

ERRATA

Illinois Route 31
 Illinois Route 176 to Illinois Route 120
 McHenry County, Illinois
 Job No. P-91-135-99

May 30, 2018
 Updated January 29, 2020

This Erratum includes corrections, revisions, and/or additions to the November 2016 Environmental Assessment (EA), for the proposed improvements of Illinois Route 31 (IL Route 31) from Illinois Route 176 to Illinois Route 120 in McHenry County, Illinois. The EA was approved for public release by the Federal Highway Administration (FHWA) on November 29, 2016. Corrections, revisions, and/or additions are shown in *italics*.

Chapter 3: Alternatives

3.7 Best Management Practices (BMPs) for the Preferred Alternative

Page 3-26. Revise second bullet point on page, keeping the sub bullet intact, to:

- *“Approximately 2,112 feet of bioswales are planned in the Sleepy Hollow Creek and Fox River watersheds.”*

Page 3-27: Revise text descriptions for two of the nine wildlife crossings (the first and third bullet points), as indicated below:

- *Brighton Lane, east of IL Route 31: Crossing to be designed in Phase II (contract plan preparation), with a minimum waterway opening of 15 square feet and to be designed as a wildlife crossing.*
- *Squaw Creek south of Stream Outfall 7: Crossing to be designed in Phase II (contract plan preparation), with a minimum waterway opening of 40 square feet and to be designed as a wildlife crossing.*

Page 3-28: Update Table 3-5 based on public hearing revisions to the proposed roadway design. The revisions were made to reduce impacts to oak and hickory trees, residences, and commercial properties.

Table 3-5
Water Quality Volume Comparison
USACE Recommended Storage for 1" / 1.25" Rainfall of Additional Proposed Impervious Areas Not Including Bike Path/Sidewalk

Watershed	Required WQV Storage (ac ft)	Provided WQV Storage (ac ft)		Difference = Provided - Required (ac ft)	
		Primary Measures Revised Design	Secondary Measures Revised Design	Primary Measures	Primary & Secondary Measures
<i>Silver Creek (Outfall 1-3)</i>	<i>0.11</i>	<i>0.03</i>	<i>2.40</i>	<i>-0.08</i>	<i>2.32</i>
<i>Sleepy Hollow Creek (Outfall 4-13, Outfall 17)</i>	<i>0.68</i>	<i>0.65</i>	<i>0.00</i>	<i>-0.03</i>	<i>-0.03</i>
<i>Fox River (Outfall 14-16, Outfall 18-27)</i>	<i>0.79</i>	<i>1.01</i>	<i>0.00</i>	<i>0.22</i>	<i>0.22</i>
TOTAL	1.58	1.69	2.40	0.11	2.51

Chapter 4: Environmental Resources, Impacts, and Mitigation

Page 4-1: Revise portions of Table 4-1, Environmental Impacts of the Preferred Alternative, as indicated below:

Criterion	Unit of Measure	Preferred Alternative
Water Quality/ Water Resources	Floodplain (acres affected)	10.04
	Sensitive Aquifer Recharge Area (acres affected)	108.40
Trees	Oak/Hickory Trees (number affected)	153 / 35
Businesses	Businesses with Site Impacts Only (no structure acquisition)	15
	Businesses with Structure Acquisition	1
Traffic Noise	Representative Receptors with Predicted Noise Impacts	23
Right-of-Way	Proposed Right-of-Way/Temporary Easements (acres)	59.63 / 9.75

4.1 Social and Economic Resources

Section 4.1.4 Public Facilities and Services, Page 4-4. Insert text after second full paragraph:

“McHenry Public Library and McHenry County College Shah Center are two public facilities located within the project study area. A strip of proposed right-of-way and temporary easement along IL Route 31 is required from both. There are no recreational uses within these areas. A private sanitary sewer for the library will be located within the proposed right-of-way and will need to be relocated. Existing parking stalls at these facilities will not be impacted. These public facilities should benefit from the expected roadway operational benefits of the project, as well as the proposed bicycle and pedestrian facilities on IL Route 31.”

Section 4.1.5 Change in Travel Patterns, Page 4-4. Change the second sentence of the section:

“The project would maintain most existing access points, but would convert several intersections to right-in, right-out access. A cul-de-sac was proposed for Waukegan Road at IL Route 31, but was revised to maintain the existing right-in as requested by the City of McHenry and the adjacent property owner.

Section 4.1.6 Relocations, Page 4-4. Revise the first sentence:

“The IL Route 31 Preferred Alternative will require one business relocation and one residential relocation.”

Section 4.1.6 Business Relocations, Page 4-5. Replace this section with the following:

“The project will require the acquisition of one commercial structure, The Vape Shop building at 3815 W. Elm Street as shown in Exhibit 4. The business must be relocated due to the proposed IL Route 31/IL Route 120 intersection widening. These improvements impact much of the off-street parking for the business. Web search investigations identified available comparable commercial properties³¹ in McHenry. Following the public hearing, the right-of-way was reduced from a full take to a partial take as requested by the property owner.

A cul-de-sac was proposed for Waukegan Road at IL Route 31 which required the displacement of the Boost Mobile building at 3817 W. Elm Street. Following the public hearing, the cul-de-sac was eliminated in lieu of the existing right-in access as requested by the City of McHenry and the property owner of the displacement. As a result, the business is no longer a displacement”

Section 4.1.6 Residential Relocations, Page 4-5. Revise the first sentence:

“The Preferred Alternative will require the acquisition of one single-family residence, located at 518 Front Street in McHenry, as shown in Exhibit 4.”

Section 4.1.7 Economic Impacts, Page 4-5. Revise the section:

“In addition to the business relocation (see Section 4.1.6.1) and maintenance of business access during construction (see Section 4.1.5), the Preferred Alternative will require the removal of off-street business parking or other (non-structure) business amenities at 15 parcels. The majority of these impacts are off-street parking, but business display areas would be impacted through the corridor. Parking impacts were minimized through roadway and site design as much as possible. The one business that will be acquired by the project could be relocated within McHenry. Web search investigations identified available comparable commercial properties³³ in McHenry. The Preferred Alternative will impact approximately 72 parking stalls along IL Route 31 and IL Route 120, based on a review of the proposed ROW and aerial imagery. IDOT has minimized parking impacts at certain locations through the project study area with the use of retaining walls, select reduction of landscape buffer areas, and the use of paved swales. An estimated 338 parking stalls would be impacted temporarily due to temporary easement use but are expected to be restored at the conclusion of project construction. A total of eleven on-street parking stalls will be lost on the west side of IL Route 31 south of the IL Route 120 intersection. Within the project study area, there are four on-street parking stalls on IL Route 120; the proposed IL Route 31 project will require removing all four parking stalls. There is available parking on adjacent lots to balance this loss. Businesses that would have some off-street parking removal due to the project would have sufficient remaining parking. Other than the loss of business parking and display areas and the acquisition of several businesses, the Preferred Alternative is not expected to disrupt the economic vitality of the community.”

Section 4.1.10 Pedestrian and Bicycle Facilities, Page 4-6. Revise the first sentence:

“The Preferred Alternative includes a shelf for an 8 or 10-foot shared-use path along the east side of IL Route 31, and a shelf for a 5-foot sidewalk along the west side of IL Route 31, for the extent of the proposed improvements.”

Section 4.5 Traffic Noise

Page 4-10. Revise the second, third, and fourth full paragraphs on this page:

“Traffic noise modeling uses the FHWA Traffic Noise Model (TNM), Version 2.5. Noise monitoring of existing ambient noise conditions was used to validate TNM models; for this project, monitoring occurred in August 2014 for a portion of the studied receptors. Traffic noise model results found traffic noise impacts (as defined by the NAC) at 23 of the 42 representative receptors (see Appendix C which summarizes the results of the noise and noise abatement analysis).

IDOT noise policy requires that a noise abatement measure must be feasible and reasonable to be implemented. It was determined that noise abatement (in the form of noise barriers) would not be constructible for seven of the 23 CNEs because their construction would result in building impacts. Fourteen noise barriers were studied for the remaining 16 CNEs that had impacted receptors.

Six of the analyzed barriers did not achieve a 5 dB(A) reduction to the impacted receptors in their respective CNEs, so these were not considered feasible. Five of the analyzed areas were considered feasible but did not achieve the IDOT noise reduction design goal of 8 dB(A) for benefitted receptors, so these were not considered reasonable. The three remaining barriers (B4, B5, and B14) provided the 5 dB(A) and 8 dB(A) noise reductions but were not reasonable because the cost to build each barrier exceeds the IDOT allowable cost per benefitted receptor.”

4.6 Natural Resources

Section 4.6.1 Upland Plant Communities, Existing Conditions, Page 4-11. Revise first sentence of the first paragraph to:

“Surveys to identify tree resources within the project study area were conducted in March, May, and December 2014, March 2016, and March 2018.”

Section 4.6.1 Upland Plant Communities, Existing Conditions, Page 4-11. Revise first sentence of the second paragraph to:

“The total number of trees within the project study area was estimated at 6,905 (5,049 trees estimated by subplots in heavily wooded areas, and 1,856 individually surveyed trees).”

Section 4.6.1 Upland Plant Communities, Existing Conditions, Page 4-12. Revise last sentence of “Trees: Existing Conditions” subsection to:

“A total of 286 oaks and hickories are present within the project study area.”

Section 4.6.1 Upland Plant Communities, Specimen and Exceptional Trees, Page 4-11. Revise section to:

“Ninety-six (96) exceptional trees were identified within the project study area; no specimen trees were identified in the project study area.”

Section 4.6.1 Upland Plant Communities, Tree Impacts:

Page 4-12. Revise the first sentence of the first paragraph to:

“Based on the tree survey and final roadway alignment, an estimated 5,176 trees will be removed by the project (see Exhibit 4). The impacted trees include 3,923 impacted subplot trees and 1,253 individually surveyed trees.”

Page 4-12. Revise the first two sentences of the third paragraph to:

“The Preferred Alternative will impact approximately 153 oak trees (see Exhibit 4), 35 shagbark hickories, and 49 exceptional trees. The exceptional trees that will be impacted are 23 white oaks, 13 bur oaks, five red oaks, one shagbark hickory, one American elm, one Austrian pine, one black walnut, one Norway maple, one Norway spruce, and one sugar maple.”

Page 4-12. After the last paragraph, add:

“Following the public hearing, measures to further reduce impacts to trees were investigated. These measures included adjustments to the proposed improvement design and adding retaining walls.

Of the investigated methods, eliminating bioswales and adjusting ditches and sidewalk/path were effective measures that were implemented. Retaining walls were not a cost-effective solution due to their high construction cost and long-term maintenance. In lieu of retaining walls, IDOT will work with adjacent municipalities and the McHenry County Conservation District to identify replacement areas near the project.

A recommendation to harvest oak and hickory seeds (acorns and hickory nuts) from trees located within the vicinity of the proposed improvements which can then be propagated and used for local reforestation efforts is included for this project. Acorns and hickory nuts harvested throughout the project area could be utilized by nurseries, both public and private. This effort would be consistent with the Oak Ecosystems Recovery Plan for northeastern Illinois outlined by Chicago Wilderness (October 2015).

The following Special Design/Construction Consideration will be added to the Combined Design Report:

“Phase II should consider a special provision to harvest oak and hickory seeds (acorns and hickory nuts) from trees located within the vicinity of the proposed improvements which can then be propagated and used for local reforestation efforts is included for this project.”

Section 4.6.4 Threatened and Endangered Species, Page 4-15. After the last paragraph of the Federally Listed Species section, insert text:

*“This project will not affect the federally endangered rusty patched bumble bee (*Bombus affinis*) per US Fish and Wildlife guidance. This project is located within “low potential zones” for the rusty patched bumble bee as identified by the USFWS (USFWS March 2017). “High potential zones” are not located within the project limits; however, one “high potential zone” is located approximately a mile and a half east of the corridor near Prairie Grove, IL.”*

Section 4.6.4 Threatened and Endangered Species, Page 4-15. After the last sentence of the State Listed Species section, insert text:

*“There are records of the state listed yellow headed blackbird (*Xanthocephalus xanthocephalus*), black tern (*Chlidonias niger*), and common gallinule (*Gallinula galeata*) at Thunderbird Lake; these species will not be impacted by the project.”*

4.7 Water Resources and Aquatic Habitats

Section 4.7.4, Page 4-19, Maintenance subsection. Revise third sentence of second paragraph to:

“A proposed infiltration trench will be installed near Outfall 10 (north of Half Mile Trail) that is anticipated to reduce chloride concentrations to the unnamed Tributary to Thunderbird Lake to an acceptable level (see Section 3.7).”

4.9 Floodplains

Page 4-21. Revise second to last sentence of paragraph:

“The Preferred Alternative footprint impacts 10.04 acres of 100-year floodplain in the project study area; nearly all impacts are transverse at each floodplain crossing. The realignment of Squaw Creek constitutes a longitudinal floodplain impact east of IL Route 31; however, the stream realignment is considered a net benefit. A 100-year flood event will likely not result in overtopping of IL Route 31, but will result in overflow to the east due to the lower elevation of Squaw Creek compared to the proposed IL Route 31 improvements. The modifications to avoid roadway overtopping will not adversely affect properties east of IL Route 31. Additionally, depressions for water quality are proposed along the realigned Squaw Creek that in combination with the stabilized stream banks, riffles, and pools, will slow water volume (both from roadway runoff and within the stream) and reduce soil erosion.

Page 4-21. Add to the second paragraph of the section:

“Half an acre of compensatory storage will be provided west of IL Route 31 along the Unnamed Tributary to the Fox River near downtown McHenry.”

Chapter 5: Commitments

Remove the two commitments and add the following:

*“Pollinator habitat shall be incorporated into the project where practical. The Phase II landscaping and signing plans shall be developed in accordance with the current version of the FHWA publication *Pollinators and Roadsides: Best Management Practices for Managers and Decision Makers* and coordinated through the IDOT’s Bureau of Maintenance/Landscaping Unit.”*

Chapter 6: Comments and Coordination

6.2 Public and Agency Coordination

Section 6.2.2 Public Meetings, Page 6-2. Following the last paragraph in the section, insert the following paragraph:

“A public hearing was held on February 8, 2017 from 4:00 to 7:00 p.m. at the McHenry County College Shah Center in the City of McHenry. The public hearing was conducted in an open house format with a public comment forum. Public notice was placed in the January 18 and February 1, 2017 editions of the Northwest Herald newspaper. The EA was made available for public review on the project website, as well as at IDOT – District 1 office, Prairie Grove Village Hall, and the City of McHenry Public Library on January 18, 2017. The public hearing was attended by 169 people including elected officials, local agencies and groups, businesses and local residents. A court reporter was present to transcribe verbal comments and the public comment forum. The comment period ended on March 10, 2017 and 92 comments were submitted. The most common comment topics related to the Oak Grove subdivision, oak and hickory tree impacts, access, new traffic signals, pedestrian and bicyclist accommodations, and general support.

Comments were responded to via a newsletter as well as individual correspondence. All those that attended the public hearing or provided comments were e-mailed (or mailed) the newsletter. The newsletter is also posted on the project website at www.idot.illinois.gov/projects/il31. The public hearing summary, attendance roster, certification of publication, transcripts/comments and responses are included in Appendix K.”

Chapter 9: References

Page 9-3. Add one reference:

*“US Fish and Wildlife Service Regions 3, 4, 5, and 6. The Rusty Patched Bumble Bee (*Bombus affinis*): Interagency Cooperation under Section 7(a)(2) of the Endangered Species Act, Voluntary Implementation Guidance. Version 1.1. March 20, 2017.”*

Exhibit 4 - Environmental Resource Map

Replace Exhibit 4 (Environmental Resources Map) to show updates to proposed right-of-way, business site impacts, and tree impacts.

Exhibit 5 - Best Management Practices Map

Replace Exhibit 5 (Best Management Practices Map) to show updates to water quality Best Management Practices.

Coordination: Agency Coordination

Insert the following at the end of Environmental Resources/Special Waste:

- ESR-F PESA Validation – February 28, 2018

Insert the following at the end of NEPA/404 Merger Meeting Summaries and Concurrences:

- US Environmental Protection Agency - February 27, 2017
- US Army Corps of Engineers - March 15, 2017

Insert the following at the end of FHWA Coordination Meeting Summaries:

- FHWA/BDE - January 10, 2018

Insert the following at the end of Other Agency Coordination:

- McHenry County Council of Governments - January 31, 2017
- Nunda Township - March 17, 2017
- McHenry County Conservation District - May 24, 2017
- Village of Prairie Grove - September 19, 2017
- City of McHenry - February 1, 2018

Appendix A - Social and Economic Resources

Revise the final paragraph of the Appendix to state:

“In addition to the aforementioned business relocation listed in Section 3.1.6, the IL Route 31 Preferred Alternative will require the removal of private business parking spaces or other business amenities at 15 total parcels in the project corridor. The Preferred Alternative will also impact business display areas at several businesses in the corridor.”

Appendix C – Traffic Noise Analysis

Replace the traffic noise analysis technical report that was updated to include an analysis of noise abatement measures for R14 and R16 (Oak Grove subdivision in Prairie Grove).

Appendix K – Public Hearing Materials

Add this appendix that includes the public hearing summary, attendance roster, certification of advertisement, transcripts/comments and responses.

Attachments
Provided on CD

Exhibits

Exhibit 4 - Environmental Resources Map
Exhibit 5 - Best Management Practices Map

Coordination

US Environmental Protection Agency - February 27, 2017
US Army Corps of Engineers - March 15, 2017
FHWA/BDE - January 10, 2018
McHenry County Council of Governments - January 31, 2017
Nunda Township - March 17, 2017
McHenry County Conservation District - May 24, 2017
Village of Prairie Grove - September 19, 2017
City of McHenry - February 1, 2018

Appendices and Technical Reports

Appendix C – Traffic Noise Analysis
Appendix K – Public Hearing Materials

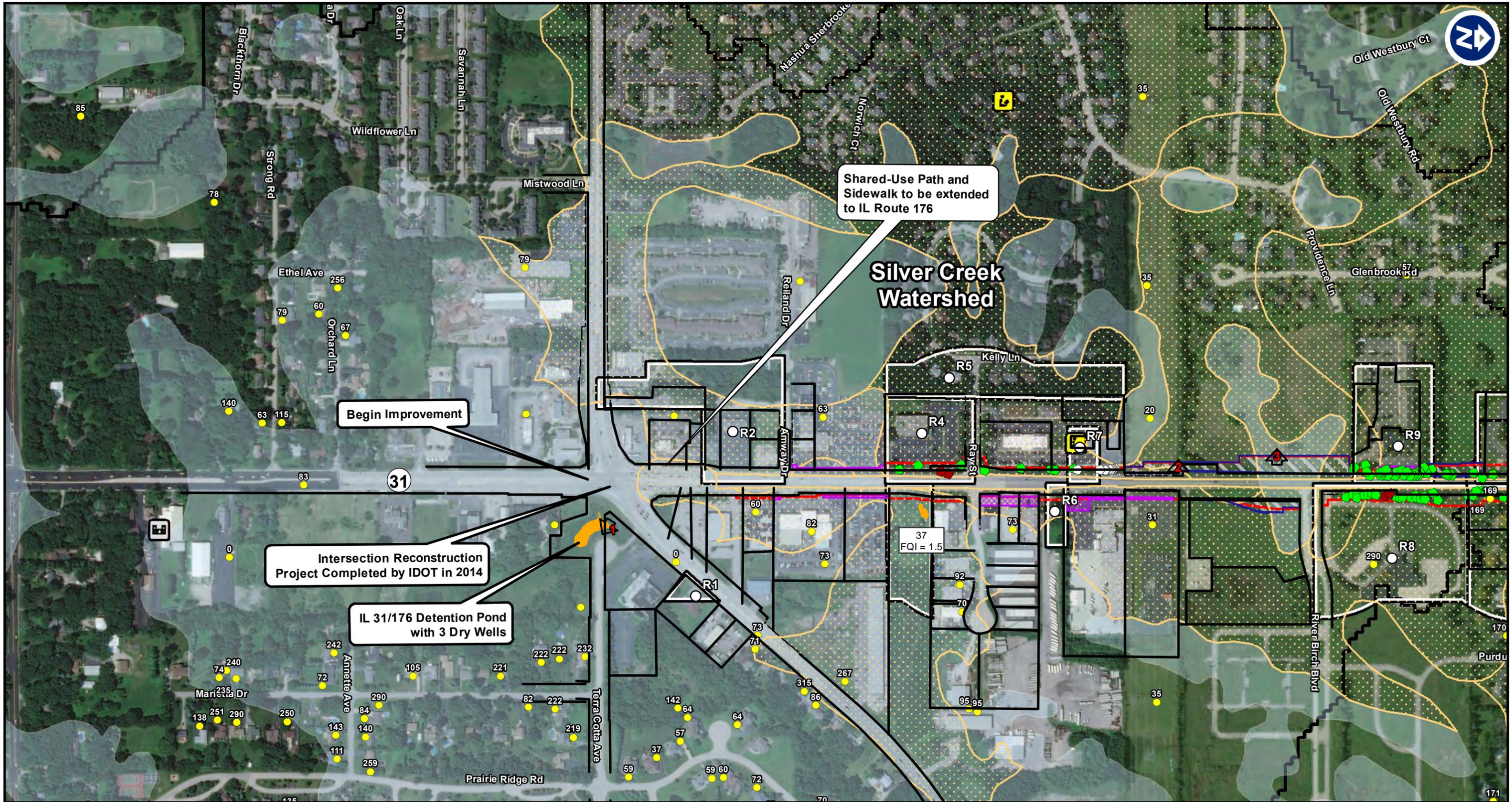
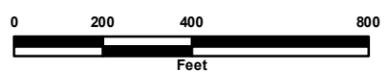


Exhibit 4
Environmental Resources Map
Illinois Route 31
Illinois Route 176 to Illinois Route 120
McHenry County, Illinois
Sheet 1 of 6



Legend

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|----------------------------|---------------------------|---|--------------------------|--|------------------|-----------------------------------|
| Wastewater Treatment Plant | All Community Wells | Tree Impact | Traffic Noise Receptor | Outfall | Stream | Impacted Farm Parcel |
| Existing ROW | All Non Community Wells | Oak Tree Impact | Common Noise Environment | WOUS (INHS) | Floodplain | Prime & Important Farmland Soil |
| Proposed ROW | Non Community Well Impact | Shagbark Hickory Tree Impact | Business Site Impact | Lakes and Rivers | Floodway | Prairie Grove Town Center |
| Prairie Trail | School | Residential Acquisition | Highly Erodible Soil | Proposed Detention Pond / Proposed Retention Basin | ADID Wetland | Census Tracts 8706.05 and 8706.06 |
| Proposed Retaining Wall | Place of Worship | Business Structure Acquisition | Park | Sensitive Aquifer Recharge Area | NON-ADID Wetland | |
| | | Owned by Land Conservancy of McHenry County | | | | |

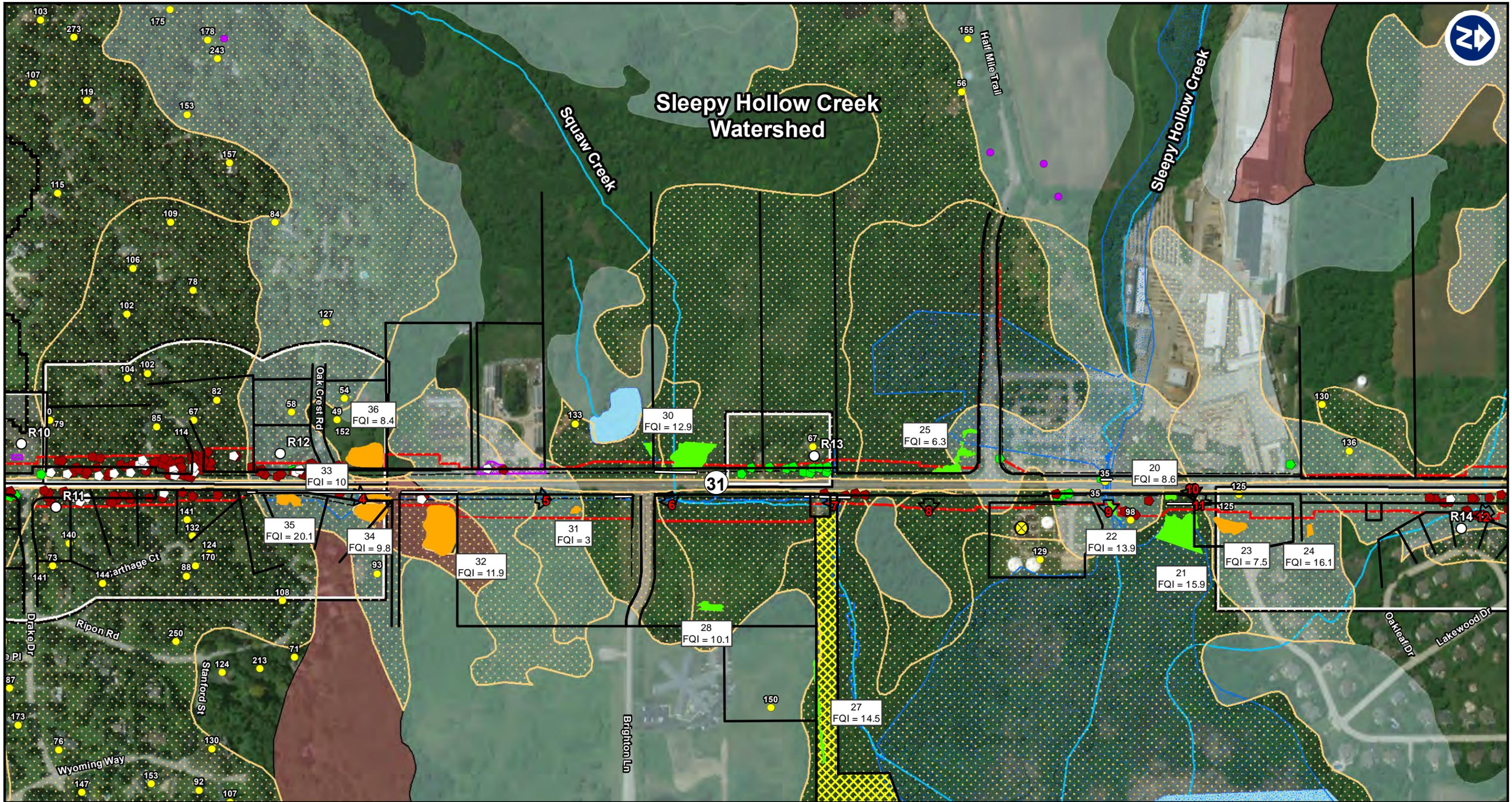
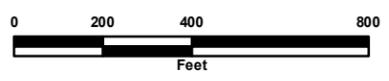


Exhibit 4
Environmental Resources Map
Illinois Route 31
Illinois Route 176 to Illinois Route 120
McHenry County, Illinois
Sheet 2 of 6



Legend	
	Wastewater Treatment Plant
	All Community Wells
	All Non Community Wells
	Non Community Well Impact
	School
	Place of Worship
	Existing ROW
	Proposed ROW
	Prairie Trail
	Proposed Retaining Wall
	Tree Impact
	Oak Tree Impact
	Shagbark Hickory Tree Impact
	Residential Acquisition
	Business Structure Acquisition
	Traffic Noise Receptor
	Common Noise Environment
	Business Site Impact
	Highly Erodible Soil
	Park
	Owned by Land Conservancy of McHenry County
	Outfall
	WOUS (INHS)
	Lakes and Rivers
	Proposed Detention Pond / Proposed Retention Basin
	Sensitive Aquifer Recharge Area
	Stream
	Floodplain
	Floodway
	ADID Wetland
	NON-ADID Wetland
	Impacted Farm Parcel
	Prime & Important Farmland Soil
	Prairie Grove Town Center
	Census Tracts 8706.05 and 8706.06

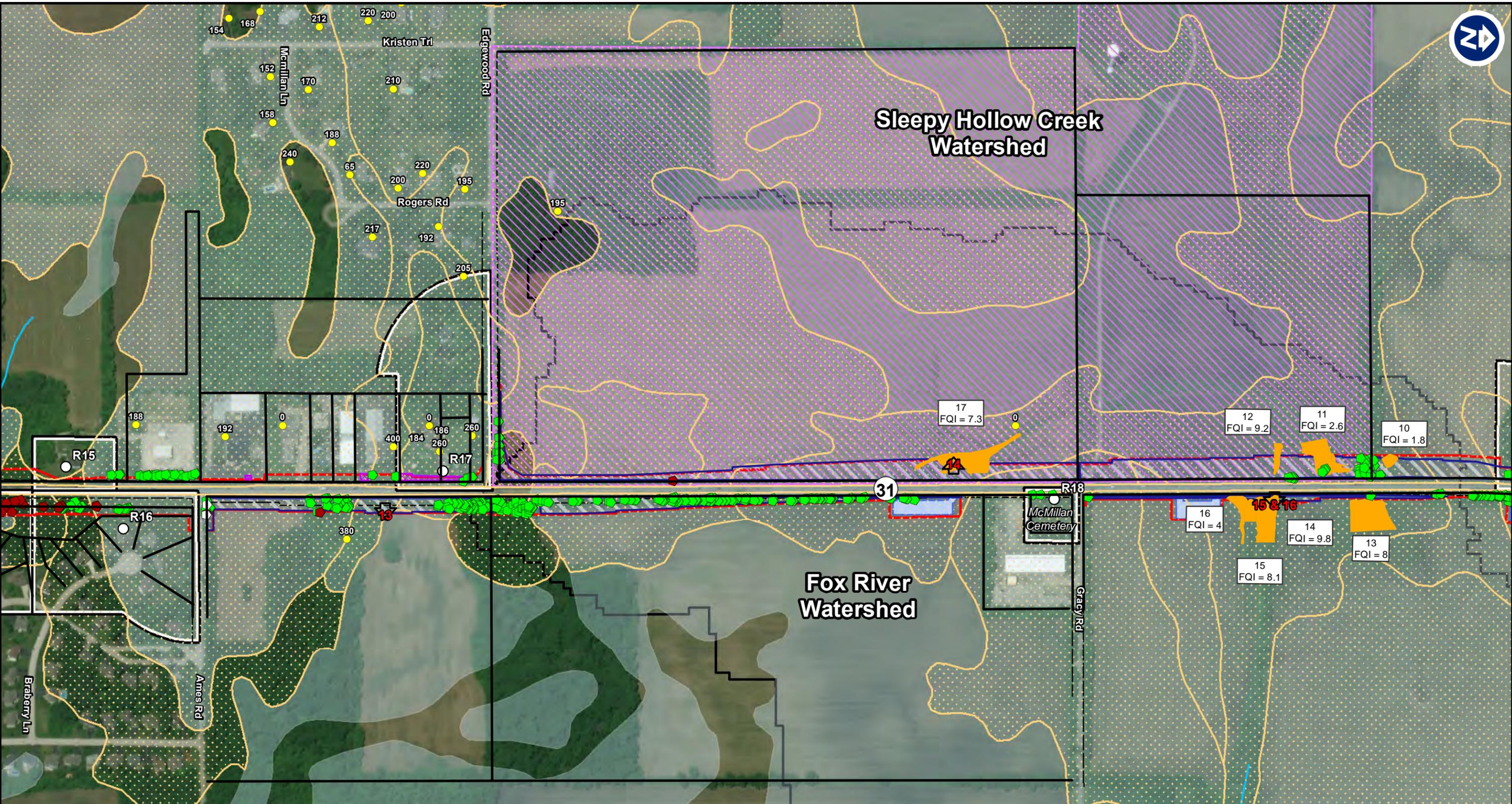
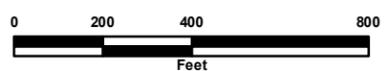


Exhibit 4
Environmental Resources Map
Illinois Route 31
Illinois Route 176 to Illinois Route 120
McHenry County, Illinois
Sheet 3 of 6



Legend

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|----------------------------|---------------------------|---|---------------------------------|--|-----------------------------------|
| Wastewater Treatment Plant | All Community Wells | Tree Impact | Traffic Noise Receptor | Stream | Impacted Farm Parcel |
| Existing ROW | All Non Community Wells | Oak Tree Impact | Common Noise Environment | Floodplain | Prime & Important Farmland Soil |
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| Prairie Trail | School | Residential Acquisition | Highly Erodible Soil | ADID Wetland | Census Tracts 8706.05 and 8706.06 |
| Proposed Retaining Wall | Place of Worship | Business Structure Acquisition | Park | NON-ADID Wetland | |
| | | Owned by Land Conservancy of McHenry County | Outfall | WOUS (INHS) | |
| | | | Lakes and Rivers | Proposed Detention Pond / Proposed Retention Basin | |
| | | | Sensitive Aquifer Recharge Area | | |

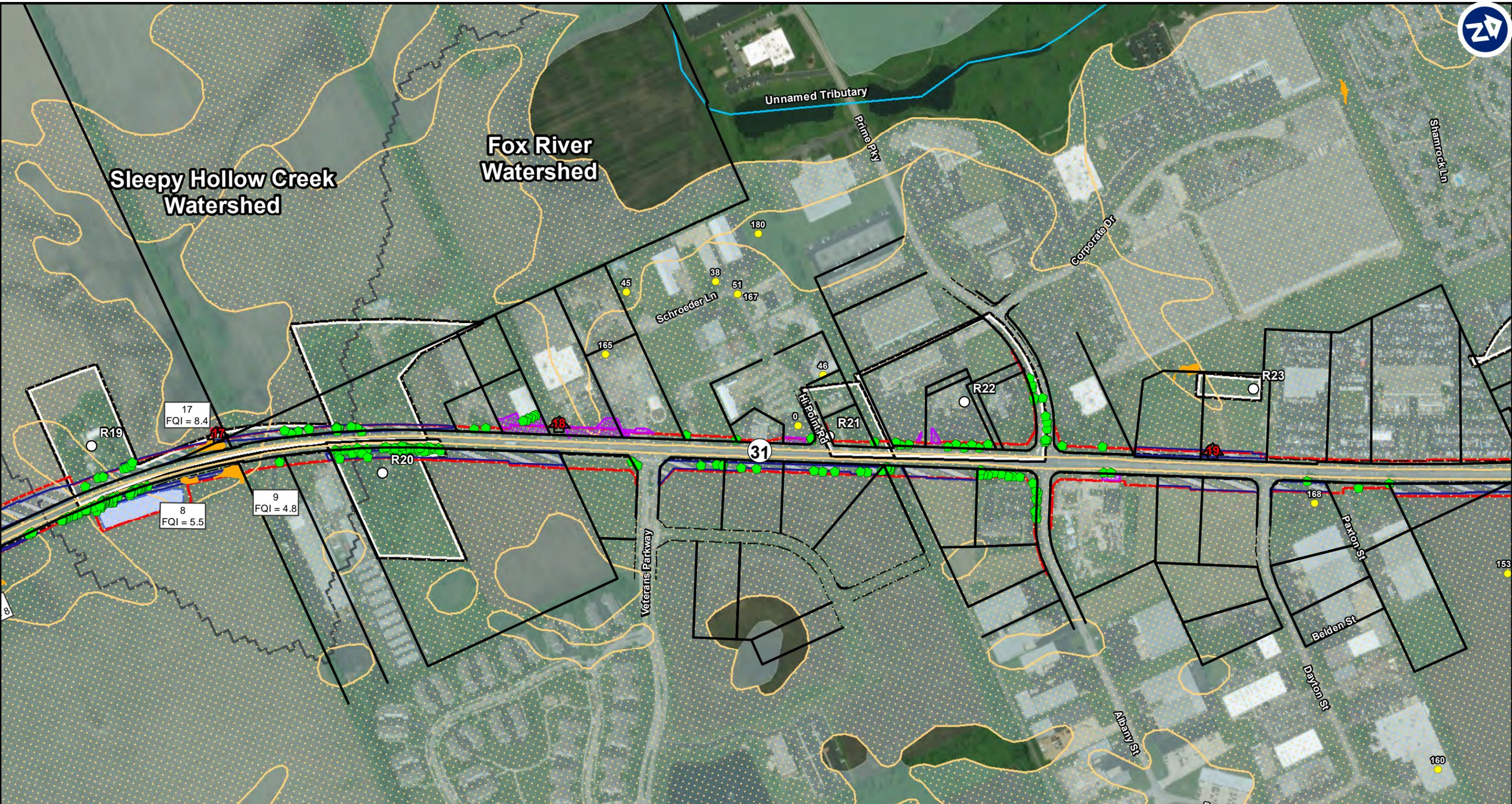
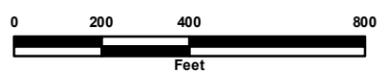


Exhibit 4
Environmental Resources Map
Illinois Route 31
Illinois Route 176 to Illinois Route 120
McHenry County, Illinois
Sheet 4 of 6



Legend

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|----------------------------|---------------------------|---|--------------------------|---------------------------------|--|
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| | | Owned by Land Conservancy of McHenry County | WOUS (INHS) | Lakes and Rivers | Proposed Detention Pond / Proposed Retention Basin |
| | | | Outfall | Sensitive Aquifer Recharge Area | |

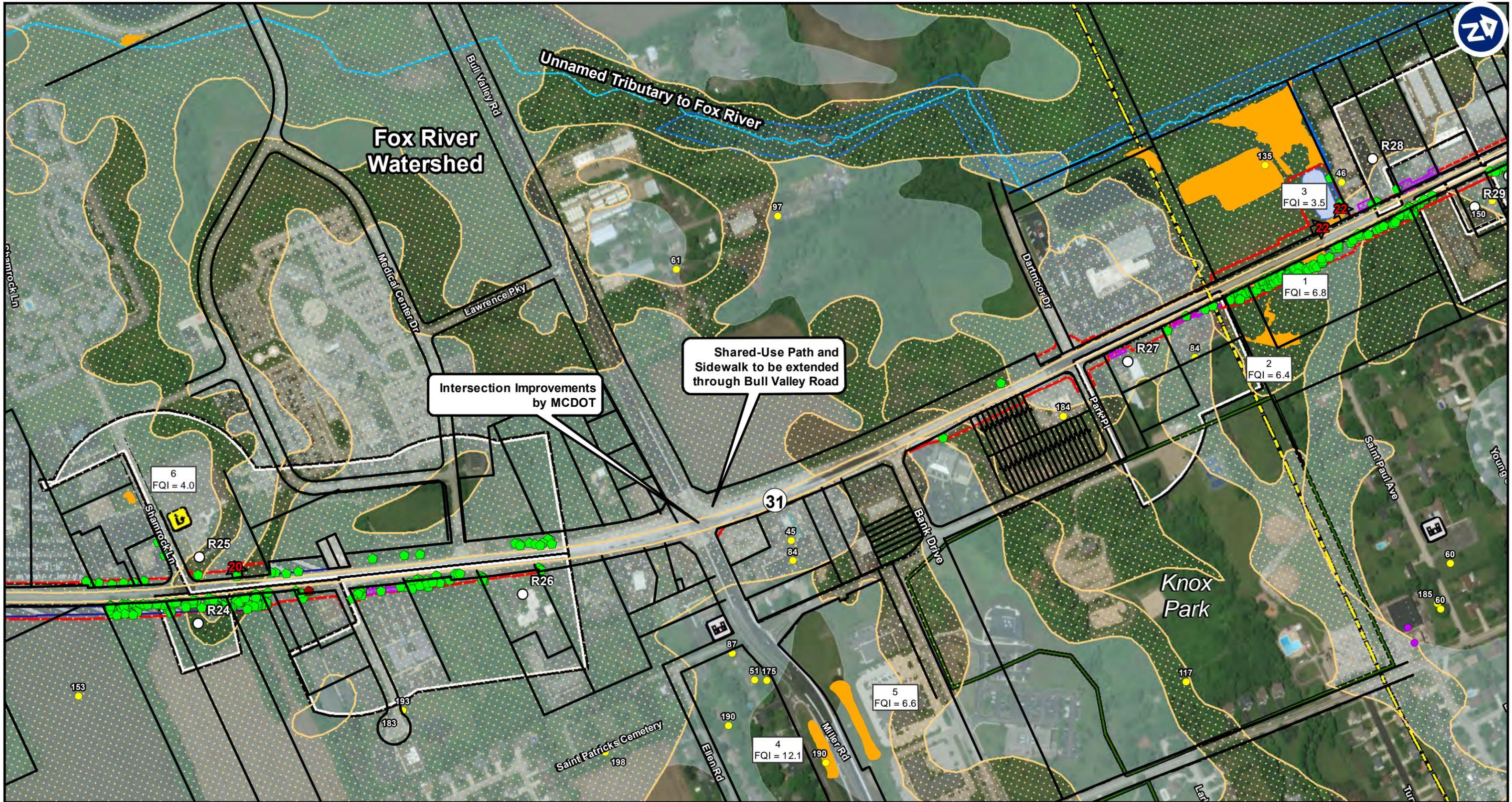
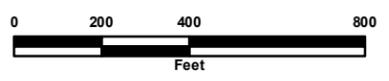


Exhibit 4
Environmental Resources Map
Illinois Route 31
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McHenry County, Illinois
Sheet 5 of 6



Legend

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| | | | Proposed Detention Pond / Proposed Retention Basin | Sensitive Aquifer Recharge Area | |
| | | | Outfall | | |

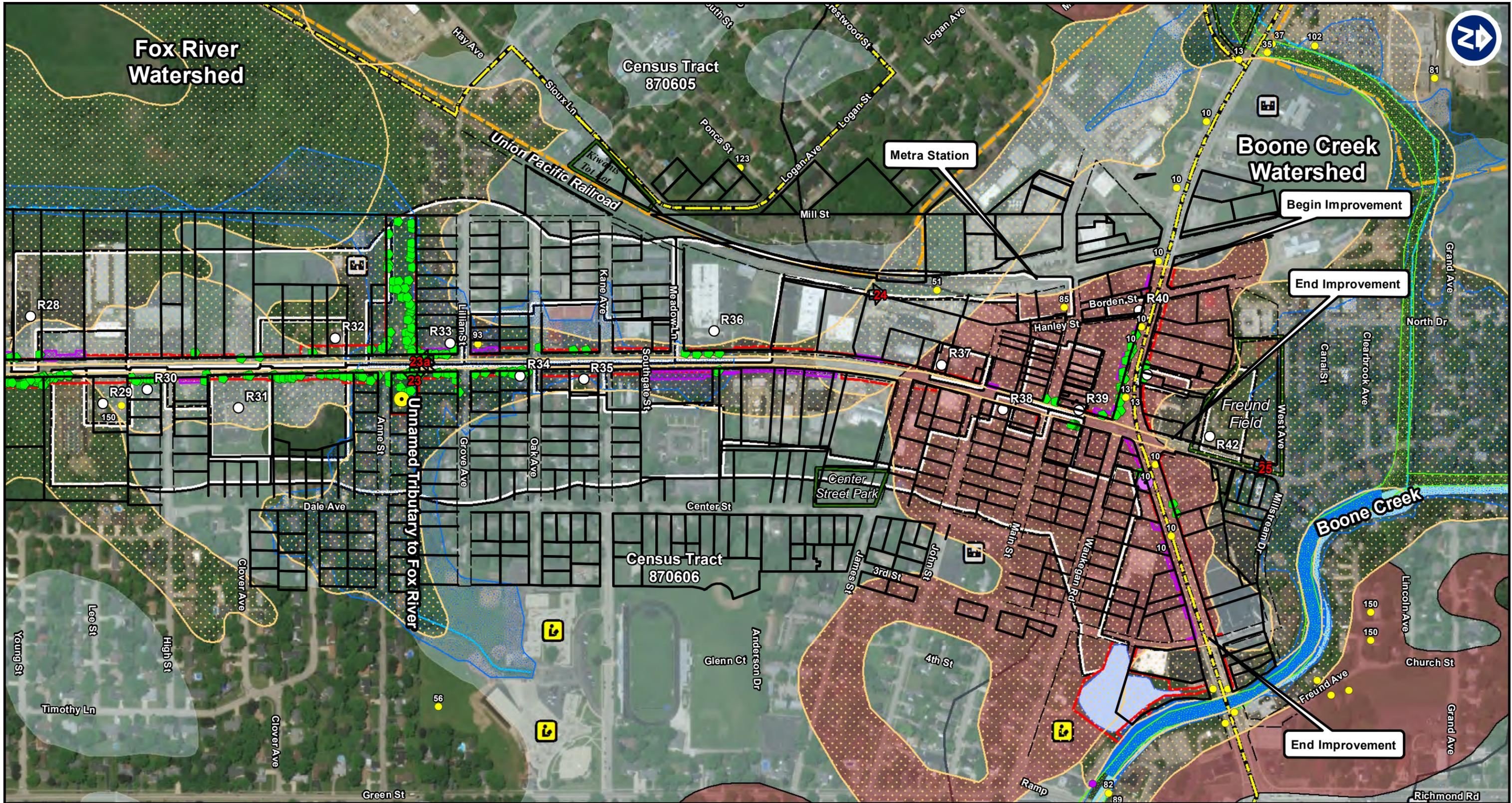
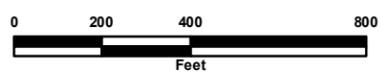


Exhibit 4
Environmental Resources Map
Illinois Route 31
Illinois Route 176 to Illinois Route 120
McHenry County, Illinois
Sheet 6 of 6



Legend

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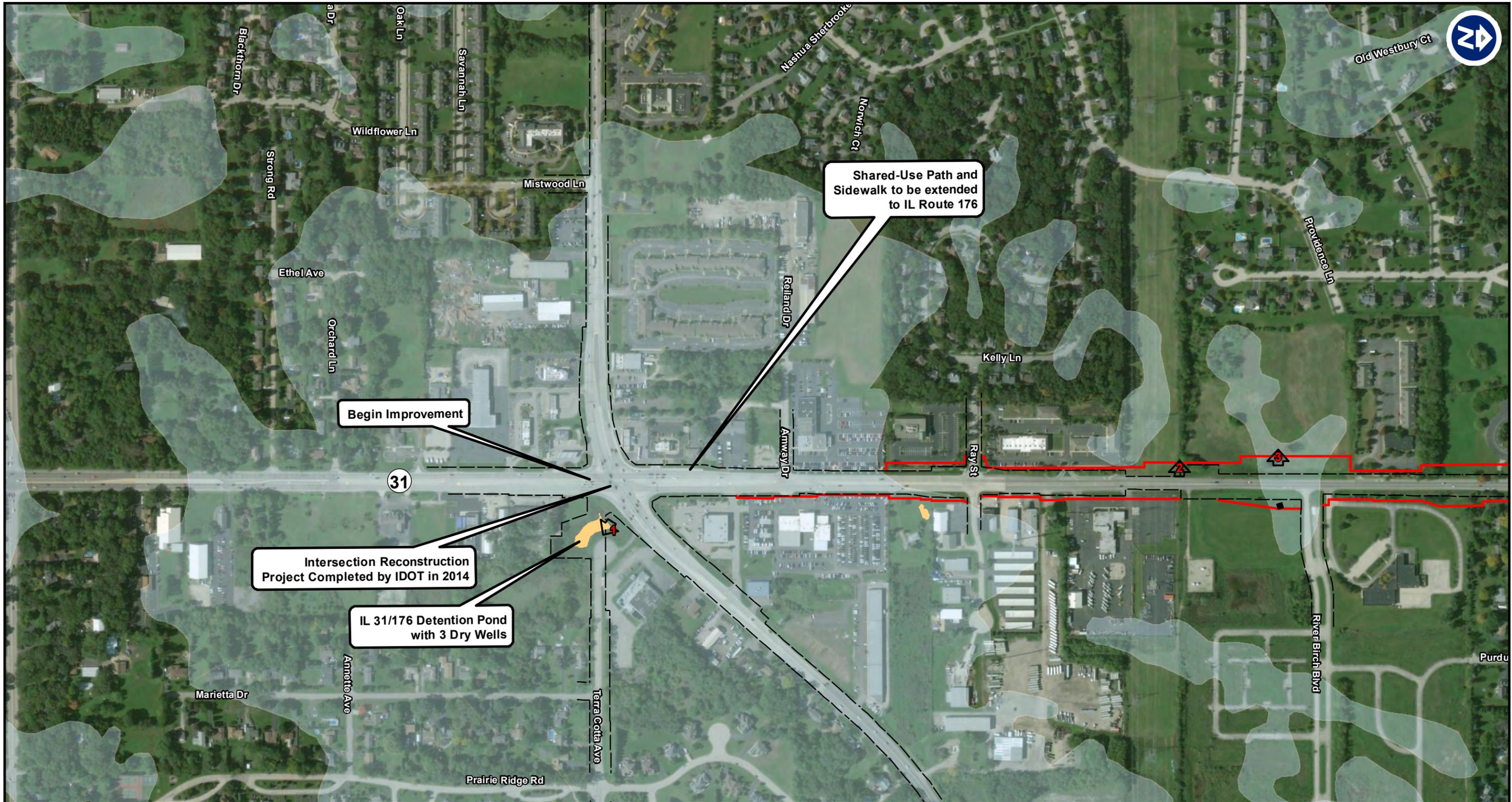
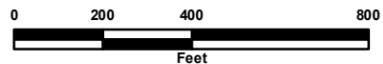
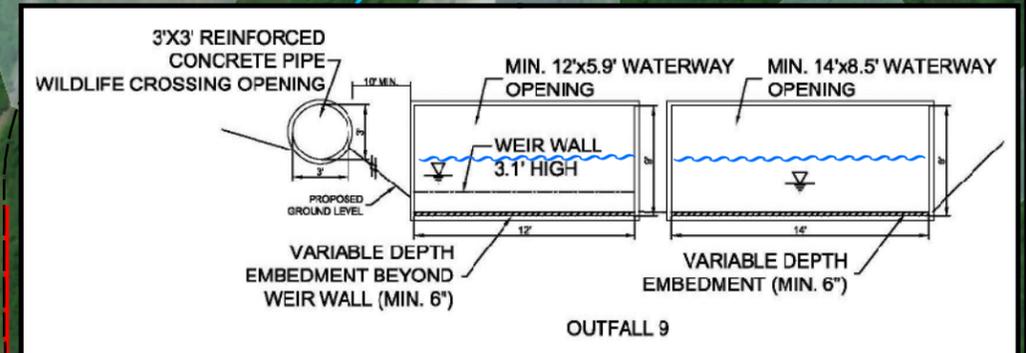
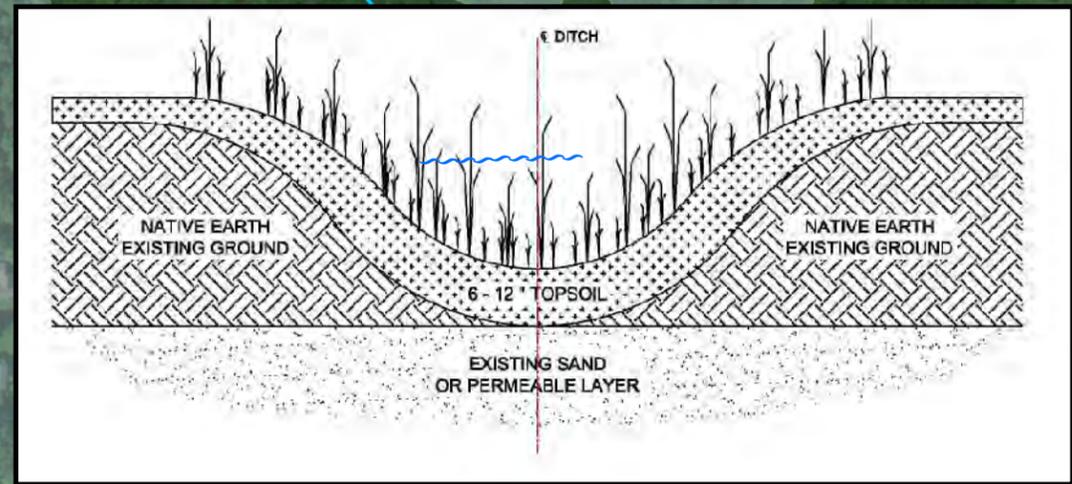
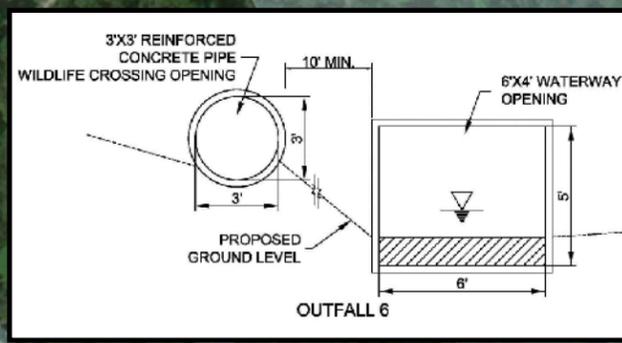


Exhibit 5
Best Management Practices Map
 Illinois Route 31
 Illinois Route 176 to Illinois Route 120
 McHenry County, Illinois
 Sheet 1 of 6



Legend

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|---------------------------|--|---------------------------------|--|--|
| --- Existing ROW | Meandering Stream | Perennial Stream | WOUS (INHS) | Stream |
| — Proposed ROW | Proposed Bioswale | ADID Wetland | Lakes and Rivers | Outfall |
| — Proposed Retaining Wall | Proposed Infiltration Trench | NON-ADID Wetland | Proposed Detention Pond/
Proposed Retention Basin | Embedded Culvert -
Typical 1' Depth |
| Water Level | Proposed Natural Bottom Culvert/Bridge | Sensitive Aquifer Recharge Area | | |



Proposed Natural Bottom Culvert (Embedded Box Culvert) with Separate Wildlife Crossing Culvert (Reinforced Concrete Pipe)

To be Designed in Phase II (Contract Plan Preparation), with Minimum Waterway Opening of 40 sqft, to be Designed as a Wildlife Crossing

Proposed Natural Bottom Culvert (Embedded Box Culvert) with Separate Wildlife Crossing Culvert (Reinforced Concrete Pipe)

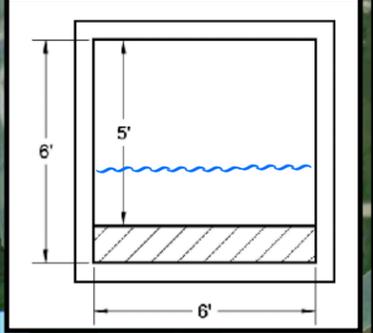
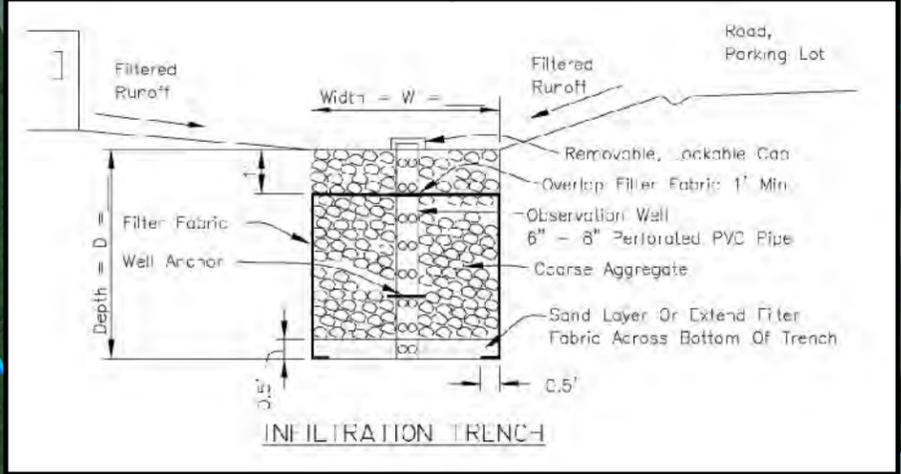
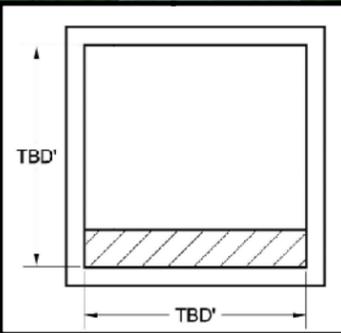
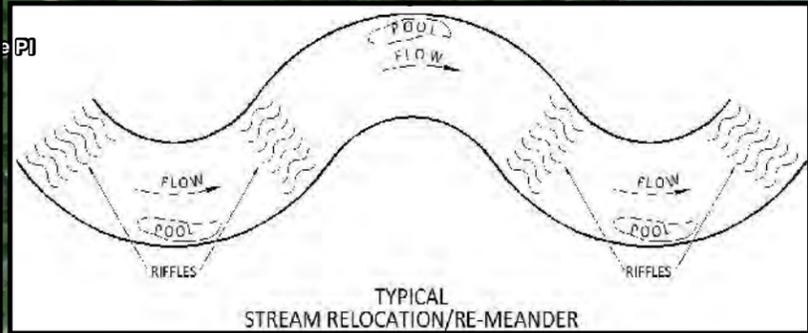
Proposed Squaw Creek Meandering Stream

To be Designed in Phase II (Contract Plan Preparation), with a Minimum Waterway Opening of 15 sqft, to be Designed as a Wildlife Crossing.

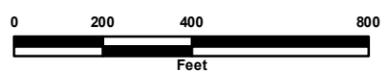
Proposed Natural Bottom Culvert (Embedded Box Culvert)

Proposed Infiltration Trench

Proposed Natural Bottom Culvert (Embedded Box Culvert)

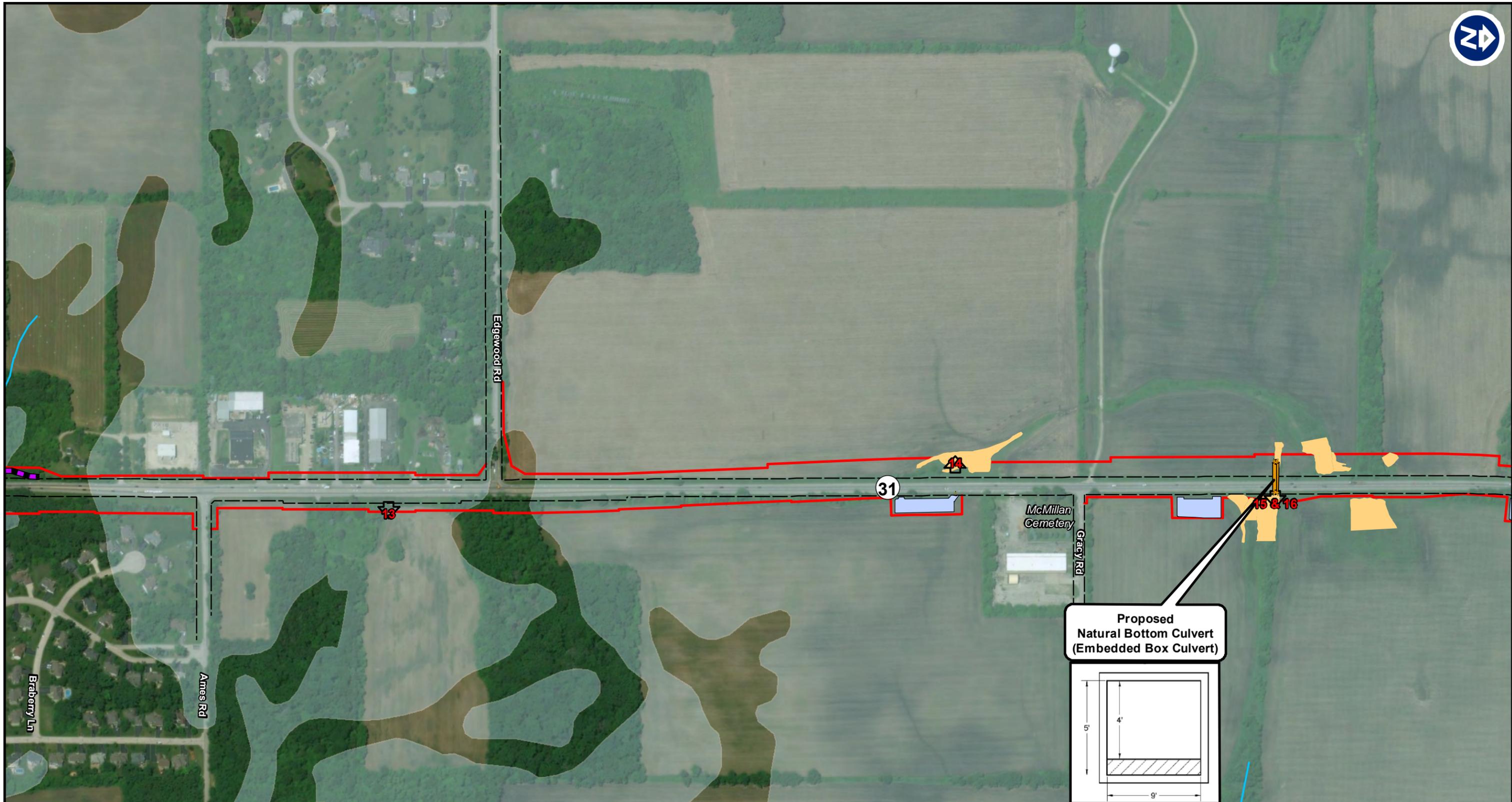


**Exhibit 5
Best Management Practices Map
Illinois Route 31
Illinois Route 176 to Illinois Route 120
McHenry County, Illinois
Sheet 2 of 6**



Legend

- Existing ROW
- Proposed ROW
- Proposed Retaining Wall
- ~ Water Level
- Meandering Stream
- Proposed Bioswale
- Proposed Infiltration Trench
- Proposed Natural Bottom Culvert/Bridge
- Perennial Stream
- ADID Wetland
- NON-ADID Wetland
- Sensitive Aquifer Recharge Area
- WOUS (INHS)
- Lakes and Rivers
- Proposed Detention Pond/Proposed Retention Basin
- Stream
- Outfall
- Embedded Culvert - Typical 1' Depth



Proposed
Natural Bottom Culvert
(Embedded Box Culvert)

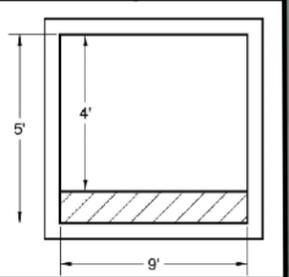
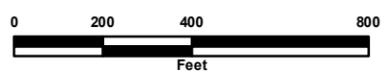


Exhibit 5
Best Management Practices Map
Illinois Route 31
Illinois Route 176 to Illinois Route 120
McHenry County, Illinois
Sheet 3 of 6

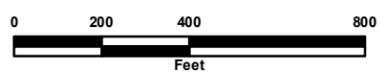


Legend

- Existing ROW
- Proposed ROW
- Proposed Retaining Wall
- ~ Water Level
- Meandering Stream
- Proposed Bioswale
- Proposed Infiltration Trench
- Proposed Natural Bottom Culvert/Bridge
- Perennial Stream
- ADID Wetland
- NON-ADID Wetland
- Sensitive Aquifer Recharge Area
- WOUS (INHS)
- Lakes and Rivers
- Proposed Detention Pond/
Proposed Retention Basin
- Stream
- Outfall
- Embedded Culvert -
Typical 1' Depth

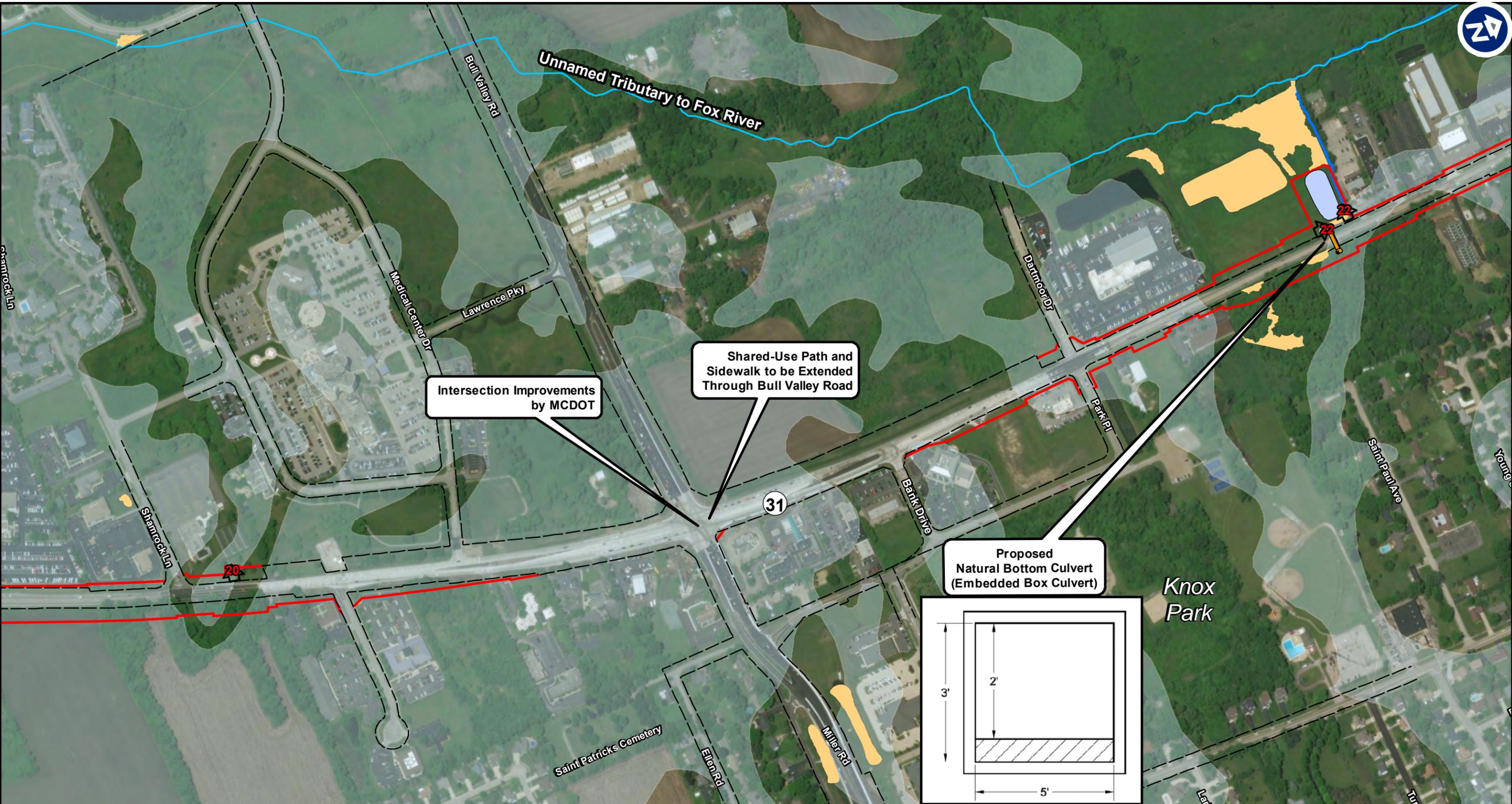


Exhibit 5
Best Management Practices Map
 Illinois Route 31
 Illinois Route 176 to Illinois Route 120
 McHenry County, Illinois
 Sheet 4 of 6



Legend

- | | | | | |
|---------------------------|--|---------------------------------|--|--|
| --- Existing ROW | Meandering Stream | ▽ Perennial Stream | WOUS (INHS) | Stream |
| — Proposed ROW | Proposed Bioswale | ADID Wetland | Lakes and Rivers | Outfall |
| — Proposed Retaining Wall | Proposed Infiltration Trench | NON-ADID Wetland | Proposed Detention Pond/
Proposed Retention Basin | Embedded Culvert -
Typical 1' Depth |
| Water Level | Proposed Natural Bottom Culvert/Bridge | Sensitive Aquifer Recharge Area | | |



Intersection Improvements by MCDOT

Shared-Use Path and Sidewalk to be Extended Through Bull Valley Road

Proposed Natural Bottom Culvert (Embedded Box Culvert)

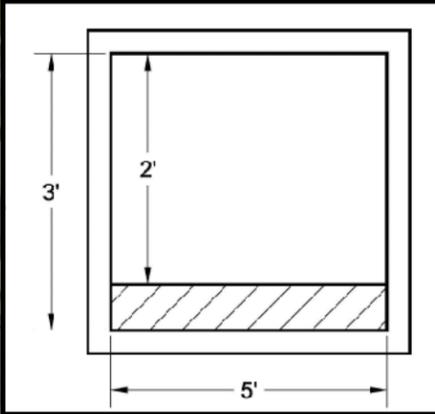
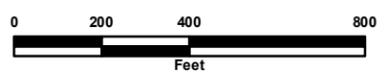


Exhibit 5
Best Management Practices Map
Illinois Route 31
Illinois Route 176 to Illinois Route 120
McHenry County, Illinois
Sheet 5 of 6



Legend

- Existing ROW
- Proposed ROW
- Proposed Retaining Wall
- ~ Water Level
- Meandering Stream
- Proposed Bioswale
- Proposed Infiltration Trench
- Proposed Natural Bottom Culvert/Bridge
- ▽ Perennial Stream
- ADID Wetland
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- WOUS (INHS)
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- Proposed Detention Pond/Proposed Retention Basin
- Stream
- Outfall
- Embedded Culvert - Typical 1' Depth

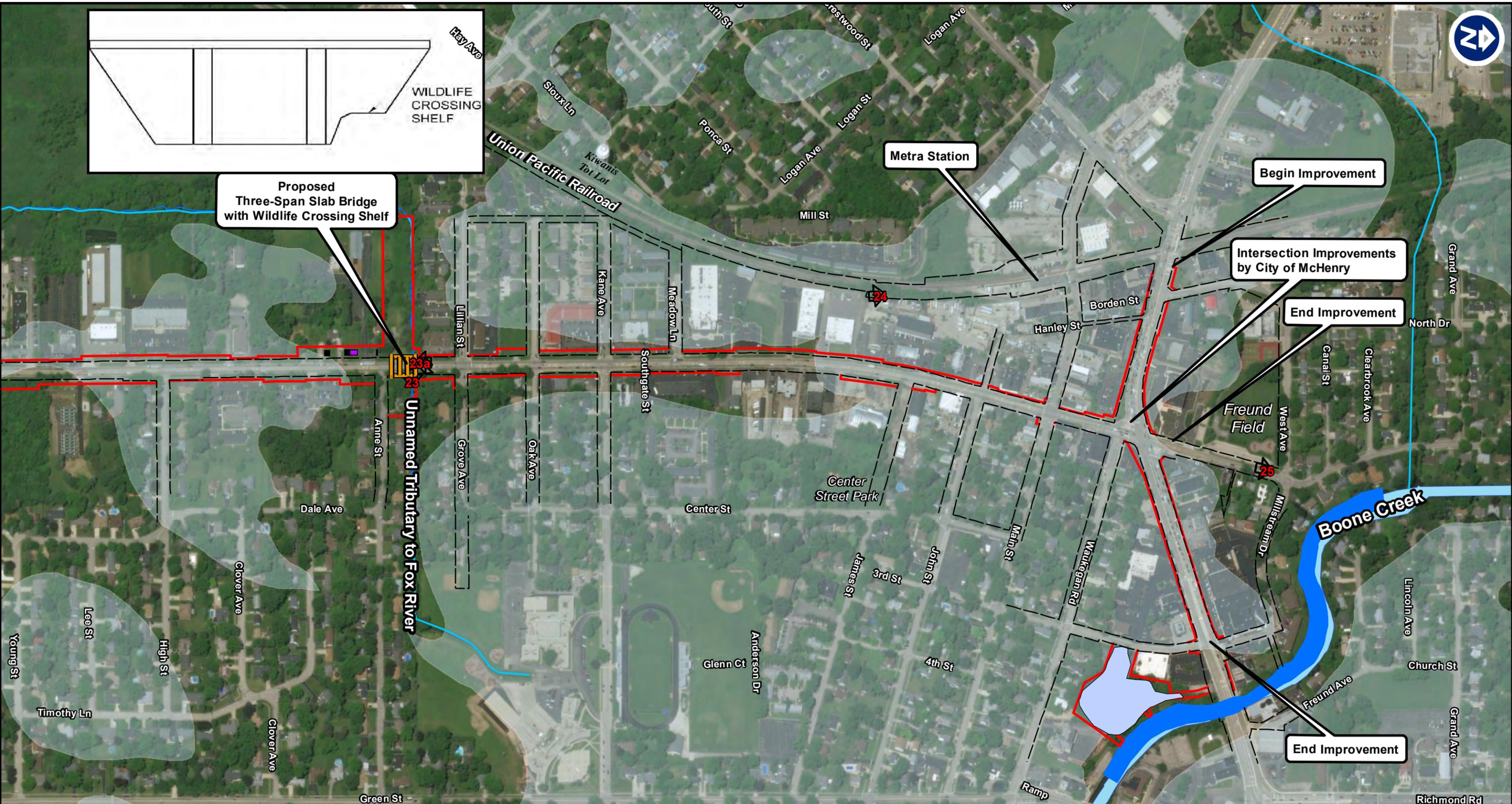
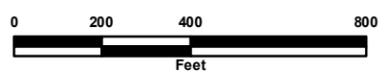


Exhibit 5
Best Management Practices Map
 Illinois Route 31
 Illinois Route 176 to Illinois Route 120
 McHenry County, Illinois
 Sheet 6 of 6



Legend

- Existing ROW
- Proposed ROW
- Proposed Retaining Wall
- ~ Water Level
- Meandering Stream
- Proposed Bioswale
- Proposed Infiltration Trench
- Proposed Natural Bottom Culvert/Bridge
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- ADID Wetland
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- WOUS (INHS)
- Lakes and Rivers
- Proposed Detention Pond/ Proposed Retention Basin
- Stream
- Outfall
- Embedded Culvert - Typical 1' Depth



Illinois Department of Transportation

Memorandum

To:	Anthony Quigley	Attn:	Tyler Petersen
From:	Scott E. Stitt	By:	Jim Curtis
Subject:	Seq. #1340F: PESA Validation Request		<i>James R. Curtis</i>
Date:	February 28, 2018		

Refer to : FAU 336 (IL 31)
 Bull Valley Road (IL120) to IL 176
 McHenry County
 P-91-135-99
 Sequence # 1340F

We have received the Environmental Survey Request (ESR) for a validation to be completed for PESA #1108V3. After reviewing this project, it is the opinion of BDE that in this case, there is sufficient information provided in ISGS PESA #1108V3 dated February 25, 2015 for the project to move ahead. The information provided in PESA #1108V3 should be used as necessary to prepare the project for letting. Should you have any questions, please contact Jim Curtis at 217/558-4653.

SES: jmv

Attachments



UNITED STATES ENVIRONMENTAL PROTECTION AGENCY
REGION 5
77 WEST JACKSON BOULEVARD
CHICAGO, IL 60604-3590

FEB 27 2017

REPLY TO THE ATTENTION OF:

E-19J

Matt Fuller
Federal Highway Administration – Illinois Division
3250 Executive Park Drive
Springfield, Illinois 62703

**Re: Draft Environmental Assessment for the IL Route 31 (From IL Route 176 to IL 120)
Project, McHenry County, Illinois**

Dear Mr. Fuller:

The U.S. Environmental Protection Agency (EPA) has reviewed the referenced document dated January 18, 2017, which was produced by the Illinois Department of Transportation (IDOT), in coordination with the Federal Highway Administration (FHWA). We are providing comments pursuant to our authorities under the National Environmental Policy Act (NEPA), Council on Environmental Quality regulations (40 CFR Parts 1500-1508), Section 309 of the Clean Air Act, Section 404 of the Clean Water Act, and in accordance with EPA's role in the NEPA-CWA Section 404 Merger Process.

The proposed project involves addition of roadway infrastructure on IL Route 31, between IL Route 176 and IL Route 120. The proposed project is broken into three separate sections: South Section, North Section, and IL Route 120 Intersection.

Alternatives carried forward for the South Section:

- Alternative S-5 (Preferred Alternative for the South Section). Widen to 4 lanes with 30-foot raised median; and
- Alternative S-6. Widen to 4 lanes with 30-foot depressed median and 10-foot shoulders.

Alternatives carried forward for the North Section:

- Alternative N-2 (preferred alternative for the North Section). Widen to 4 lanes with an 18-foot raised curb median.

Alternatives carried forward for the IL Route 120 Intersection:

- Alternative 120INT-1 (preferred alternative for the IL Route 120 Intersection). Construct a five-lane cross section (two lanes in each direction with a two-way left turn lane); and

- Alternative 120INT-3. Widen the IL Route 120 intersection for additional capacity. A 30-foot raised median with two 12-foot through lanes would be constructed in each direction.

Other Alternatives carried forward for consideration:

- No Action Alternative. The project does not proceed; and
- Congestion Management Process Alternative. Perform safety and infrastructure alterations without adding any new infrastructure.

We do not object to any of the three preferred alternatives. Additionally, we have comments that we urge FHWA and IDOT to consider before producing a Final EA (FEA) and Finding of No Significant Impact (FONSI), as stated below.

Stormwater Management

We support the stormwater management features and erosion control best management practices (BMPs) incorporated into this project. We do, however, urge FHWA and IDOT to consider constructing enough stormwater storage capacity for the volume of stormwater associated with a 500-year flood event.

Air Quality

We recommend FHWA and IDOT commit to reducing air emissions from internal combustion vehicles during the construction phase of this project. A list of BMPs is found in the enclosed document *U.S. Environmental Protection Agency Diesel Emission Reduction Checklist*.

Pollinator Habitat

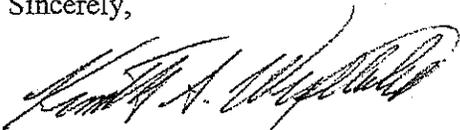
The 2014 Presidential Memorandum entitled, *Creating a Federal Strategy to Promote the Health of Honey Bees and Other Pollinators*, responds to evidence of steep declines in certain pollinator populations. Pollinators are critical contributors to our nation's economy, food system, and environmental health. Vegetation within the project area can provide much needed habitat for pollinators, providing food, shelter, and connections to other patches of habitat. Maintenance staff and landscape designers can all take steps to improve the quality of vegetation to benefit pollinators, steps that can also reduce costs, maintain public safety, and improve public good will. The FEA and FONSI should include specific commitments by FHWA and IDOT to incorporate pollinator habitat into the project where practical.

Transportation Linkages

We applaud FHWA and IDOT for committing to constructing sidewalks and providing linkages between sidewalks and local trails, consistent with IDOT's Complete Streets Policy. We recommend FHWA and IDOT identify potential locations where constructing sidewalk or trail crossings over or under IL 31 and IL 120 would be beneficial for the safety of pedestrians and bicyclists. This would include (but not limited to) sidewalk routes that lead from residential areas to schools and/or shopping centers that would require crossing IL 31 or IL 120. FHWA and IDOT should also consider the proposed wildlife crossings as potential trail and sidewalk crossings.

We are available to discuss these comments at your convenience. Please feel free to contact Mike Sedlacek of my staff at 312-886-1765, or by email at sedlacek.michael@epa.gov.

Sincerely,



Kenneth A. Westlake, Chief
NEPA Implementation Section
Office of Enforcement and Compliance Assurance

Enclosure: U.S. Environmental Protection Agency Diesel Emission Reduction Checklist

cc: Scott Czaplicki, Illinois Department of Transportation
Shawn Cirton, U.S. Fish and Wildlife Service
Soren Hall, U.S. Army Corps of Engineers – Chicago District

U.S. Environmental Protection Agency
Diesel Emission Reduction Checklist

- Use low-sulfur diesel fuel (15 ppm sulfur maximum) in construction vehicles and equipment.
- Retrofit engines with an exhaust filtration device to capture diesel particulate matter before it enters the construction site.
- Position the exhaust pipe so that diesel fumes are directed away from the operator and nearby workers, reducing the fume concentration to which personnel are exposed.
- Use catalytic converters to reduce carbon monoxide, aldehydes, and hydrocarbons in diesel fumes. These devices must be used with low sulfur fuels.
- Use enclosed, climate-controlled cabs pressurized and equipped with high efficiency particulate air (HEPA) filters to reduce the operators' exposure to diesel fumes. Pressurization ensures that air moves from inside to outside. HEPA filters ensure that any incoming air is filtered first.
- Regularly maintain diesel engines, which is essential to keep exhaust emissions low. Follow the manufacturer's recommended maintenance schedule and procedures. Smoke color can signal the need for maintenance. For example, blue/black smoke indicates that an engine requires servicing or tuning.
- Reduce exposure through work practices and training, such as turning off engines when vehicles are stopped for more than a few minutes, training diesel-equipment operators to perform routine inspection, and maintaining filtration devices.
- Repower older vehicles and/or equipment with diesel- or alternatively-fueled engines certified to meet newer, more stringent emissions standards. Purchase new vehicles that are equipped with the most advanced emission control systems available.
- Use electric starting aids such as block heaters with older vehicles to warm the engine reduces diesel emissions.
- Use respirators, which are only an interim measure to control exposure to diesel emissions. In most cases, an N95 respirator is adequate. Workers must be trained and fit-tested before they wear respirators. Depending on work being conducted, and if oil is present, concentrations of particulates present will determine the efficiency and type of mask and respirator. Personnel familiar with the selection, care, and use of respirators must perform the fit testing. Respirators must bear a NIOSH approval number.
- Per Executive Order 13045 on Children's Health¹, EPA recommends operators and workers pay particular attention to worksite proximity to places where children live, learn, and play, such as homes, schools, daycare centers, and playgrounds. Diesel emission reduction measures should be strictly implemented near these locations in order to be protective of children's health.

¹ Children may be more highly exposed to contaminants because they generally eat more food, drink more water, and have higher inhalation rates relative to their size. Also, children's normal activities, such as putting their hands in their mouths or playing on the ground, can result in higher exposures to contaminants as compared with adults. Children may be more vulnerable to the toxic effects of contaminants because their bodies and systems are not fully developed and their growing organs are more easily harmed. EPA views childhood as a sequence of lifestages, from conception through fetal development, infancy, and adolescence.



Illinois Department of Transportation

Office of Highways Project Implementation / Region 1 / District 1
201 West Center Court / Schaumburg, Illinois 60196-1096

FILE COPY

Project and Environmental Studies
Illinois Route 31
Illinois Route 176 to Illinois Route 120
McHenry County

December 19, 2017

Mr. Kenneth A. Westlake
Chief, NEPA Implementation Section
Office of Enforcement and Compliance Assurance
US Environmental Protection Agency, Region 5
77 West Jackson Boulevard
Chicago, IL 60604-3590

Dear Mr. Westlake:

Thank you for your February 27, 2017 letter regarding the Illinois Department of Transportation's (Department) proposed improvement of Illinois Route 31 (IL 31) from Illinois Route 176 to Illinois Route 120 in McHenry County. Funding for Phase II engineering is included in the Department's FY 2018-2023 Proposed Highway Improvement Program. The remaining elements of the project will be included in our priorities for future funding consideration among similar improvement needs throughout the region. The general scope of work for this improvement is anticipated to consist of roadway widening and reconstruction to provide two lanes in each direction separated by a median, bicycle and pedestrian accommodations, and intersection and drainage improvements.

In your letter, the US Environmental Protection Agency (USEPA) provided comments on the IL 31 Environmental Assessment. Responses to your comments are provided below:

Stormwater Management

The project has been designed to provide stormwater detention to the greatest extent practicable. Additional detention storage would result in increased impacts to adjacent property and environmental resources, and would require additional right-of-way. It should be noted that the drainage plan was redesigned after receiving concurrence on the preferred alternative to meet the US Army Corps of Engineer's Stormwater Performance Standards Guidance for water quality volume retention which resulted in nine additional acres of right-of-way.

Air Quality

Attached is a table correlating the BMPs listed in the USEPA's *Diesel Emission Reduction Checklist* to the Department's *Standard Specifications for Road and Bridge Construction* and other specifications. In addition, the following Special Design/Construction Consideration will be added to the Combined Design Report.

During Phase II, contract plan preparation, and Phase III, construction, the Department will develop construction special provisions to address possible environmental effects related to construction activities conducted within close proximity to residential areas, schools, parks, childcare locations and other sensitive receptors. The special provisions will address the following areas of concern:

Communication Plan - Notify local communities about construction activities that may contribute to temporary noise and air quality effects. The special provision will include requirements for developing notification procedures regarding regional air quality, local construction activities, and emergency situations.

Dust Control Plan – This would augment the existing dust control methods currently in place in Department's Standard Specifications. The Dust Control Plan special provision will require development of methods and procedures that the contractor will employ during construction activities to reduce or contain construction dust in urban environments. These may be best practices from other similarly sized and located projects that are applicable to this project at the time that the special provision is drafted in Phase II, contract plan preparation.

Contractor's Site Health and Safety Plan - Although contractors guarantee the Department a safe work place for their employees, this special provision will consider the application of additional best practices for worker protection that may be applicable at that time the special provision is drafted in Phase II, contract plan preparation. This special provision will improve contractor awareness of worker protection best practices as identified by the USEPA and other environmental agencies.

Pollinator Habitat

The following commitment will be added to the Environmental Assessment and Combined Design Report.

Pollinator habitat shall be incorporated into the project where practical. The Phase II landscaping and signing plans shall be developed in accordance with the current version of the FHWA publication *Pollinators and Roadsides: Best Management Practices for Managers and Decision Makers*, and coordinated through the Department's Bureau of Programming/ Environmental Studies Unit. (USEPA letter dated February 27, 2017).

Mr. Kenneth A. Westlake
December 19, 2017
Page three

Transportation Linkages

Please note that grade separated structures for pedestrians and bicyclists would be entirely a local agency cost for design, construction, and land acquisition. At this time, there are no local agency sponsors for grade separated structures. It should be noted that there are eight existing or new signalized intersections along the project that include at-grade pedestrian crossing accommodations such as ADA ramps, pedestrian signals, and crosswalks.

The proposed culverts identified for wildlife crossings are only appropriate for small to medium sized mammals as large as foxes or coyotes. To accommodate pedestrians and bicyclists, the roadway profile would need to be raised several feet to provide appropriate vertical clearance while keeping the path above normal water levels. For instance, the tallest culvert identified as a wildlife crossing is six feet high. The IL 31 profile would need to be raised four feet to meet design standard for a path crossing. This would have compounding impacts to adjacent areas. The wildlife crossings are typically located in low areas connecting wetlands and waters. Raising the roadway profile at these locations would result in additional impacts to these resources.

If you have any questions or need additional information, please contact me or Kimberly Murphy, Consultant Studies Unit Head, at (847) 705-4791.

Very truly yours,

Anthony J. Quigley, P.E.
Region One Engineer



By:
Issam Rayyan, P.E.
Acting Bureau Chief of Programming

cc: Robin Helmerichs, FHWA
John Sherrill, IDOT-BDE

bcc: Sam Mead
Perry Masouridis
File

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USEPA Response.docx

Table
Air Quality Best Management Practice Reference

USEPA Diesel Emission Reduction Checklist	Illinois Department of Transportation Specification ^{1 2}
Use low-sulfur diesel fuel (15 ppm sulfur maximum) in construction vehicles and equipment.	SSRBC Article 107.41 (b) Construction Air Quality
Retrofit engines with an exhaust filtration device to capture diesel particulate matter before it enters the construction site.	BDE Special Provision – Construction Air Quality – Diesel Retrofit
Position the exhaust pipe so that diesel fumes are directed away from the operator and nearby workers, reducing the fume concentration to which personnel are exposed.	Special Design/Construction Consideration for project
Use catalytic converters to reduce carbon monoxide, aldehydes, and hydrocarbons in diesel fumes. These devices must be used with low sulfur fuels.	SSRBC Article 107.41 (b) Construction Air Quality
Use enclosed, climate-controlled cabs pressurized and equipped with high efficiency particulate air (HEPA) filters to reduce the operators' exposure to diesel fumes. Pressurization ensures that air moves from inside to outside. HEPA filters ensure that any incoming air is filtered first.	Special Design/Construction Consideration for project
Regularly maintain diesel engines, which is essential to keep exhaust emissions low. Follow the manufacturer's recommended maintenance schedule and procedures. Smoke color can signal the need for maintenance. For example, blue/black smoke indicates that an engine requires servicing or tuning.	SSRBC Article 105.03 (d) Diesel Vehicle Emissions Control Deduction
Reduce exposure through work practices and training, such as turning off engines when vehicles are stopped for more than a few minutes, training diesel-equipment operators to perform routine inspection, and maintaining filtration devices.	SSRBC Article 107.41 (a) Construction Air Quality, and Article 105.03 (c) Idling Restriction Deficiency Deduction
Repower older vehicles and/or equipment with diesel- or alternatively-fueled engines certified to meet newer, more stringent emissions standards. Purchase new vehicles that are equipped with the most advanced emission control systems available.	Special Design/Construction Consideration for project
Use electric starting aids such as block heaters with older vehicles to warm the engine reduces diesel emissions.	Special Design/Construction Consideration for project

¹ Standard Specification for Road and Bridge Construction, 2016 (SSRBC)

² Bureau of Design and Environment Special Provision (BDE Special Provision)

Table (continued)
Air Quality Best Management Practice Reference

USEPA Diesel Emission Reduction Checklist	Illinois Department of Transportation Specification
<p>Use respirators, which are only an interim measure to control exposure to diesel emissions. In most cases, an N95 respirator is adequate. Workers must be trained and fit-tested before they wear respirators. Depending on work being conducted, and if oil is present, concentrations of particulates present will determine the efficiency and type of mask and respirator. Personnel familiar with the selection care and use of respirators must perform the fit testing. Respirators must bear a NIOSH approval number.</p>	<p>Special Design/Construction Consideration for project</p>
<p>Per Executive Order 13045 on Children's Health, EPA recommends operators and workers' pay particular attention to worksite proximity to places where children live, learn, and play, such as homes, schools, daycare centers, and playgrounds. Diesel emission reduction measures should be strictly implemented near these locations in order to be protective of children's health.</p>	<p>SSRBC Article 107.41 (b) Construction Air Quality (staging in sensitive areas) also covered under Special Design and Construction Considerations (Communication Plan)</p>



REPLY TO
ATTENTION OF:

DEPARTMENT OF THE ARMY
CHICAGO DISTRICT, CORPS OF ENGINEERS
231 SOUTH LA SALLE STREET
CHICAGO, ILLINOIS 60604-1437

March 15, 2017

Technical Services Division
Regulatory Branch
LRC-2011-336

SUBJECT: NEPA/404 Merger Process Comments on the Draft Environmental Assessment for the IL Route 31 (from IL Route 176 to IL 120) Project, McHenry County, Illinois

Catherine Batey
Federal Highway Administration
3250 Executive Park Drive
Springfield, Illinois 62703

Dear Ms. Batey:

This letter is in response to your request that the Department of the Army (Corps) provide comments on the Draft Environmental Assessment for the IL Route 31 (from IL Route 176 to IL 120) Project, McHenry County, Illinois (EA). Various Federal and state agencies are providing a concurrent review of the project under the terms and conditions as set forth in the "Statewide Implementation Agreement National Environmental Policy Act And Clean Water Act Section 404 Concurrent NEPA/404 Processes For Transportation Projects in Illinois".

Following a review of the EA, the Corps provides the following comments:

1. Best Management Practices (BMP) for the Preferred Alternative

The Water Quality Volume does not meet the requirement around Sleepy Hollow Creek, a perennial stream where two pollution intolerant fish species were identified. The permit application will need to provide a detailed BMP proposal that includes information on what efforts were made to meet the 1.25" recommended storage.

In addition, Exhibit 5 appears to show outfalls discharging directly into wetlands. Stormwater outfalls should be located as far from the resource as possible. This information should be made available in a detailed BMP proposal. Finally, please make sure to label all wetlands and streams in the BMP proposal.

2. Water Resources Impacts

In addition to indicating whether proposed impacts are permanent or temporary, please make sure to qualify each proposed impact. Avoidance and minimization should also be discussed. For example, Sheet 13 in Exhibit 3 shows temporary impacts proposed in W6. It appears that a detention basin is proposed at this location, but the particular impacts are unclear.

If so, what avoidance and minimization measures were considered? It should be noted that detention in-line with a stream is generally not permissible. This information could be presented in the form of a table.

The information presented for culverts does not contain any information on the stream channel width. For culverts spanning a stream, this information should be provided in the application. Culverts should be designed to prevent the restriction of expected high water flows, and should be designed so as not to impede low water flows or the movement of aquatic organisms. Typically, a stream channel cross section taken upstream, at and downstream of the culvert should help determine if proposed culvert is meeting this requirement.

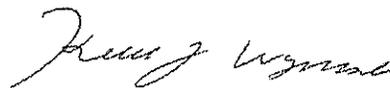
3. Proposed Mitigation

The proposed mitigation for impacts to Squaw Creek is on-site, in-kind replacement with meanders. The proposal will need to include existing and proposed cross sections of the stream. A monitoring and management plan will be required.

The EA states that a mitigation ratio of 1.5:1 will be applied to all non-ADID wetlands and a mitigation ratio of 3:1 will be applied to HQAR wetlands. There are instances where mitigation ratio of up to 6:1 are applied for ADID wetlands depending on the type of ADID classification. Note that stream mitigation will be required for areas that are considered stream loss.

Thank you for the opportunity to comment. If you have any questions, please contact Julie Rimbault of my staff by telephone at 312-846-5542, or email at Julie.C.Rimbault@usace.army.mil.

Sincerely,



Keith L. Wozniak
Chief, West Section
Regulatory Branch

Copy Furnished:

U.S. Environmental Protection Agency (Ken Westlake)
U.S. Fish and Wildlife Service (Shawn Cirton)
Illinois Department of Natural Resources (Sheldon Fairfield)
Illinois Department of Transportation (Anthony Quigley)



Illinois Department of Transportation

Office of Highways Project Implementation / Region 1 / District 1
201 West Center Court / Schaumburg, Illinois 60196-1096

Project and Environmental Studies
Illinois Route 31
Illinois Route 176 to Illinois Route 120
McHenry County

December 19, 2017

Mr. Keith Wozniak
Chief, CELRC-TS-RW
U.S. Army Corps of Engineers
Chicago District - Regulatory Branch
231 South LaSalle Street
Suite 1500
Chicago, IL 60604

Dear Mr. Wozniak:

Thank you for your March 15, 2017 letter regarding the Illinois Department of Transportation's (Department) proposed improvement of Illinois Route 31 (IL 31) from Illinois Route 176, to Illinois Route 120 in McHenry County. Funding for Phase II engineering is included in the Department's FY 2018-2023 Proposed Highway Improvement Program. The remaining elements of the project will be included in our priorities for future funding consideration among similar improvement needs throughout the region. The general scope of work for this improvement is anticipated to consist of roadway widening and reconstruction to provide two lanes in each direction separated by a median, bicycle and pedestrian accommodations, and intersection and drainage improvements.

In your letter, the US Army Corps of Engineers (USACE) provided comments on the IL 31 Environmental Assessment. Responses to your comments are provided below.

Best Management Practices

The drainage plan was redesigned to meet the USACE's Stormwater Performance Standards Guidance for water quality volume (WQV) retention. As a result, nine acres of additional right-way is proposed to meet the guidance. The guidance was met for two of the three watersheds and for the project as a whole. The guidance could not be met for the Sleepy Hollow Creek watershed due to the existing terrain and environmental resources. The watershed is located in a 150-foot deep valley along IL 31 that has steep longitudinal grades which do not allow for effective storm water storage. In addition, environmental resources such as wetlands, waters, and oak and hickory trees are concentrated at the bottom of the valley. Even though the guidance was subsequently rescinded, the proposed retention measures are still included in the project.

Discharging storm water directly into wetlands and waters has been avoided where feasible. Locations that could not be avoided include culverts at existing low points. BMPs are proposed upstream of these locations to the greatest extent practicable. A detailed BMP plan will be included in the Section 404 permit application.

Water Resource Impacts

Avoidance and minimization measures were discussed during the four NEPA/404 Merger meetings as well as five USACE meetings for the project. Measures include alignment shifts, retaining walls, and lane and median width reduction. Impacts are anticipated to be further reduced during Phase II, contract plan preparation. Impacts to wetlands and waters are generally a result of roadway widening. As you noted, there are some locations where the impacts are not apparent and are discussed below:

- W6 (Sheet 13) – Potential temporary impacts due to the construction of the adjacent detention basin
- W1 (Sheets 14 and 15) – The existing box culvert is proposed to be replaced with a 90-foot span bridge. As a result, the Unnamed Tributary to the Fox River (W1) will be regraded upstream and downstream of the bridge and will include a low-flow channel
- W5 (Sheet 16) – Temporary impacts for the construction of outlet pipes from proposed detention basin

There are three waters identified as permanent streams that cross IL 31. The Section 404 permit application will include upstream and downstream channel cross sections.

Proposed Mitigation

The detailed design for the proposed meandering of Squaw Creek will be performed during Phase II, contract plan preparation. The proposed cross sections and monitoring and management plan will be provided as part of the Section 404 permit application. The Department acknowledges that the actual mitigation ratios will be determined as part of the application process.

Mr. Keith Wozniak
December 19, 2017
Page three

If you have any questions or need additional information, please contact me or Kimberly Murphy, Consultant Studies Unit Head, at (847) 705-4791.

Very truly yours,

Anthony J. Quigley, P.E.
Region One Engineer

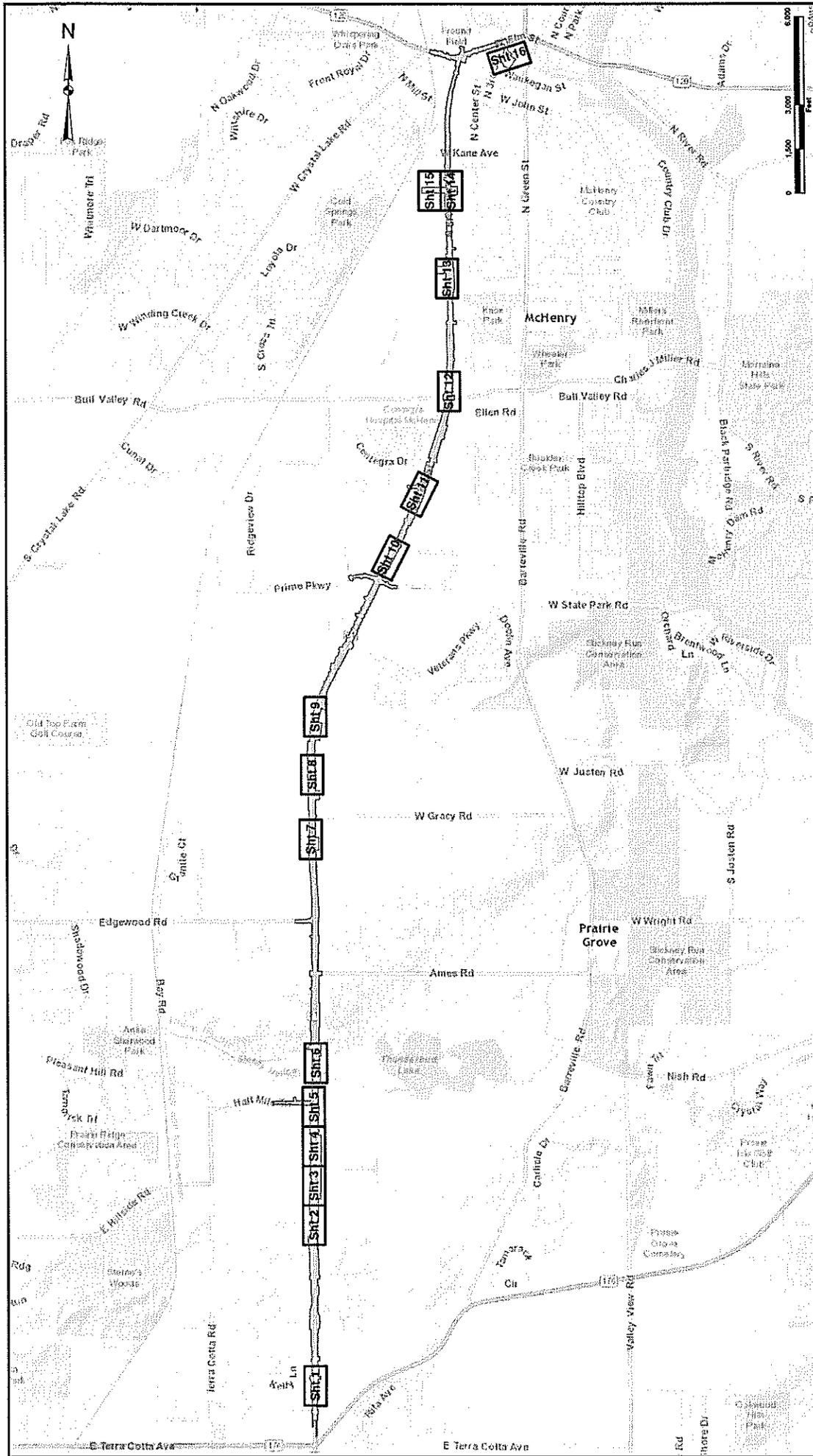


By:
Issam Rayyan, P.E.
Acting Bureau Chief of Programming

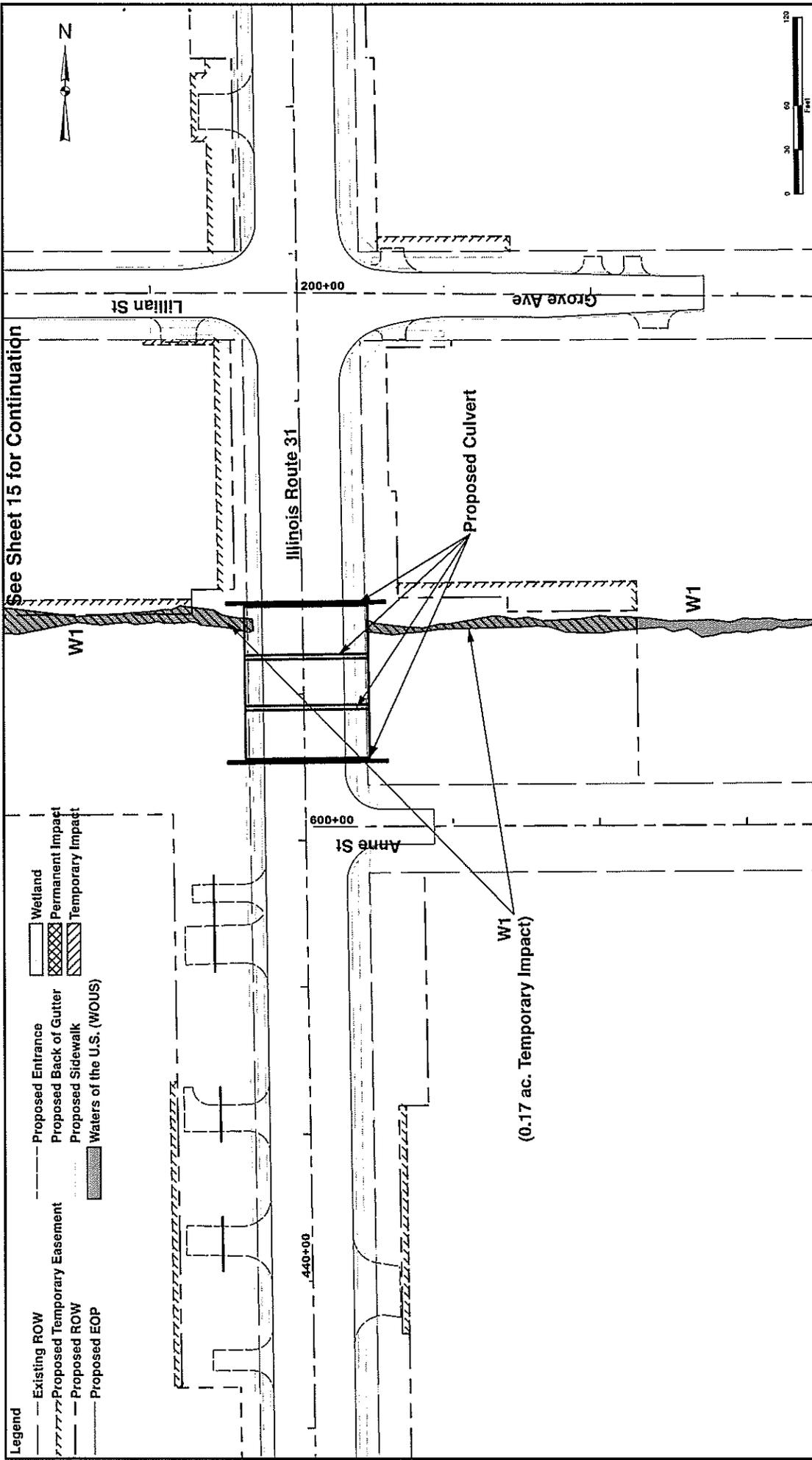
cc: Robin Helmerichs, FHWA
John Sherrill, BDE

bcc: Sam Mead
Perry Masouridis
File

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USACE Response.docx



State of Illinois Department of Transportation		Illinois Route 31 Wetlands Impact Exhibit Key Map	
Date:	Designed:	Revised:	Total Sheet No.
Plot Scale:	Drawn:	Revised:	
Plate Date:	Checked:	Revised:	
	Date:	Revised:	Scale: 1" = 3000'



W1
(0.17 ac. Temporary Impact)

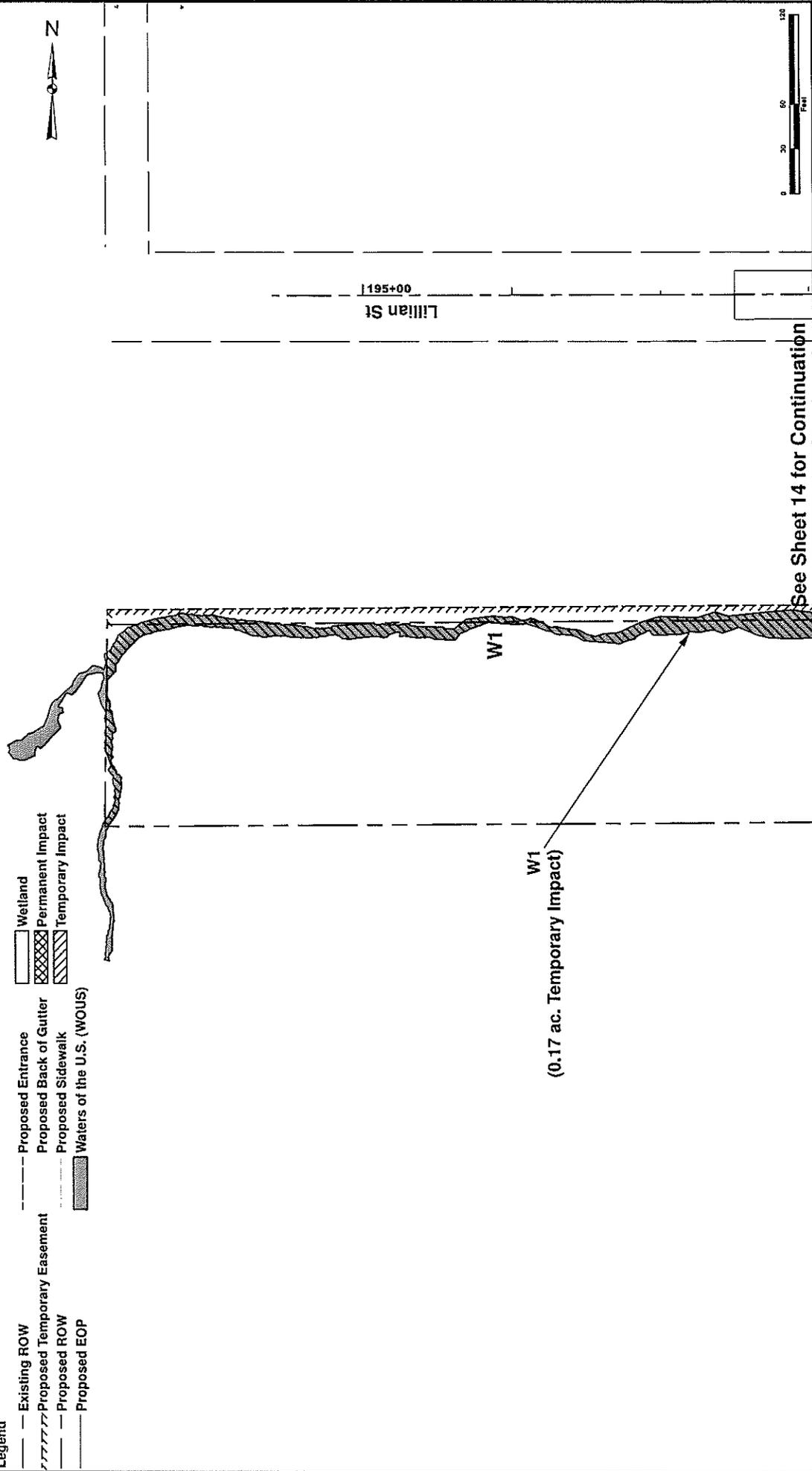
See Sheet 15 for Continuation

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Plate Date:		Checked:	Revised:	WOUS W1		16	14
		Date:	Revised:			Scale: 1" = 60'	

TEMPORARY IMPACT SHEET

Legend

- Existing ROW
- - - Proposed Temporary Easement
- - - Proposed ROW
- - - Proposed EOP
- Proposed Entrance
- Proposed Back of Gutter
- Proposed Sidewalk
- Waters of the U.S. (WOUS)
- Wetland
- ▨ Permanent Impact
- ▧ Temporary Impact



Date:	Designed:	Revised:	Illinois Route 31 Wetlands Impact Exhibit WOUS W1	Total Sheet Sheets 16	Total Sheet No. 15
Plot Scale:	Drawn:	Revised:			
Plot Date:	Checked:	Revised:			
			State of Illinois Department of Transportation	Scale: 1" = 60'	

AGENDA ITEM #4

Illinois Route 31
Illinois Route 176 to Illinois Route 120
Job. No. P-91-135-99
McHenry County

January 10, 2018

This is the 28th presentation for this project. The last presentation was on March 15, 2017. The purpose of this meeting is to provide an update on the Environmental Assessment (EA) Errata and Finding of No Significant Impact (FONSI).

Since the last presentation, the plans were updated where feasible to address public hearing comments, comments were responded to via a newsletter and individual correspondence, an Initial Financial Plan was prepared, and the draft FONSI was drafted. In addition, response letters to the U.S. Army Corps of Engineers and U.S. Environmental Protection Agency were mailed. The presentation used annotated public hearing exhibits where notable additional analysis and plan revisions were made.

- Location 1 – Potential cross access across from River Birch Boulevard. River Birch Boulevard is permitted for a future traffic signal once subdivision to the east is built.
- Locations 2, 9 and 12 - The District performed updated traffic counts and signal warrants after the public hearing. Traffic signals are not warranted at Drake Drive, Ames Road, or Veterans Parkway. The City of McHenry noted Veterans Parkway is their top priority for a new traffic signal.
- Location 3 - In response to comments from the McHenry County Conservation District, Environmental Defenders of McHenry County, Nunda Township and others impacts to further minimize impacts were evaluated. It was discovered that one of the four tree surveys was not counted in the impact analysis. As a result there were 149 and 33 oak and hickory impacts instead of the reported 84 and 17. Measures such as adjusting ditches and sidewalk were effective methods that were implemented and saved approximately 20 oak/hickories. Retaining walls were evaluated and could save approximately 38 trees between Drake Drive and Shady Oaks Lane; however their high construction and long term maintenance costs were not reasonable. In lieu of retaining walls, the Department will work with adjacent municipalities and the McHenry County Conservation District to identify replacement areas near the project.
- Location 4 - Proposed bioswales were eliminated at the Greenhouses of Crystal Lake to avoid septic and parking impacts
- Location 5 – Completed agreements were received from Terra Cotta Industries for their northern access drive, and with Direct Steel across from Ames Road.

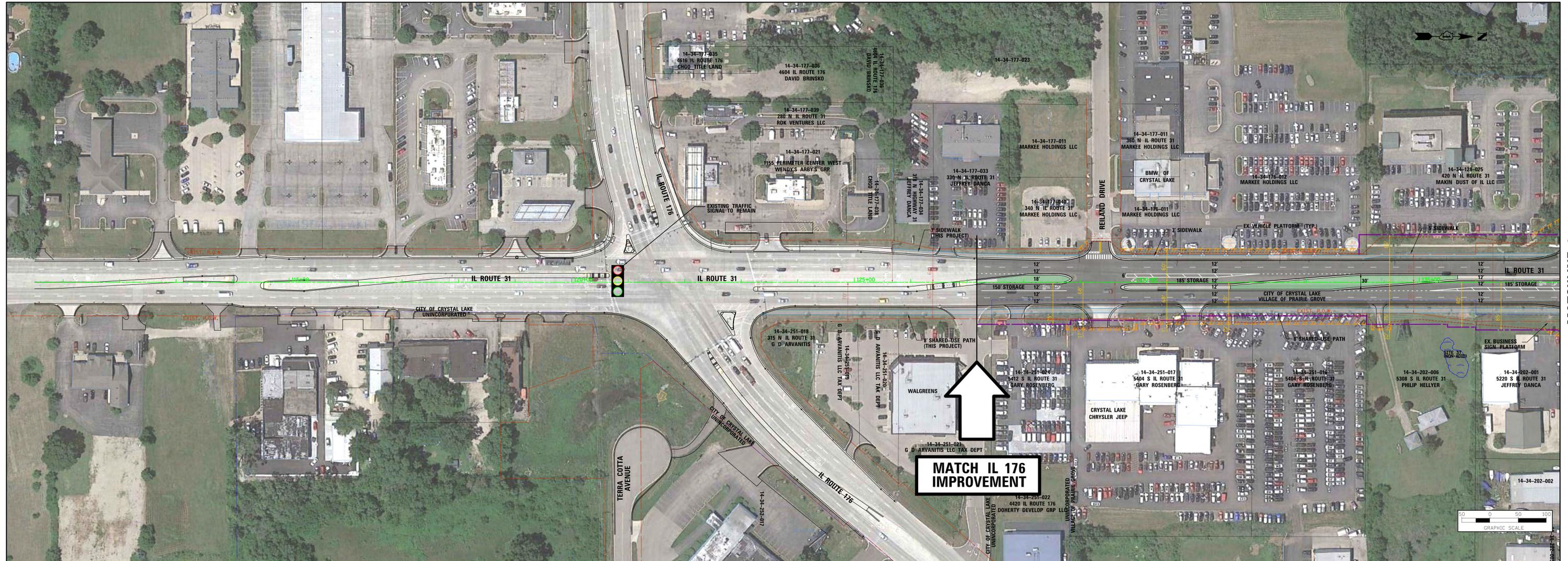
- Locations 6, 7 and 8 (Oak Grove Subdivision) – The traffic noise analysis was revised to include an evaluation of a noise abatement wall along the subdivision. The barrier was found to be feasible and acoustically reasonable, but exceeded the allowable cost per benefitted receptor. The plans were revised to maintain the existing landscape berm along IL 31 south of Ames Road. A drainage berm was constructed by the subdivision which shifted the flow at Outfall #12 (south of Ames Road) south along IL 31 instead of through the subdivision. The berm and associated subdivision drainage features were resurveyed after the public hearing and modifications were made to maintain the drainage berm. One of the subdivision's main complaints was that the District did not coordinate with them during the study. The residences along IL 31 were invited to previous public meetings. Residents beyond those invited attended the meetings, including one that was on the Village of Prairie Grove commission. Extensive coordination took place with the Village throughout the study; however none of that staff is currently at the Village.
- Location 10 – In follow-up to a letter from the Village of Prairie Grove and correspondence from three property owners, a meeting was held with the property owners, Senator Althoff and the Village engineer to discuss median breaks and cross access. A median break will be allowed if a cross access agreement is received.
- Location 11 – Potential cross access with four property owners to the median opening at Veterans Parkway.
- Locations 13, 14 and 15 - Side slopes, sidewalks and swales were revised to avoid utility boxes at a battery farm, McDonald's retaining wall and business parking impacts.
- Location 16 and 17 - A meeting with City of McHenry and Mr. Ron Bykowski, a multi-parcel property owner along IL 120, was held. Mr. Bykowski requested the right-of-way be minimized to only what is needed for the improvement, and not take the two full parcels where the two commercial displacements were identified due to elimination of parking. The proposed improvements will directly impact the buildings, but the proposed right-of-way will be changed to go through the buildings. Temporary easements will be proposed for the remaining area needed to remove the building. The City and Mr. Bykowski also requested the existing right-in at Waukegan Road be maintained instead of the proposed cul-de-sac. The District will evaluate this when the City sends a letter to the Department requesting this. One of the commercial buildings would not be displaced if the right-in proposed in lieu of the cul-de-sac.
- Location 18 - Second Amendment Sports, located at IL 120 and 3rd Street, constructed a new building last summer which affects the design of the regional detention basin. A seventeen foot tall retaining wall will be required to avoid impacting the new building.

The revisions did not affect most environmental resources. Outstanding items pending District review include revised right-of-way and water quality analysis. The Errata and FONSI will be submitted to BDE prior to the next coordination meeting. The draft Initial Financial Plan will be submitted to FHWA for review.

A Special Design/Construction Consideration will be added to the Combined Design Report to complete a value engineering study no later than when construction plans are 30% complete to allow for the implementation of the recommendations without delaying the project.

Kimberly Murphy/Ryan Dettmann– IDOT/Programming
Scott Czaplicki – IDOT/Programming-Consultant

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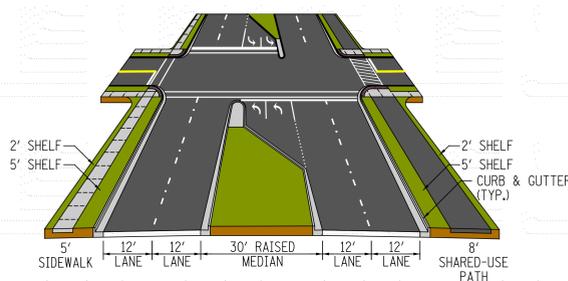


MATCHLINE 1
(SEE SHEET 2)

LEGEND

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	EX. PROPERTY LINE		EX. WETLANDS		PROP. TRAFFIC SIGNAL
	EX. RIGHT OF WAY LINE		PROP. GRASS MEDIAN		
	PROP. CENTERLINE		PROP. MEANDERING STREAM / BASIN		
	PROP. LIMIT OF CONST. (APPROXIMATE)		PROP. PAVEMENT SURFACE		
	PROP. RIGHT OF WAY LINE		PROP. RAISED CURB MEDIAN		
	PROP. TEMP. EASEMENT LINE		COMMERCIAL DISPLACEMENT		
			RESIDENTIAL DISPLACEMENT		
			DRAINAGE OUTFALL		

PROPOSED TYPICAL SECTION



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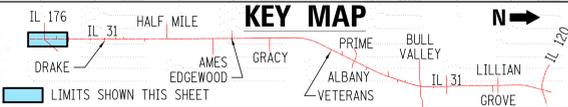
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PUBLIC HEARING
FEBRUARY 8, 2017

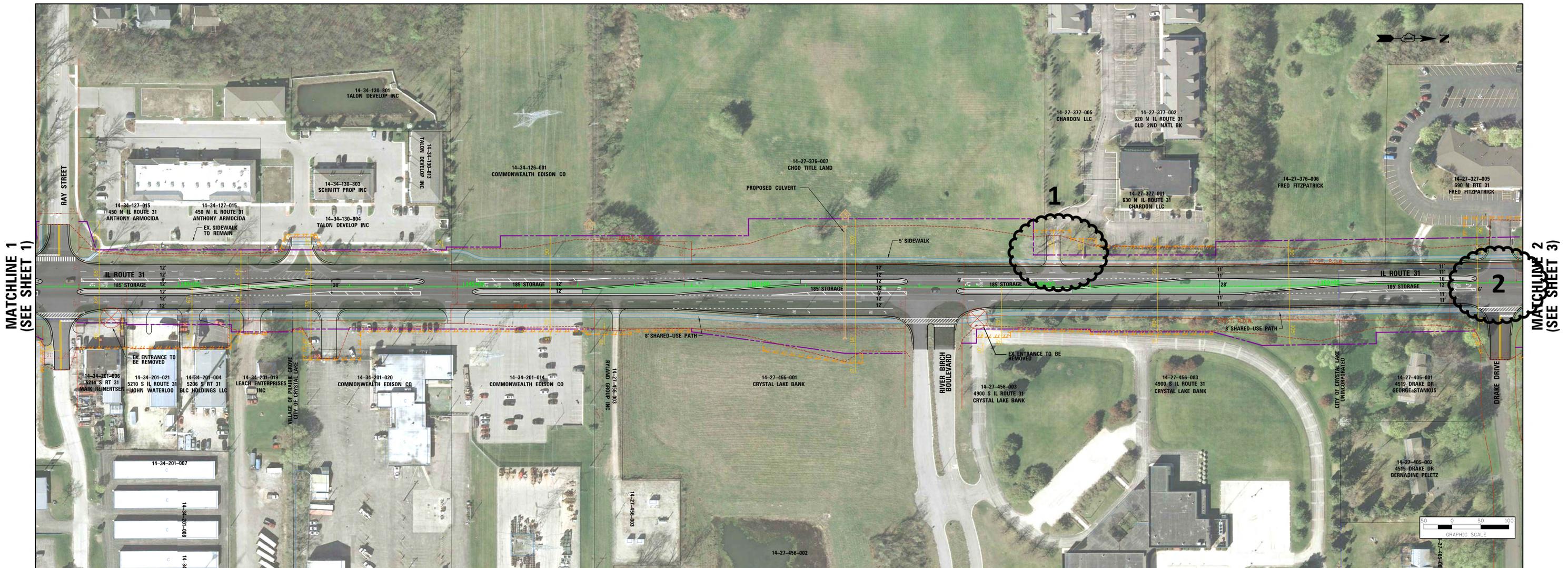
ILLINOIS ROUTE 31
SOUTH SECTION: ILLINOIS ROUTE 176 TO SOUTH OF BULL VALLEY ROAD

PREFERRED ALTERNATIVE
SHEET 1 OF 15



Illinois Route 31
ROUTE 176 TO ROUTE 120
www.idot.illinois.gov/projects/il31

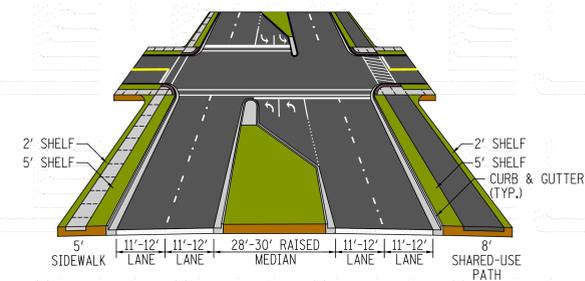




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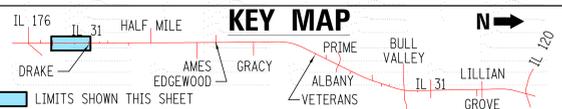
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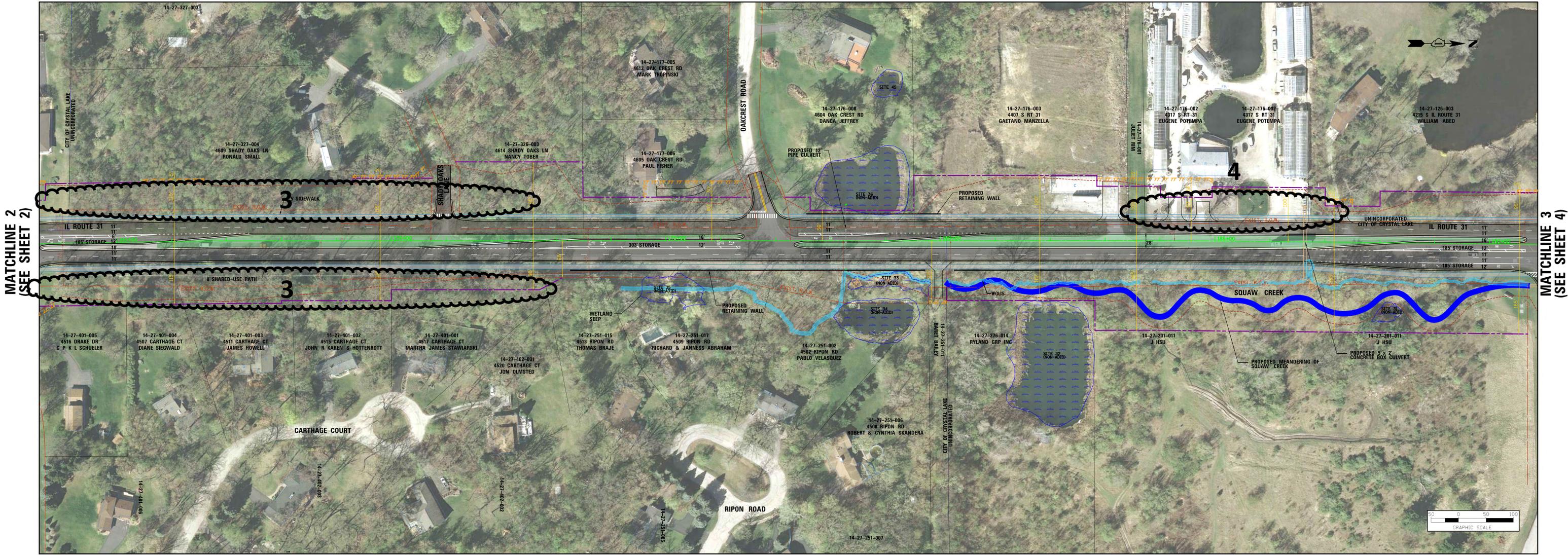
PROPOSED TYPICAL SECTION



PRELIMINARY

SUBJECT TO CHANGE
PUBLIC HEARING
FEBRUARY 8, 2017



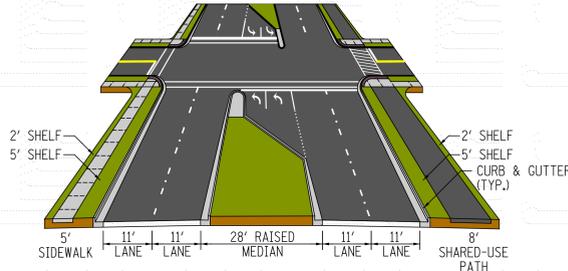


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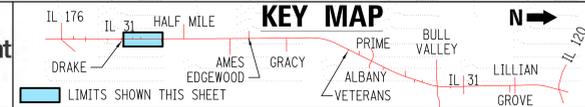
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	PROP. RIGHT OF WAY LINE
	PROP. TEMP. EASEMENT LINE
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	EX. WATERS OF THE U.S. (WOUS)
	EX. WETLANDS
	PROP. GRASS MEDIAN
	PROP. MEANDERING STREAM / BASIN
	PROP. PAVEMENT SURFACE
	PROP. RAISED CURB MEDIAN
	COMMERCIAL DISPLACEMENT
	RESIDENTIAL DISPLACEMENT
	DRAINAGE OUTFALL
	EX. TRAFFIC SIGNAL TO REMAIN
	EX. TRAFFIC SIGNAL TO BE REPLACED
	PROP. TRAFFIC SIGNAL

PROPOSED TYPICAL SECTION



PRELIMINARY
SUBJECT TO CHANGE
PUBLIC HEARING
FEBRUARY 8, 2017

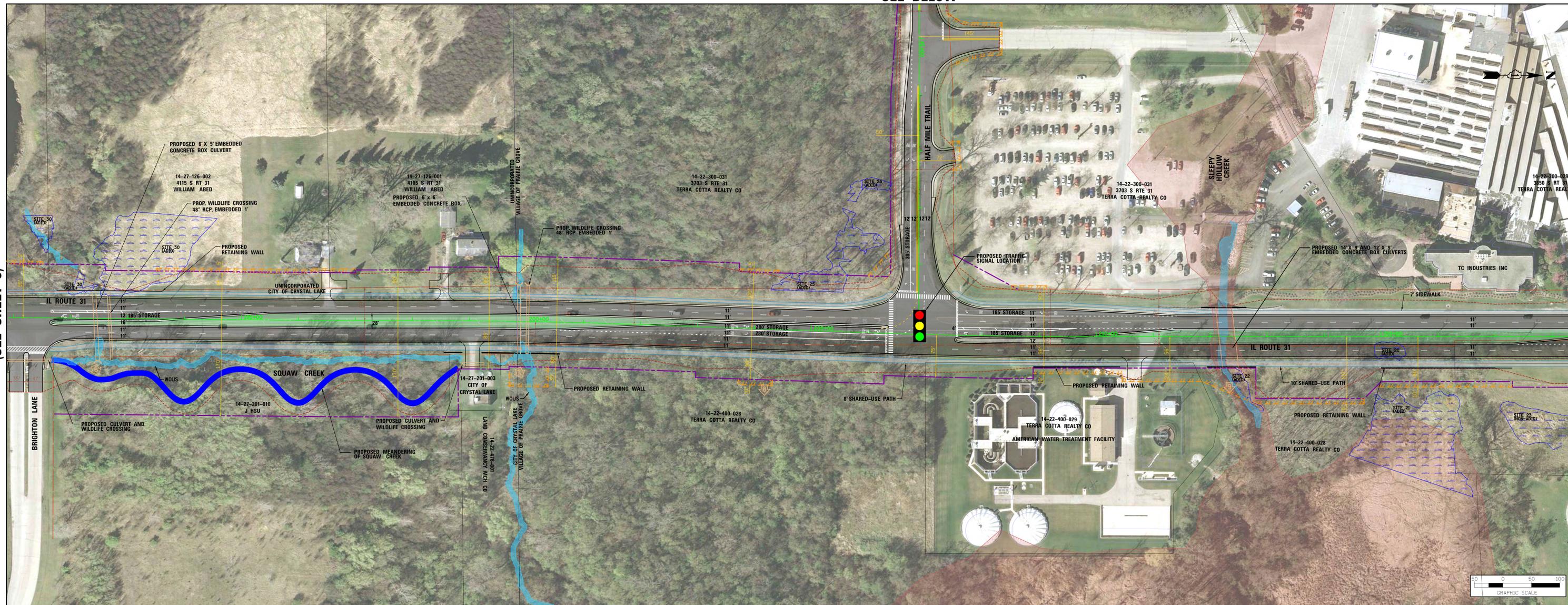


ILLINOIS ROUTE 31
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MATCHLINE
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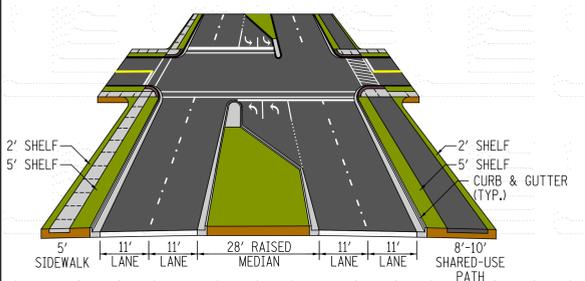
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MATCHLINE 4
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			DRAINAGE OUTFALL		

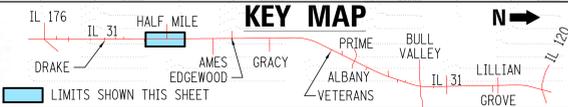
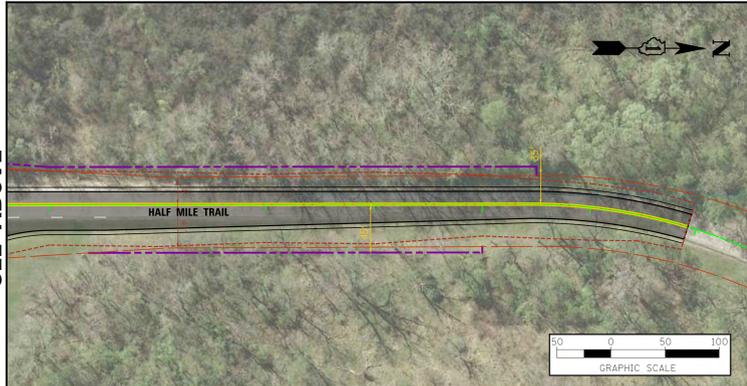
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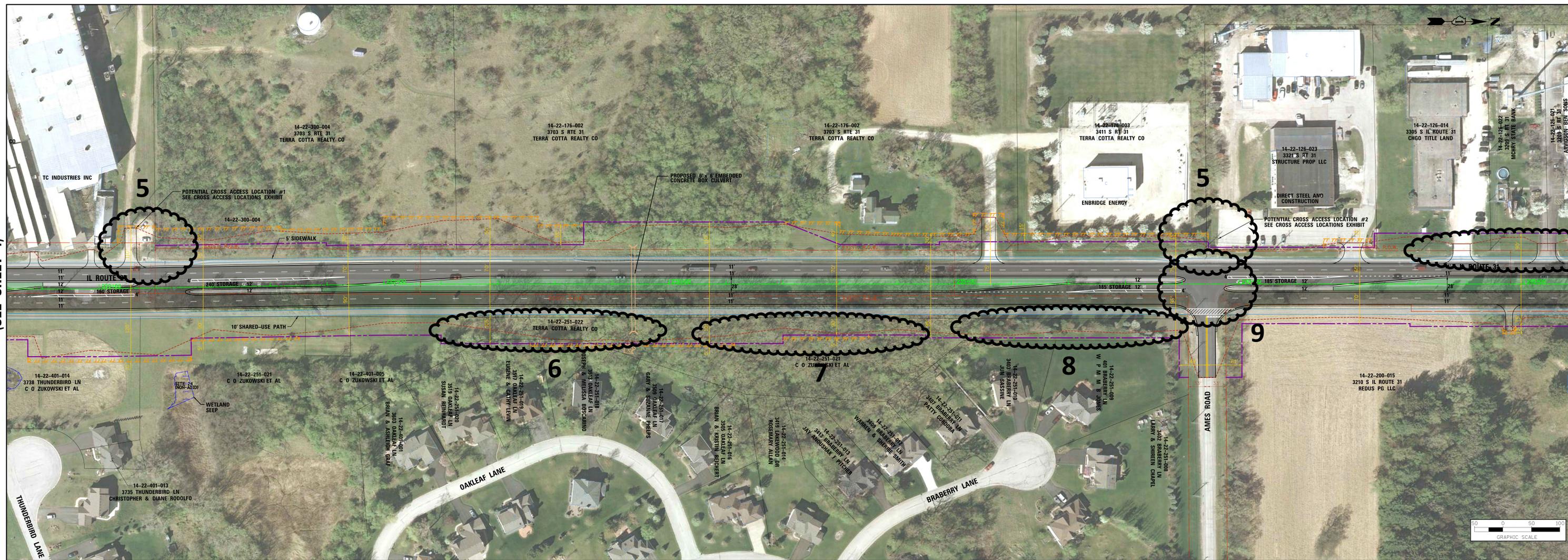
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PUBLIC HEARING
FEBRUARY 8, 2017

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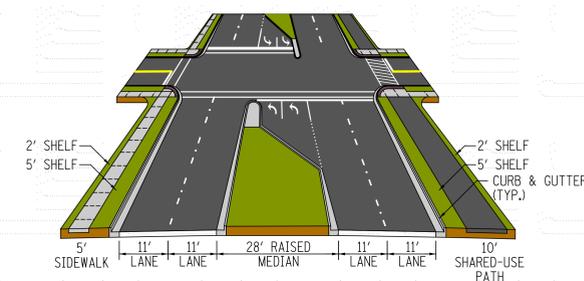
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MATCHLINE 5
(SEE SHEET 6)



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			DRAINAGE OUTFALL		

PROPOSED TYPICAL SECTION

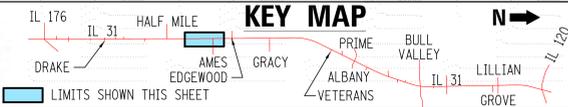


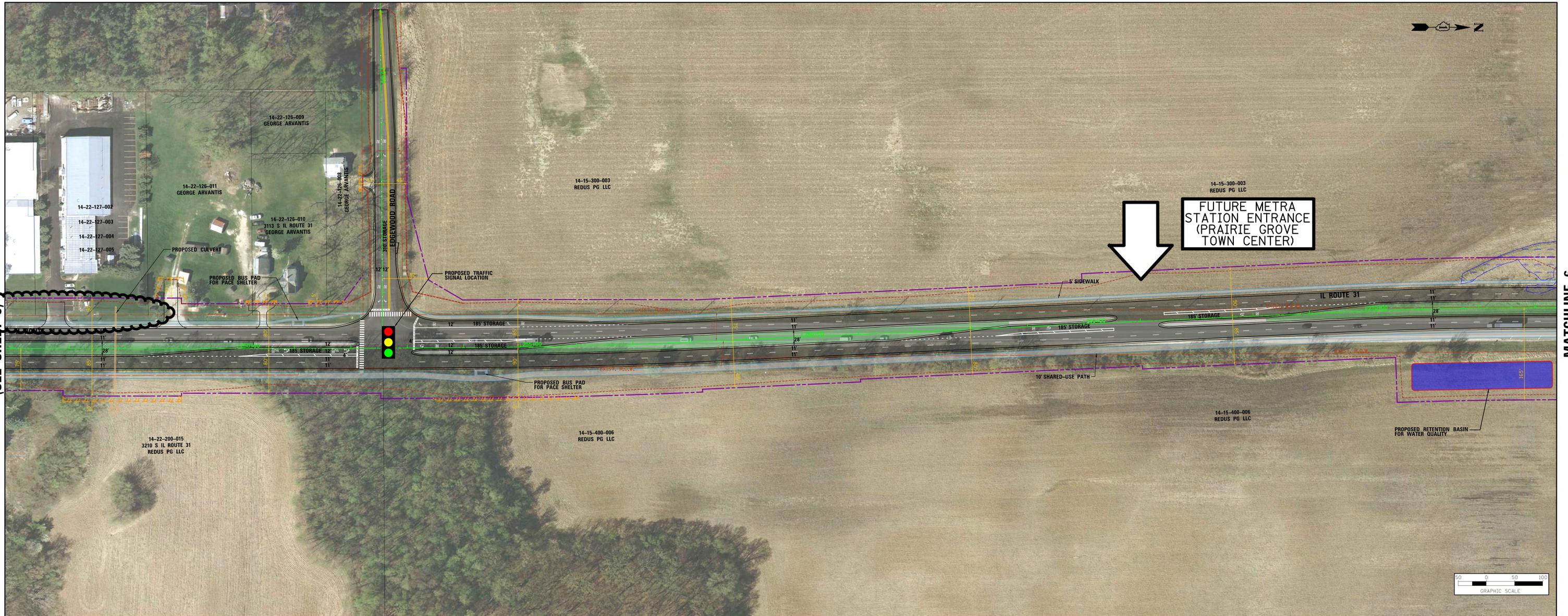
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PUBLIC HEARING
FEBRUARY 8, 2017

ILLINOIS ROUTE 31
SOUTH SECTION: ILLINOIS ROUTE 176 TO SOUTH OF BULL VALLEY ROAD

PREFERRED ALTERNATIVE
SHEET 5 OF 15

Illinois Route 31
ROUTE 176 TO ROUTE 120
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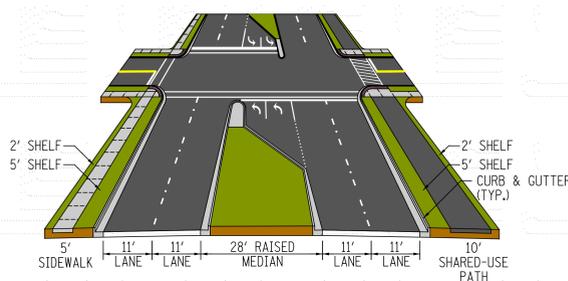


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MATCHLINE 5
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MATCHLINE 6
(SEE SHEET 7)

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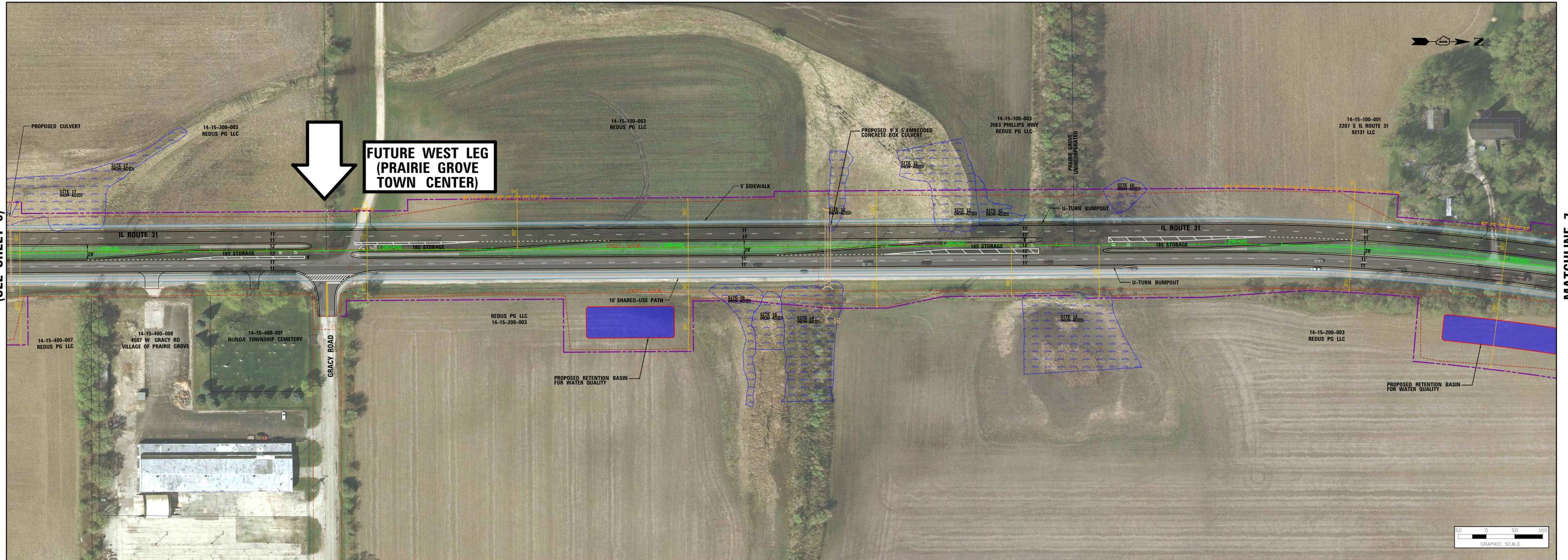
PROPOSED TYPICAL SECTION



PRELIMINARY
SUBJECT TO CHANGE
PUBLIC HEARING
FEBRUARY 8, 2017

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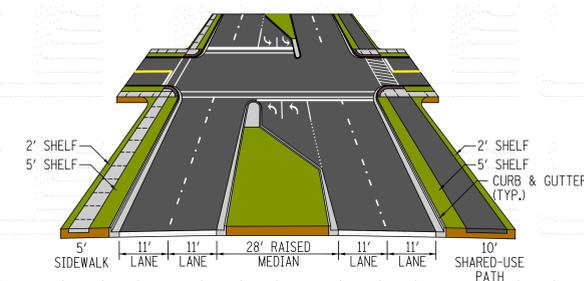
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			RESIDENTIAL DISPLACEMENT		
			DRAINAGE OUTFALL		

PROPOSED TYPICAL SECTION



PRELIMINARY

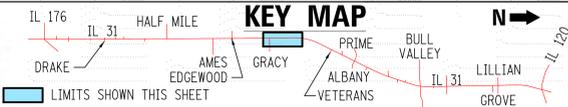
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PUBLIC HEARING
FEBRUARY 8, 2017

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PREFERRED ALTERNATIVE
SHEET 7 OF 15



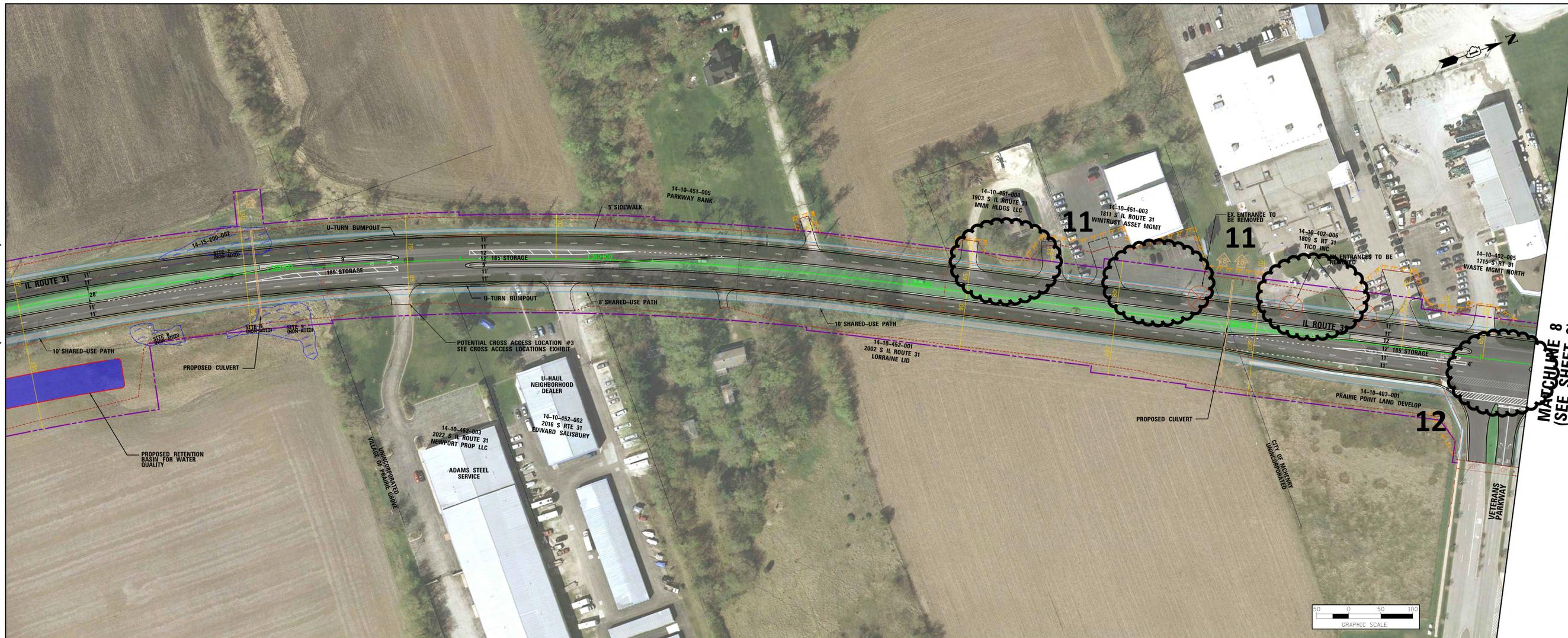
Illinois Route 31
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KEY MAP
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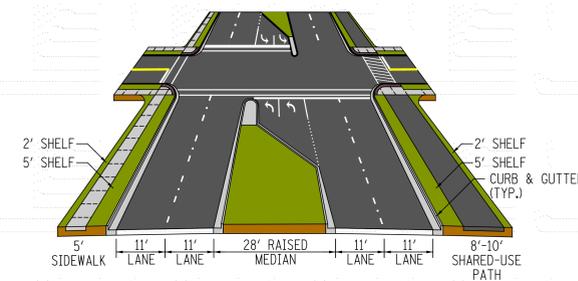
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			DRAINAGE OUTFALL		

PROPOSED TYPICAL SECTION



PRELIMINARY

SUBJECT TO CHANGE
PUBLIC HEARING
FEBRUARY 8, 2017

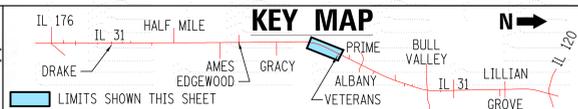
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PREFERRED ALTERNATIVE

SHEET 8 OF 15



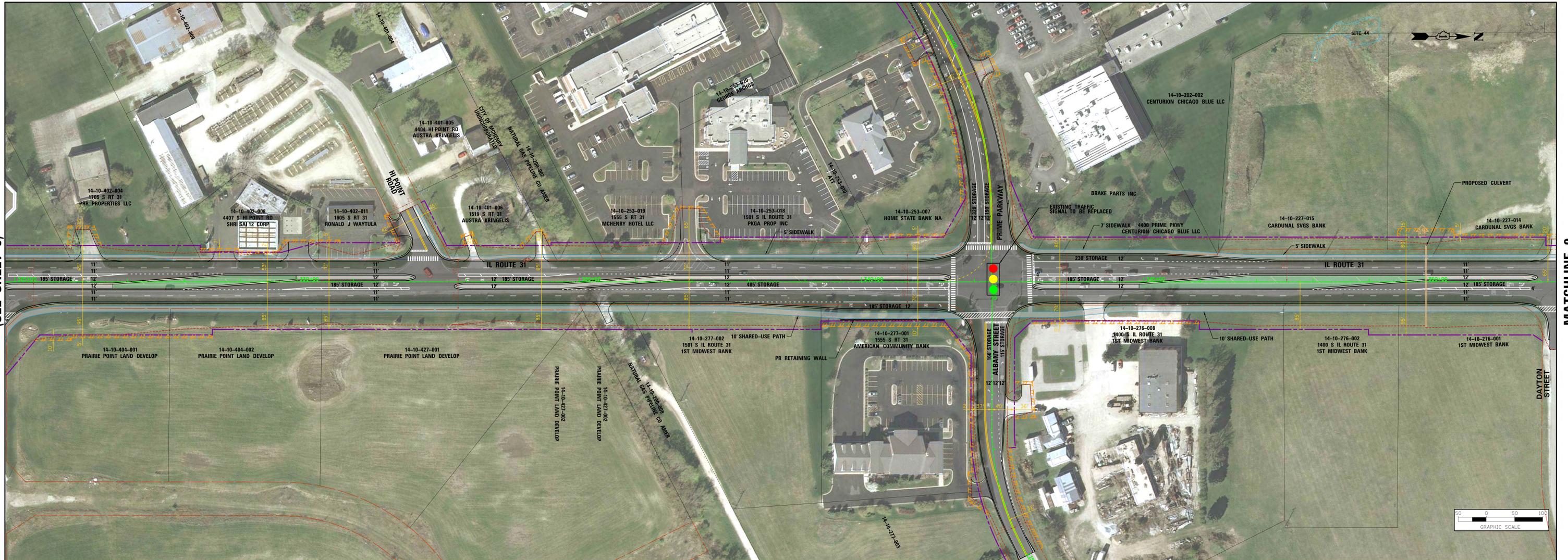
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FOR CONTINUATION
SEE BELOW

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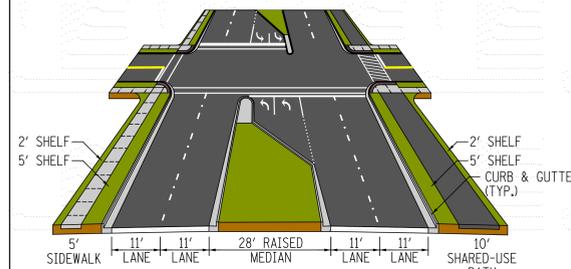
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	PROP. TEMP. EASEMENT LINE		COMMERCIAL DISPLACEMENT		
			RESIDENTIAL DISPLACEMENT		
			DRAINAGE OUTFALL		

PROPOSED TYPICAL SECTION



PRELIMINARY

SUBJECT TO CHANGE
PUBLIC HEARING
FEBRUARY 8, 2017



FOR CONTINUATION
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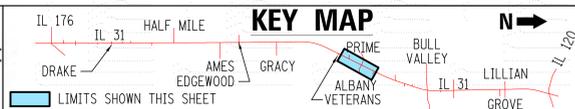
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PREFERRED ALTERNATIVE

SHEET 9 OF 15



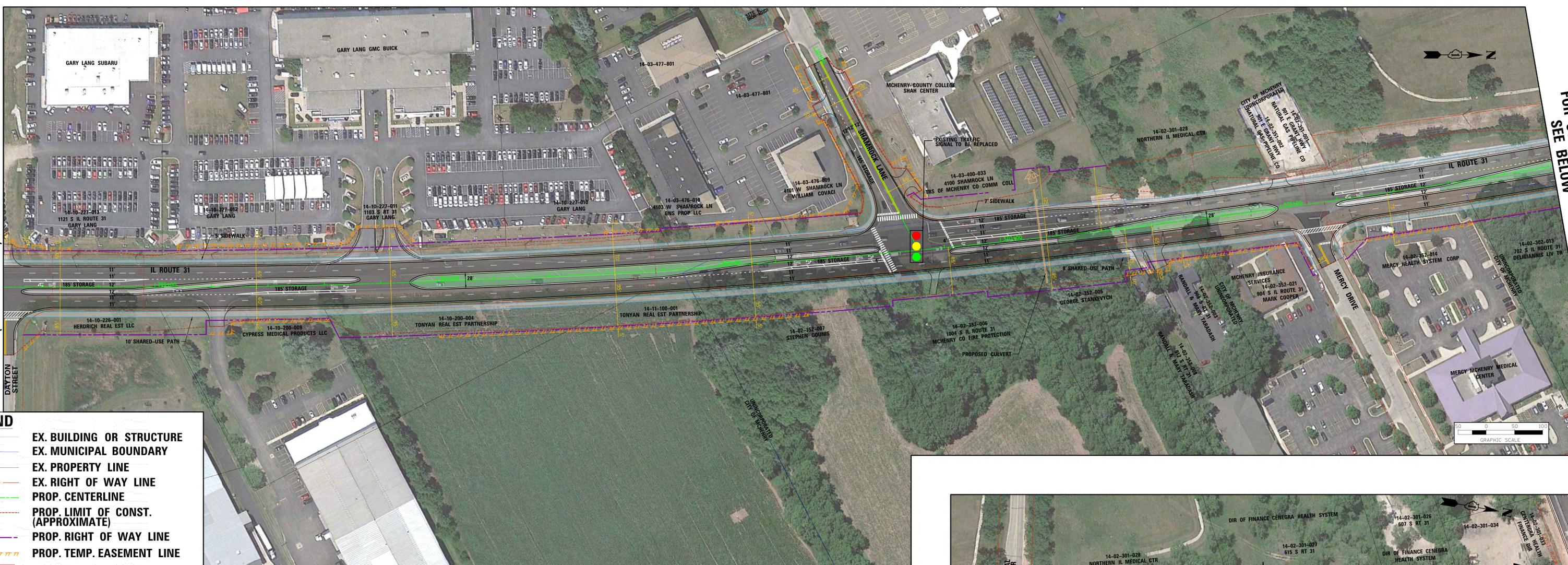
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ROUTE 176 TO ROUTE 120
www.idot.illinois.gov/projects/il31



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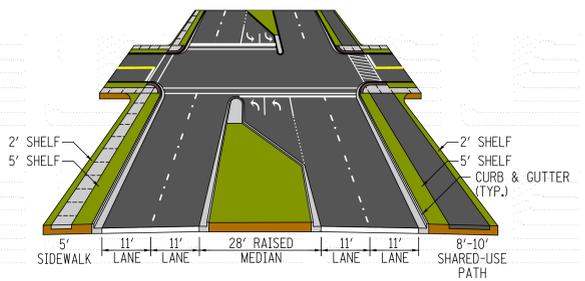
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FOR CONTINUATION
SEE BELOW



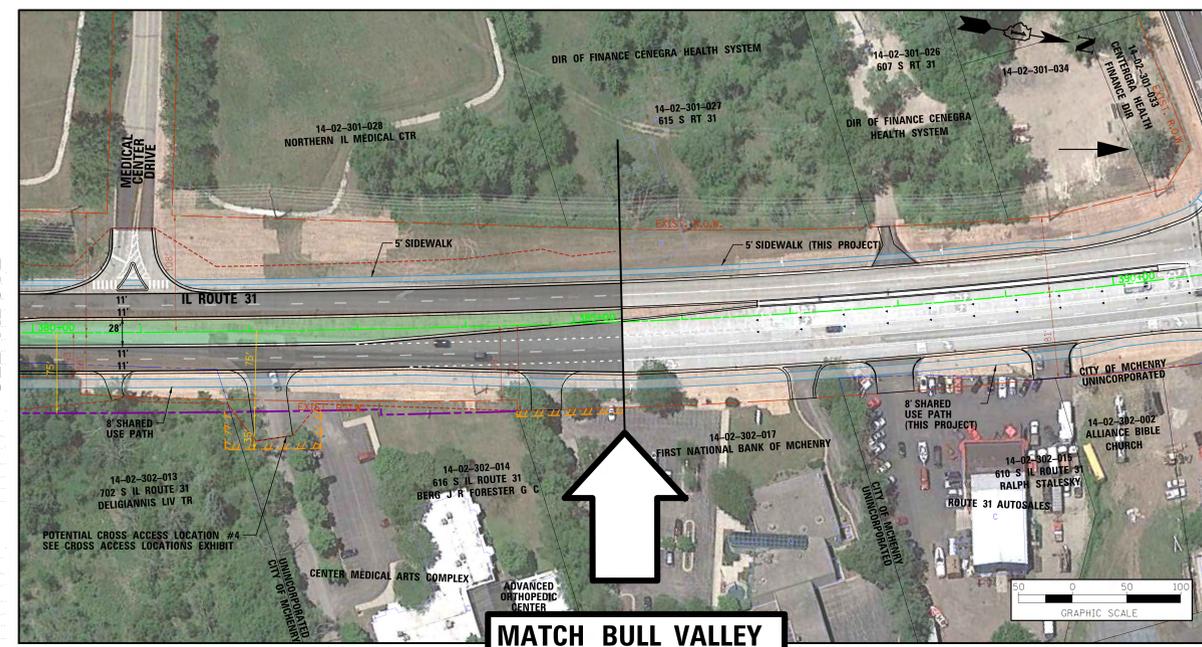
- LEGEND**
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 - EX. MUNICIPAL BOUNDARY
 - EX. PROPERTY LINE
 - EX. RIGHT OF WAY LINE
 - PROP. CENTERLINE
 - PROP. LIMIT OF CONST. (APPROXIMATE)
 - PROP. RIGHT OF WAY LINE
 - PROP. TEMP. EASEMENT LINE
 - EX. FLOODPLAIN
 - EX. WATERS OF THE U.S. (WOUS)
 - EX. WETLANDS
 - PROP. GRASS MEDIAN
 - PROP. MEANDERING STREAM / BASIN
 - PROP. PAVEMENT SURFACE
 - PROP. RAISED CURB MEDIAN
 - COMMERCIAL DISPLACEMENT
 - RESIDENTIAL DISPLACEMENT
 - DRAINAGE OUTFALL
 - EX. TRAFFIC SIGNAL TO REMAIN
 - EX. TRAFFIC SIGNAL TO BE REPLACED
 - PROP. TRAFFIC SIGNAL

PROPOSED TYPICAL SECTION



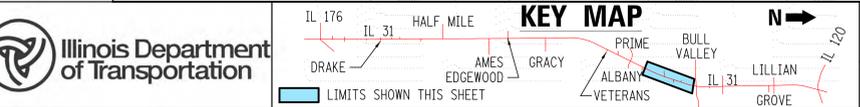
PRELIMINARY
SUBJECT TO CHANGE
PUBLIC HEARING
FEBRUARY 8, 2017

FOR CONTINUATION
SEE ABOVE



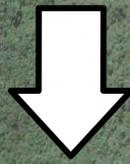
MATCHLINE 10
(SEE SHEET 11)

MATCH BULL VALLEY ROAD IMPROVEMENT

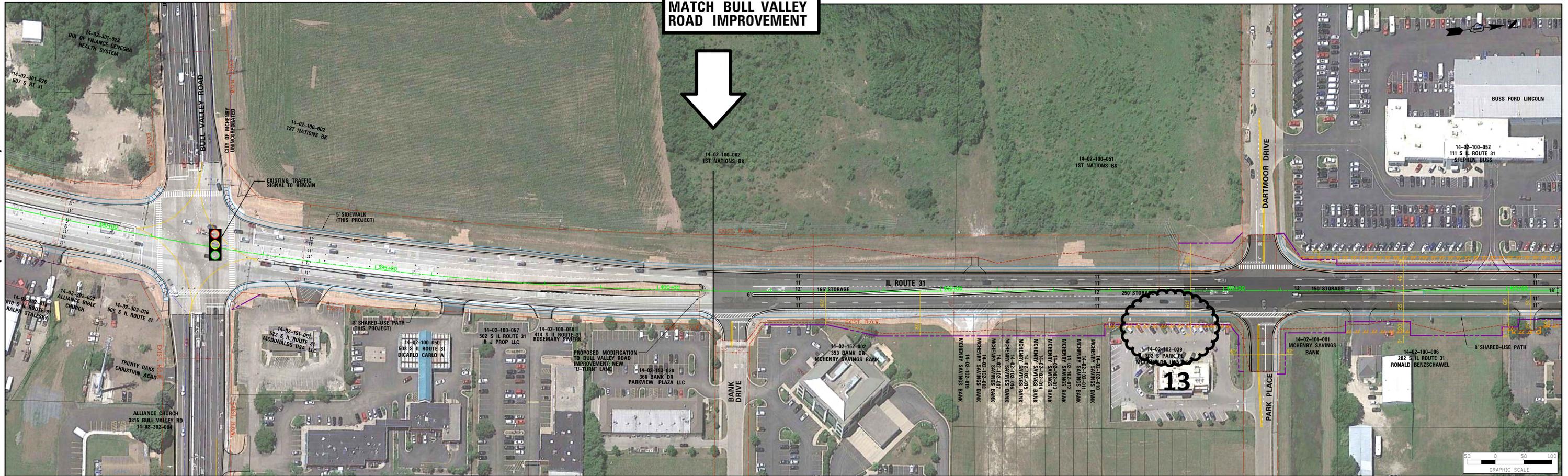


MATCHLINE 10
(SEE SHEET 10)

MATCH BULL VALLEY
ROAD IMPROVEMENT

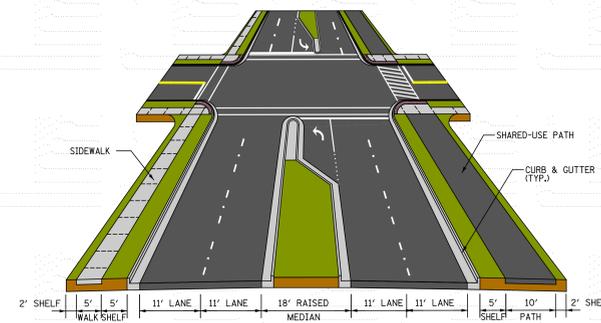


MATCHLINE 11
(SEE SHEET 12)



LEGEND	
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	PROP. LIMIT OF CONST. (APPROXIMATE)
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	EX. WATERS OF THE U.S. (WOUS)
	EX. WETLANDS
	PROP. GRASS MEDIAN
	PROP. MEANDERING STREAM / BASIN
	PROP. PAVEMENT SURFACE
	PROP. RAISED CURB MEDIAN
	COMMERCIAL DISPLACEMENT
	RESIDENTIAL DISPLACEMENT
	DRAINAGE OUTFALL
	EX. TRAFFIC SIGNAL TO REMAIN
	EX. TRAFFIC SIGNAL TO BE REPLACED
	PROP. TRAFFIC SIGNAL

PROPOSED TYPICAL SECTION
BANK DRIVE TO HIGH STREET



PRELIMINARY

SUBJECT TO CHANGE
PUBLIC HEARING
FEBRUARY 8, 2017

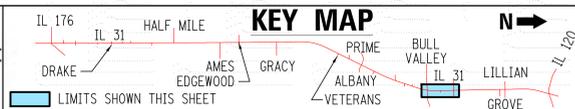
ILLINOIS ROUTE 31
NORTH SECTION: NORTH OF BULL VALLEY ROAD TO ILLINOIS ROUTE 120

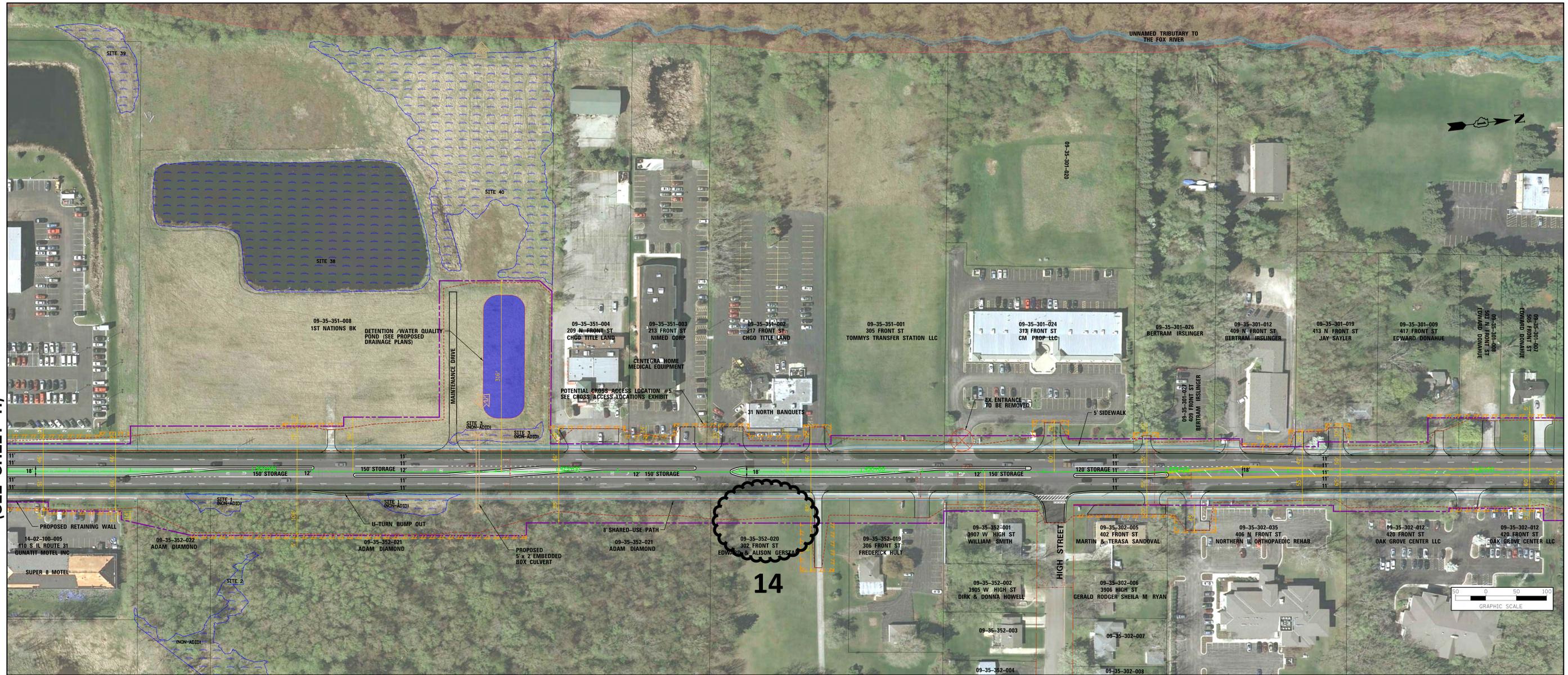
PREFERRED ALTERNATIVE

SHEET 11 OF 15



Illinois Route 31
ROUTE 176 TO ROUTE 120
www.idot.illinois.gov/projects/il31



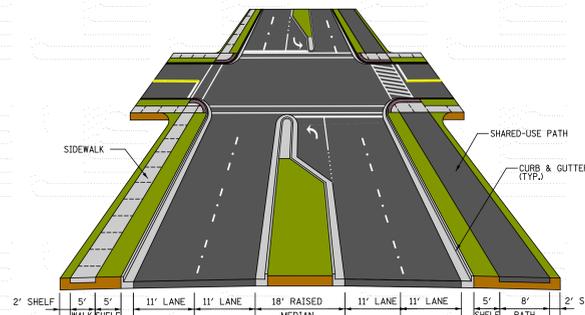


MATCHLINE 11
(SEE SHEET 11)

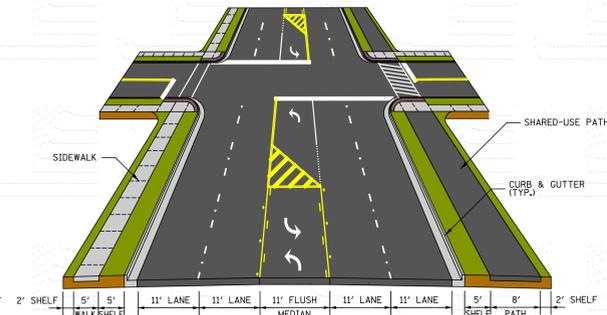
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(SEE SHEET 13)

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	PROP. LIMIT OF CONST. (APPROXIMATE)
	PROP. RIGHT OF WAY LINE
	PROP. TEMP. EASEMENT LINE
	EX. FLOODPLAIN
	EX. WATERS OF THE U.S. (WOUS)
	EX. WETLANDS
	PROP. GRASS MEDIAN
	PROP. MEANDERING STREAM / BASIN
	PROP. PAVEMENT SURFACE
	PROP. RAISED CURB MEDIAN
	COMMERCIAL DISPLACEMENT
	RESIDENTIAL DISPLACEMENT
	DRAINAGE OUTFALL
	EX. TRAFFIC SIGNAL TO REMAIN
	EX. TRAFFIC SIGNAL TO BE REPLACED
	PROP. TRAFFIC SIGNAL

PROPOSED TYPICAL SECTION
BANK DRIVE TO HIGH STREET



PROPOSED TYPICAL SECTION
HIGH STREET TO JOHN STREET



PRELIMINARY
SUBJECT TO CHANGE
PUBLIC HEARING
FEBRUARY 8, 2017

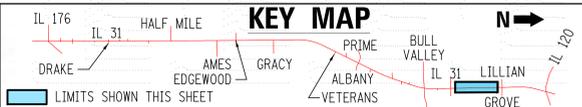
ILLINOIS ROUTE 31
NORTH SECTION: NORTH OF BULL VALLEY ROAD TO ILLINOIS ROUTE 120

PREFERRED ALTERNATIVE

SHEET 12 OF 15

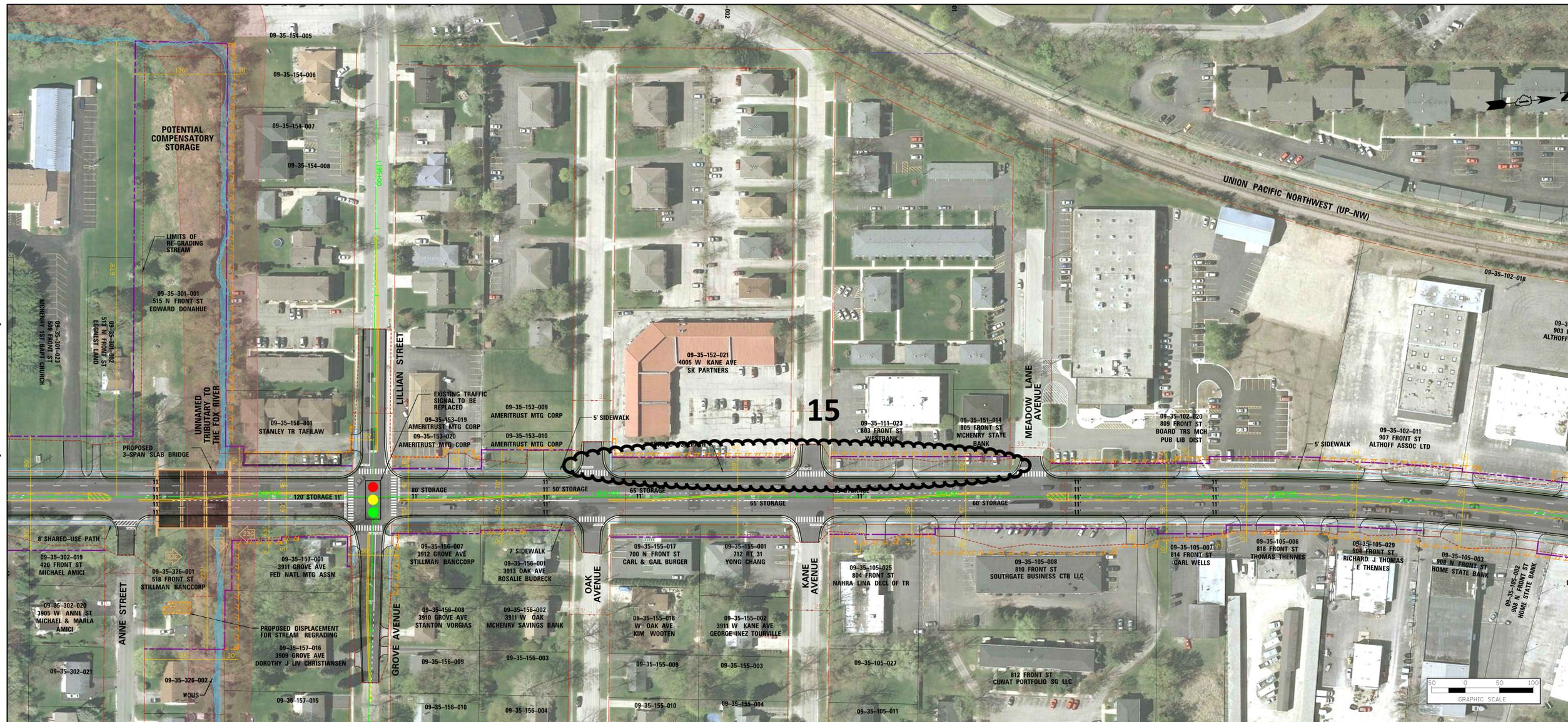


Illinois Route 31
ROUTE 176 TO ROUTE 120
www.idot.illinois.gov/projects/il31



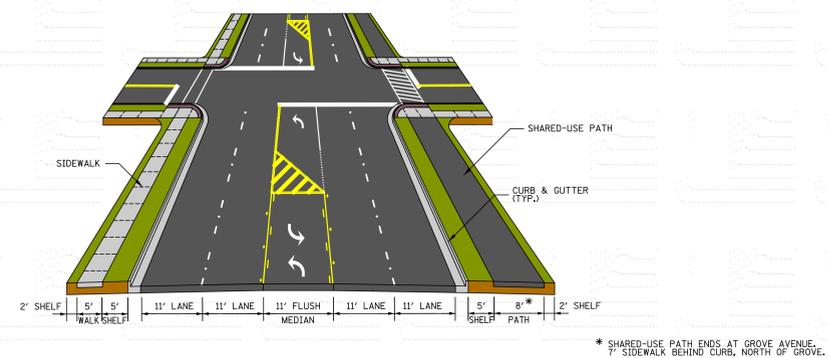
MATCHLINE 12
(SEE SHEET 12)

MATCHLINE 13
(SEE SHEET 14)



LEGEND	
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	PROP. LIMIT OF CONST. (APPROXIMATE)
	PROP. RIGHT OF WAY LINE
	PROP. TEMP. EASEMENT LINE
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	EX. WATERS OF THE U.S. (WOUS)
	EX. WETLANDS
	PROP. GRASS MEDIAN
	PROP. MEANDERING STREAM / BASIN
	PROP. PAVEMENT SURFACE
	PROP. RAISED CURB MEDIAN
	COMMERCIAL DISPLACEMENT
	RESIDENTIAL DISPLACEMENT
	DRAINAGE OUTFALL
	EX. TRAFFIC SIGNAL TO REMAIN
	EX. TRAFFIC SIGNAL TO BE REPLACED
	PROP. TRAFFIC SIGNAL

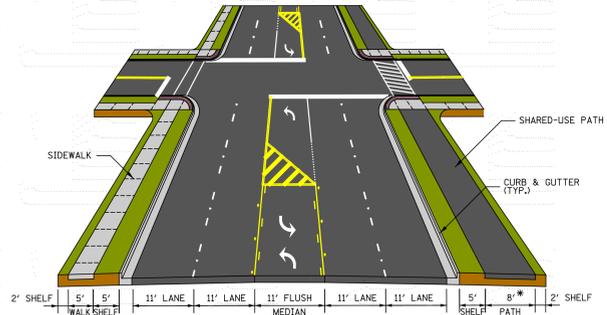
PROPOSED TYPICAL SECTION
HIGH STREET TO JOHN STREET



PRELIMINARY
SUBJECT TO CHANGE
PUBLIC HEARING
FEBRUARY 8, 2017

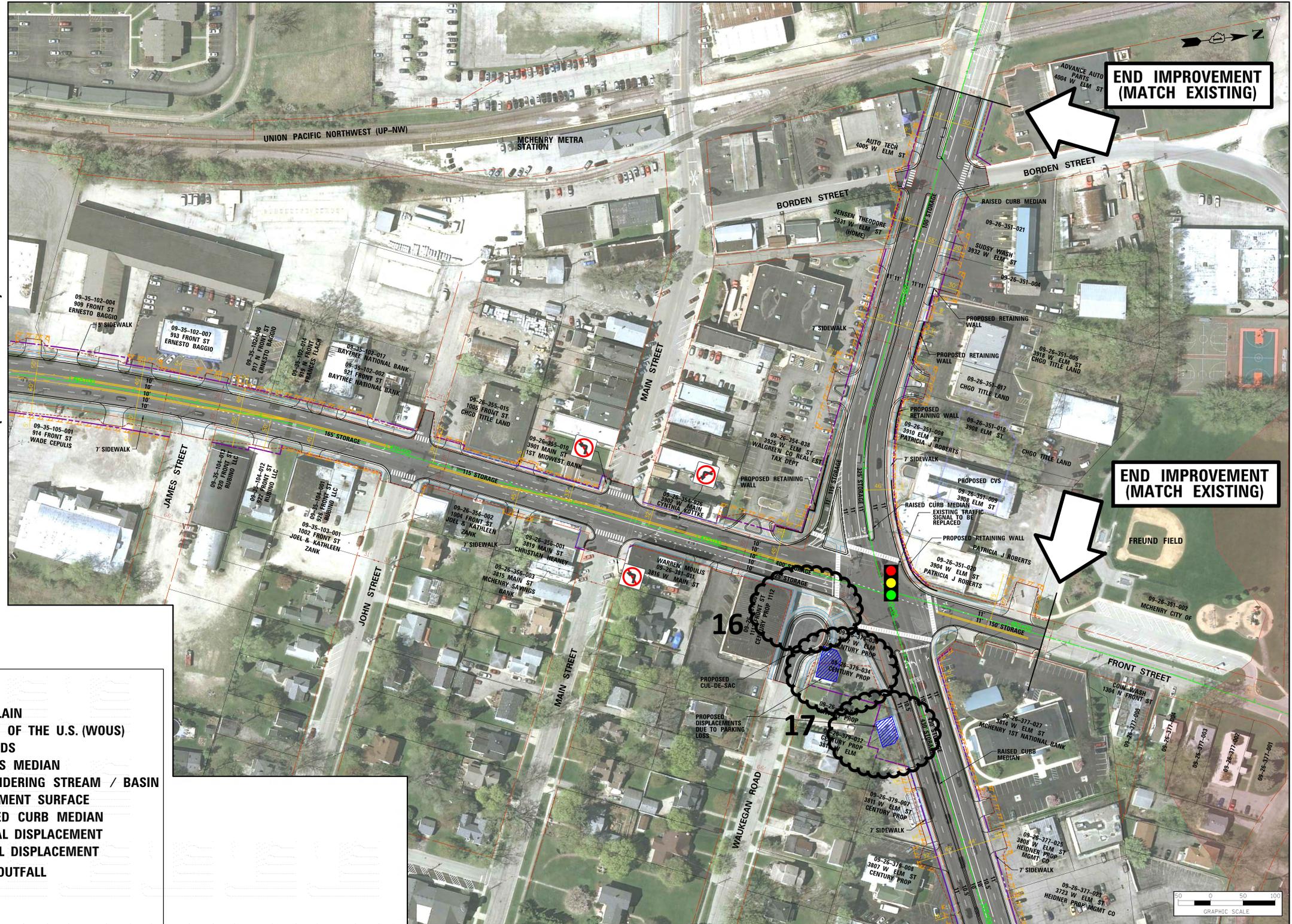
PROPOSED TYPICAL SECTION

HIGH STREET TO JOHN STREET



* SHARED-USE PATH ENDS AT GROVE AVENUE, 7' SIDEWALK BEHIND CURB, NORTH OF GROVE.

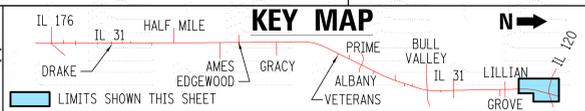
MATCHLINE 13
(SEE SHEET 13)



MATCHLINE 14
(SEE SHEET 15)

LEGEND

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| | EX. PROPERTY LINE | | EX. WETLANDS |
| | EX. RIGHT OF WAY LINE | | PROP. GRASS MEDIAN |
| | PROP. CENTERLINE | | PROP. MEANDERING STREAM / BASIN |
| | PROP. LIMIT OF CONST. (APPROXIMATE) | | PROP. PAVEMENT SURFACE |
| | PROP. RIGHT OF WAY LINE | | PROP. RAISED CURB MEDIAN |
| | PROP. TEMP. EASEMENT LINE | | COMMERCIAL DISPLACEMENT |
| | | | RESIDENTIAL DISPLACEMENT |
| | EX. TRAFFIC SIGNAL TO REMAIN | | DRAINAGE OUTFALL |
| | EX. TRAFFIC SIGNAL TO BE REPLACED | | |
| | PROP. TRAFFIC SIGNAL | | |



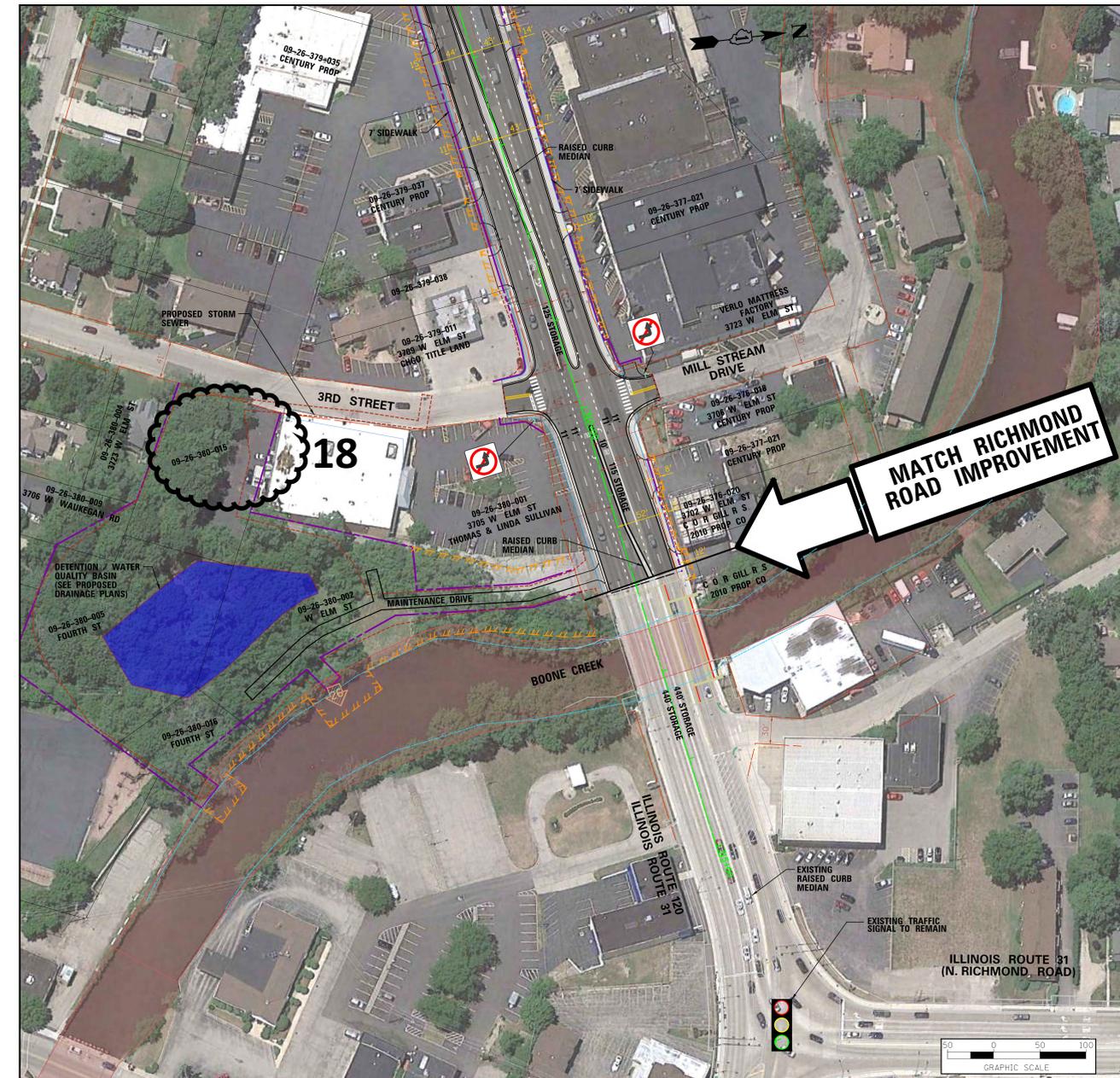
ILLINOIS ROUTE 31
NORTH SECTION: NORTH OF BULL VALLEY ROAD TO ILLINOIS ROUTE 120

PRELIMINARY

SUBJECT TO CHANGE
PUBLIC HEARING
FEBRUARY 8, 2017

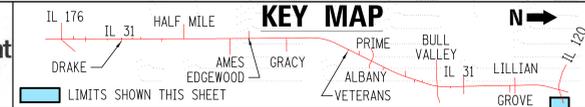
PREFERRED ALTERNATIVE

MATCHLINE 14
(SEE SHEET 14)



LEGEND					
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	EX. MUNICIPAL BOUNDARY		EX. WATERS OF THE U.S. (WOUS)		EX. TRAFFIC SIGNAL TO BE REPLACED
	EX. PROPERTY LINE		EX. WETLANDS		EX. TRAFFIC SIGNAL TO BE REPLACED
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	PROP. RIGHT OF WAY LINE		PROP. RAISED CURB MEDIAN		
	PROP. TEMP. EASEMENT LINE		COMMERCIAL DISPLACEMENT		
			RESIDENTIAL DISPLACEMENT		
			DRAINAGE OUTFALL		

PRELIMINARY
SUBJECT TO CHANGE
PUBLIC HEARING
FEBRUARY 8, 2017



ILLINOIS ROUTE 31
NORTH SECTION: NORTH OF BULL VALLEY ROAD TO ILLINOIS ROUTE 120



McHenry County Council of Governments

BUREAU OF PROGRAMMING
RECEIVED

FEB 10 2017

DISTRICT #1

January 31, 2017

Mr. Scott Czaplicki
Illinois Department of Transportation
201 W. Center Court
Schaumburg, IL 60196-1096

Dear Mr. Czaplicki:

Thank you for providing the opportunity to comment on the IL 31 Project from IL 176 in the City of Crystal Lake to IL 120 in the City of McHenry in McHenry County. This is an important project for McHenry County and will greatly benefit the residents and commuters in McHenry County.

For the past several years, the McHenry County Council of Governments (MCCG) has included the IL 31 project on their list of transportation priority projects. The list is approved by our full membership (29 municipal and township members) in January and used in meetings with legislators, state officials and local elected officials.

We are excited to see that the project continues to move forward and that Phase I Engineering and the Environmental Assessment have been completed. In addition we are pleased that Phase II Engineering is programmed in the FY 2017-2022 Proposed Highway Improvement Program. The MCCG will continue to support this project through completion and look forward to working with IDOT and our municipal members along the corridor.

Enclosed with this letter is a copy of our 2017 Legislative and Transportation Priorities. Please feel free to contact me with any questions.

Sincerely,

Chalen Daigle
Executive Director

Executive Committee

President Rick Mack
Village of Ringwood
MCCG President

Mayor Mark Kownick
Village of Cary
MCCG Vice-President

Mayor Charles Sass
Village of Huntley
MCCG Treasurer

Mayor Donald Lockhart
City of Marengo
MCCG Secretary

Supervisor Craig Adams
McHenry Township
Chairman of the
Finance Committee

President Peter Koenig
Village of Richmond
Chairman of the
Legislative Committee

President John Schmitt
Village of Algonquin
Chairman of the
Transportation Committee

President Terry Counley
Village of McCullom Lake
Chairman of the Water Policy
Task Force

President Robert Nunamaker
Village of Fox River Grove
Chairman of the
Mayors Caucus

Chairman Jack Franks
McHenry County Board
Ex-Officio Member

Chalen Daigle
Executive Director
44 N. Virginia
Suite 2-A
Crystal Lake, IL 60014
815-477-2090 (p)
847-767-0440 (c)
cdaigle@mchenrycountycog.org
www.mchenrycountycog.org

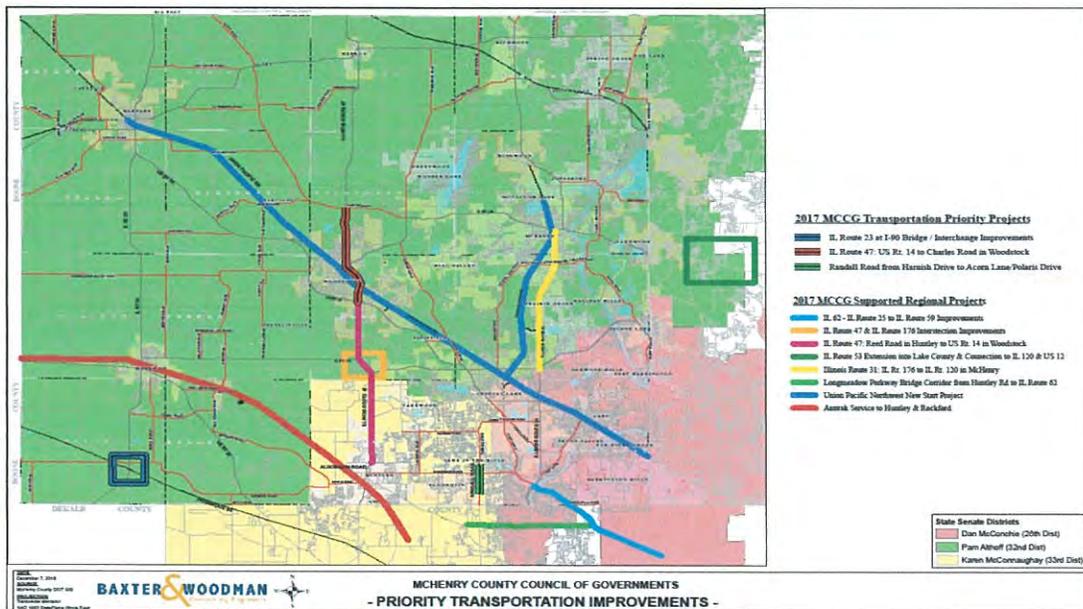
2017 MCCG Transportation Priority List

Key Corridor Projects

- *IL 47 from US 14 to Charles Road in Woodstock*
- *Randall Road from Harnish Drive to Acorn Lane/Polaris Drive*
- *IL Route 23 at I-90 Bridge/Interchange Improvements*
- *IL 47 and IL 176 Intersection Improvements*

Regional Projects for which the MCCG Supports

- *IL 31 from IL 176 in Crystal Lake to IL 120 in McHenry*
- *IL 47 from Reed Road in Huntley to US 14 in Woodstock*
- *Union Pacific Northwest Line New Start Projects*
- *Longmeadow Parkway*
- *IL 53 Extension into Lake County and connection to IL 120 and US 12 with the addition of an IL 31 Connector project that would help ease possible congestion in Lakemoor, McHenry, Ringwood, Johnsburg and Richmond.*
- *IL 62 Phase I Study in Barrington Hills*
- *Amtrak Service to Huntley and Rockford*





MIKE LESPERANCE
HIGHWAY COMMISSIONER

February 15, 2017

Mr. Jose Rios, P.E.
Illinois Dept. of Transportation
Office of Highways Project Implementation / Region 1 / District 1
201 West Center Court
Schaumburg, Illinois 60196-1096

Dear Mr. Rios,

As the Nunda Township Highway Commissioner looking at the overall project improvement of Route 31 from Route 176 to Route 120, I think it is long overdue, much needed and very much appreciated.

As an elected official and concerned citizen I would like to request some changes to the plans.

First, I would like to request retaining walls / sound barriers from Drake Drive Latitude and Longitude 42.260286, -88.286761 to L.L. 42.265039, -88.286778 and from 42.275065, -88.286846 to Ames Road 42.281133, -88.286859. In these two areas there are many residents with very rural settings. On their properties there exist many very old growth and healthy Oak trees. In my opinion, rather than removing them and cutting a steep slope, we could take less land and extend the retaining walls above grade to create a sound barrier.

Second, Ames Road should be re-aligned to Edgewood Road in order to allow all of the residents of Prairie Grove to turn left at the stop light to go to Crystal Lake.

It seems to me now is the time to address this issue while we are already doing a \$75 million dollar improvement. The danger and time, especially in turning left or turning right with a U-turn, could be easily rectified either with a frontage road or an S curve on Ames Road.

I offer as much time and energy as it will take to see these suggestions come to pass. Please call on me for any assistance you may need.

Sincerely,

NUNDA TOWNSHIP ROAD DISTRICT

Mike Lesperance
Highway Commissioner

cc: Scott Czaplak



BUREAU OF PROGRAMMING
RECEIVED

FEB 24 2017

DISTRICT #1

February 22, 2017

Scott Czaplicki, Bureau of Programming
Illinois Department of Transportation
201 West Center Court
Schaumburg, IL 60196-1096

RE: *Illinois Route 31
Route 176 to Route 120*

Dear Mr. Czaplicki:

Thank you for the opportunity to provide comment on the above project. The McHenry County Conservation District does not have any property directly impacted by the proposed project as presented at the February 8, 2017 public hearing. Several staff members of the District attended and have provided the following comments.

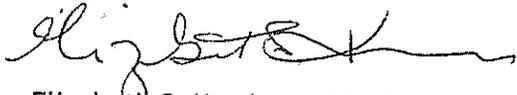
First, it should be noted that the McHenry County Conservation District's existing 26-mile linear multiuse Prairie Trail runs parallel to the proposed shared use path and is separated by a distance of only 400-600 feet within the City of McHenry and does not show a connection between the two trails. A portion of the proposed shared use trail would be a duplication of services and it is our understanding that the proposed shared use trail is not included in the state funding request. The cost of construction and ongoing maintenance would fall to the local governments. The District is not interested in participating in this part of the project at this juncture but would encourage the placement of signage and on-street routes to direct users to the existing multiuse trail to safely traverse north and south through the county and to the western businesses within the City of McHenry.

There is concern that the 84 Oak trees and 17 Hickory trees would be impacted by the current alignment and that some of these critical local species could be avoided with a shifting of the project either to the east or west. If such a move is unavoidable, several nearby Conservation Areas (Stickney Run and Silver Creek) could benefit from replacement trees which may be required to mitigate such an impact.

The District may also be able to assist in the anticipated wetland mitigation measures by providing off-site local wetland improvements within the Regional watershed. The District will continue to stay informed on this project as it moves into Phase II.

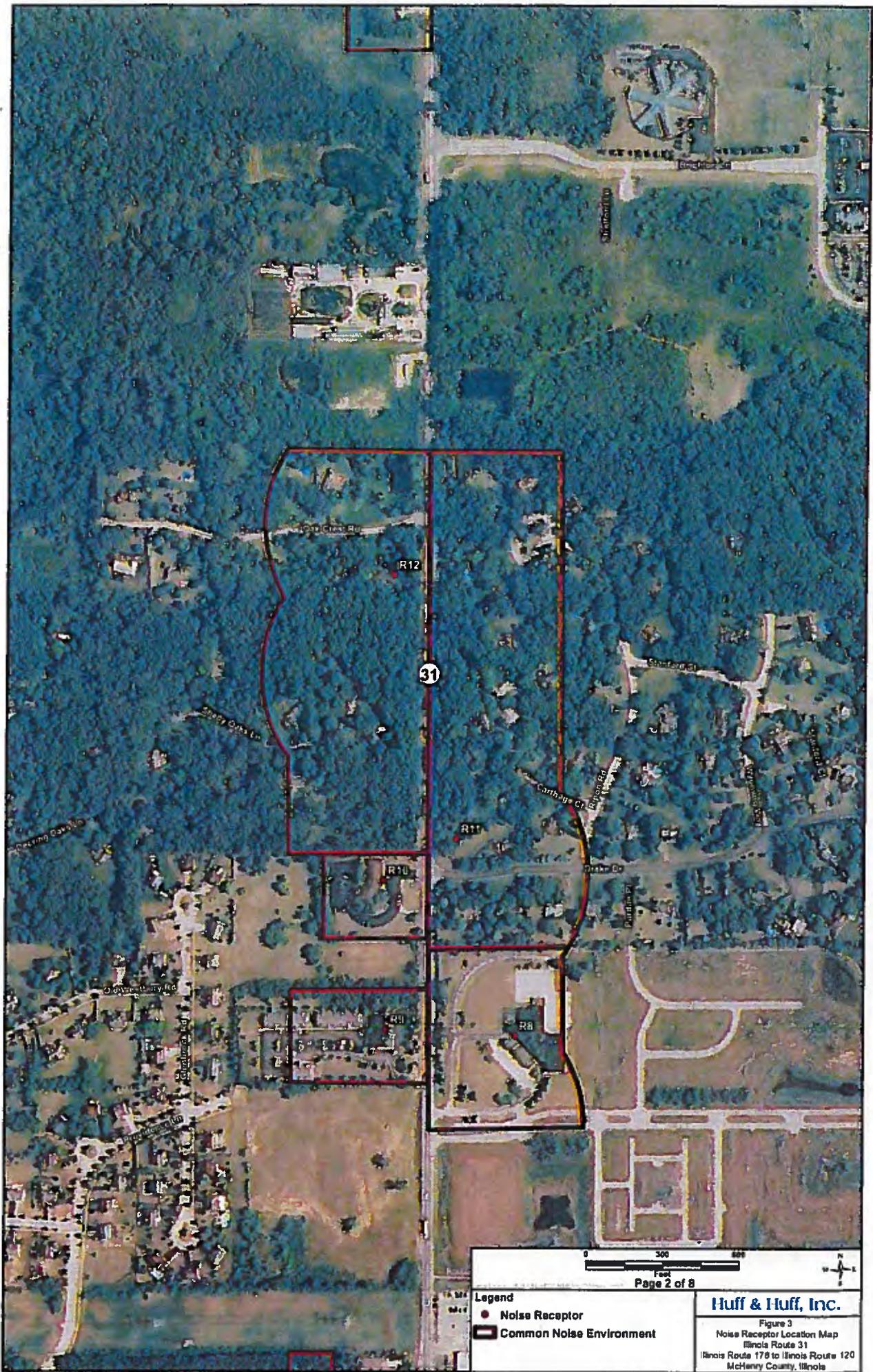
If you have any questions or we can be of further assistance, please do not hesitate to give us a call.

Sincerely,
McHENRY COUNTY CONSERVATION DISTRICT



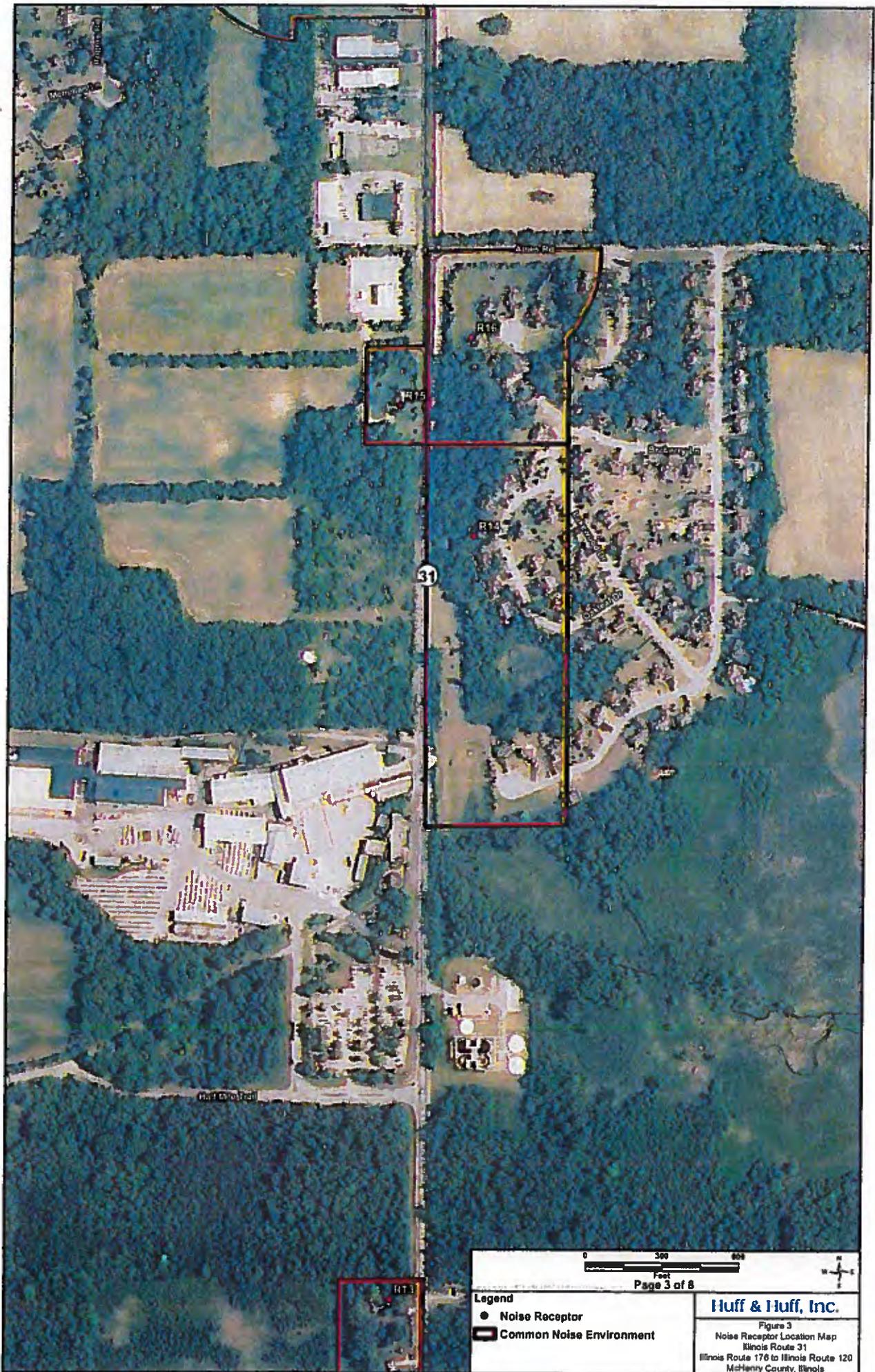
Elizabeth S. Kessler, MBA, CPRE
Executive Director

- c:
- Bona Heinsohn, President, Board of Trustees
 - Stephen Barrett, Vice President
 - Brandon Thomas, Treasurer
 - Dave Kranz, Secretary
 - Pete Merkel, Trustee
 - Vern Scacci, Trustee
 - Dave Brandt, Trustee
 - Robert Nowak, Liaison, McHenry County Board
 - Ed Collins, Director of Land Preservation & Natural Resources
 - John Kremer, Director of Operations & Public Safety
 - Val Siler, Land Preservation Manager
 - Amy Peters, Planning Manager



Legend	
●	Noise Receptor
▭	Common Noise Environment

Huff & Huff, Inc.
 Figure 3
 Noise Receptor Location Map
 Illinois Route 31
 Illinois Route 178 to Illinois Route 120
 McHenry County, Illinois



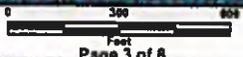
31

R15

R16

R14

R13



Page 3 of 8

- Legend**
- Noise Receptor
 - ▭ Common Noise Environment

Huff & Huff, Inc.
 Figure 3
 Noise Receptor Location Map
 Illinois Route 31
 Illinois Route 176 to Illinois Route 120
 McHenry County, Illinois

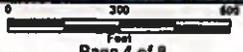


31

R19

R18

R17



Page 4 of 8

- Legend**
- Noise Receptor
 - ▭ Common Noise Environment

Huff & Huff, Inc.
 Figure 3
 Noise Receptor Location Map
 Illinois Route 31
 Illinois Route 178 to Illinois Route 120
 McHenry County, Illinois



Illinois Department of Transportation

Office of Highways Project Implementation / Region 1 / District 1
201 West Center Court / Schaumburg, Illinois 60196-1096

March 17, 2017

Mr. Mike Lesperance
Highway Commissioner
Nunda Township
3518 Bay Road
Crystal Lake, IL 60012

Dear Mr. Lesperance:

Thank you for your February 15, 2017 letter regarding the Illinois Department of Transportation's (Department) proposed improvement of IL 31 from IL 176 to IL 120 in McHenry County. Partial funding for Phase II engineering is included in the Department's FY 2017-2022 Proposed Highway Improvement Program. The remaining elements of the project will be included in our priorities for future funding consideration among similar improvement needs throughout the region. The general scope of work for this improvement is anticipated to consist of roadway widening and reconstruction to provide two lanes in each direction separated by a median, bicycle and pedestrian accommodations, and intersection and drainage improvements.

The Department appreciates your support for the project. In your letter, the Township requested retaining walls along IL 31 between Drake Drive and Ames Road to reduce tree impacts and provide a noise barrier. The preservation of trees has been a concern throughout the study. Retaining walls will be evaluated to assess their effectiveness in reducing tree impacts.

A traffic noise analysis was performed using projected year 2040 noise levels and concluded that four of the seven receptors in this area (R11, R12, R13, and R15) merited an evaluation of traffic noise abatement walls. However, none of the walls met the criteria to implement them as part of this project. Due to public comments received after the February 8, 2017 public hearing for this project, the Department is evaluating a traffic noise abatement wall for the Oak Grove subdivision located along the east side of IL 31 south of Ames Road (R14 and R16). See attached Noise Receptor Maps for receptor locations. The original noise analysis is part of the Environmental Assessment and can be found on the project website at www.idot.illinois.gov/projects/il31.

Mr. Mike Lesperance
March 17, 2017
Page 2

In addition, Ames Road was requested to be realigned with Edgewood Road so Prairie Grove residents can make a left turn onto IL 31 at a signalized intersection to go south. Ames Road is under the jurisdiction of the Village of Prairie Grove (Village) and this realignment is identified in their Town Center & Transit-Oriented Development Plan dated June 2010. The Village could initiate engineering and construction for an improvement on Ames Road through either the Department's Bureau of Local Roads and Streets or the Bureau of Traffic Operations Permit Section. More information on the funds available to local governments for infrastructure improvements can be found at <http://www.idot.illinois.gov/transportation-system/local-transportation-partners/county-engineers-and-local-public-agencies/index>.

As a result of comments from the public hearing, new traffic counts will be performed and a traffic signal warrant performed at the IL 31 and Ames Road intersection. Regardless of additional improvements, we would like to note that Ames Road connects to Barreville Road which runs north-south between IL 176 and IL 120 and provides access to other east-west roadways that are signalized at IL 31.

If you have any questions or need additional information, please contact Kimberly Murphy, Consultant Studies Unit Head, at (847) 705-4791.

Very truly yours,

Anthony J. Quigley, P.E.
Region One Engineer

By: 
Jose Rios, P.E.
Engineer of Program Development

Attachments

bcc: Jose Rios
Brian Carlson
File

Prepared By: Scott Czaplicki, Ext. 4678
Bureau of Programming



Illinois Department of Transportation

Office of Highways Project Implementation / Region 1 / District 1
201 West Center Court / Schaumburg, Illinois 60196-1096

May 24, 2017

Ms. Elizabeth S. Kessler, MBA, CPRE
Executive Director
McHenry County Conservation District
18410 US Highway 14
Woodstock, IL 60098

Dear Ms. Kessler:

Thank you for your February 22, 2017 letter regarding the Illinois Department of Transportation's (Department) proposed improvement of IL 31 from IL 176 to IL 120 in McHenry County. Partial funding for Phase II engineering is included in the Department's FY 2017-2022 Proposed Highway Improvement Program. The remaining elements of the project will be included in our priorities for future funding consideration among similar improvement needs throughout the region. The general scope of work for this improvement is anticipated to consist of roadway widening and reconstruction to provide two lanes in each direction separated by a median, bicycle and pedestrian accommodations, and intersection and drainage improvements.

In your letter, concerns were noted that the proposed shared-use path is a duplication of the Prairie Trail, and its construction cost and maintenance would be entirely a local agency responsibility. While the proposed path and Prairie Trail are parallel, they would provide different uses. The proposed path will provide opportunities for both short and long distance trips along the corridor to connect to residential, commercial and industrial areas along IL 31. In addition, the proposed path will collect users from the east side of IL 31 and provide crossing opportunities to the west at the eight signalized intersections along the project. Streets under local jurisdiction such as Edgewood Road, Bull Valley Road, and Lillian Street provide a direct connection to the Prairie Trail. Regarding local agency participation, the local cost share for new pedestrian and bicyclist facilities is 20% of the construction cost, plus a 15% engineering fee. The local agency, as you state in your letter, must agree to accept long-term responsibility for the administration, control, reconstruction and maintenance of the shared-use path and/or sidewalk.

Regarding impacts to oak and hickory trees, the preservation of trees has been a Department concern throughout the study. We are currently evaluating the proposed design to further reduce impacts to oak and hickory trees. The McHenry County Conservation District (MCCD) and public will be notified of the results of this analysis through a project newsletter this summer. Tree replacement at nearby conservation areas can be considered, as well as at areas along the project, as outlined in Department Policy D&E-18, Preservation and Replacement of Trees. A link to the policy is available on the project website at www.idot.illinois.gov/projects/il31 under Info Center/Newsletters.

Ms. Elizabeth S. Kessler, MBA, CPRE
May 24, 2017
Page 2

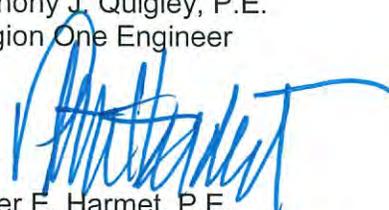
A Special Design/Construction Consideration (SDCC) will be added to the Phase I Combined Design Report to work with the MCCD during Phase II, contract plan preparation, to identify the location, type and number of trees to be planted. The MCCD would need to donate any temporary easements required for this work.

A SDCC will also be added to the Phase I report for the Department to work with MCCD during Phase II, contract plan preparation, to identify potential wetland mitigation sites.

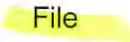
If you have any questions or need additional information, please contact Scott Czaplicki, Project Manager, at (847) 705-4678.

Very truly yours,

Anthony J. Quigley, P.E.
Region One Engineer

By: 
Peter E. Harmet, P.E.
Bureau Chief of Programming

Attachments

bcc:  File

S:\Gen\WP\p&es\CONSULT\Projects - Active\IL 31 (IL 176 to IL 120)\Correspondence\Tickles\Internal Tickle 1704 Response .docx



VILLAGE OF PRAIRIE GROVE

September 19, 2017

John A. Baczek, P.E.
Project & Environmental Studies Section Chief
Illinois Department of Transportation District 1
201 West Center Street
Schaumburg, IL 60196

VILLAGE HALL
3125 Barreville Road
Prairie Grove, IL 60012
815-455-1411 Office
815-455-0783 Fax

RE: IL Route 31 – Route 176 to Route 120

Dear Mr. Baczek:

Thank you for meeting with our local business owners, Senator Althoff, and our Village engineers on August 24, 2017 to discuss access control along the proposed IL 31 widening project at the 3 businesses located on the west side of IL 31 between Edgewood Road and Ames Road. The Village is extremely interested in accommodating these business owner's requests of maintaining full access to their driveways for large truck deliveries in order to ensure these remain viable businesses in the Village of Prairie Grove and the State of Illinois.

My understanding from that meeting is that IDOT is willing to include a left turn lane and median opening with full access at the middle driveway (Arvidson's Pools and Spas) because it has the potential to serve multiple businesses. There is an existing access road behind the businesses between Arvidson's Pools and Spas and the property immediately south, that would allow vehicles and delivery trucks to access the business to the south of Arvidson's via the proposed median opening across from Arvidson's.

While the Village is truly appreciative of IDOT's willingness to make this change to the proposed improvements, it is still concerned about providing delivery truck access to the property north of Arvidson's, especially since the intersection of IL 31 and Edgewood Road will not accommodate large delivery trucks making U-turns, and the surrounding roads prohibit large trucks.

The ideas discussed at the August 24th meeting included providing access between Arvidson's and the property to the north via access across either the front or rear of those businesses. Upon further discussion with those property owners we have determined that those are not likely feasible options because of the topography in the rear of the properties and because of the limited frontage in the front of the properties to accommodate turning movements of large delivery trucks.

One idea from the August 24th meeting was to provide access to both Arvidson's and the property to the north by providing an extended left turn lane with access to both Arvidson's and the property to the north. IDOT is concerned about the safety of this option because there could be confusion over which entrance a vehicle was going to turn left into from IL 31.

The Village requests IDOT to consider a variation of this option. A left turn lane would be installed in the median at the entrance to Arvidson's, as discussed at the August 24th meeting. Immediately north of the left turn lane and median opening would be a mountable concrete median that only large delivery trucks and construction equipment could use to access the property north of Arvidson's property from IL 31. This option would eliminate the safety concern of having two possible turning points along the left turn lane while providing infrequent, large truck only access to this business. This mountable median would be similar to the mountable median on IL 120, east of the City of McHenry.

Without this mountable median, northbound delivery trucks attempting to access the property north of Arvidson's would be forced to attempt a 3-point U-turn at Edgewood Road to enter the property, and then again at Ames Road when exiting the property and continuing north.

I am available to meet with you or your staff at a time and place that is convenient to discuss this request, or an alternative to provide delivery truck access to this business in greater detail. In the meantime, please do not hesitate to contact me for further information or assistance regarding this matter.

Very truly yours,



David K. Robak
Village President

cc: Senator Pamela Althoff
State Representative, Barb Wheeler
John Ambrose, Baxter & Woodman (Village Engineer)



Office of the Mayor
333 S. Green Street
McHenry, IL 60050
www.ci.mchenry.il.us

February 1, 2018

Scott Czaplicki, P.E.
Illinois Department of Transportation
201 Center Court
Schaumburg, IL 60196

Re: Letter of Support for Right-In Access off of Front Street to Waukegan Road and Retention of Usable Developable Land at the Southeast Corner of Front Street and Elm Street in the City of McHenry

Dear Mr. Czaplicki:

The purpose of this letter is to express my complete support for Mr. Bykowski's right-in from Illinois Route 31/Front Street at Waukegan Road. This is paramount for the business located at the southeast corner of Front Street and Waukegan Road (1112 Front Street) to deliver cars. There are no alternative routes for car carriers if Waukegan Road is transformed into a cul-de-sac.

Waukegan Road, in its current configuration, is operating well and safe and is enforced by police. Mr. Bykowski owns most of the southeast corner of Illinois Route 31 and Illinois Route 120 (Elm Street) and has plans to redevelop it. Any future development will be prohibited from accessing westbound Waukegan Road if the right-in is maintained.

Additionally, Mr. Bykowski would like to retain as much land as possible in the parcels which are identified as displacements for redevelopment purposes, at the southeast corner of Illinois Route 120 and Illinois Route 31.

I greatly appreciate your time and consideration of my requests. If you have any questions regarding this letter, please do not hesitate to contact me at 815-363-2108.

Sincerely,

Wayne Jett, Mayor
City of McHenry

The City of McHenry is dedicated to providing the citizens, businesses and visitors of McHenry with the highest quality of programs and services in a customer-oriented, efficient and fiscally responsible manner.

APPENDIX C

Traffic Noise Analysis

(Errata)

TRAFFIC NOISE TECHNICAL REPORT
for the
Proposed Improvements of Illinois Route 31
From Illinois Route 176 to Illinois Route 120
McHenry County, Illinois

Prepared for:

Illinois Department of Transportation

Prepared by:
Huff & Huff, Inc.
May 2017

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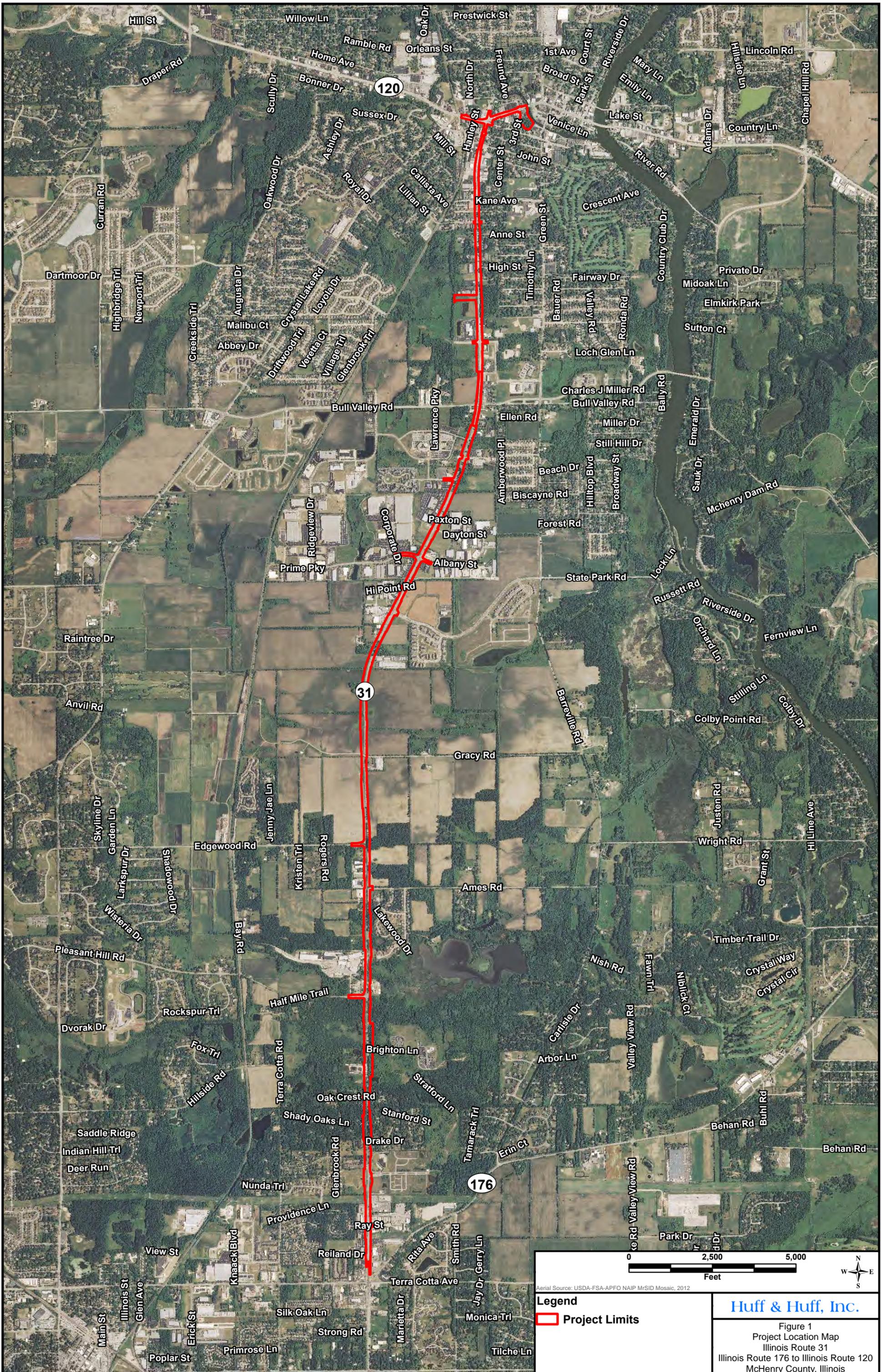
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APPENDIX A: LETTERS TO LOCAL OFFICIALS

1. INTRODUCTION

Improvements to Illinois Route 31 in McHenry County, Illinois, are proposed by the Illinois Department of Transportation. The study area, shown in Figure 1, is from just north of Illinois Route 176 to Illinois Route 120. Improvements to the Illinois Route 176 and IL Route 31 intersection have already been completed. Improvements to the Bull Valley Road intersection with IL Route 31 are being analyzed under a different, concurrent project. The project area is within unincorporated McHenry County, the Cities of McHenry and Crystal Lake, and the Village of Prairie Grove. Existing IL Route 31 is one lane in each direction with certain sections having a center turn-lane. Proposed improvements include adding a second lane to IL Route 31 in both directions.

This report presents a background on noise and the Federal and state noise regulations (Section 2), a discussion of noise sensitive receptors (Section 3), field noise monitoring (Section 4), a description of the noise analysis methodology (Section 5), the analysis of the existing and future noise levels (Section 6), the noise abatement analysis (Section 7), coordination with local officials for undeveloped lands (Section 8), construction noise (Section 9), and the noise analysis conclusion (Section 10).



Aerial Source: USDA-FSA-APFO NAIP MrSID Mosaic, 2012

Legend

Project Limits

Huff & Huff, Inc.

Figure 1
 Project Location Map
 Illinois Route 31
 Illinois Route 176 to Illinois Route 120
 McHenry County, Illinois

2. NOISE BACKGROUND AND REGULATIONS

2.1 Noise Background

Sound is caused by the vibration of air molecules, and loudness is measured on a logarithmic scale using units of decibels (dB). Sound is composed of a wide range of frequencies; however, the human ear is not uniformly sensitive to all frequencies. Therefore, the "A" weighted scale was devised to correspond with the sensitivity of the human ear. Decibels reported using the A-weighted scale are noted as dBA.

The equivalent sound level is the steady-state, A-weighted sound level, which contains the same amount of acoustic energy as the actual time-varying, A-weighted sound level over a specified period of time. If the time period is one hour, the descriptor is the hourly equivalent sound level or $L_{eq}(h)$, which is widely used by state highway agencies as a descriptor of traffic noise. The A-weighted unit is used because:

- 1) it is easily measured,
- 2) it approximates the sensitivity of the human ear to sounds of different frequencies,
- 3) it matches attitudinal surveys of noise annoyance better than other noise measurements, and
- 4) it has been adopted as the basic unit of environmental noise by many agencies around the world in dealing with community noise issues.

2.2 Federal Regulations

Traffic noise analyses are required for all projects considered a Type I project. The federal regulations define Type I projects as any of the following:

- The construction of a highway on new location,
- The physical alteration of an existing highway where there is either:
 - *Substantial Horizontal Alteration*. A project that halves the distance between the traffic noise source and the closest receptor between the existing condition to the future build condition or
 - *Substantial Vertical Alteration*. A project that removes shielding therefore, exposing the line-of-sight between the receptor and the traffic noise source. (This is done by either altering the vertical alignment of the highway or by altering the topography between the highway traffic noise source and the receptor.)
- The addition of a through-traffic lane(s). (This includes the addition of a through-traffic lane that functions as a HOV lane, High-Occupancy Toll (HOT) lane, bus lane, or truck climbing lane.)
- The addition of an auxiliary lane, except for when the auxiliary lane is a turn lane,
- The addition or relocation of interchange lanes or ramps added to a quadrant to complete an existing partial interchange,
- Restriping existing pavement for the purpose of adding a through-traffic lane or an auxiliary lane, or,

- The addition of a new or substantial alteration of a weigh station, rest stop, ride-share lot or toll plaza.

This proposed improvement to IL Route 31 would be characterized as a Type I noise project as it includes the addition of a through-lane.

The Federal regulations establish noise abatement criteria to establish noise levels where noise abatement should be evaluated. Five separate noise abatement criteria (NAC) based upon land use are used by the FHWA to assess potential noise impacts. A traffic noise impact occurs when noise levels approach or exceed the NAC listed in Table 1.¹ In determining the applicable noise activity category for the study area, existing land use was reviewed. The applicable NAC for all residential noise receptors evaluated is 67 dB(A).

**TABLE 1
NOISE ABATEMENT CRITERIA - HOURLY WEIGHTED SOUND LEVEL**

Activity Category ¹	L _{eq} (h)	Evaluation Location	Activity Description
A	57	Exterior	Lands on which serenity and quiet are of extraordinary significance and serve an important public need and where the preservation of those qualities is essential if the area is to continue to serve its intended purpose.
B	67	Exterior	Residential.
C	67	Exterior	Active sport areas, amphitheatres, auditoriums, campgrounds, cemeteries, day care centers, hospitals, libraries, medical facilities, parks, picnic areas, places of worship, playgrounds, public meeting rooms, public or nonprofit institutional structures, radio studios, recording studios, recreation areas, Section 4(f) sites, schools, television studios, trails and trail crossings.
D	52	Interior	Auditoriums, day care centers, hospitals, libraries, medical facilities, places of worship, public meeting rooms, public or nonprofit institutional structures, radio studios, recording studios, schools, and television studios.
E	72	Exterior	Hotels, motels, offices, restaurants/bars, and other developed lands, properties or activities not included in A-D or F.
F	---	---	Agriculture, airports, bus yards, emergency services, industrial, logging, maintenance facilities, manufacturing, mining, rail yards, retail facilities, shipyards, utilities (water resources, water treatment, electrical), and warehousing.
G	---	---	Undeveloped lands that are not permitted.

¹ Based on 23 Code of Federal Regulations Part 772, Procedures for Abatement of Highway Traffic Noise and Construction Noise. (adopted 2010).

2.3 IDOT Policy

Based on the FHWA regulations, State Highway Authorities are allowed to define the noise impacts as 1) the noise level determined to approach the NAC and 2) the increase in noise levels determined to be a substantial increase. The Illinois Department of Transportation (IDOT) defines noise impacts as follows:

- Design-year traffic noise levels approach, meet or exceed the NAC, with approach defined as 66 dB(A) for the residential NAC of 67 dB(A).
- Design-year traffic noise levels are a substantial increase over existing traffic generated noise levels, defined as an increase greater than 14 dB(A).

3. NOISE RECEPTOR SELECTION

The land use within the study limits consists of residences, a library, a recreation area, a park, a school, medical facilities, offices, hotels, restaurants, light industrial, retail, and agricultural use. Figure 2 depicts existing land use based on field reviews and available aerial photography.

Receptor locations were selected based on land use adjacent to the project corridor to represent the land uses with established NAC. For this project, this includes Activity Categories B (residences), C (a library, a cemetery, a recreational area, a park, a school, and medical facilities) and E (offices and a restaurant). The remaining land uses along the project corridor either do not have outdoor use areas or are industrial and agricultural areas characterized as land use Activity Categories F or G, which do not have an established NAC.

The traffic noise study evaluates the study area using common noise environments (CNEs). A CNE is a group of receptors within the same activity category that are exposed to similar noise sources and levels. Within each of the CNEs, the closest receptor was selected to represent the CNE, thereby representing the worst-case traffic noise condition. The represented receptors within the CNEs will have similar traffic noise levels as the selected receptor.

Forty-three receptors were selected to represent the study area; one of the receptors was later removed from the analysis as the Preferred Alternative ended south of the receptor. Each receptor represents a CNE. Receptor types include residences, a library, a recreation area, a park, a school, medical facilities, offices, and a restaurant. According to IDOT policy, when determining traffic noise impacts, primary consideration shall be given to exterior areas where frequent human use occurs for Activity Categories A, B, C and E. Traffic noise impacts for land uses within Activity Category D shall be predicted for interior areas only if no exterior use areas are identified. Therefore, receptor locations were selected at outdoor locations of frequent human use. This includes front yards or back yards of the residential receptors; the front entrance of the school, medical facilities, offices, library, and hotel; a green of the golf course; the open field of the recreational area; and a bench in the cemetery. Because exterior areas of frequent human use were identified for all receptors, no interior noise monitoring or prediction occurred.

Table 2 lists the receptor number, the land use category and associated NAC, and the receptor type. Figure 3 depicts the aerial photograph of the study area with the receptors and CNEs depicted.

The vacant and undeveloped areas within the project area, shown as land use activity category G in Figure 2, were reviewed along to determine if any were permitted for development. Based on the information available from the governing agencies with permitting jurisdiction, there are no existing permits for development within the project limits.

**TABLE 2
SUMMARY OF NOISE RECEPTORS**

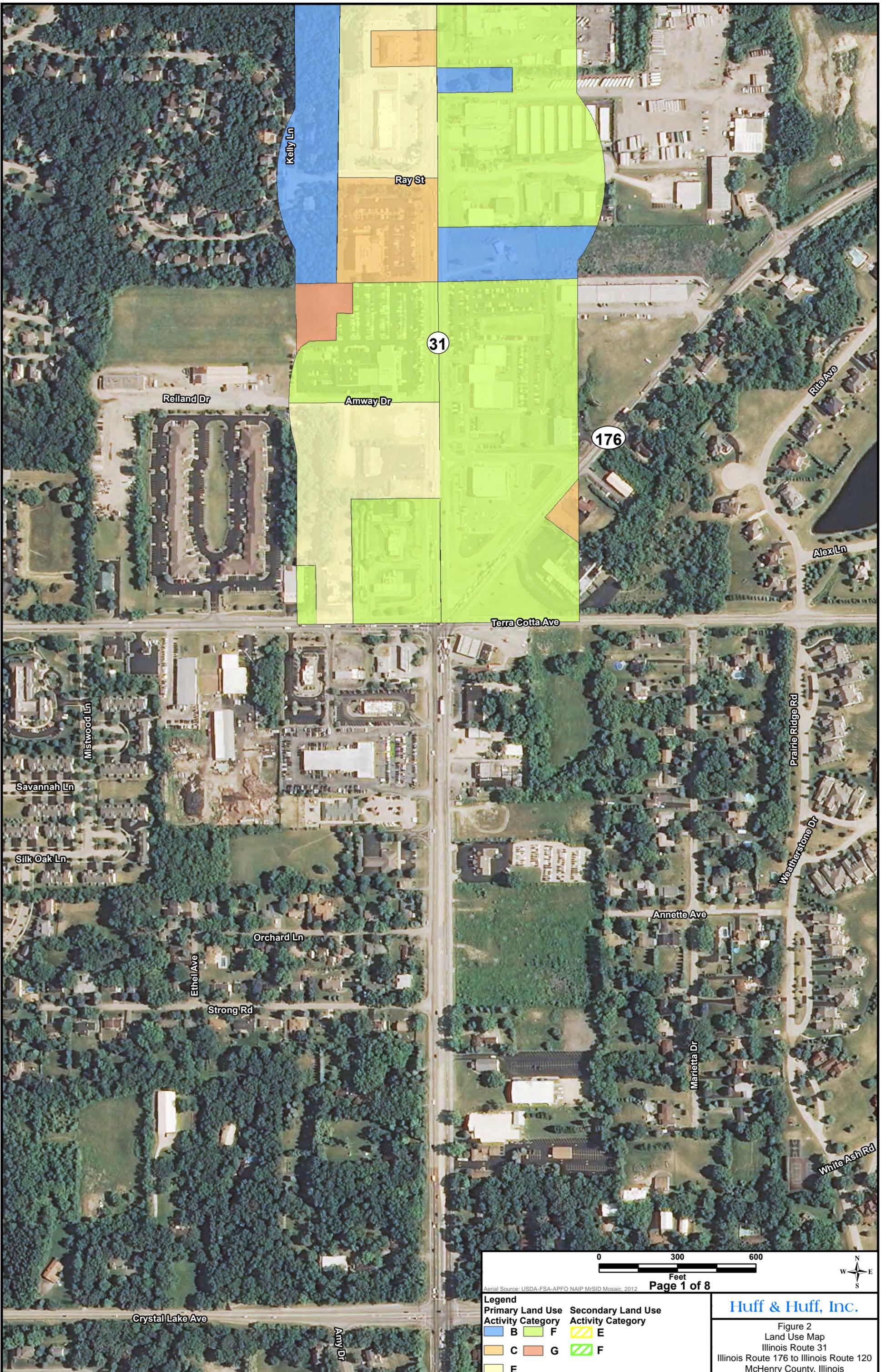
Receptor/ CNE No.	NAC Activity Category / NAC	Type
R1	C / 67	Medical Facility (Chiropractic Center)
R2	E / 72	Offices
R3	B / 67	SFR
R4	C / 67	Medical Facility (Orthopedic Center)
R5	B / 67	SFR
R6	B / 67	SFR
R7	C / 67	School (Columbia College)
R8	C / 67	Medical Facility (Immediate Care Facility)
R9	C / 67	Day School
R10	C / 67	Medical Facility (Dentist & Orthodontist office)
R11	B / 67	SFR
R12	B / 67	SFR
R13	B / 67	SFR
R14	B / 67	SFR
R15	B / 67	SFR
R16	B / 67	SFR
R17	B / 67	SFR
R18	C / 67	Cemetery
R19	B / 67	SFR
R20	B / 67	SFR
R21	B / 67	SFR
R22	E / 72	Restaurant
R23	C / 67	Recreational Area (Soccer Field)
R24	B / 67	SFR
R25	E / 72	Offices
R26	C / 67	Medical Facility (Medical Complex)
R27	C / 67	Medical Facility (Medical Center)
R28	C / 67	Medical Facility (Health System Center)
R29	C / 67	Medical Facility (Health Services)

Receptor/ CNE No.	NAC Activity Category / NAC	Type
R30	B / 67	SFR
R31	C / 67	Medical Facility (Physical Therapy Center)
R32	B / 67	SFR
R33	B / 67	MFR
R34	B / 67	SFR
R35	C / 67	Medical Facility (Dentist office)
R36	C / 67	Library
R37	B / 67	SFR
R38	B / 67	SFR
R39	B / 67	SFR
R40	B / 67	SFR
R41	B / 67	SFR
R42	C / 67	Park
R43*	B / 67	SFR

**R43 was later removed from analysis, as the Preferred Alternative ends south of R43.*

SFR = Single family residence

MFR = Multi family residence

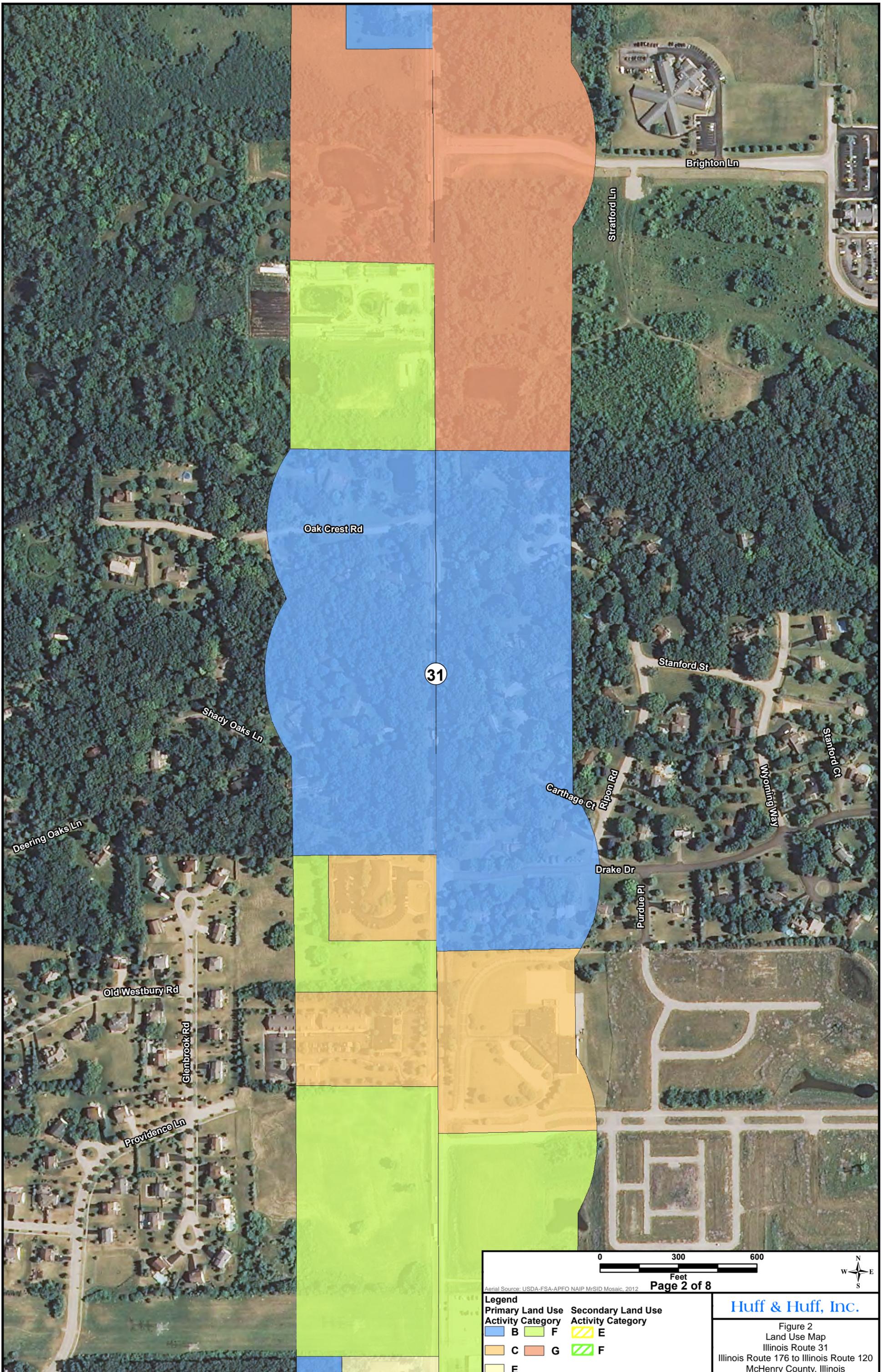


Legend	
Primary Land Use Activity Category	Secondary Land Use Activity Category
■ B	■ F
■ C	■ E
■ E	■ G
	■ F

Huff & Huff, Inc.

Figure 2
 Land Use Map
 Illinois Route 31
 Illinois Route 176 to Illinois Route 120
 McHenry County, Illinois

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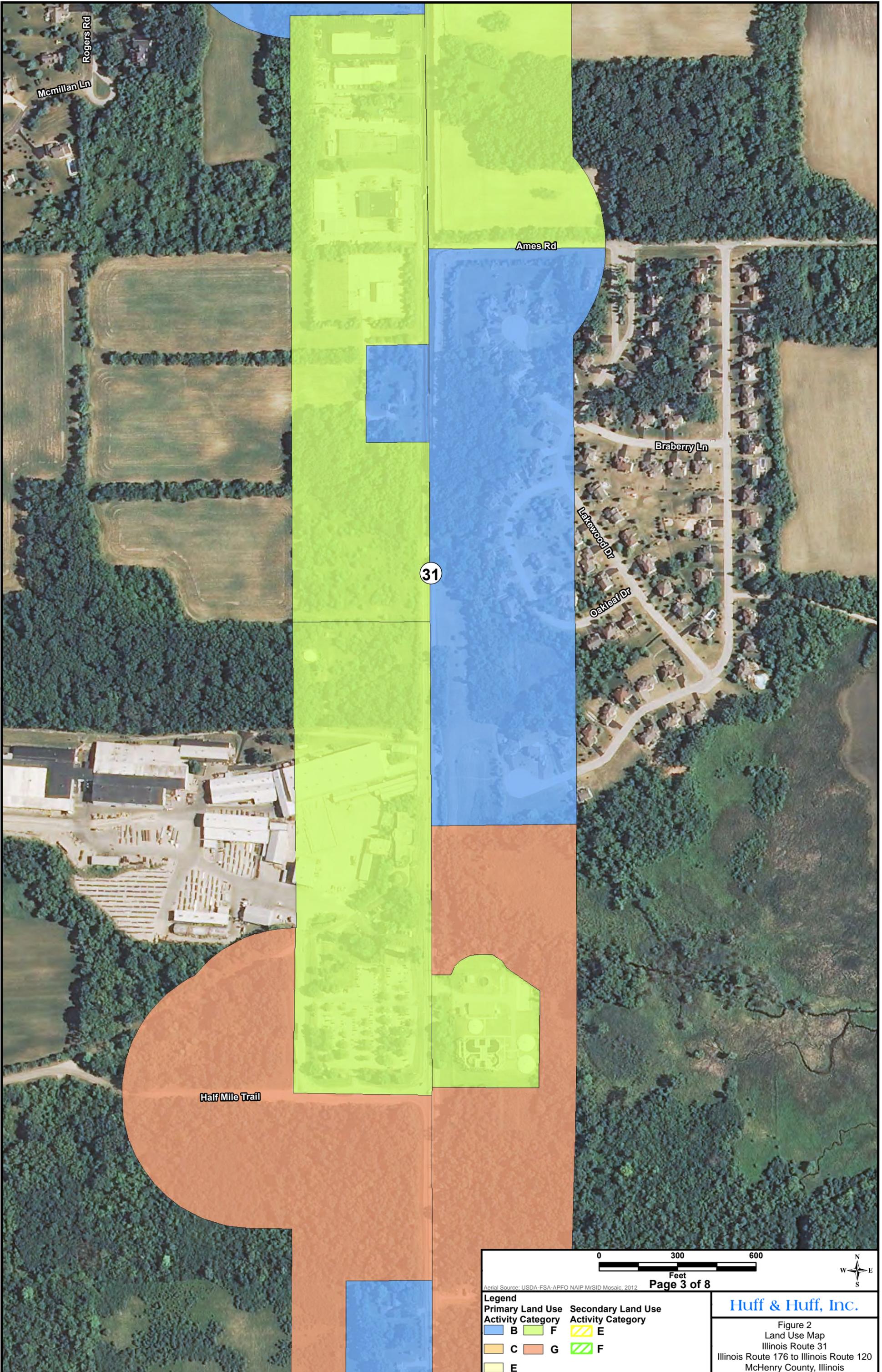
Aerial Source: USDA-FSA-APFO NAIP MRSID Mosaic, 2012

Legend	
Primary Land Use Activity Category	Secondary Land Use Activity Category
 B	 F
 C	 G
 E	 E

Huff & Huff, Inc.

Figure 2
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 Illinois Route 176 to Illinois Route 120
 McHenry County, Illinois

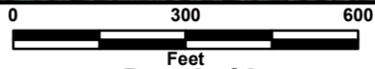
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31

Braberry Ln
Lakewood Dr
Oakleaf Dr

Half Mile Trail



Aerial Source: USDA-FSA-APFO NAIP MrSID Mosaic, 2012

Legend	
Primary Land Use Activity Category	Secondary Land Use Activity Category
 B	 F
 C	 G
 E	 H

Huff & Huff, Inc.

Figure 2
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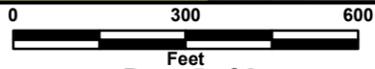
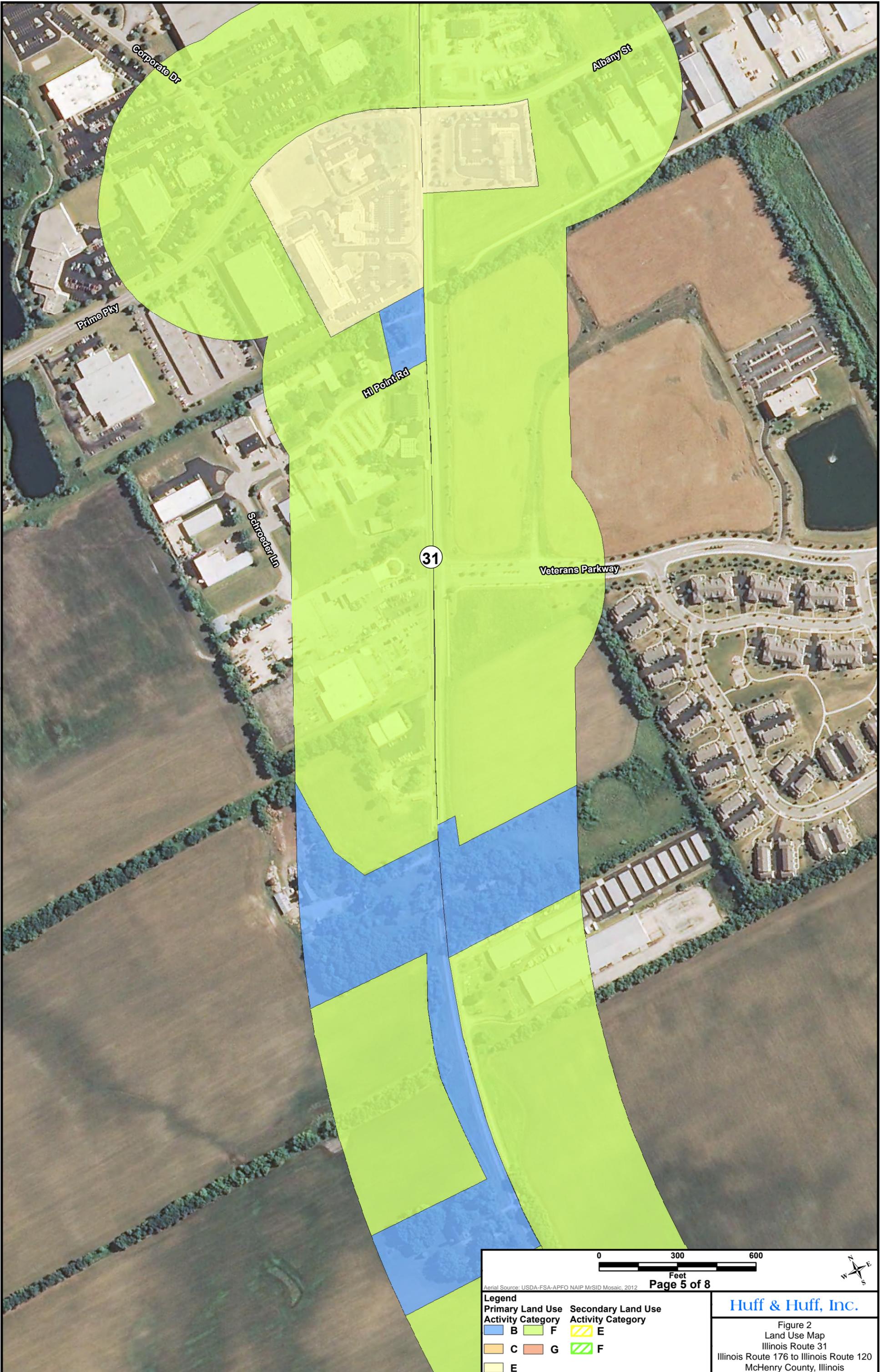


Aerial Source: USDA-FSA-APFO NAIP MrSID Mosaic, 2012

Legend	
Primary Land Use Activity Category	Secondary Land Use Activity Category
 B	 E
 F	 G
 C	 F
 E	

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McHenry County, Illinois

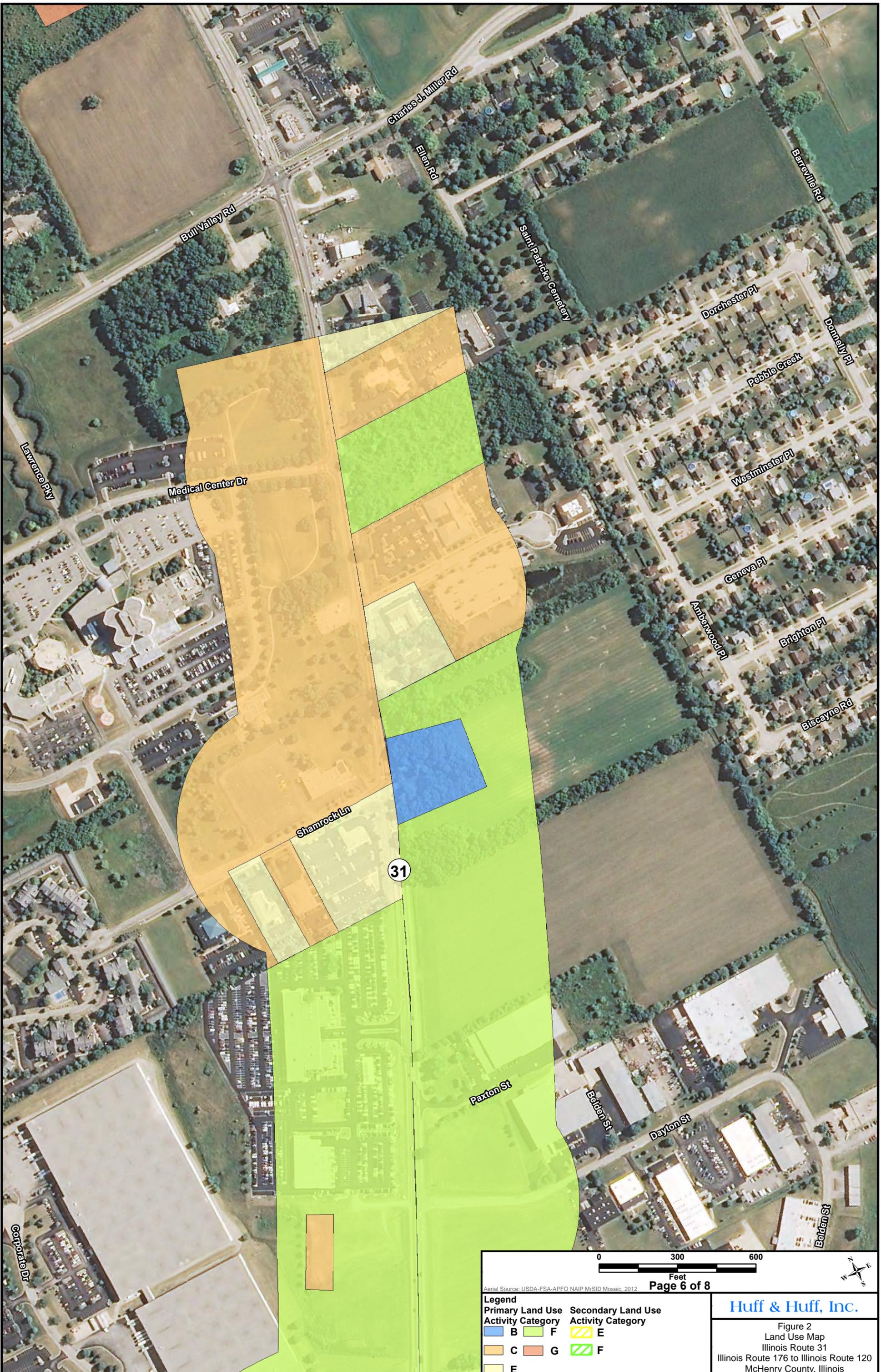


Aerial Source: USDA-FSA-APFO NAIP MrSID Mosaic, 2012
 Page 5 of 8

Legend	
Primary Land Use Activity Category	Secondary Land Use Activity Category
■ B	■ E
■ F	■ G
■ C	■ F
■ E	

Huff & Huff, Inc.

Figure 2
 Land Use Map
 Illinois Route 31
 Illinois Route 176 to Illinois Route 120
 McHenry County, Illinois



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Feet

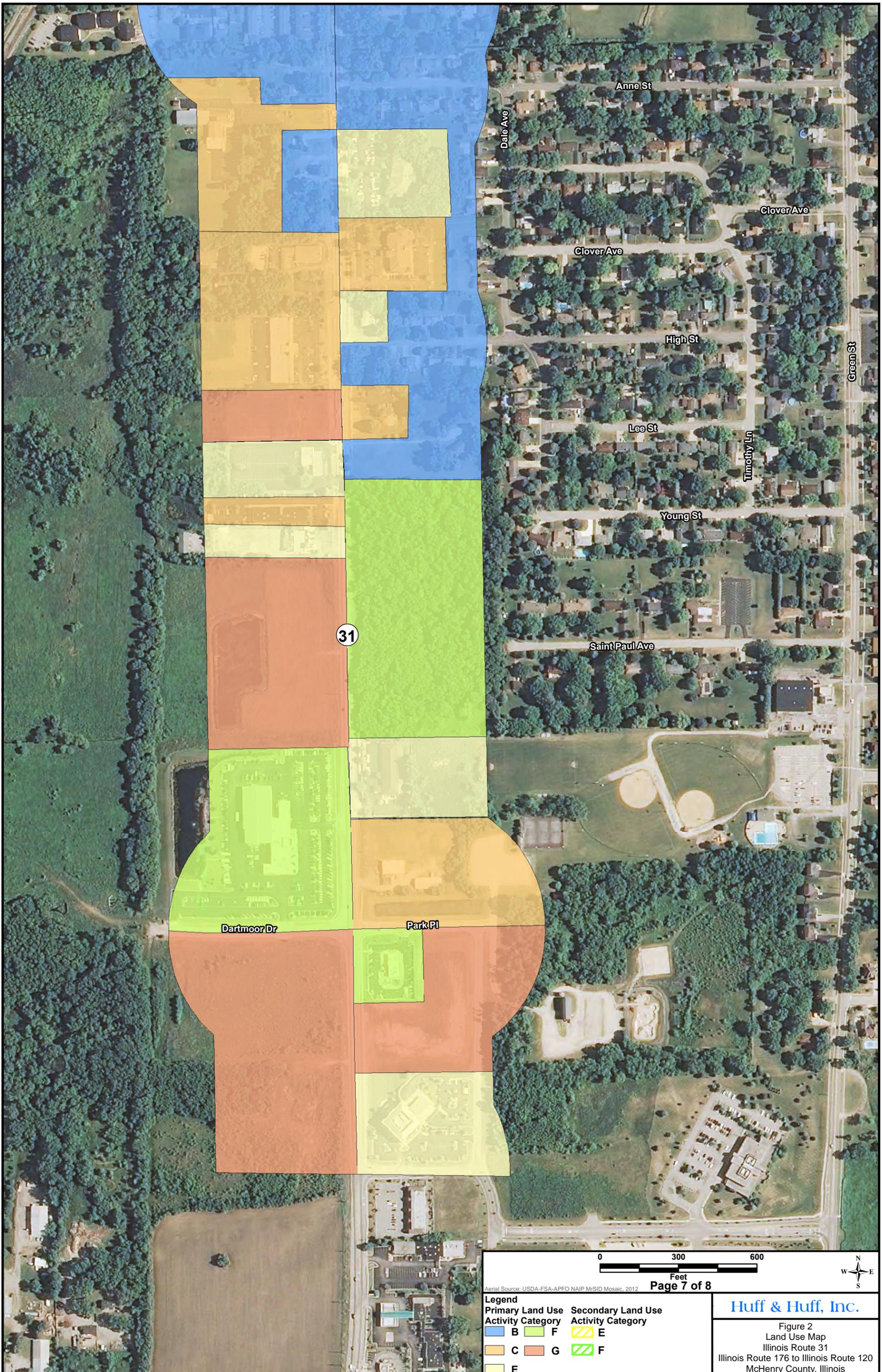
Page 6 of 8

Aerial Source: USDA-FSA-APFO NAIP MrSID Mosaic, 2012

Legend	
Primary Land Use Activity Category	Secondary Land Use Activity Category
■ B	■ E
■ C	■ F
■ E	■ G
	■ F

Huff & Huff, Inc.

Figure 2
Land Use Map
Illinois Route 31
Illinois Route 176 to Illinois Route 120
McHenry County, Illinois



31

Anne St

Clover Ave

Clover Ave

High St

Green St

Lee St

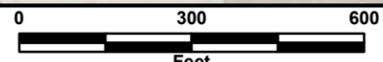
Timothy Ln

Young St

Saint Paul Ave

Dartmoor Dr

Park Pl



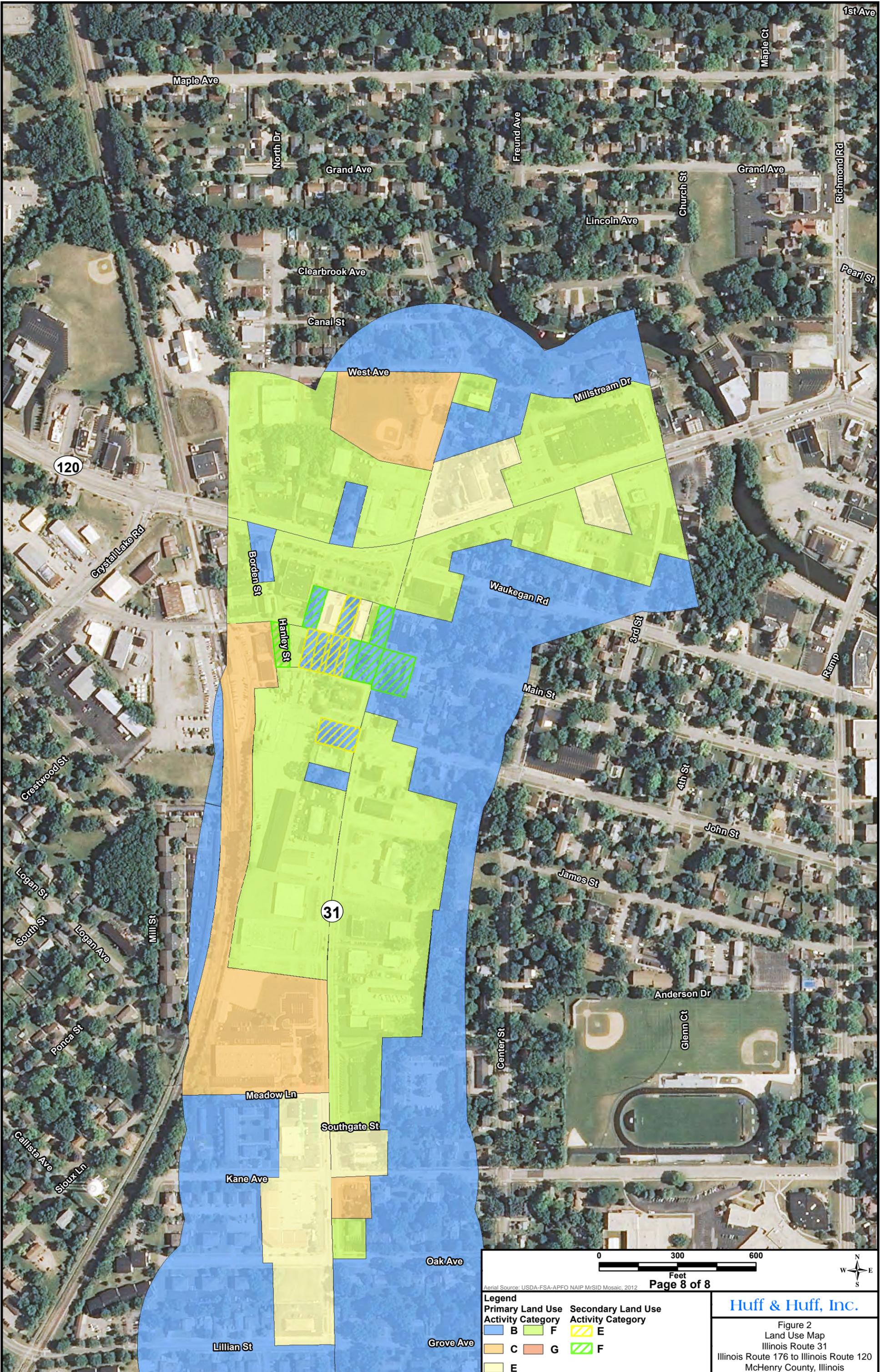
Aerial Source: USDA-FSA-APFO NAIP MrSID Mosaic, 2012

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Legend	
Primary Land Use Activity Category	Secondary Land Use Activity Category
■ B	■ F
■ C	■ G
■ E	■ F

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Figure 2
 Land Use Map
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 Illinois Route 176 to Illinois Route 120
 McHenry County, Illinois



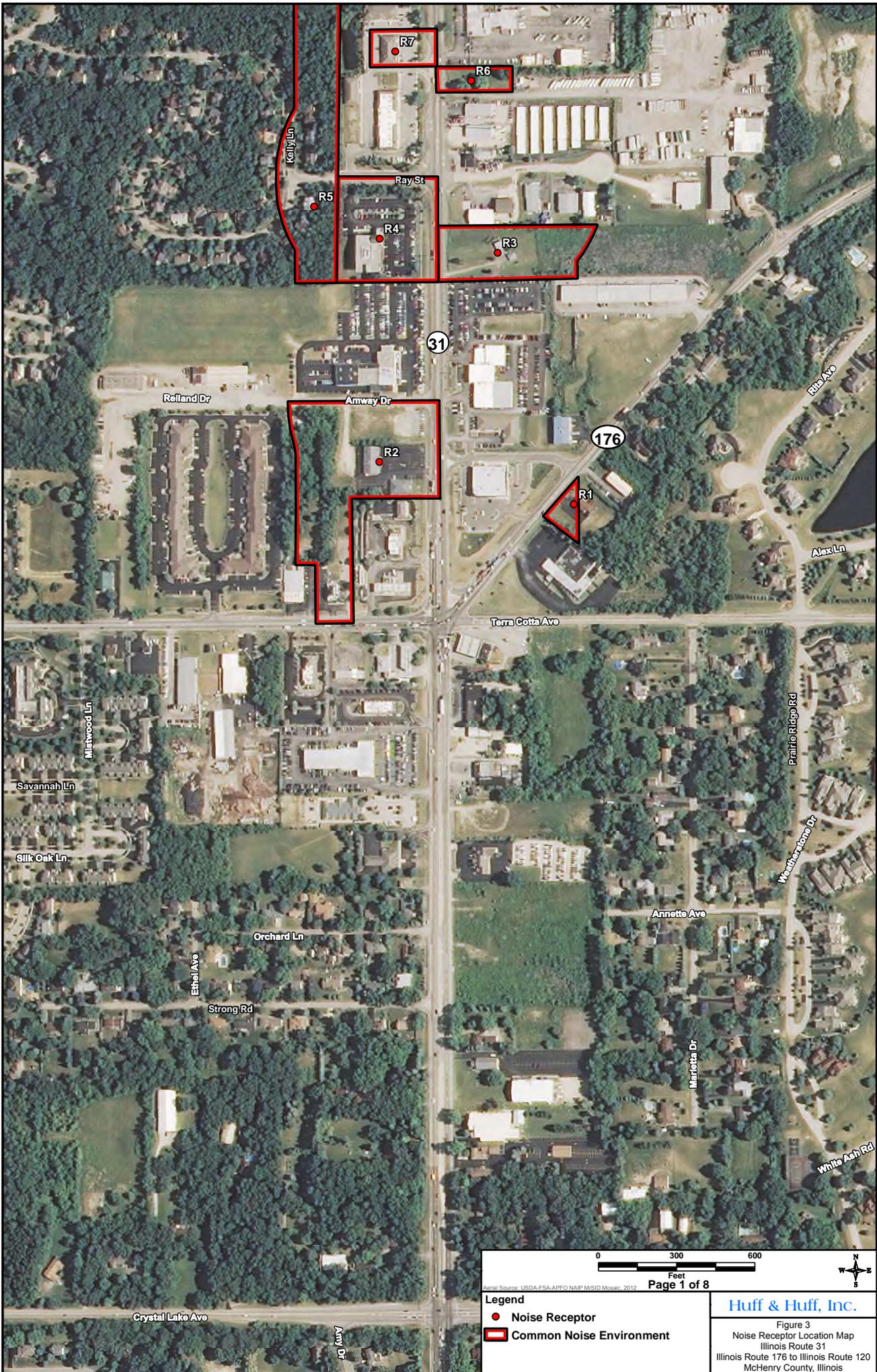
Aerial Source: USDA-FSA-APFO NAIP MrSID Mosaic, 2012

Page 8 of 8

Legend	
Primary Land Use Activity Category	Secondary Land Use Activity Category
 B	 F
 C	 G
 E	 E
	 F

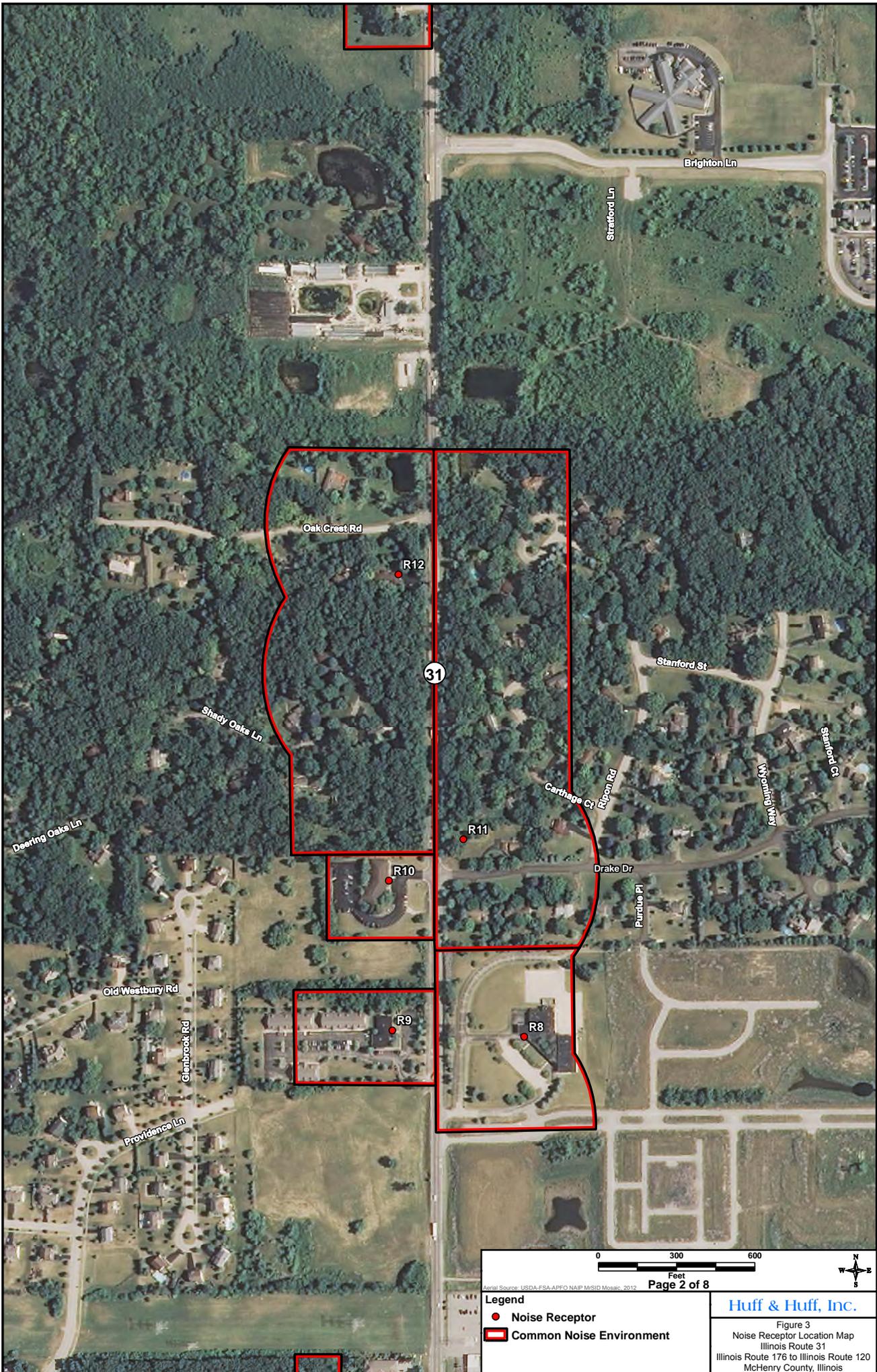
Huff & Huff, Inc.

Figure 2
 Land Use Map
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 Illinois Route 176 to Illinois Route 120
 McHenry County, Illinois



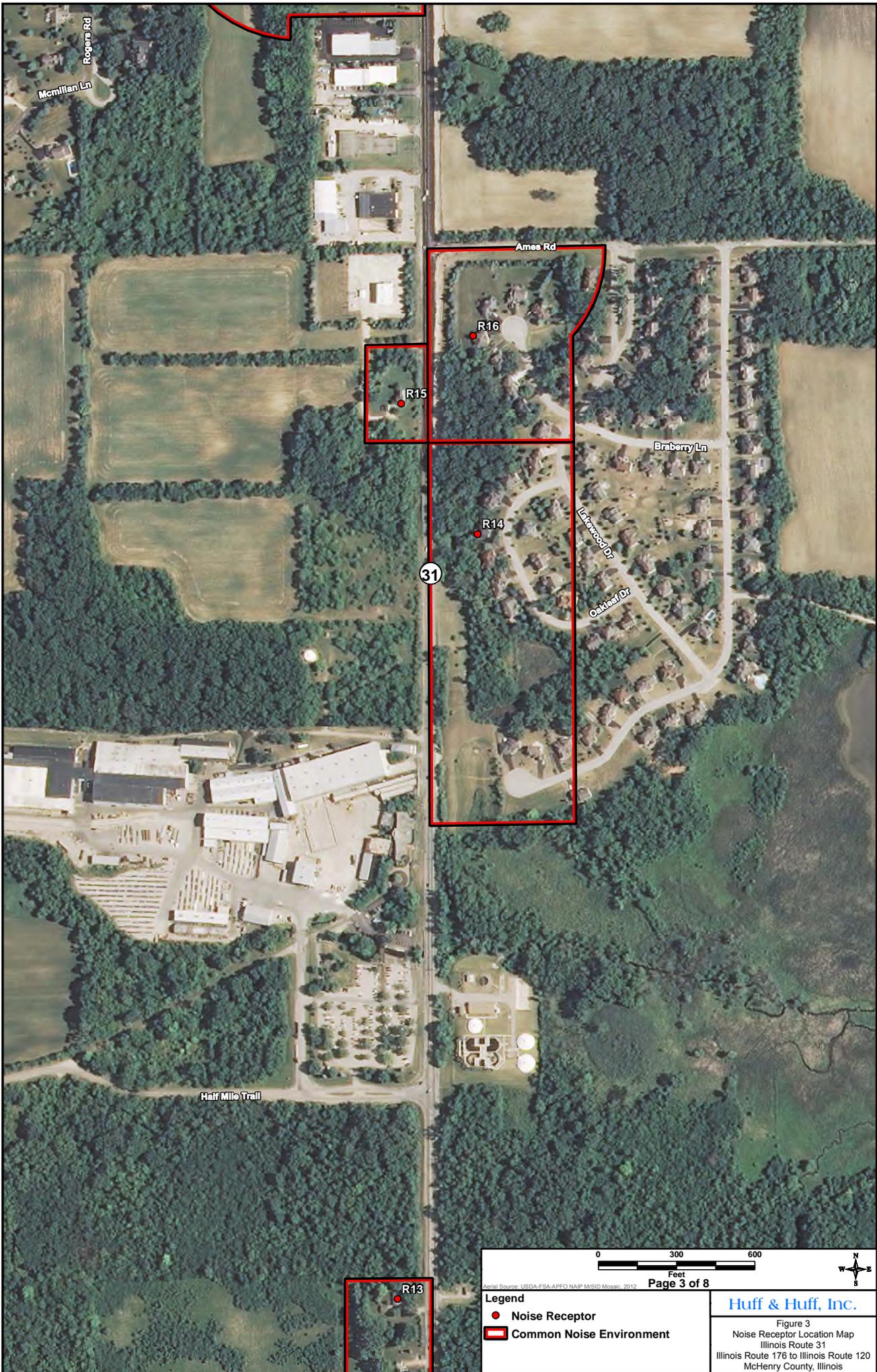
Legend
 ● Noise Receptor
 ■ Common Noise Environment

Huff & Huff, Inc.
 Figure 3
 Noise Receptor Location Map
 Illinois Route 31
 Illinois Route 176 to Illinois Route 120
 McHenry County, Illinois



Legend
 ● Noise Receptor
 ■ Common Noise Environment

Huff & Huff, Inc.
 Figure 3
 Noise Receptor Location Map
 Illinois Route 31
 Illinois Route 176 to Illinois Route 120
 McHenry County, Illinois



Aerial Source: USDA-FSA-APFO NAIP MxSID Mosaic, 2012

Legend	
●	Noise Receptor
	Common Noise Environment

Huff & Huff, Inc.

Figure 3
 Noise Receptor Location Map
 Illinois Route 31
 Illinois Route 176 to Illinois Route 120
 McHenry County, Illinois



Aerial Source: USDA-FSA-APFO NAIP M/SID Mosaic, 2012

Legend	
●	Noise Receptor
	Common Noise Environment

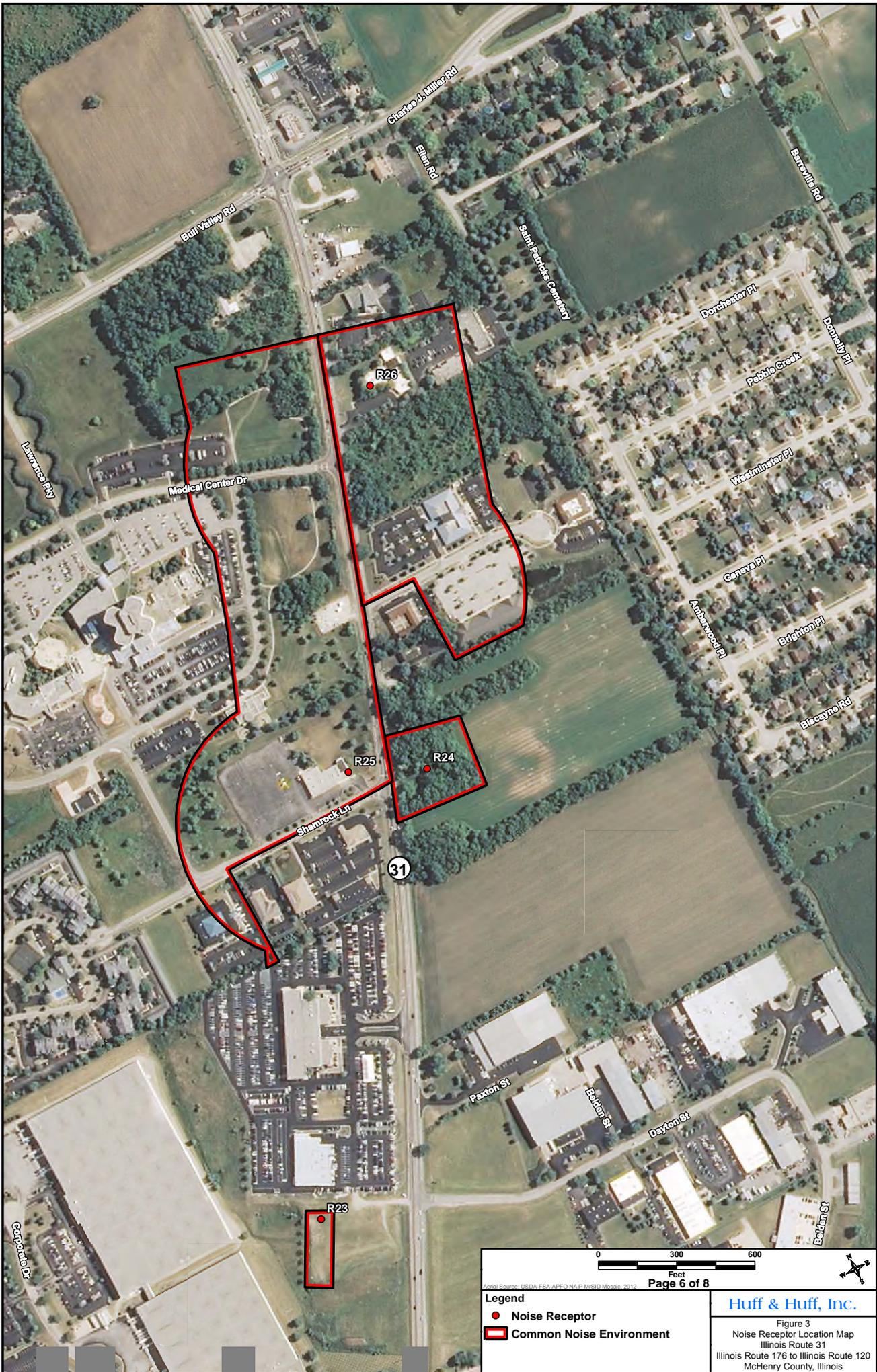
Huff & Huff, Inc.

Figure 3
Noise Receptor Location Map
Illinois Route 31
Illinois Route 176 to Illinois Route 120
McHenry County, Illinois



Legend
 ● Noise Receptor
 ■ Common Noise Environment

Huff & Huff, Inc.
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 McHenry County, Illinois



Aerial Source: USDA-FSA-APFO NAIP MxSID Mosaic 2012

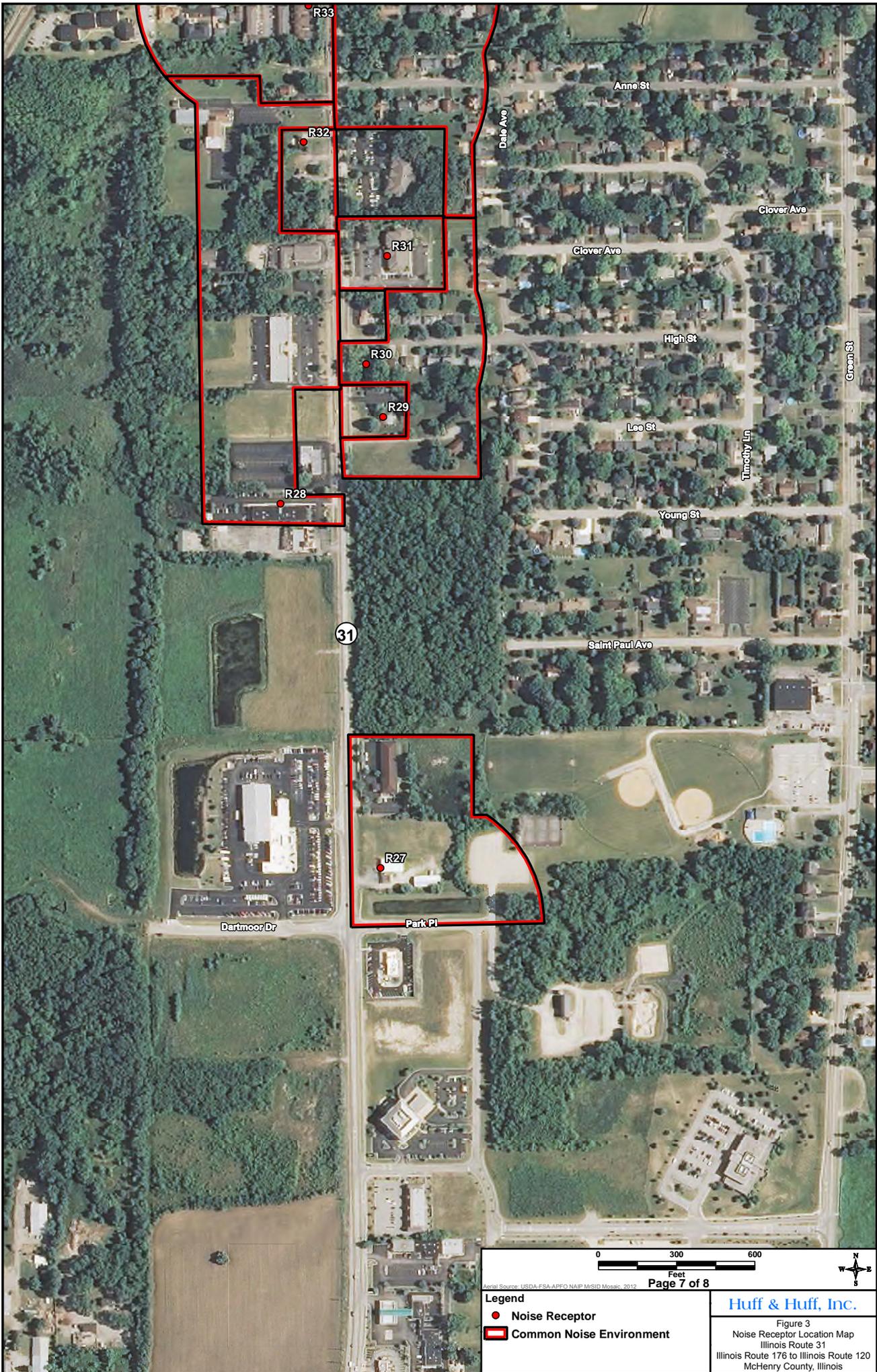
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Legend

- Noise Receptor
- Common Noise Environment

Huff & Huff, Inc.

Figure 3
 Noise Receptor Location Map
 Illinois Route 31
 Illinois Route 176 to Illinois Route 120
 McHenry County, Illinois



0 300 600
Feet

Aerial Source: USDA-FSA-APFO NAIP MxSID Mosaic, 2012

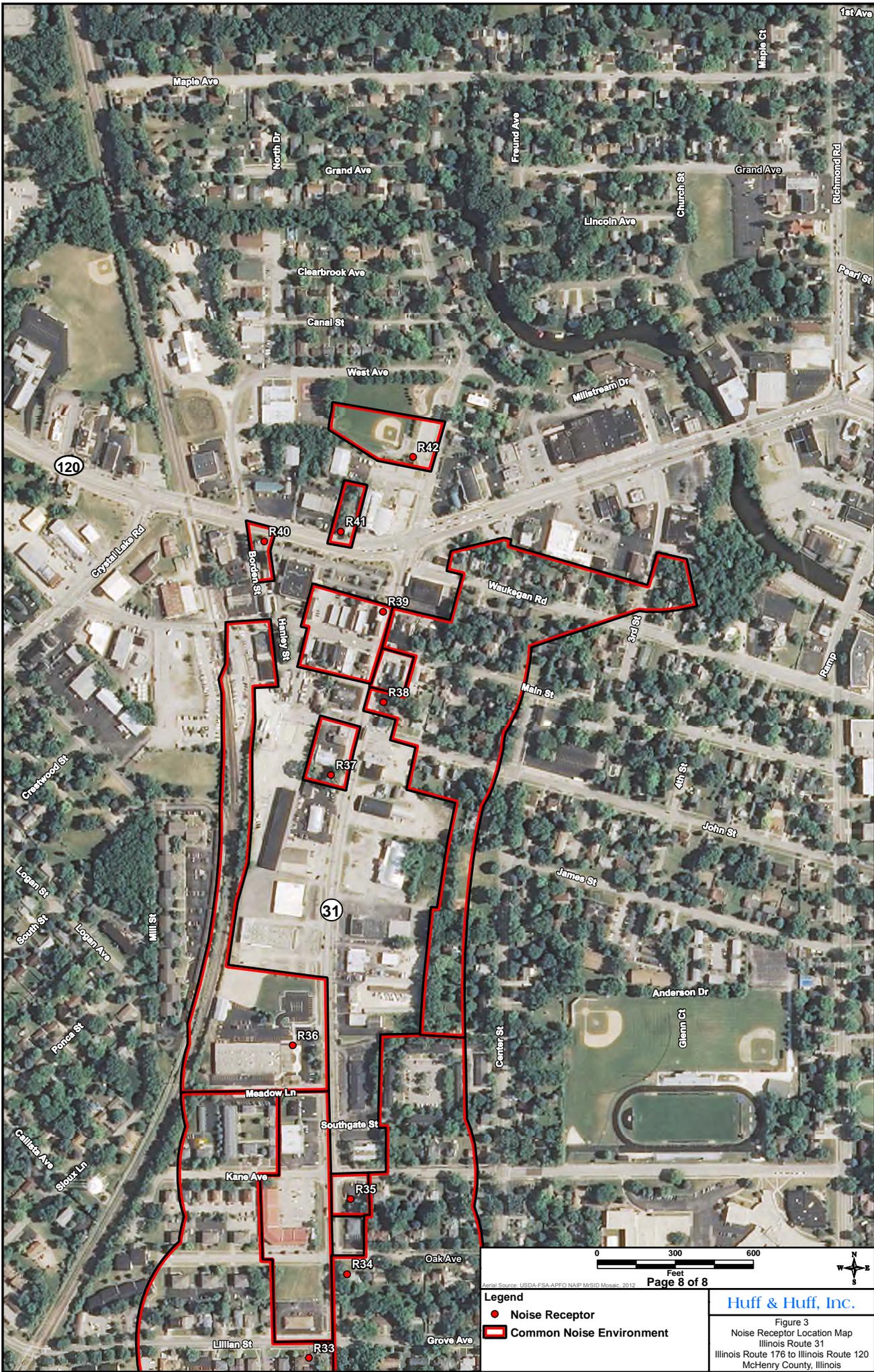
Page 7 of 8

Legend

- Noise Receptor
- ▭ Common Noise Environment

Huff & Huff, Inc.

Figure 3
Noise Receptor Location Map
Illinois Route 31
Illinois Route 176 to Illinois Route 120
McHenry County, Illinois



Aerial Source: USDA-FSA-APFO NAIP MxSID Mosaic 2012

Legend

- Noise Receptor
- Common Noise Environment

Huff & Huff, Inc.

Figure 3
 Noise Receptor Location Map
 Illinois Route 31
 Illinois Route 176 to Illinois Route 120
 McHenry County, Illinois

4. FIELD NOISE MEASUREMENTS

Ambient noise level measurements show existing site conditions. The traffic volumes and conditions during the actual noise level measurements need to be considered when evaluating field measurements as typical for the area. The following methodology was used to collect noise level measurements.

Traffic noise levels measured during monitoring events are representative of the traffic characteristics (volume, speed and composition) for the period of time measured. This may or may not be the peak-hour noise condition at the location being measured. In addition, the noise levels are also influenced by other noise sources in the area other than the traffic noise and the characteristics of the location, such as shielding afforded by existing berms or structures. Consequently, comparison of the noise levels between locations needs to also consider the variations in site characteristics in addition to varying traffic conditions. Noise monitoring was conducted at receptors R9, R10, R18, R21, R22, R26, R28, R30, R33, R36, and R42. The IDOT Highway Traffic Noise Assessment Manual states that between 25 to 50 percent of receptors that are included in the noise analysis should be evaluated by noise monitoring. The eleven monitored sites are 26% of the total 42 receptors, which is within the range recommended by IDOT. These receptors were selected so that noise monitoring would occur throughout the project corridor, with the exception of the extreme south portion of the corridor; during monitoring, active road construction was occurring at the IL Route 176/IL Route 31 intersection, which did not represent typical ambient noise levels. The noise monitoring results are compared to noise modeling results for the existing conditions to validate the noise model. Traffic noise modeling is completed using the FHWA-approved Traffic Noise Model (TNM 2.5).

4.1 Traffic Volumes

Traffic volumes along IL Route 31 were counted during field monitoring. The number of cars and trucks were recorded separately along with any other noise sources observed during monitoring. The traffic volumes were counted as a total during the 10-minute noise monitoring periods. The traffic volumes counted were extrapolated to hourly volumes. This procedure is accepted by the Federal Highway Administration as a representative noise monitoring method, detailed in IDOT's "Highway Noise Assessment Manual," Section 3.5.2.

4.2 Time and Day for Measurements

Noise monitoring is typically conducted during the period representing the worst hourly noise level. This may or may not be during the peak-hour traffic volumes, as traffic may be stop-and-go during this period or at a reduced travel speed. Traffic was moving steadily on adjacent roadways during the measurements. Noise monitoring was conducted at all sites on August 7, 2014 between the hours of 11 am to 4 pm.

4.3 Weather Conditions

Weather conditions have some effect on noise measurement readings. Noise measurements cannot be taken if wind speed exceeds 12 mph. A wind screen was used at all times during the monitoring to reduce wind noise. The conditions during the monitoring are summarized as follows:

WEATHER CONDITIONS DURING THE NOISE MONITORING

Condition	Required	Actual*
Pavement	Dry	Dry
Humidity	Less than 90%	65%
Temperature	14 to 112 degrees F	75 degrees F
Wind Speed	Less than 12 mph**	7 mph

* National Weather Service Data

** Miles per hour

The weather conditions during the noise monitoring were within the recommended ranges for all parameters listed.

4.4 Instrumentation

A Brüel & Kjaer Type 2250L sound level meter was used for monitoring the actual noise level. The L_{eq} was recorded for the "A" weighted scale. L_{eq} is the equivalent level of sound (in decibels or dB(A)) which represents the level of sound, held constant over a specified period of time. This reflects the same amount of energy as the actual fluctuating noise over that time period. The instrument was calibrated prior to use. The instrument was set up approximately five (5) feet from the ground and the measurement was conducted for 10 minutes. The noise meter was placed in an outdoor location where human activity typically occurs or in a location representative of that location.

4.5 Field Noise Monitoring Results

Table 3 compares the noise monitoring results for the eleven monitored locations to the TNM 2.5 modeled existing noise levels. Noise monitored levels ranged from 55 dB(A) to 74 dB(A). The difference between modeled and monitored noise levels provides an indication of the representativeness of the TNM 2.5 model. Section 5 describes the TNM 2.5 modeling methodology and results. Monitored noise levels are within 3 dB(A) of the modeled noise levels, which validates the TNM 2.5 model. The impact analysis and abatement evaluation will be conducted using the build traffic noise model results.

TABLE 3
NOISE MONITORING RESULTS, L_{eq}

Receptor	Noise Level Monitored, dB(A)	Modeled Existing Noise Level, dB(A)*	Difference Between Modeled and Monitored, dB(A)
R9	60	61	1
R10	59	59	0
R18	74	76	2
R21	68	65	-3
R22	56	58	2
R26	55	57	2
R28	55	55	0
R30	66	64	-2
R33	65	67	2
R36	61	58	-3
R42	56	59	3

*Modeling methodology and results are presented in Section 5 and Section 6, respectively.

5. NOISE ANALYSIS METHODOLOGY

Modeling of traffic noise levels at the receptors located within the project limits was conducted utilizing the FHWA-approved TNM 2.5. Traffic noise levels for the receptor sites were predicted using existing (2013) and future (2040) traffic volumes.

TNM 2.5 data inputs include traffic volume, traffic mix (cars, heavy trucks, and medium trucks), traffic controls, receptor distance, elevation, and average speeds during free flowing conditions. Information sources used in the analysis are briefly described in the following subsections.

5.1 Traffic Volumes

STV, Inc. provided average daily traffic (ADT) for the years 2013 and 2040 for IL Route 31 and the major crossroads within the project limits. The daily volumes were converted to peak hour volume using a K factor of 10 percent.

5.2 Traffic Composition

Three types of vehicles, including cars, medium trucks, and heavy trucks, are input into TNM 2.5. Truck composition for the roadways was determined based on the traffic counts provided. The percentage of automobiles for the existing condition on IL Route 31 is estimated to be between 91 percent and 100 percent with medium and heavy trucks combined accounting for between 0 percent and 9 percent. Truck traffic is assumed to be half medium trucks and half heavy trucks. The range in values is associated with the variation in traffic volumes that occur within the project limits.

5.3 Receptor Distance/Elevation

The selected representative receptors include residences, a library, a recreation area, a park, a school, medical facilities, offices, and a restaurant. The distance and elevation of each receptor directly affects the predicted traffic noise level. Receptor locations are between 10 feet and 430 feet from the existing IL Route 31 edge of pavement. The specific location of the receptor is based upon identifying the location where outdoor activity occurs.

5.4 Speed Conditions

The average free-flow speeds for the corridor (posted speed limits) were included as listed:

- IL Route 176 to Ray Street: 40 mph
- Ray Street to Drake Drive: 45 mph
- Drake Drive to 1,200 feet south of Veterans Parkway: 55 mph
- South of Veterans Parkway to High Point Road: 50 mph
- High Point Road to south of Dartmoor Drive/Park Place: 45 mph
- South of Dartmoor Drive/Park Place to south of Anne Street: 40 mph
- South of Anne Street to Meadow Lane: 35 mph
- Meadow Lane to IL Route 120: 30 mph
- IL Route 120: 30 mph

6. TNM 2.5 RESULTS

6.1 Existing, No-Build and Build Receptor Noise Evaluation

Existing (2013), No-Build (2040), and Build (2040) traffic noise levels were predicted for the forty-two receptor sites utilizing TNM 2.5. Table 4 presents the existing (2013) and projected (2040) noise levels for the analyzed receptor sites, as well as the anticipated difference in noise levels for these two time periods.

The existing 2013 modeled noise levels range from 56 dB(A) at R23 and R28 to 78 dB(A) at R18. The projected No-Build 2040 traffic noise levels range from 57 dB(A) at R5, R23, and R28 to 79 dB(A) at R18. Receptor noise levels were found to either remain the same or increase between one dB(A) and two dB(A) from the existing scenario to the 2040 No Build scenario. Any increase in traffic noise levels between the existing and 2040 No Build scenarios is due to increased traffic volumes for the 2040 No Build condition.

The projected Build 2040 traffic noise levels range from 59 dB(A) at R5, R23, and R28 to 74 dB(A) at R18. The projected Build 2040 noise levels change from -4 dB(A) and five dB(A) from the existing condition. One receptor, R18, showed a noise level decrease in the Build 2040 condition, due to IL Route 31 being moved approximately 12 feet west in this location, away from R18. The speed limit on IL Route 31 is also proposed to decrease to 45 mph near R18, from the existing 55 mph speed limit. Increases in noise levels between the existing and 2040 Build conditions are due to an increase in traffic volumes and the widening of IL Route 31, which moves traffic closer to some receptors.

Under the proposed 2040 Build scenario, 23 receptor locations approach, meet, or exceed the FHWA NAC in the Build condition, and therefore warrant a noise abatement analysis. None of the receptors are considered impacted due to a substantial increase (greater than 14 dB(A) increase) in traffic noise levels.

TABLE 4
NOISE IMPACT SUMMARY – TNM 2.5 MODELING RESULTS

Receptor Number	Activity Category/ NAC (dB(A))	Distance to Existing Edge of IL Route 31 Pavement, ft.	Existing 2013 Noise Level, dB(A)	No-build 2040 Noise Level, dB(A)	Build 2040 Noise Level, dB(A)	Increase in Build Noise Levels over Existing Noise Levels, dB(A)
R1	C / 67	70	68	69	69	1
R2	E / 72	180	62	63	65	3
R3	B / 67	210	63	64	64	1
R4	C / 67	180	63	63	65	2
R5	B / 67	430	57	57	59	2
R6	B / 67	100	67	67	68	1
R7	C / 67	120	65	65	67	2
R8	C / 67	315	60	60	60	0
R9	E / 72	135	65	65	66	1
R10	C / 67	140	63	63	65	2
R11	B / 67	130	68	68	69	1
R12	B / 67	100	66	67	69	3
R13	B / 67	80	65	66	68	3
R14	B / 67	160	60	61	63	2
R15	B / 67	60	67	68	68	1
R16	B / 67	160	64	65	67	1
R17	B / 67	50	69	70	70	1
R18	C / 67	70	78	79	74	-4
R19	B / 67	90	63	63	65	2
R20	B / 67	130	64	65	66	2
R21	B / 67	90	67	68	71	4
R22	E / 72	185	60	61	63	3
R23	C / 67	360	56	57	59	3
R24	B / 67	150	60	61	64	4
R25	E / 72	105	64	65	68	4
R26	C / 67	185	58	59	60	2
R27	C / 67	105	60	61	65	5
R28	C / 67	240	56	57	59	3
R29	C / 67	125	58	59	61	3
R30	B / 67	90	66	67	69	3
R31	C / 67	150	58	59	61	3
R32	B / 67	100	62	62	66	4
R33	B / 67	60	67	68	67	0
R34	B / 67	40	68	68	69	1
R35	C / 67	50	67	67	68	1
R36	C / 67	105	61	61	66	5
R37	B / 67	45	64	65	69	5
R38	B / 67	20	66	67	71	5
R39	B / 67	30	73	74	76	3
R40	B / 67	35	69	71	71	2
R41	B / 67	10	72	74	73	1
R42	C / 67	85	59	61	62	3

Boldface indicates the noise levels approach, meet or exceed the NAC in future build condition

7. ABATEMENT ANALYSIS

7.1 Abatement Alternatives

Traffic noise abatement measures were considered for the 23 impacted receptors that approach, meet, or exceed the appropriate FHWA NAC. The most feasible approach to abating noise impacts in this area would be to construct a noise barrier. This may include a noise wall, an earth berm or a combination of both. Noise barriers placed adjacent to the roadway will attenuate traffic-related noise and are the most practical measure for this project. An effective noise barrier must be tall enough to break the line-of-sight between the receptor and source and typically extends beyond the last receptor four times the distance between the receptor and noise barrier. Noise barriers have a zone of effectiveness, or shadow zone, which is generally within 200 feet of the noise barrier; therefore, less noise reduction is achieved as the distance between the receptor and the noise barrier increases.

TNM 2.5 was used to perform the noise barrier feasibility and reasonability evaluation for the 23 impacted receptors. When determining if an abatement measure is feasible and reasonable, the noise reductions achieved, number of residences benefited, total cost, and total cost per residence benefited are considered.

7.2 Feasibility and Reasonability

An analysis of noise abatement measures (noise barriers) was conducted in conformance with FHWA requirements contained in Title 23 *Code of Federal Regulations* Part 772 for each of the impacted receptors. In order for a noise abatement measure to be constructed, it must meet both the feasibility and reasonability criteria, described below.

Feasibility

The feasibility evaluation is a combination of acoustical and engineering factors considered in the evaluation of a noise abatement measure. The acoustical portion of the IDOT policy, as required by FHWA regulations, considers noise abatement to be feasible if it achieves at least a 5 dB(A) traffic noise reduction at an impacted receptor. Factors including but not limited to safety, barrier height, topography, drainage, utilities, maintenance, and access issues are also considered.

Reasonability

As per the FHWA regulations, a noise abatement measure is determined to be reasonable when all three of the following reasonableness evaluation factors are met:

- cost effectiveness of the highway traffic noise abatement measure;
- achievement of IDOT's noise reduction design goal; and,
- consideration of the viewpoints of the benefited receptors (property owners and residents) if all other criterion are achieved.

A noise abatement measure is considered cost-effective to construct if the noise wall construction cost per benefited receptor is less than the allowable cost per benefited receptor. A benefited receptor is any receptor that is afforded at least a 5 dB(A) traffic noise reduction from the proposed noise abatement measure. The FHWA regulations allow each State Highway Authority to establish cost criteria for determining cost effectiveness.

IDOT policy establishes the actual cost per benefited receptor shall be based on a noise wall cost of \$25 per square foot, which includes engineering, materials, and construction. The base value allowable cost is \$24,000 per benefited receptor, which can be increased based on three factors as summarized below:

- the absolute noise level of the benefited receptors in the design year build scenario before noise abatement;
- the incremental increase in noise level between the existing noise level at the benefited receptor and the predicted build noise level before noise abatement; and
- the date of development compared to the construction date of the highway. These factors are considered for all benefited receptors.

Absolute Noise Level Consideration

Predicted Build Noise Level Before Noise Abatement	Dollars Added to Base Value Cost per Benefited Receptor
Less than 70 dB(A)	\$0
70 to 74 dB(A)	\$1,000
75 to 79 dB(A)	\$2,000
80 dB(A) or greater	\$4,000

Source: IDOT Highway Traffic Noise Assessment Manual

Increase in Noise Level Consideration

Incremental Increase in Noise Level Between the Existing Noise Level and the Predicted Build Noise Level Before Noise Abatement	Dollars Added to Base Value Cost per Benefited Receptor
Less than 5 dB(A)	\$0
5 to 9 dB(A)	\$1,000
10 to 14 dB(A)	\$2,000
15 dB(A) or greater	\$4,000

Source: IDOT Highway Traffic Noise Assessment Manual

New Alignment / Construction Date Consideration

Project is on new alignment OR the receptor existed prior to the original construction of the highway	Dollars Added to Base Value Cost per Benefited Receptor
No for both	\$0
Yes for either	\$5,000

Note: No single optional reasonableness factor shall be used to determine that a noise abatement measure is unreasonable.

Source: IDOT Highway Traffic Noise Assessment Manual

The IDOT noise reduction design goal is to achieve an 8 dB(A) traffic noise reduction at a minimum of one benefited receptor. If a noise abatement measure is feasible, achieves the cost-effective criterion, and achieves the IDOT noise reduction design goal, then the viewpoints of benefited receptors are solicited on the construction of the noise wall.

7.3 Noise Wall Analysis

TNM 2.5 was used to perform the noise wall feasibility and reasonability check for the 23 impacted receptors. When determining if an abatement measure is feasible and reasonable, the noise reductions achieved, number of residences benefited, total cost, and total cost per residence benefited are considered.

Noise barriers were found to be not constructible at R34, R35, R37, R38, R39, R40, and R41, due to existing building setbacks. Because noise barriers at these receptors are not constructible, they are not considered to be feasible, and no further noise abatement analysis occurred at these receptors.

Fourteen noise walls were evaluated for the remaining 16 impacted receptors, all of which are in locations where noise barriers could feasibly be constructed (this includes a shared barrier at R32, R33, and R36). All noise walls were modeled along the proposed right-of-way. The barriers studied (denoted with a “B” prefix) included the following:

- B1: Barrier for R1
- B2: Barrier for R6
- B3: Barrier for R7
- B4: Barrier for R11
- B5: Barrier for R12
- B6: Barrier for R13
- B7: Barrier for R15
- B8: Barrier for R17
- B9: Barrier for R18
- B10: Barrier for R20
- B11: Barrier for R21
- B12: Barrier for R30

- B13: Barrier for R32, R33, and R36
- B14: Barrier for R16 (including R14)

Six of the analyzed noise walls did not achieve the feasibility criterion of a 5 dB(A) reduction at an impacted receptor (B1, B2, B3, B6, B10, and B11). Five of the analyzed noise walls are considered feasible, as they are constructible and achieve the feasibility criterion of a 5 dB(A) reduction at an impacted receptor; however, these barriers did not achieve the Noise Reduction Design Goal of an 8 dB(A) reduction at a benefited receptor (B7, B8, B9, B12, and B13). Three of the fourteen noise barriers would be considered acoustically reasonable (B4, B5, and B14) as well as feasible, as they achieve the IDOT noise reduction design goal of at least an 8 dB(A) traffic noise reduction at one or more benefited receptor locations, in addition to the 5 dB(A) reduction at an impacted receptor. The barriers that do not meet the Noise Reduction Design Goal in Table 5 would not do so because of gaps in the barrier to maintain driveways and crossroads in that area.

The three noise barriers that were feasible and met the noise reduction design goal (B4, B5, B14) were evaluated for cost-effectiveness. Table 5 summarizes the results of the adjusted allowable cost per benefited receptor determination. Table 6 summarizes the results of the noise abatement evaluation.

**TABLE 5
ADJUSTED ALLOWABLE COST PER BENEFITED RECEPTOR SUMMARY**

Barrier / CNE	Benefited Receptors	Adjustment Factor	Adjusted Allowable Cost per Benefited Receptors
B4 / R11	7	\$0 - \$1,000	\$24,143
B5 / R12	1	\$0 - \$1,000	\$24,000
B14 / R16	17	\$0 - \$1,000	\$24,353

Note: No values are provided in the table where a noise wall does not meet noise reduction design goal/noise reduction criterion

**TABLE 6
NOISE WALL COST REASONABLENESS EVALUATION**

Barrier / CNE	Benefited Receptors	Length, ft.	Height, ft.	Total Noise Wall Cost¹	Actual Cost per Benefited Receptor	Adjusted Allowable Cost per Benefited Receptor
B4 / R11	7	13,130	18	\$5,908,500	\$844,071	\$24,143
B5 / R12	1	835	16	\$334,000	\$334,000	\$24,000
B14 / R16	17	2,414	14	\$844,900	\$49,700	\$24,353

¹ Based on the IDOT policy value of \$25 per square foot

The three barriers found to be feasible and acoustically reasonable (B4, B5, B14) were both found not to be cost effective, as the cost to build the noise barrier exceeded the allowable cost to construct the barrier, based on IDOT allowable costs per benefitted receptor. Barrier B4 had a total barrier cost of \$5,908,500 (\$844,071 per benefitted receptor), and the allowable total barrier cost was \$169,000 (\$24,143 per benefitted receptor). Barrier B5 had a total barrier cost of \$334,000

(\$334,000 for the one benefitted receptor), and the allowable total barrier cost was \$24,000 (\$24,000 for the one benefitted receptor). Barrier B14 had a total barrier cost of \$844,900 (\$49,700 per benefitted receptor), and the total allowable barrier cost of \$414,00 (\$24,353 per benefitted receptor).

Based on the evaluation, there are no noise walls that would be considered both feasible and reasonable; therefore, highway traffic noise abatement measures are not likely to be implemented for the IL Route 31 project, based on preliminary design.

8. COORDINATION WITH LOCAL OFFICIALS FOR UNDEVELOPED LANDS

Figure 2 depicts the land use within the project limits. Several undeveloped parcels of land exist along the corridor. For planning purposes, the Year 2040 Build scenario was analyzed to predict traffic noise levels on the undeveloped areas. The 66 dB(A) and 71 dB(A) noise contours in the undeveloped areas between Oak Crest and Thunderbird Lane are located approximately 150 feet and 50 feet, respectively, from the edge of pavement of the nearest planned traffic lane. The 66 dB(A) and 71 dB(A) noise contours in the undeveloped areas between Bank Drive and High Street are located approximately 75 feet and 30 feet, respectively, from the edge of pavement of the nearest planned traffic lane. Appendix A includes information that was sent to the local officials having jurisdiction over the undeveloped lands, and includes an exhibit depicting the approximate distances where the NAC is approached.

9. CONSTRUCTION NOISE

Trucks and machinery used for construction produce noise which may affect some land uses and activities during the construction period. Residents along the alignment will at some time experience perceptible construction noise from implementation of the project. To minimize or eliminate the effect of construction noise on these receptors, mitigation measures have been incorporated into the Illinois Department of Transportation's Standard Specifications for Road and Bridge Construction as Article 107.35.

10. CONCLUSION

Improvements to Illinois Route 31 in McHenry County, Illinois, are being proposed by the Illinois Department of Transportation. A noise analysis was performed for forty-two noise sensitive receptors within the project limits, including residences, a library, a recreation area, a park, a school, medical facilities, offices, and a restaurant.

Existing 2013 modeled noise levels range from 56 dB(A) to 78 dB(A). The projected No-Build 2040 traffic noise levels range from 57 dB(A) to 79 dB(A). The projected Build 2040 traffic noise levels range from 59 dB(A) to 74 dB(A). Under the proposed 2040 Build scenario, 23 receptor locations approach or exceed the FHWA NAC in the Build condition, and therefore warrant a noise abatement analysis.

Noise barriers were found to be not constructible at R34, R35, R37, R38, R39, R40, and R41, due to the minimal available right-of-way for barrier construction, and existing building setbacks. Fourteen noise walls were evaluated for the remaining 16 impacted receptors, all of which are in locations where noise barriers could feasibly be constructed. This includes shared barriers at select locations due to their close proximity. Eleven of the studied barriers would not provide adequate noise reductions to be considered feasible or reasonable. The three remaining noise walls that would provide adequate noise reductions were determined to not be economically reasonable, as the actual cost per benefitted receptor would exceed the allowable barrier cost per benefitted receptor as specified in the IDOT noise policy. For this reason, noise abatement measures are not proposed for the IL Route 31 project.

Appendix A



915 Harger Road, Suite 330
Oak Brook, IL 60523
Phone (630) 684-9100
Fax (630) 684-9120
Website: <http://huffnhuff.com>

CONVERSATION RECORD

Date: December 12, 2014

To: Scott Czaplicki, Illinois Department of Transportation District 1
John Clark and Sanjay Joshi, STV Incorporated

From: Jamie Bents, Huff & Huff, a subsidiary of GZA GeoEnvironmental, Inc.

Subject: Undeveloped Lands Coordination with Local Agencies
Traffic Noise Analysis
IL Route 31 Improvement Project
McHenry County, IL

The IL Route 31 noise contour mapping for undeveloped lands was discussed with representatives from the City of Prairie Grove (Jeannine Smith, Village Administrator), City of Crystal Lake (Elizabeth Maxwell, Planner), and the City of McHenry (Jon Schmitt, Director of Public Works) during the November 201, 2014 Community Advisory Group (CAG) meeting for the IL Route 31 project.

As a follow-up to these conversations, Illinois DOT will send (via e-mail) representatives of the City of Prairie Grove, the City of Crystal Lake, the City of McHenry, and McHenry County a copy of the noise contour map and a fact sheet summarizing how local officials can use the undeveloped lands analysis during site plan reviews for future land use development.

Traffic Noise Considerations - Undeveloped Lands

December 2014

As part of the preliminary engineering and environmental study (Phase I) for this proposed project, projected future traffic noise levels were evaluated for lands (either currently under your jurisdiction or land that may come under your jurisdiction) near the proposed roadway improvement. For your information, this study area includes undeveloped or agriculture land that is zoned for uses other than agriculture, or land that is planned for future development in a comprehensive land use plan. For developed lands, a traffic noise study has been completed for this project and will be included in the Combined Design Report which will be presented at the public hearing and transmitted to you upon completion of the Phase I study.

Attached for your information is an exhibit showing the predicted design year (2040) build traffic noise levels for these undeveloped lands identified along the project corridor. We hope this information will be useful to you in planning and permitting future development in your area. Although noise abatement is not warranted, we recommend that you carefully consider the future predicted noise levels to avoid potential issues of public concern over incompatible noise levels.

To help with your future planning and discernment regarding permitting decisions, we encourage you to obtain the Federal Highway Administration (FHWA) publication titled *Entering the Quiet Zone: Noise Compatible Land Use Planning* from their website at http://www.fhwa.dot.gov/environment/noise/noise_compatible_planning/federal_approach/land_use/quietezon.pdf.

For additional information regarding traffic noise, regulations and policy, noise analyses or noise abatement, visit the Department's website at <http://www.idot.illinois.gov/transportation-system/environment/index> under the Community tab.

The draft version of the Traffic Noise Technical Report is available on the project website at <http://www.ilroute31.com/projectdocuments.html>.

If you have any questions or concerns, please contact:

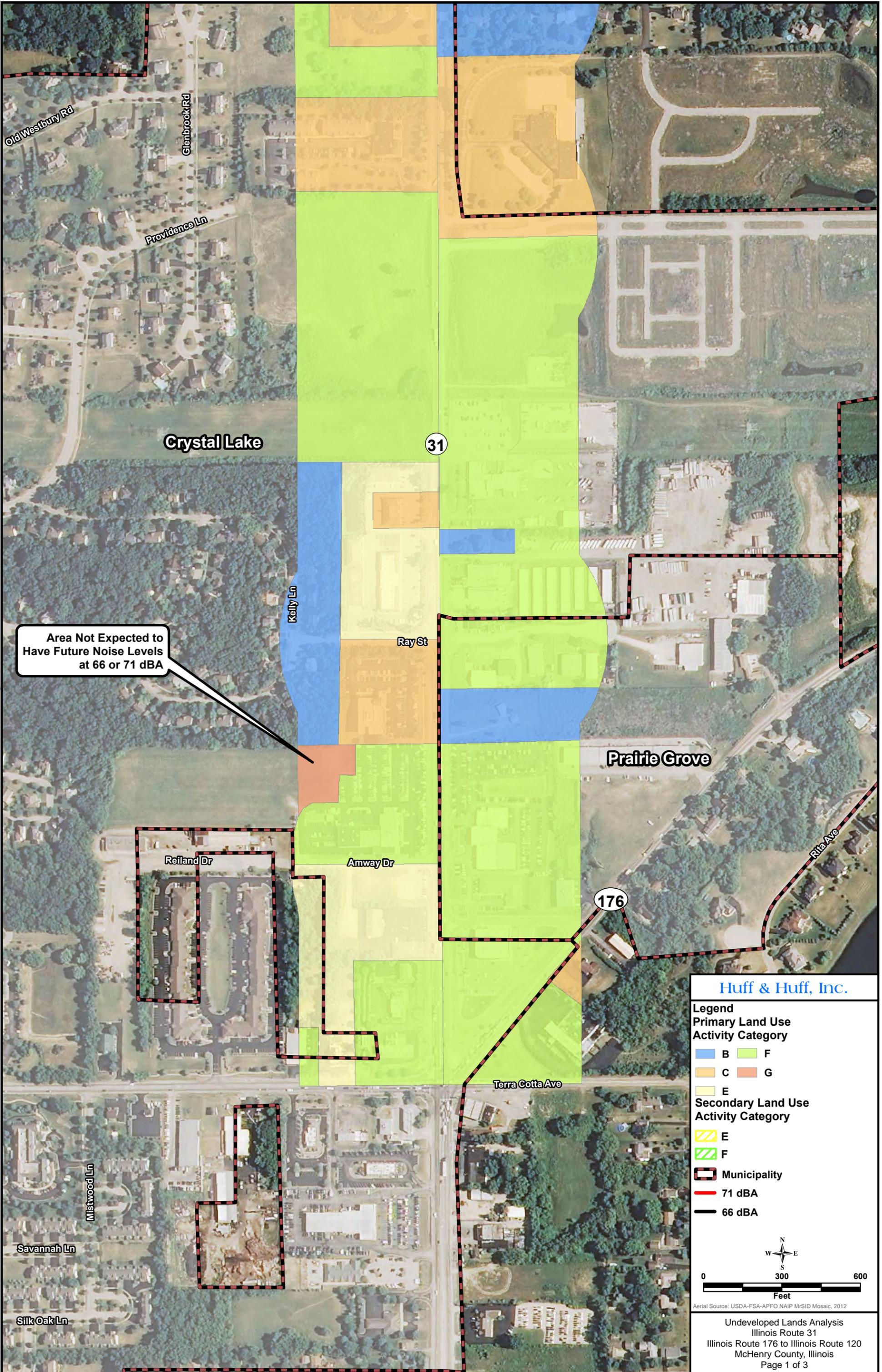
Illinois Department of Transportation
Bureau of Programming
201 W. Center Court
Schaumburg, IL 60196-1096
Attention: Scott Czaplicki, Project Manager
(847) 705-4107
scott.czaplicki@illinois.gov

Copies to: City of Crystal Lake, Village of Prairie Grove, City of McHenry,
Nunda Township, McHenry County



TABLE 1
NOISE ABATEMENT CRITERIA - HOURLY WEIGHTED SOUND LEVEL

Activity Category ¹	L _{eq} (h)	Evaluation Location	Activity Description
A	57	Exterior	Lands on which serenity and quiet are of extraordinary significance and serve an important public need and where the preservation of those qualities is essential if the area is to continue to serve its intended purpose.
B	67	Exterior	Residential.
C	67	Exterior	Active sport areas, amphitheatres, auditoriums, campgrounds, cemeteries, day care centers, hospitals, libraries, medical facilities, parks, picnic areas, places of worship, playgrounds, public meeting rooms, public or nonprofit institutional structures, radio studios, recording studios, recreation areas, Section 4(f) sites, schools, television studios, trails and trail crossings.
D	52	Interior	Auditoriums, day care centers, hospitals, libraries, medical facilities, places of worship, public meeting rooms, public or nonprofit institutional structures, radio studios, recording studios, schools, and television studios.
E	72	Exterior	Hotels, motels, offices, restaurants/bars, and other developed lands, properties or activities not included in A-D or F.
F	---	---	Agriculture, airports, bus yards, emergency services, industrial, logging, maintenance facilities, manufacturing, mining, rail yards, retail facilities, shipyards, utilities (water resources, water treatment, electrical), and warehousing.
G	---	---	Undeveloped lands that are not permitted.



Area Not Expected to Have Future Noise Levels at 66 or 71 dBA

Huff & Huff, Inc.

Legend

Primary Land Use Activity Category

- B
- C
- E
- F
- G

Secondary Land Use Activity Category

- ▨ E
- ▨ F

Municipality

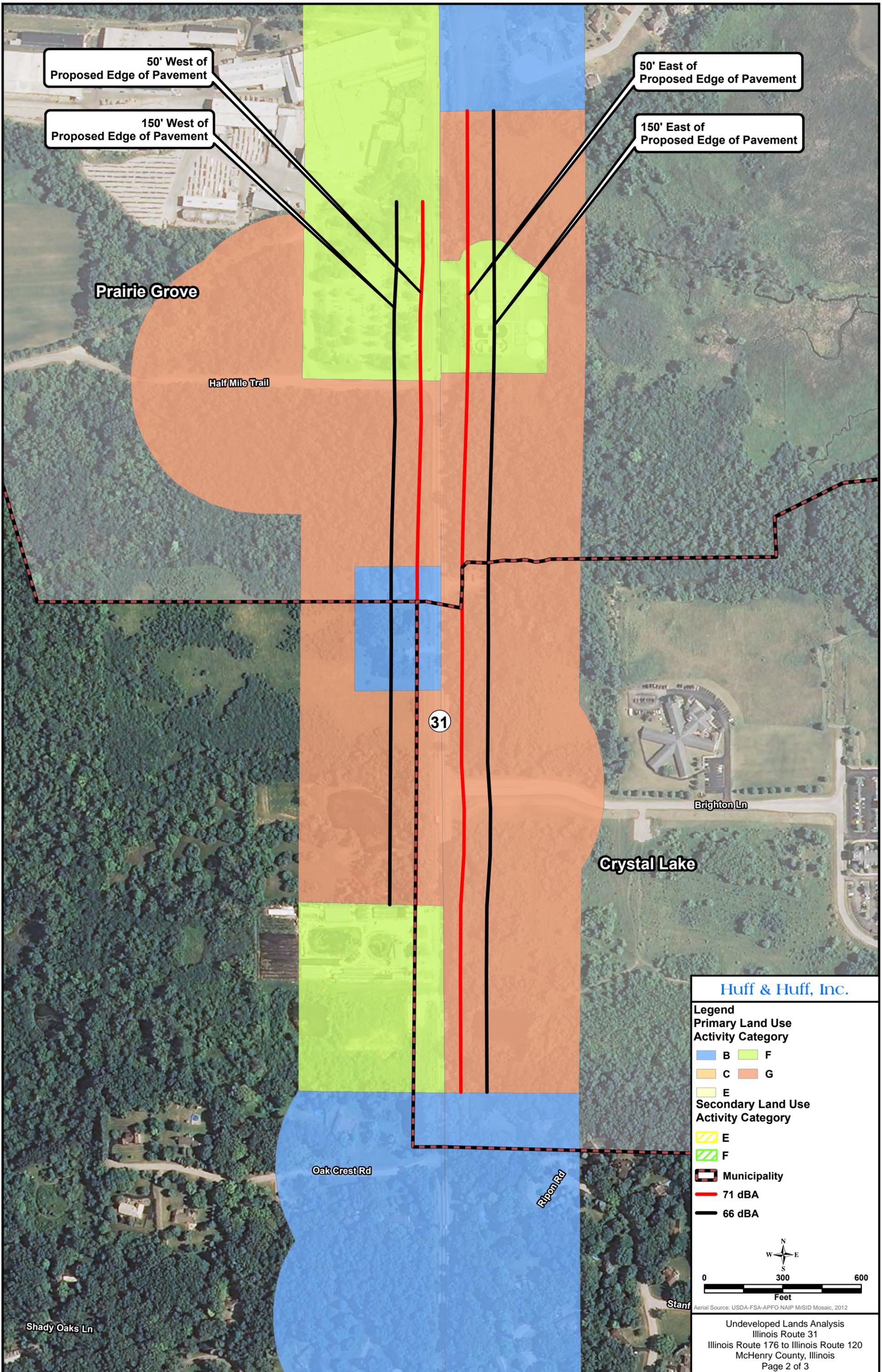
- 71 dBA
- 66 dBA

Scale

0 300 600
Feet

Aerial Source: USDA-FSA-APFO NAIP MrSID Mosaic, 2012

Undeveloped Lands Analysis
 Illinois Route 31
 Illinois Route 176 to Illinois Route 120
 McHenry County, Illinois
 Page 1 of 3



Huff & Huff, Inc.

Legend

Primary Land Use Activity Category

 B	 F
 C	 G
 E	

Secondary Land Use Activity Category

 E	 F
--	---

Municipality

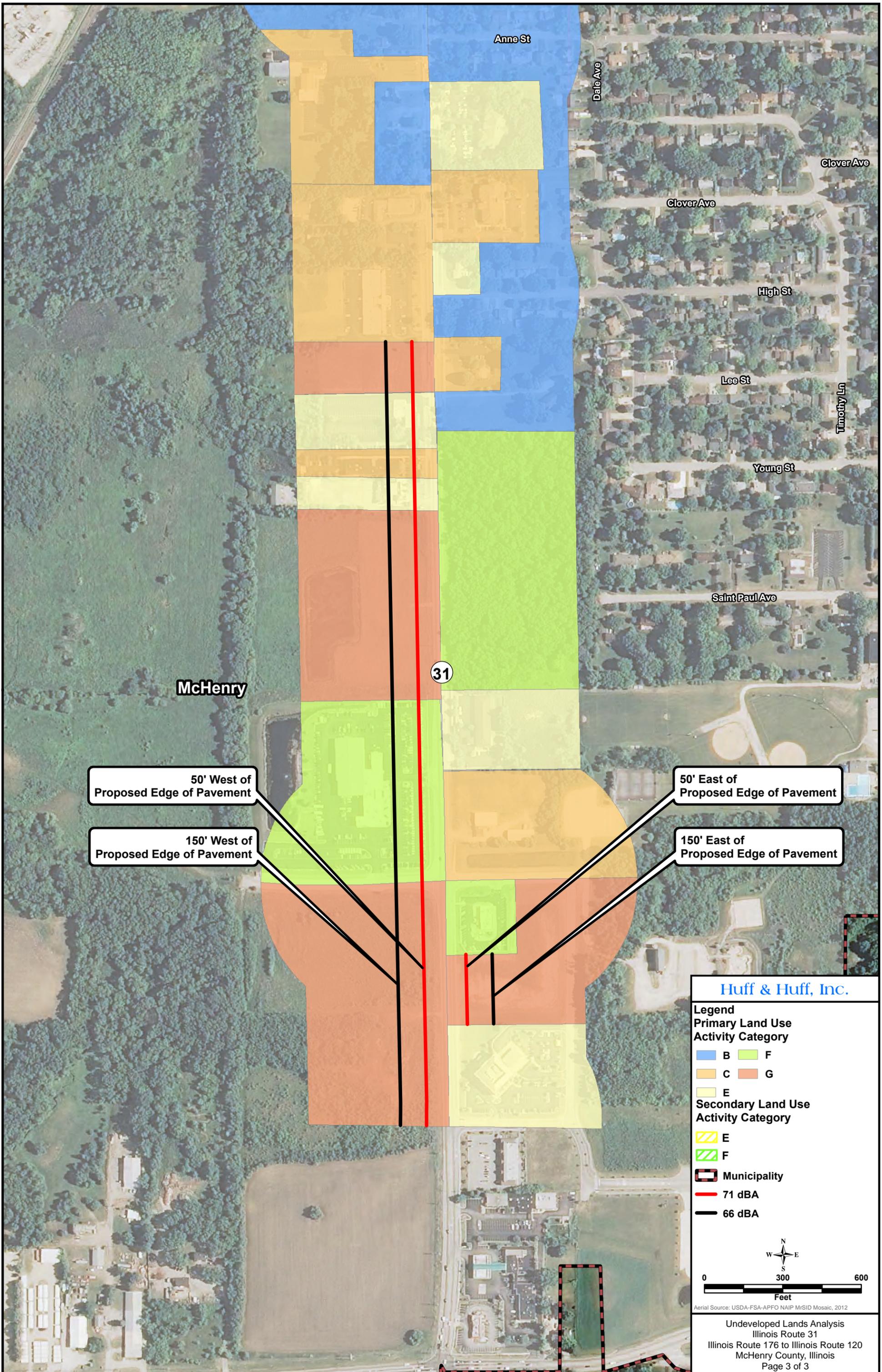
71 dBA

66 dBA

0 300 600
Feet

Aerial Source: USDA-FSA-APFO NAIP MrSID Mosaic, 2012

Undeveloped Lands Analysis
Illinois Route 31
Illinois Route 176 to Illinois Route 120
McHenry County, Illinois
Page 2 of 3



50' West of Proposed Edge of Pavement

150' West of Proposed Edge of Pavement

50' East of Proposed Edge of Pavement

150' East of Proposed Edge of Pavement

Huff & Huff, Inc.

Legend

Primary Land Use Activity Category

- B
- C
- E
- F
- G

Secondary Land Use Activity Category

- E
- F

Municipality

71 dBA

66 dBA

0 300 600
Feet

Aerial Source: USDA-FSA-APFO NAIP MrSID Mosaic, 2012

APPENDIX K

Public Hearing Materials

(Errata)

IL 31
IL 176 to IL 120
McHenry County



PUBLIC HEARING SUMMARY

McHenry County College Shah Center
4100 W. Shamrock Lane
McHenry, Illinois 60050
February 8, 2017

A public hearing for the proposed improvement of IL 31 from IL 176 to IL 120 in McHenry County was held on Wednesday, February 8, 2017 at the Shah Center in the City of McHenry from 4:00 p.m. to 7:00 p.m. The hearing was an open house format with exhibits, an audio-visual presentation, and a public forum at 6:00 p.m. The hearing was attended by 169 people. 92 comment were submitted at the during the 30-day comment period. Four people made statements during the public forum. There was no organized opposition, but there was a group of several residents from the subdivision southeast of the IL 31 and Ames Road subdivision who gathered in a large group to express their concerns regarding the project.

The following elected officials were in attendance:

- Office of State Representative Steve Reick: Carrie Andre (representative)
- McHenry County Board: Chairman, Joe Gottemoller
- Nunda Township: Highway Commissioner Mike Lesperance

The following municipalities and agencies were represented:

- City of McHenry
- City of Crystal Lake
- Village of Prairie Grove
- McHenry County DOT
- Nunda Township Highway Department
- Metra
- Pace
- McHenry County Conservation District
- FHWA

Other organizations/businesses represented include:

- McHenry Chamber of Commerce
- McHenry County Economic Development Corporation
- The Land Conservancy of McHenry County
- Environmental Defenders of McHenry County
- McHenry Public Library
- Oak Grove Homeowners Association
- Deering Oaks Association
- Buss Ford
- Gary Lang Auto
- Terra Cotta Realty / TC Industries
- Centegra
- Adam's Steel
- Althoff Industries
- Structure Properties LLC / Direct Steel
- McHenry Savings Bank
- Approximately 40 business owners / representative from project area

The following media was represented:

- None

The general themes of comments included:

- Access concerns
- Support project
- Property impacts
- Tree impacts / Traffic noise impacts
- Need for additional traffic signals (primarily Ames Road)

Certificate of the Publisher

Northwest Herald

Description: IDOT ROUTE 31

RECEIVED

FEB 21 2017

STV INCORPORATED

STV INCORPORATED
200 W. MONROE STREET - SUITE 1
CHICAGO IL 60606-5015

Shaw Media certifies that it is the publisher of the Northwest Herald. The Northwest Herald is a secular newspaper, has been continuously published daily for more than fifty (50) weeks prior to the first publication of the attached notice, is published in the City of Crystal Lake, County of McHenry, State of Illinois, is of general circulation throughout that county and surrounding area, and is a newspaper as defined by 715 ILCS 5/5.

A notice, a true copy of which is attached, was published 2 time(s) in the Northwest Herald, namely one time per week for two successive week(s). Publication of the notice was made in the newspaper, dated and published on 01/18/2017 02/01/2017

This notice was also placed on a statewide public notice website as required by 5 ILCS 5/2.1.

In witness, Shaw Media has signed this certificate by John Rung, its publisher, at Crystal Lake, Illinois, on 1st day of February, A.D. 2017

Shaw Media By:



John Rung, Publisher

Account Number 10052006

Amount \$776.64



Illinois Department of Transportation
PUBLIC HEARING

The Illinois Department of Transportation (Department) cordially invites you to attend an open house public hearing concerning the preliminary engineering and environmental study (Phase I) for the improvement of IL 31 from IL 170 to IL 120 in McHenry County. The scope of work for this improvement is anticipated to consist of roadway widening and reconstruction to provide two lanes in each direction separated by a median, intersection and drainage improvements, and pedestrian and bicycle improvements. The details of the public hearing are as follows:

Date: Wednesday, February 8, 2017
Time: 4:00 p.m. to 7:00 p.m.
Location: McHenry County College Shah Center
4100 W. Shamrock Lane
McHenry, IL 60050

Purpose of the hearing:

- To present the Environmental Assessment (EA)
- To present the preferred alternative
- To obtain public input

Interested persons may attend anytime between 4:00 p.m. and 7:00 p.m. Exhibits will be on display and an audio-visual presentation will be shown continuously during the hearing. Project team members will be present to discuss the project and answer questions. A public forum will begin at 6:00 p.m., where the public will be invited to make a statement to a court reporter. Right-of-way acquisition and relocation assistance information will be available.

The EA has been completed and will be presented at the public hearing. The EA describes the purpose and need of the proposed project, alternatives considered, the recommended preferred alternative, anticipated environmental impacts, project benefits, and potential mitigation measures. It will be available on January 18, 2017 for public review and comment on the project website at www.idot.illinois.gov/projects/i131, the Prairie Grove Village Hall, McHenry Public Library, and the Department's District One office in Schaumburg. A complete list of these locations can be found on the project website. Comments will be accepted through March 10, 2017.

The improvement is anticipated to impact 1.53 acres of wetlands, and encroach into the floodplain at two locations for the replacement of the structures carrying IL 31 over Sleepy Hollow Creek and the Tributary to the Fox River. The Department will process a permit for construction in the regulated floodway at these two locations.

This hearing will be accessible to people with disabilities. Anyone needing special assistance should contact Scott Czaplicki, Project Manager at (847) 705-4107. Persons planning to attend who will need a sign language interpreter or other similar accommodations should notify the Department's TTY/TTD number (800) 526-0844/or 711; TTY users (Spanish) (800) 501-0884/or 711; and Telebraille (877) 526-6670 at least five (5) days prior to the meeting.

All correspondence regarding this project should be sent to:

Illinois Department of Transportation
201 W. Center Court
Schaumburg, IL 60196-1096
Attn: Bureau of Programming
Scott Czaplicki

or submitted through the project website: www.idot.illinois.gov/projects/i131

Sign-In Sheet

Public Hearing
 February 8, 2017



	Name	Representing	Telephone Number	E-Mail and/or Mailing Address
1	Joel & Kathleen Zerk	Butch's ^{Auto} Service		
2	Jim McNally	Baxter & Woodman		
3	Lisa Haderlein	The Land Conservancy		
4	Brian KILINSKI			
5	STAN VORGIS	LAND OWNERS ON 31 McHenry		
6	Chris Rodolfo	Self		
7	Doug Marynde	Arnoldson Parks & Sp		
8	BRIAN REICHERT	Self		
9	Tina Napolitano	Self		
10	Carmela Yociu	AICA		

Sign-In Sheet

Public Hearing
 February 8, 2017



Name		Representing	Telephone Number	E-Mail and/or Mailing Address
1	Matt Miller	IHWTS		
2	STEVE CUSA	FATHER-IN-LAW		
3	BILL KENNELLY	JA FRATE		
4	Mike Harper			
5	TODD TROCKI	KATHEEN'S BEARZ		
6	Jun Powell	Self		
7	LINDA + RON BYKOWSKI	SELF		
8	Joe Gottemolle	County Board		
9	Rosemary Smerk	STRUCTURE PROPERTY		
10	Alice Runkik	Self		

Sign-In Sheet

Public Hearing
 February 8, 2017



Name		Representing	Telephone Number	E-Mail and/or Mailing Address
1	Bill Westermann	SELF		
2	Karen Nylén Swidorski	Self		
3	GREGORY DIONNE	"		
4	Sue Phelps	SELF		
5	Christine Kustra	Environmental Defenders of MC		
6	ED RADWANSKI	PRAIRIE GROVE		
7	ERICA RILINSKI	McHenry		
8	Matt Schramm	OMEGA		
9	DAN ARVIDSON	ARVIDSON PARS		
10	Jon Meyer	McHenry		

Sign-In Sheet

Public Hearing
 February 8, 2017



	Name	Representing	Telephone Number	E-Mail and/or Mailing Address		
1	Kevin Meyer	Oak Grove Subdivision	[REDACTED]	[REDACTED]		
2	William Busse	CAG Member				
3	Cynthia Skandera	Self				
4	Nancy Schietze	Environmental Defenders				
5	Gene Potempa	The Green House of Crystal Lake				
6	Bill Swenson	IL REALTY				
7	G. ROSENBERG	SELF				
8	Bill Jones	Self				
9	Ed Salisbury	2016 SR 31				
10	Ted Hamilton	Self				

Sign-In Sheet

Public Hearing
 February 8, 2017



	Name	Representing	Telephone Number	E-Mail and/or Mailing Address
1	Ed Buss	Buss Ford		
2	Bill Mullins	Oak Grove HOA		
3	Tim Shoemaker	RTM		
4	Sunita Parikh			
5	Jan Nela	(self)		
6	JEFF DANCA			
7	Kathy Marinangeli	McHenry Savings Bank		
8	James Speake			
9	TODD Wheeler	myself		
10	MAYENA & Bill Smith	Selves		

Sign-In Sheet

Public Hearing
 February 8, 2017



	Name	Representing	Telephone Number	E-Mail and/or Mailing Address		
1	Keith Powell	Resident	[REDACTED]	[REDACTED]		
2	John Dargen	Defender				
3	Denise Strom	MCDONALD'S				
4	Ivek Johnson	SELF 803 W. FRONT ST				
5	Nancy A. Jober	self				
6	Douglas Marten	City of McHenry				
7	Alan D. Simon	Resident				
8	Curt Beltran	RESIDENT				
9	Wayne Jett	Resident				
10	JEFF SIMON	FH PASCHEN				

Sign-In Sheet

Public Hearing
 February 8, 2017



Name		Representing	Telephone Number	E-Mail and/or Mailing Address
1	BOB ZITEK	FH PASCHEN	7	[REDACTED]
2	Jon Schmitt	City of McHenry	8	
3	Lari McConville	Envi Kependis McHenry	8	
4	Erica Carlsson	Digital Pix	8	
5	MIKE WOLFF	SELF	8	
6	Jeff Pittman	The Chapel	2	
7	Rick Mack	METRA	3	
8	ROGER MIRS	SELF	8	
9	BERT IRLINGER JR	Second Amendment Sports	8	
10	STEVE CARLUTHERS	CRYSTAL LAKE	8	

Sign-In Sheet

Public Hearing
 February 8, 2017



Name		Representing	Telephone Number	E-Mail and/or Mailing Address
1	GONS PROPERTIES	Self	[REDACTED]	[REDACTED]
2	SAM SHAM	SELF		
3	Brian Smith	self		
4	Ron Small	SELF		
5	DARREN MARSH	ED'S RENTAL		
6	DR. PERTRAM IERLINGER	Ierlingers		
7	Eric Konieczski	MUNDA ROAD		
8	Mike Lesperance	Munda Road		
9	Tim Wiegman	self		
10	Susan Bonucki	MCDOT		

ct.
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Sign-In Sheet

Public Hearing
 February 8, 2017



	Name	Representing	Telephone Number	E-Mail and/or Mailing Address
1	William Abd		8	
2	Mark Olson	Self	8	
3	Jim Scholtz	McHenry Public Library		
4	PERRY MOY	PLUM GARDEN REST		
5	Michael Chambers	Adams Steel	8	
6	Rick Bruski	ALTHOFF INC		
7	TROY STRANGE	CITY OF MCHENRY PUBLIC WORKS		
8	JOHN KOSS	OAK GROVE SUBDIVISION	8	
9	Carrie André	State Rep. Steve Reick	8	
10	Patricia Szapik			

Sign-In Sheet

Public Hearing
 February 8, 2017



	Name	Representing	Telephone Number	E-Mail and/or Mailing Address
1	WAYNE KURCHINA	IL REALTY		
2	Gary Overbay	SELF		
3	Scott Henning S	MCDOT		
4	Pick Hickey	CHAS. HERDRICH & SON, INC		
5	Sharon & Paul Christensen			
6	Ryan McGuire	Digital PIX		
7	DAVID W. GELWICKS	SELF		
8	Perry D. Maglieri	FIRST Baptist Church 509 Pratt		
9	Jules & Pat Waldman	Self		
10	Max Crum	Steffens Law		

Sign-In Sheet

Public Hearing
 February 8, 2017



Name		Representing	Telephone Number	E-Mail and/or Mailing Address
1	John Sherrill	IDOT		
2	VAL SILER	MCCD		
3	Jim Mowery			
4	RUBEN RAQUEL	FBC, McHenry		
5	JON T. ARMSTRONG	ME		
6	JAMIE & DAVID VASKIN	US		
7	Jim Hicks	RESIDENT EDGEWOOD RD REGION		
8	Norm Nepermann	FARMER		
9	Sam Rummel	MCCD		
10	Julie Hoberg	resident.		



60012

Sign-In Sheet

Public Hearing
 February 8, 2017



Name		Representing	Telephone Number	E-Mail and/or Mailing Address
1	FREDERICK DOSE	DEERING OAKS ASSOCIATION	8	[REDACTED]
2	Jim Haisler	Heartland REACTOR organization	8	
3	CHAD PIEPER	HR GREEN	E	
4	MARK & FRAN TROPINSKI		E	
5	KATHLEEN MARTINEZ	TERRA COTTA REALTY CO.	S	
6	FRANCES FLAHERTY		8	
7	MARTIN & NANCY VAZZANO	prop owners themselves		
8	STEVE SCHWARZ	HR GREEN		
9	Dirk Hewell			
10	AMY PETERS	MCCO	E	

Sign-In Sheet

Public Hearing
 February 8, 2017



	Name	Representing	Telephone Number	E-Mail and/or Mailing Address	
1	Gary Lary	SM	[REDACTED]		
2	Dan Shepard	PPCD OAKS			
3	Randy Schietzel	Land Conservancy of McHenry Co			COM
4	Kristine Hall	DR. Robert Hall			COM
5	Jim Bishop				
6	TIM DAUM	self			
7	TOM ZAWLEK	Legislative + Wine Properties			
8	ROBIN HEIMBRICHS				
9	Margaret Barranco	self			COM
10	JACQUEMINO				

Sign-In Sheet

Public Hearing
 February 8, 2017



	Name	Representing	Telephone Number	E-Mail and/or Mailing Address
1	ERNESTO BAGGIO	913-917,909	8	
2	MEREDITH SARKEES	self	8	
3	ERNEST J. VARGA	MCDOT	8	
4	Karen S J Varga	self		
5	John Swick	STRUCTURE PROPERTIES		
6	Jack Melhuish	SELF	8	
7	Angu Frey	self	8	
8	Jamie Snider	Elstrom Hall		
9	Vic Santi	City of Mt.		
10	c. Shuck	self		Sasei 5101 @ yanson, Cal

Sign-In Sheet

Public Hearing
 February 8, 2017



Name		Representing	Telephone Number	E-Mail and/or Mailing Address
1	ED ADRESZKO	SELF	[REDACTED]	[REDACTED]
2	Pam Ciupak	MCEDC		
3	JAMES STAWORSKI	SELF		
4	Kelly Weaver	ALCA		
5	Mike Rogulic	McHenry Heating		
6	Emily Kalal	Self Oak Grove sub.		
7	Craig Wilcox	Self		
8	Randy Jeschik	Self		
9	Herb Burnap	Alliance Bible Church		
10	Kay Betts	McHenry Chamber		

Sign-In Sheet

Public Hearing
 February 8, 2017



	Name	Representing	Telephone Number	E-Mail and/or Mailing Address		
1	Larry Kaplan	home owner	[REDACTED]	[REDACTED]		
2	MARY KOSS	HOA OAK GROVE				
3	Rhonda Nering	Oak Grove				
4	Michael Walker	Glen Rd.				
5	Carol [unclear]	Home Owner				
6	Anthony Jones	Home owner				
7	Christine Campo	Fantasy Factory				
8	[unclear]	Self				
9	Mary L. [unclear]	Pace				
10	Ben Redding	MCDOT				[unclear]

Sign-In Sheet

Public Hearing
 February 8, 2017



	Name	Representing	Telephone Number	E-Mail and/or Mailing Address
1	Matt Rogulic	McHenry Excavating		
2	Kantha Stawianski			
3	Scott Gles	Community Home Inspections		
4	Rich Carter			
5	Nanatte Jones			
6	Bill/Bonnie Merriman			
7	Philip Baggio	B & B Auto		
8	Leonard Kerny			
9	RALPH HAYWARD	SOUTHGATE BUS CENTER		
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PUBLIC HEARING
ILLINOIS DEPARTMENT OF TRANSPORTATION
ILLINOIS ROUTE 31, ROUTE 176 TO ROUTE 120
Public Comments

McHenry County College - Shah Center
4100 West Shamrock Lane
McHenry, Illinois 60654

Wednesday, February 8, 2017

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STATEMENT OF

BARB DWORAK

Organization: McHenry County College - Shah Center

Home Address:

714 Nashua Court

Crystal Lake, Illinois 60012

(815) 814-2071

BarbDworak@Gmail.com

I thought the video was a good explanation of the impacts and the different agencies that are included, and all of the statistics that they provided. I know it's a pain to go through changes like that, but I totally support it and feel that it's necessary.

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STATEMENT OF

GARY OVERBAY

1625 Flagstone Drive

Crystal Lake, Illinois 60014

I'm curious as to the need for the wide median option over the entire length of the improvement. Based on development patterns anticipated over the next 20 years, it would be surprising if dual left turn lanes are required anywhere, with the exception of Route 176 and Bull Valley Road.

Please provide traffic information for crossroads to confirm that dual left turn lanes would be necessary.

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STATEMENT OF
PATRICIA KNAPIK
3711 Freund Avenue
McHenry, Illinois 60050

What I would like is a copy of that section where my property is at. In fact, it's that first sheet that's over there.

Please send me a copy of Sheet 14 of 15. It's the North Section, North of Bull Valley Road to Route 120. Sheet 14 of 15.

My complaint is that the property on Main Street and Route 31, they're taking away five parking spaces, which is not good, because it's a business that faces Main Street and there's no other parking. Just four in the front of the building, and that's all. Where right next door is a business, they're going to be taking those spaces up also. So we're going to be short on parking. That's my complaint.

1 STATEMENT OF
2 MICHAEL WOLFF
3 Organization: Myself
4 321 Neville Drive
5 Grayslake, Illinois 60030
6 (847) 223-2479
7 M_Wolff@LCGC.com
8

9 The address that I'm concerned about is
10 2207 South Illinois Route 31. And it's a farm on the
11 west side of 31. And I have to make sure that I'm
12 going to have a wide driveway to get semi trucks in
13 there for the farmer to get the grain out.

14 And then I also have a farm entrance to the
15 south of the house and the driveway, and I have to
16 maintain that farm entrance. And it's got to be
17 wider than what it is now, because the trucks have to
18 go into the other lane in order to make the turn.

19 If the entrances are wider, they can get in
20 there from the lane. They'll be able to turn in off
21 of the southbound lane. Because it doesn't look like
22 there's going to be a cut in there so they can go
23 left. So I'd like to have right and left at those
24 entranceways, especially the driveway.

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STATEMENT OF

ERICA CARLSON AND RYAN McGUIRE

Organization: Digital Pix

1811 South Illinois Route 31

McHenry, Illinois 60050

(815) 363-2800

Erica@DPCPix.com

ERICA: So it looks like the, I guess, easement in the construction line comes all the way up to our dock. We have a dock that holds 53-foot trucks right now, and it's right here. And so we have an issue because there's no way they're going to be able to get in and out during the construction process. That's going to be a huge problem.

RYAN: Also our front door is really close to that also.

ERICA: Also our front door is right in the middle of the building, so that's going to be a little bit difficult.

We have a septic field in the back, so it's not like we can really move a lot of our parking back, so we're going to lose half of our parking.

What are our other concerns?

1 RYAN: I mean, just the biggest concern is the
2 trucks coming and going. Deliveries and pickups.
3 And our employees' places to park. Access to the
4 parking lot.

5 ERICA: Because, like, here there's a red X.

6 RYAN: They're going to remove that and move it
7 here. Which is a positive. But we're losing half
8 our parking lot.

9 ERICA: But there's no way the trucks can get in
10 there.

11 RYAN: No.

12 ERICA: Okay. That's all we have to say.

13 RYAN: That's our biggest issue.

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STATEMENT OF

DR. BERTRAM IRSLINGER, SR.

409 Front Street

McHenry, Illinois 60050

DRBPI@Mc.net

This is in reference to PIN
Number 09-35-301-027.

It's an existing parking lot for the
AA Club. It houses 30 to 40 cars a day. I need to
have access to the north from the driveway. I'm
asking them to shorten the median so that the Alano
Club can exit to the north, or make a left-hand turn,
out of that parking lot.

I also need the curb cut to be a minimum of
24 feet.

Now the other property, 09-35-301-012, the
traffic pattern is around the building. There's
going to be temporary work done on the northeast
corner.

During that temporary work, I have to be
guaranteed that my patients can exit around my
building and exit onto Route 31. And that's it.

1 STATEMENT OF
2 BERT IRSLINGER, JR.
3 Second Amendment Sports
4 3705 West Elm Street
5 McHenry, Illinois 60050
6

7 My name is Bert Irslinger, Jr. I'm at
8 Second Amendment Sports at 3705 West Elm Street
9 in McHenry. That would be property PIN Number
10 09-26-380-001. As well as PIN Number 09-26-380-015.

11 I have a few concerns with the project,
12 specifically on PIN Number 001. That is the property
13 south of Route 120, but east of Third Street. And we
14 have already gone through the discussion of left
15 turns from Third Street to 120. And they had lowered
16 that median to allow the left turns.

17 The proposed plan again removes the left
18 turns from Third Street to 120, but still allows the
19 left turns from 120 to Third Street. The issue for
20 me is loss of business as an inability to get my
21 customers back to southbound 31.

22 So I'll have a real traffic flow problem
23 for any of my customers based out of, whether it be
24 southern McHenry, Crystal Lake, Fox River Grove,

1 Cary, Barrington, anywhere south of us. I won't be
2 able to get those customers back to southbound 31
3 without either routing them through Green Street, or
4 routing them through the neighborhood.

5 And with the no left turns allowed at Main
6 Street, as well as a few others, I would have to
7 route them all the way through the neighborhood down
8 Center Street, all the way to Lillian Street. So it
9 would be a very, very difficult routing for my
10 customers. I believe it would be a serious loss of
11 business.

12 As far as the property at -- ending in 015,
13 we've had discussions with IDOT regarding the take on
14 that property. And we would request, number one,
15 we're in for permitting on an accessory building that
16 we need for storage on our building, which would be
17 partially in that take, as well as that take is our
18 rear parking lot for our employees. So we're losing
19 parking spots in the front of the building, plus it's
20 proposed to lose all of our parking spots behind the
21 building.

22 So we would request that that take get
23 shrunk to leave the rear parking lot and still allow
24 for the accessory building. But we would be open to

1 a conversation regarding the rest of the property, if
2 it's needed for the drainage.

3 Oh, and then just lastly, I would also be
4 interested to see what the time frame is on the
5 construction, the storm construction directly on 120.
6 That's a concern to me, obviously, for traffic flow
7 and access and egress to my building. So I would
8 want to know what that time frame is going to look
9 like so I know sort of what the loss of business and
10 what a delay that would bring to our customers, just
11 from getting in and out of our building while that's
12 under construction.

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STATEMENT OF
MEREDITH SARKEES
4802 Wyoming Way
Crystal Lake, Illinois 60012
(815) 444-0692

MEREDITH: I'm concerned -- we were looking at the map, and it looks like they're eliminating the right-hand turn lane onto Drake Drive.

MS. CYNTHIA SKANDERA: Which is Station 136. He said to say --

MEREDITH: I thought 163.

MS. CYNTHIA SKANDERA: You're right.
Station 163.

MEREDITH: At Station 163, there is currently a right-hand turn lane from 31 into Drake Drive as you're heading north. And from the plan, and what we've been told, they're eliminating that. And we think that that's a serious safety concern.

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STATEMENT OF

JAMIE LEE

1001 Cherry Valley Road
Bull Valley, Illinois 60050
(224) 242-2795
Jamie.M.Lee@ICloud.com

I am the partner, own the business, at 616, 618, and 620 South Illinois Route -- or State Route 31, McHenry, 60050.

Direct concerns are access to my building. We cannot currently make a left turn out of the building. Nor coming southbound, you cannot make a left-hand turn into my building.

Currently, there are U-turns on both Mercy Medical and at 31 and Bull Valley Road causing safety concerns, as one.

You cannot make a U-turn over 21 feet, which is causing multiple vehicles that are not able to make turns to have any access to my building.

More concerning is the U-turn at Mercy Medical Center is dangerous. The U-turn is supposed to have the right of way, and that's never the case. You do need to go into the Mercy Medical Building to

1 then wait for oncoming traffic to then make this
2 U-turn.

3 Large vehicles can't do this. Our
4 buildings are currently 35,000 square feet total and
5 have the ability to house 24 different tenants.
6 There's 24 different units. The problem right now
7 has caused the bank next to us to sell their
8 business. There's also a McDonald's that moved about
9 a block north in order to avoid this median that was
10 placed. And then we've lost large tenants. So we
11 had a 20-year tenant, the VA Building, move out due
12 to the issue.

13 So really what we're looking for is some
14 sort of access points in order to make a left-hand
15 turn coming southbound and/or take a left-hand turn
16 out of our building to head southbound.

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1 STATEMENT OF
2 DAVID SOSKIN
3 P.O. Box 1297
4 McHenry, Illinois 60051
5 (224) 622-3238
6

7 My concern is for safety. You know, the
8 U-turns that are both at Bull Valley and 31 and
9 the U-turns that are made at Mercy Drive and 31.
10 Both are limiting by the vehicle size. It says
11 nothing over 21 feet. I have a truck and a 45-foot
12 trailer, so I'm not able to make a turn there. Which
13 means, how far does somebody have to drive -- and I'm
14 just 45 feet. What if it's a 60-foot semi? How far
15 do they have to drive out of their way to make a
16 delivery to, you know, I mean, 24 businesses? They
17 need deliveries. And a lot of times, semis deliver
18 stuff. Or box trucks. Or UPS trucks. Or a FedEx
19 truck. So how large are those? Over 21 feet.
20 Because my truck is 20 feet, you know, without a
21 trailer.

22 So it's dangerous because not everybody is
23 going to not make a -- if you're 25 feet or 26 feet
24 or 28 feet, they're probably going to make a turn

1 anyway, and it's dangerous. It blocks the street.

2 If I make a U-turn in my own minivan, when
3 I make a U-turn on 31 by Mercy Drive, I have to block
4 Mercy Drive, okay? And then wait for oncoming
5 traffic to go, before I can go, while I'm literally,
6 like, blocking a street. That's dangerous.

7 My other concerns are, you know, of a
8 financial nature to both the county, the village,
9 their tax dollars, because a flourishing complex was
10 sold for cheap. And many businesses left and went
11 out of business, including a 20-year tenant, because
12 you can't make a left turn going southbound into the
13 complex.

14 So the concern lies with -- I mean, I have
15 physically passed up the building. Multiple people
16 have. My own friends, you know, get lost and don't
17 want to come because they come from the other way.
18 It's confusing. They don't see it, you know?

19 I think that kind of sums it up.

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1 lanes of traffic. The school buses will not be able
2 to get out.

3 The widening project involves a bike lane,
4 a multi-use lane, 10 feet wide on one side, a
5 sidewalk on the other side of the road. Nobody walks
6 on Route 31. There's no place to walk to.

7 Bicyclists ride in the street. They're
8 prohibited by law from riding on sidewalks. They
9 have right-of-way in the street. And all the
10 bicyclists that come through our neighborhood always
11 ride on the street. They won't ride on the sidewalk.

12 So you don't need two of them. It just
13 adds to the width. It takes more of my property
14 away.

15 There's a berm in my backyard along 31
16 which is supposedly going to be cut down, taken out,
17 all the trees taken out, and any noise abatement that
18 that served is going to go away. It's noisy enough
19 now. My house shakes when trucks go down Route 31.
20 Add two more lanes of traffic, it will be untenable.
21 The noise will be ridiculous. And there's no
22 planning for a noise abatement. There's no planning
23 for a noise wall to prevent this kind of thing.
24 There's only a couple houses that are affected by

1 this, and nobody's doing anything for us.

2 I think it's great that Terra Cotta's a
3 lovely industry. I'm sure they pay their share of
4 property taxes to the county and the State. But, you
5 know what? They're a Canadian company. And I'm an
6 American citizen. And I own property. And I don't
7 like that they're flexing their muscle and deciding
8 what side of the road the property's going to be
9 taken away from so that they don't have their
10 property taken away.

11 That's pretty much it. That's what I have
12 to say.

13 The planning -- we were not involved in the
14 planning. And this is a travesty. There is no
15 barrier, whatsoever, between my backyard and the
16 road, and my neighbor's backyard and the road.
17 There's nothing to prevent a kid from walking out of
18 his backyard onto a four-lane highway with a
19 45-mile-an-hour speed limit. There's no guardrails.
20 There's no fencing. There's no nothing. And there's
21 certainly no wall to keep anybody from doing that.

22 Thank you.

23

24

1 STATEMENT OF
2 MARGARET BARRANCO
3 3401 Braberry Lane
4 Crystal Lake, Illinois 60012
5

6 I also am a property owner at 3401 Braberry
7 Lane, Crystal Lake, Illinois. Our home backs up to
8 Highway 31 on the corner of Ames and 31.

9 I also have the same concerns as William
10 Jones, as he has previously stated.

11 In addition, I have a challenge to the
12 noise study that was done, as the noise study
13 reflects properties that are not within our
14 subdivision. They also were not monitored during
15 rush hour. And they were not monitored when a road
16 is four lanes versus two lanes. So to estimate the
17 noise that would be occurring is impossible, in my
18 estimation, and I would like to know how, with this
19 planning, there is going to be noise abatement.

20 With our property in the cul-de-sac, there
21 is a high potential for loss of property value when
22 there is noise. Plus we will be losing, as my
23 husband, William Jones, has stated, safety issues
24 because of the lack of barrier from the road directly

1 into our yard.

2 So my requests are the same as my husband,
3 William Jones.

4 But also I am very concerned regarding the
5 noise and the effect on property value. I do not
6 believe that a berm and/or a fence is enough. There
7 has to be some additional ways to cut down on the
8 noise and I would like to know how they can work
9 through that in order for property values, for those
10 of us that live in the cul-de-sac, to be maintained.

11 We are already paying \$12,000 annually in
12 property tax. And this is a moral issue to me, at
13 this point, that our property values could be
14 affected in the case of the State of Illinois doing
15 this road expansion, especially knowing that we were
16 not provided any opportunity to provide input when
17 this planning has been going on for well over
18 24 months.

19 Thank you.

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STATEMENT OF

JIM HICKS

4701 Edgewood Road

Prairie Grove, Illinois 60012

UJHicks2015@Gmail.com

My wife and I have lived there for 48 years. And Edgewood Road is a beautiful road lined with oak trees and so forth.

I just want to go on record and say two things.

Number one, I'm very impressed with the people who are here with IDOT. And that was Sanjay, John, Jim, and Scott.

And this has been ongoing for many years, because I've attended all of the CAG meetings. And I was told that the oak trees on the south side of Edgewood Road, that are right near the corner of Route 31 and Edgewood, would not be touched.

And when I arrived here today, I noticed that there were going to be three trees removed.

I talked to Scott first, and he said that was a mistake.

I talked to John and Jim, and they both

1 agreed that the trees would be impacted, but it would
2 just be fill. That there would be no need to remove
3 trees on the south side of Edgewood Road, next to the
4 corner of Route 31. So we're talking the southwest
5 corner of 31 and Edgewood Road. The southwest
6 corner.

7 All four agreed that those trees will
8 remain as they are, the oak trees, and they will not
9 be touched.

10 So I would like for it to be recorded, for
11 all the construction people, or whatever, that they
12 are not to be touched.

13 And I'm very, very happy I came today. I
14 will have to admit that IDOT has listened. I've been
15 here many times, and they put forth exactly what I
16 wanted.

17 So, thank you.

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STATEMENT OF

EUGENE POTEMPA

The Greenhouse of Crystal Lake

4317 South State Route 31

Crystal Lake, Illinois 60012

(847) 331-8659

Potempa8@Gmail.com

I presently operate a greenhouse retail facility. And it's five acres. It has, like, 60,000 square feet of greenhouses.

We have hundreds of cars that come into our place every day. This project would adversely affect my business. Because right now, I have access from the north and the south. The proposal restricts access going northbound to my property. And that's unacceptable. It will have a negative effect, and it will put me out of business.

I would like to propose a turning lane into my property, because there's also two other properties that could benefit from a turning lane and get access. So it would benefit three different properties, if we could put a turning lane onto my property.

1 That's one of the problems I have.

2 Another problem is they are proposing to
3 acquire my septic field, which would also put me out
4 of business, because the septic field I have right
5 now is the only piece of land on my property that
6 would be acceptable for a septic field.

7 Additionally, they're proposing a culvert
8 in front of my entire property, which takes away more
9 than 50 percent of my parking, which would also be a
10 real detriment. And I feel that will also put me out
11 of business.

12 I would like to propose either that we get
13 a left turn lane or the median be a drivable median
14 that you can drive over and turn into our property.
15 And I would like that to be considered because the
16 proposal, as is, will definitely put me out of
17 business.

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STATEMENT OF

KELLY WEAVER AND PAM YOCIUS
Alexander Leigh Center for Autism
620 North Illinois Route 31
Crystal Lake, Illinois 60012
(815) 477-2522

KELLY: We were just talking with Kimberly Murphy of IDOT regarding the traffic pattern outside our school. We have a school located at 620. We currently have approximately 30 school buses and 80 staff. And the proposed plan will not allow us to make a left turn.

PAM: Into the property or out.

KELLY: Or out of the property. Our expansion plans will begin this summer and will take us up to approximately 40 to 45 buses and over 200 staff.

And we enter and exit the property two times a day for buses, the beginning of school and end of school, and we need to find a solution for how we can get in and out of our parking lot of our place of business.

Anything you want to add?

PAM: No.

1 STATEMENT OF
2 ANTHONY AND NANATTE JONES
3 4515 Carthage Court
4 Crystal Lake, Illinois 60012
5 (847) 571-8331
6 T4RealEstate@Gmail.com
7 NanatteJones@Yahoo.com
8

9 ANTHONY: The comment is basically that we
10 don't -- there's been a lot of accidents right where
11 you come out of our subdivision, because we're on the
12 hill coming up 31. And right as you get to the top
13 of the hill, Drake is right here. It's on Drake and
14 31. And it's very difficult getting out of there.
15 And with four lanes, it's going to be worse.

16 And somebody got killed coming out of our
17 subdivision, and one person got paralyzed. He ended
18 up losing his home and everything. Because when you
19 come out of there, even school buses in the morning,
20 we wait, like, sometimes 15 minutes just to make a
21 turn out because of 31. The traffic's going this way
22 because they're turning off -- it wasn't as bad
23 before they did the 176. Once they redid 176, the
24 people are turning coming this way. And then once

1 that light changes, people are this way. And then
2 cars are going north and south. We can never
3 get out.

4 What I end up having to do is just pull out
5 and go to my right, go down, and then try to make a
6 U-turn to come back up. And it takes me, like,
7 almost six blocks that I have to drive for safety
8 when I have my kid in the car because, you know, it's
9 dangerous. It's a very dangerous corner.

10 So we were interested in seeing if we can
11 get a light put there. A light that's actuated where
12 it doesn't have to -- you know, it can be always
13 green going this way until a car pulls up, and then
14 it changes. But we've got to have something there.
15 Because this is not going to work. I mean, it's not
16 going to work. I mean, we will never get out of
17 there. And all of the neighbors, we're all concerned
18 about that. With four lanes as opposed to two, we
19 can't get out. With two lanes, we're struggling to
20 get out. With four lanes, we'll never get out.

21 Thank you.

22 NANATTE: We would like a stoplight or a
23 four-way stop at 31 and Drake.

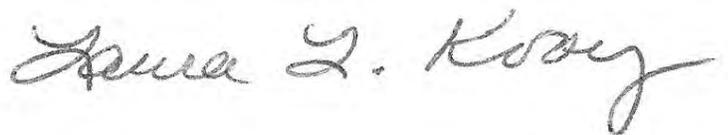
24

1 I, Laura L. Kooy, do hereby certify that I
2 reported in shorthand the proceedings as appears from
3 my stenographic notes so taken and transcribed under
4 my direction.

5 IN WITNESS WHEREOF, I have hereunto set my
6 hand and affixed my seal of office this 11th day of
7 February, 2017.

8

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LAURA L. KOOY, CSR, RDR, CRR
Notary Public
CSR License No. 084-002467

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PUBLIC HEARING
ILLINOIS DEPARTMENT OF TRANSPORTATION
ILLINOIS ROUTE 31, ROUTE 176 TO ROUTE 120
Public Forum

McHenry County College - Shah Center
4100 West Shamrock Lane
McHenry, Illinois 60654

Wednesday, February 8, 2017

1 PRESENT:

2 MR. JOHN A. CLARK, STV, Incorporated

3 * * * * *

4 MR. CLARK: Okay. We'll get started here.

5 Welcome, everybody. My name's John Clark.

6 I'm from STV, Incorporated.

7 I'm the Phase I consultant for the
8 improvement of Illinois 31 from Illinois 176 to
9 Illinois 120, McHenry County, and I'm going to be
10 serving as the moderator for tonight's public forum.

11 The purpose of the forum is to provide you
12 with the opportunity to publicly state your comments
13 regarding the proposed improvements.

14 I have three sign-ins right now. I think
15 several of you are working on giving me -- you'll
16 probably give me the sign-in form here shortly.

17 But anyone who wants to speak should give
18 me one of these forms, just so we know who is getting
19 up and talking.

20 If you didn't receive a form or you want
21 one, raise your hand. I can have one provided to
22 you. We've got some pens up here as well.

23 Anyone, again, who wants to make a
24 statement should complete the form and I'll just go

1 ahead and read these through one by one as we move
2 forward.

3 When your name is called, please approach
4 the front and state your name. You may also provide
5 the name of any organization or business you're
6 representing.

7 We ask that you please limit your statement
8 to two minutes, so that everybody has the opportunity
9 to speak.

10 Once everyone has had the opportunity to
11 speak, we will provide additional time for additional
12 statements as well. So if you want to go beyond the
13 two minutes, we can call you back up.

14 All statements will be transcribed by the
15 court reporter here, and your statements will become
16 part of the public meeting record.

17 If you'd rather not make a public
18 statement, you may provide an individual statement to
19 the court reporter after the public forum.

20 In addition, you can submit written
21 comments tonight, or they can be mailed to the
22 Department or submitted via the project website.

23 The mailing address and website are listed
24 on the comment form of the brochure.

1 Comments received by March 10, 2017, will
2 become part of the public hearing record.

3 So with that, I'd just ask that you come up
4 to the mic, and we can start the forum here.

5 Just give me a second. I'm going to get my
6 timer going.

7 First up, we have Sam Shah. Sam? Is Sam
8 here?

9 (No response.)

10 MR. CLARK: Okay. I'll put that aside.

11 Next, we have Mary Koss. Mary?

12 MS. MARY KOSS: Good evening. My name is Mary
13 Koss.

14 I am the Oak Grove President of the HOA of
15 our subdivision. And our Association wishes to
16 loudly voice our concerns with the proposed
17 construction and sternly rebuke the Illinois
18 Department of Transportation for not educating or
19 informing the member neighbors of our Association
20 about the intended expansion of Route 31, and for not
21 inviting or including our Homeowners Association in
22 the activities of the Community Advisory Group.

23 Within our subdivision, located on the
24 southeastern corner of Route 31 and Ames Road, there

1 are 82 homes, and 8 empty lots awaiting development.
2 As such, our subdivision comprises more than
3 10 percent of the entire population of Prairie Grove,
4 the single village most impacted by the proposed
5 construction.

6 As the President of the Oak Grove
7 Subdivision HOA, I am not saying that the road
8 expansion absolutely must not occur. We knew it
9 would happen, just not when or to what extent.

10 Instead, we are asking IDOT to find ways to
11 lessen the negative impact on our neighborhood.

12 I have heard from many, many members of our
13 neighbors voicing their concerns to me over the
14 proposed construction. Those concerns can be
15 narrowed to six distinct areas:

16 Number 1. HOA property. The Oak Grove
17 Subdivision HOA owns a stretch of land east of
18 Route 31 and west of the homes on the western-most
19 portion of our neighborhood. The deeded property
20 runs the length of the neighborhood adjacent to
21 Route 31 and is a minimum of 40 feet wide.

22 The current plan appears to use that
23 property for a dual use walkway. Since the
24 Association has been excluded from any planning or

1 community involvement, we are unaware of the State's
2 intentions regarding our property.

3 Traffic and safety is another concern. The
4 only direct exit from our neighborhood is via Ames
5 Road heading east one mile to Barreville. That road
6 has a 25-mile-per-hour speed limit. Or we can go
7 west to Route 31 that has a 55-mile-an-hour speed
8 limit.

9 Clearly, this expansion will bring more
10 traffic to Route 31. The plan construction does not
11 appear, in any way, to lessen the danger of turning
12 onto Route 31 from Ames Road. In fact, the plan
13 appears to increase the danger.

14 Let me remind you, we have a lot of new
15 drivers in our neighborhood that are trying to get to
16 Prairie Grove. And it's frightening, as a mother, to
17 send your children off in a car to cross 31. It's
18 very dangerous. It's very busy. And this is going
19 to make it even more dangerous, in our opinion.

20 Also, I don't think that the waterway and
21 stormwater runoff has been considered.

22 We have an entire chapter of our
23 HOA Declaration of Covenants and Restrictions that is
24 dedicated to ensuring the stormwater runoff, and

1 collection areas, on the western and southern portion
2 of the neighborhood, that it remain untouched.

3 The 31 expansion plan appears to affect,
4 significantly, the runoff design and may threaten
5 water damage to the private properties of the
6 western-most portion of the neighborhood.

7 Number 4. Visibility. The developer of
8 the subdivision went to great lengths to hide the
9 neighborhood from Route 31, and to hide Route 31 from
10 our neighborhood. He created a dirt berm on the
11 north end of the neighborhood and planted dozens of
12 trees, probably close to 40, on the north end of the
13 neighborhood, and that's also HOA property.

14 The current plan to expand 31 appears to
15 eliminate the berm and remove all of those trees by
16 bringing the right of way, literally, to the edge of
17 our backyards.

18 Fifth. Noise. We all have lived with the
19 current noise level associated with being adjacent to
20 one of the only three north-south corridors in
21 Northern McHenry County.

22 Route 31 was here before our neighborhood.
23 We get it. However, by expanding Route 31 by
24 100 percent and entirely to the east of the existing

1 roadway, the noise levels will obviously be
2 significantly increased. In your study, you state
3 noise barriers were considered, but are not
4 reasonable and feasible.

5 Noise barriers are not only reasonable,
6 they are absolutely necessary. Not only will this
7 project increase the traffic flow, it moves the
8 roadway 80 feet closer to our homes -- literally,
9 into our backyards -- while eliminating natural sound
10 barriers like trees and distance.

11 Finally, our property values. In 2007, the
12 average value of our homes in Oak Grove Subdivision
13 was nearly \$500,000.

14 In the last three months, three homes have
15 sold in our neighborhood for less than \$350,000,
16 which is a solid 30 percent reduction in property
17 value over one decade.

18 The proposed expansion of Route 31, without
19 addressing noise, visibility, safety, water runoff,
20 as well as our property values, will certainly have a
21 negative impact on those values.

22 The Route 31 project may deliver an
23 economic boon to Northern McHenry County but, please,
24 find ways to lessen the negative impact on our

1 neighborhood for the prosperity of others.

2 Thank you. (Applause.)

3 MR. CLARK: Okay. Thank you, Mary.

4 Since we only have a number of you that are
5 going to speak, I'm not going to enforce the
6 two-minute rule. But I will cut you off at around
7 five minutes, just so you know, in case there's other
8 people. I want to keep it fair.

9 Next up, we have William Jones. William,
10 are you here?

11 (No response.)

12 MR. CLARK: Okay. I'll put that aside.

13 Tim Daum.

14 MR. TIM DAUM: Hello, my name is Tim Daum.

15 I live in the Oak Grove Subdivision. And,
16 as you mentioned, there are a lot of problems with
17 this proposed plan.

18 But the one that I want to talk about is
19 the intersection. I mean, turning left from Ames
20 onto 176 today, as everything is today, is extremely
21 dangerous.

22 And believe it or not, I was actually happy
23 when I heard about this because I figured, okay,
24 good, we're finally going to get our light. And now

1 I'm learning we're not going to get our light. And
2 so I think it's just going to be worse.

3 We have so many kids in our neighborhood.
4 We have a 12-year-old daughter. A 9-year-old son.
5 They're going to be driving in a few years. The
6 thought of them trying to turn left on Ames is
7 terrifying.

8 So you've got to be able -- there's got to
9 be something that can be done to get a light in at
10 Ames.

11 That's it. Thanks. (Applause.)

12 MR. CLARK: Okay. Thank you, Tim.

13 Next up, we have Doug Marunde.

14 MR. DOUG MARUNDE: I'm Doug Marunde.

15 I'm here representing Arvidson Pools &
16 Spas, which is just north of the Ames Road
17 intersection.

18 And we're a little bit concerned also with
19 the access for our commercial property. Not only are
20 we concerned about what the timeline would be,
21 because traffic issues and rerouting things really
22 affected our business last time they did the
23 renovation on Route 31, so we're concerned about the
24 timeline, and also that -- the median that will not

1 allow people to turn left, as you're heading north
2 on 31. It can really create some, well, revenue
3 issues for us.

4 So at some point, we need to really
5 identify what other alternatives can happen for that
6 particular area.

7 I'm not nearly as eloquent as some of you,
8 but -- unprepared. Thank you. (Applause.)

9 MR. CLARK: All right. Thank you, Doug.

10 And did, by chance, Sam Shah come?

11 (No response.)

12 MR. CLARK: Okay. He had signed up. It looks
13 like he's not here.

14 And William Jones. Did William enter the
15 room?

16 (No response.)

17 MR. CLARK: Okay. Well, that's all we have for
18 the folks that came forward with sign-up cards.

19 Is there anybody else who wishes to speak?

20 MR. ED SALISBURY: Yes, I'm sorry. We didn't
21 get a chance to determine whether it was worthwhile
22 to speak until we saw the presentation.

23 If I could speak for a second.

24 MR. CLARK: Sure, not a problem.

1 MR. ED SALISBURY: I just want to get on the
2 record.

3 MR. CLARK: If you could just --

4 MR. ED SALISBURY: Should I fill one of those
5 out?

6 MR. CLARK: You can speak it into the mic. It
7 will be recorded.

8 Hi, my name is Ed Salisbury.

9 I'm from 2016 South Route 31 which, the
10 main side of the marquis is Midtown Storage.

11 We're concerned also. Our concern is from
12 the standpoint -- I understand your concern from a
13 residential standpoint. Our concern is also from a
14 negative impact that it looks like the proposal to
15 give us access both north and south is not acceptable
16 to us. We feel that there's going to be a loss of
17 parking, a loss of -- a substantial loss of revenue
18 from the standpoint we're going to lose a tenant
19 because of the change, and we feel that there's
20 probably a better alternative we can work with the
21 State to perhaps alleviate and get us better direct
22 access north and south on Route 31.

23 MR. CLARK: Okay, thank you.

24 And if you wouldn't mind filling out one of

1 those forms and just handing it to me, just so I can
2 make sure our records indicate everyone who spoke
3 tonight.

4 Is there anyone else who wishes to make a
5 comment at this time?

6 (No response.)

7 MR. CLARK: Okay. Hearing none, that concludes
8 the public forum.

9 So thank you for coming, and we appreciate
10 your comments and your input.

11 Our project team is available to answer any
12 questions you may have about the project until the
13 end of this hearing, which will be 7:00 p.m. tonight.

14 And as a reminder, any written comments
15 must be received no later than March 10, 2017.

16 And all materials presented today will be
17 available on the project website. So all the
18 exhibits and the other things can be downloaded
19 directly from the website.

20 So thank you very much for attending.

21 (Public forum adjourned at 6:15 p.m.)

22

23

24

1 I, Laura L. Kooy, do hereby certify that I
2 reported in shorthand the proceedings as appears from
3 my stenographic notes so taken and transcribed under
4 my direction.

5 IN WITNESS WHEREOF, I have hereunto set my
6 hand and affixed my seal of office this 11th day of
7 February, 2017.

8

9



10

LAURA L. KOOY, CSR, RDR, CRR
Notary Public
CSR License No. 084-002467

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From: Treehorn Properties
To: info@ilroute31.com
Subject: [External] Route 31 in McHenry
Date: Wednesday, July 26, 2017 11:22:58 AM

Hello-

We own the apartment building that is located at 3816 Main Street in McHenry. The building is at the intersection of Route 31 and Main Street. Part of the curb located at the intersection of Main St and Route 31 (on Route 31) is low-it was a driveway in the past. During heavy rain, water flows down Route 31, up the low curb area and into our basement apartments causing flooding and damage. We would like to have that section of curb converted into a regular height curb while work is being completed in the area. Is this possible? I contacted the City of McHenry and they said that I should contact IDOT directly.

Thank you.

Mary Ann Koehler

--

Property Manager
Treehorn Properties, LLC
treehornllc@gmail.com



From the Bureau of : Programming

Name:	<u>Janness (& Richard) Abraham</u>	Date:	<u>February 8, 2017</u>
Title:	<u></u>		
Organization:	<u>Homeowner</u>	Project:	<u>IL 31</u>
Address:	<u>4509 Ripon Road</u>		<u>IL 176 to IL 120</u>
	<u>Crystal Lake, IL</u>		<u>P-91-135-99</u>
Telephone:	<u>(815) 455-0408h, (815) 353-0883c</u>		
E-Mail:	<u>janness@forevergardens.com</u>	PTB No.:	<u>155-015 & 170-007</u>

Items Discussed:

Mrs. Abraham noted there is a spring under her house that is drained by a pipe, which has constant flow, and is concerned the project will affect the pipe and result in flooding the house. She also noted the spring drains to Thunderbird Lake, and there are nearby fens, glacial creek, and sedge meadow. The proposed plans were reviewed which indicated there is no Pr ROW or TE proposed at this location along the east side of IL 31 because the widening has been shifted to the west to avoid the wetland seep. A retaining wall is also proposed to minimize impacts to the east side. Squaw Creek at this location is not impacted.

Follow-Up:

Send P&P and PDP.

By: Scott Czaplicki
Bureau of Programming/Consultant

Address	71 Hampton st
City-State-Zip	Cary IL 60013
Comments	Type any comments you have here: This will greatly impact the entrance to the Scout shop. This will be a big inconvenience for many who go there on a regular basis. As the advancement chair for a troop im Car eliminating the left turn going North would impact my trip as well as anyone else heading North.
E-mail	Ccd26@yahoo.com
FirstName	Cat
LastName	Barnes
Organization	
subject	Submission
Submit	Submit

Address	4803 Barnard Mill Road
City-State-Zip	Ringwood il 60072
Comments	Type any comments you have here: I think that a gravel or dirt trail should also be added along side of the road for snomolble and maybe cross country skiers if they also pay there way as snomobilers due with state regestraions to the state, bicycles pay nothing for this project and get all the benefits of it along with not obeying the rules of the road Thanks for listening.
E-mail	kevinbauer@mchsi.com
FirstName	Kevin
LastName	Bauer
Organization	Sno-Bugs/Lakers Snomobile Club
subject	Submission
Submit	Submit
subscribebox	on

Address	231 E. Prairie St., Apt. C
City-State-Zip	Crystal Lake, IL 60014
Comments	I would support adding a continuous turn lane throughout the study area, to reduce the risk of rear-end collisions. I would also support pedestrian and bicycle accommodations being added. I would NOT support adding additional travel lanes in any section. Looking at the LOS map, most of the Route 31 study area is not expected in 2040 to have significantly higher ADT volumes from the 2009 conditions. The segments that are expected to have higher ADTs are within the cities of Crystal Lake and McHenry. I would suggest a better -- and cheaper -- way to address traffic on IDOT\'s part would be to increase bus service in McHenry County\'s cities and between them. In addition, traffic volumes are likely to be significantly disrupted over the next 20 years as autonomous vehicles come into common use. This is likely to reduce congestion all by itself because these vehicles will require shorter following distances and should decrease the incidence of single-vehicle crashes, which stop traffic during cleanup/investigation. Thank you.
E-mail	juliette.beaulieu@gmail.com
FirstName	Juliette
LastName	Beaulieu
Organization	
subject	Submission
Submit	Submit
subscribebox	on

Address	3601 Thunderbird Lane
City-State-Zip	Prairie Grove, IL 60012
Comments	<p>Scott: I have read all pertinent information regarding the Rt.31 project and am left with several concerns. As a resident of the subdivision adjacent to Rt. 31 and Aimes Road my quality of life and property value are directly impacted by this project. 1. The safe, expedient ingress and egress by residents and motorists to the Oak Grove subdivision and Aimes Road at Rt. 31 must be a priority of the project. 2. Pedestrian and vehicular safety issues associated with Aimes Road traffic dictate the placement of the 'dual use' path on the west side of Rt. 31. 3. Without attention to the ascetics of the eastern frontage between the Oak Grove subdivision and Rt. 31 (significant land encroachment as well as the removal of a consequential number of mature trees) Oak Grove residents' property values will be negatively impacted by this project. 4. The increased noise pollution generated by the project for the Oak Grove subdivision will be significant. How will this be addressed? 5. Will IDOT guarantee Oak Grove residents that storm runoff generated by the Rt. 31 project will not result in flooded back yards and basements? Are the project's proposed storm drainage plans and civil engineering studies supporting them available for public viewing? Respectfully, Fred Bencriscutto</p>
E-mail	fbenc@hotmail.com
FirstName	Fred
LastName	Bencriscutto
Organization	
subject	Submission
Submit	Submit
subscribebox	on

Address	2301 N Elmkirk Park
City-State-Zip	McHenry
Comments	<p>Dear All, I am a resident of McHenry and on a regular basis use the left turn lanes going from Charles J. Miller/BV Road onto 31 heading towards Crystal Lake. While the intersection has certainly been improved, there are some severe issues with these improvements. First and foremost would be that the entrances to the hospital are severely limited. This is followed up by the 2nd that there is an arrow directing merging traffic left onto 31 while there are others turning right into the hospital with no turn lane. There are many arguments among drivers in the morning as the merging lane has a very short time to merge if caught behind turning right into the hospital traffic. The fix for those of using this area is to only use the left turn lane to avoid having to merge but this however is now causing backups that do not allow traffic to make it through the intersection at times. I would urge that this be observed and that a right turn lane at min. be added into the hospital and am not sure why this wasn't included during the design phase of the project. Sincerely appreciated, Sally</p>
E-mail	sallyabender@gmail.com
FirstName	Sally
LastName	Bender
Organization	N/A
subject	Submission
Submit	Submit

Address	6311 Hillcrest Rd
City-State-Zip	Cary, IL 60013
Comments	The Scout Store in Crystal Lake would be affected by this change, Please consider them when making decisions about this project. It's already difficult to access the store at certain times of the day to the Autism Center traffic and buses. We need to be able to access the store from both directions off of Route 31
E-mail	mblackwell524@yahoo.com
FirstName	Melissa
LastName	Blackwell
Organization	Boy Scout Troop 657
subject	Submission
Submit	Submit

Address	3405 Prairie Trail
City-State-Zip	Johnsburg, IL 60051
Comments	I look forward to the improvement of Route 31. Nobody appreciates active construction (especially local businesses) but the inconvenience is typically rewarded when the project is completed. I have always been curious why there has never been an effort to improve the dog leg that route 31 takes through Mc Henry. I suppose the big minds see the big picture but it would be nice if one of the big minds could address that inconvenient maneuver.
E-mail	ronandgin@hotmail.com
FirstName	Ronald
LastName	BRANUM
Organization	
subject	Submission
Submit	Submit
subscribebox	on



Illinois Route 31

ROUTE 176 TO ROUTE 120

www.idot.illinois.gov/projects/il31

Comment Form

Please Print Clearly

Name DAN BROWNE

Organization OAK GROVE RESIDENT

Mailing Address 3414 THUNDERBIRD LN.

City/State/Zip PRairie GROVE, IL. 60012

Phone 847-344-1573 Email DAN BROWNE@NACSUPPLY.COM

Please provide your thoughts on the Preferred Alternative and Environmental Assessment (EA).
Add any additional information that you feel should be considered by the Project Study Group:

PLEASE SEE ATTACHED SHEETS.

If you need more space, please use the reverse side or additional forms. Please place this form in the comment box this evening, or mail it to the Illinois Department of Transportation by March 10, 2017 at the address listed below, or you may scan and email it to the project email address at: info@ILRoute31.com

Illinois Department of Transportation
Attn: Bureau of Programming, Scott Czaplicki
201 W. Center Court
Schaumburg, IL 60196-1096

February 13, 2017

Illinois Department of Transportation
Attn: Bureau of Programming, Scott Czaplicki
201 W. Center Ct.
Schaumburg, IL. 60196

Re: Rt. 31 Expansion

Dear Mr. Czaplicki,

I would like to voice my concerns on the impact the Route 31 expansion will have on my subdivision—Oak Grove. I'm sure you have already received a letter from our HOA president Mary Koss, which explains the concerns as related to our neighborhood. I have attached that letter, in case you haven't.

My greatest concern is the danger of turning south onto Route 31 from Ames. The current situation is very dangerous--adding 2 more lanes, without a traffic light will surely create a safety issue.

I am also very concerned about the natural buffers being removed. Our developer designed this neighborhood in a way that would minimize the sight of and noise from Route 31. Removing all the trees and distance will decrease the property values for those houses along Route 31 and that will filter down to the rest of the neighborhood. Why can't some of the expansion be on the west side of 31? I understand Terra Cotta suggested that the expansion would be best on the east side. It's obvious why they believe that, but it makes more sense to expand on both sides.

Please consider the impact to our subdivision and re-evaluate the plan.

Sincerely,

A handwritten signature in black ink, appearing to read 'Dan Browne', with a long horizontal flourish extending to the right.

Dan Browne
3414 Thunderbird Ln.
Prairie Grove, IL. 60012
847-344-1573

Address	3414 Thunderbird Lane
City-State-Zip	Prairie Grove
Comments	<p>We live in Oak Grove Estates off of Route 31 and Ames Road. We are extremely concerned about the plan for Route 31 and how it will effect our neighborhood. I am not comfortable with not having a stoplight at the intersection of Ames and 31. It is very difficult to get out of our neighborhood now and adding lanes will only make that even more dangerous than it already is. I am also uncomfortable with this plan as there are no plans for a noise barrier or added landscape to alleviate the noise and ugliness of a highway running through many backyards. This is also a safety concern for the houses that back up to the highway. I have seen many highways expand (Randall Road), but they seem to have additional plans for these types of things. These are our main concerns, however driving our property values down when the highway makes the bordering homes values go down is also another main concern. We were already hurt by the economy and this added obstacle will make it impossible for many of us to sell our homes for what they were once worth. Please consider our neighborhood which you are infringing upon as this plan goes forward.</p>
E-mail	wenlbrowne@gmail.com
FirstName	Wendy
LastName	Browne
Organization	Oak Grove Estates
postal-list-box	on
subject	Submission
Submit	Submit
subscribebox	on

From: [Doug Martin](#)
To: [Czaplicki, Scott D](#)
Cc: [Ron Bykowski](#); [Jon Schmitt](#)
Subject: [External] Meeting to Discuss 120/31
Date: Friday, March 03, 2017 2:54:27 PM
Attachments: [image002.png](#)
[image003.png](#)

Scott,

I met with Ron Bykowski today about his properties at the southeast corner of 31 (Front Street) and 120 (Elm) and we wanted to see if we could setup a meeting with you and potentially Michael Cullian to proactively take a look at that corner. We'd like to determine and ultimately come to some agreement as far as access points, etc. It's very hard to market that corner with the road improvements looming in the future and I know you had met with Ron a while back. Would this be possible?

Thank you for your time.

Doug

Douglas P. Martin
Director of Economic Development
City of McHenry
333 S Green Street
McHenry, IL 60050
815.363.2110 (d)
815.363.2173 (f)
815.790.4752 (c)
dmartin@ci.mchenry.il.us
www.ci.mchenry.il.us
[McHenry Market Pulse](#)
[@mchmarketpulse](#)



City of McHenry: Exhibitor Booth P3-342







Illinois Department of Transportation



Illinois Route 31

ROUTE 176 TO ROUTE 120

www.idot.illinois.gov/projects/il31

Comment Form

Please Print Clearly

Name PAUL CARISTENSON

Organization _____

Mailing Address 3501 LAKEWOOD DRCity/State/Zip PRAIRIE GROVE, ILPhone 815-477-9767 Email SANDBAG2@ATT.NET

Please provide your thoughts on the Preferred Alternative and Environmental Assessment (EA).
Add any additional information that you feel should be considered by the Project Study Group:

We would like closing Ames Rd @ 31 & extending Lakewood Drive north to Edgewood Rd to be able to use the stop light to go South on Rt 31 much safer. Please consider this
Thank you,

The Christensons
3501 Lakewood Dr.
Prairie Grove, IL

If you need more space, please use the reverse side or additional forms. Please place this form in the comment box this evening, or mail it to the Illinois Department of Transportation by March 10, 2017 at the address listed below, or you may scan and email it to the project email address at: info@ILRoute31.com

Illinois Department of Transportation
Attn: Bureau of Programming, Scott Czaplicki
201 W. Center Court
Schaumburg, IL 60196-1096



March 2, 2017

We attended your Public Hearing on Feb 8th, 2017 and made a public statement to the court reporter; however, we also wanted to make a more formal statement in writing at this time regarding 1811 S IL Route 31.

Problems: We see the following problems with the proposed Rt IL 31 expansion project in regards to our property:

- 1) Parking – Currently we have approximately 42 parking places for staff and visitors. The proposed changes cause us to lose 17 of those spaces, leaving us 25 spaces for 35 employees and customers. Because of our septic system, we cannot add parking behind our building.
- 2) Truck Access – Presently we have tractor/trailer rigs with 53’ trailers backing to our loading dock, on a daily basis, at the SE location of our building, right in the middle of the construction zone. The loss of frontage space would mean the loss of turning space required by these large rigs.
- 3) Drainage – Large paved areas require substantial drainage, and since our building is considerably below the grade of Rt 31, we have concerns about flooding/drainage relief. We have already addressed current drainage issues by installing a french drain near the entrance of our building.
- 4) Utilities – The gas line and meter will have to be moved out of Rt 31 ROW. Our water well will potentially encroach on the areas of the assessment, possibly requiring a new well. Sewage is now being handled by a septic system at the back of the property. If you suggest new parking has to extend into the present septic field, the system would have to be redesigned and moved.
- 5) Turning North – We realize your plan for the road expansion includes adding multiple U-Turn areas strategically placed. We don’t know the possible problems of our 53’ trucks not being able to turn around in these 22’ U-Turn areas anywhere near our property. If trucks aren’t able to come to our building as often, our shipping rates may be increased, supplies won’t be delivered as frequently, and our customer delivery times will be compromised.



Solutions: We see the following solutions with the proposed Rt IL 31 expansion project in regards to our property:

- 1) Parking – We could possibly be connected to City Sewer and Water, which could better allow parking and truck turnaround access to be added to the back of the building if we no longer require the Septic system.
- 2) Truck Access – Trucks will need a place to turn around, as well as have room to maneuver to the loading dock. As presently configured, your plan allows for neither. If a parking solution comes about, offering better access to the rear of the property, a truck turnaround area could possibly be added behind the building and added parking, since additional parking will still be required to accommodate our employees.
- 3) Drainage – A large culvert system needs to be installed to accommodate drainage from the highway and our parking areas.
- 4) Utilities – Regarding the impact on our water well and septic system, you could connect Digital Pix to the city water and sewage that currently exists across the road.
- 5) Turning North – Look into making some sort of ‘frontage road’ for us to drive north to the stop light that will be installed at Veteran’s Parkway. This could help the truck access problem if trailers can come in and go out of our property from the north via the traffic signal. However, this could cause a problem at high traffic times with our neighbors to the north, Waste Management and the Chapel. We do not wish for this option if it can’t solve the truck access problem as well.

Thank you for your consideration into Digital Pix’s issues with the Rt 31 proposed expansion project.

Erica Carlson

Erica Carlson

Finance/HR Manager

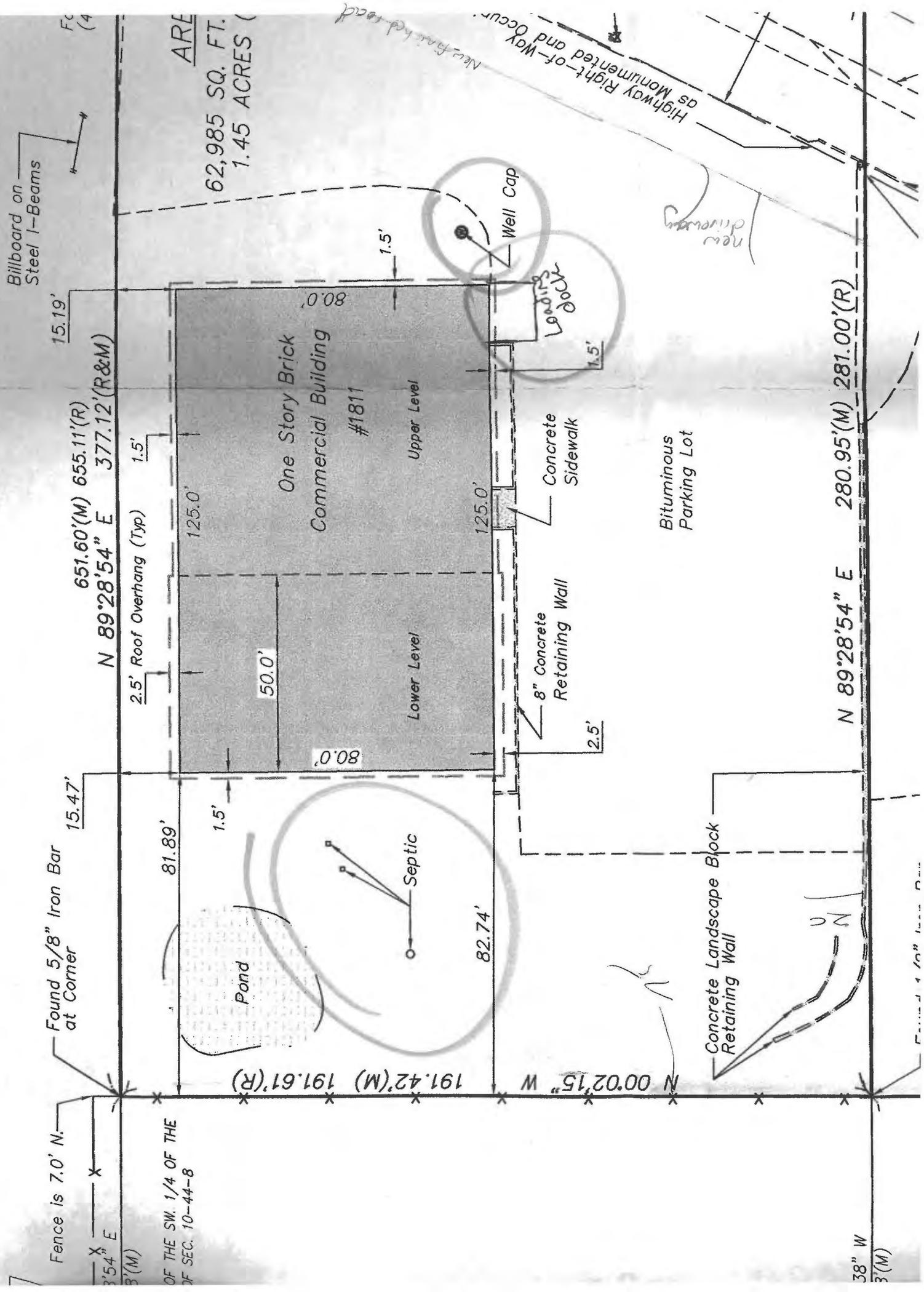
Digital Pix & Composites

1811 South IL Route 31, McHenry, IL 60050-8292

erica@dpcpix.com

815.363.2800 ext. 202

Southeast Quarter; thence Easterly along said North line, a distance of 377.12 feet to the pl... beginning, in McHenry County, Illinois.



Billboard on Steel I-Beams

15.19' 651.60'(M) 655.11'(R) 377.12'(R&M) N 89°28'54" E

ARE 62,985 SQ. FT. 1.45 ACRES

2.5' Roof Overhang (Typ) 1.5'

One Story Brick Commercial Building #1811

Lower Level 50.0' 80.0'

Upper Level 80.0'

Pond 81.89' 1.5' 82.74'

Septic

Well Cap

Loading Dock 1.5'

Concrete Sidewalk

8" Concrete Retaining Wall 2.5'

Bituminous Parking Lot

Concrete Landscape Block Retaining Wall

N 89°28'54" E 280.95'(M) 281.00'(R)

38" W 3'(M)

Found 1/4" Iron Bar at Corner

Fence is 7.0' N. 3'54" E 3'(M) OF THE SW. 1/4 OF THE SEC. 10-44-8

N 00°02'15" W 191.42'(M) 191.61'(R)

Highway Right-of-Way as Monimented and Occur New front road

new driveway



Illinois Route 31

ROUTE 176 TO ROUTE 120

www.idot.illinois.gov/projects/il31

Comment Form

Please Print Clearly

Name STEVEN CARRUTHERS BUREAU OF PROGRAMMING RECEIVED

Organization CITY OF CRYSTAL LAKE MAR 09 2017

Mailing Address 100 W. WOODSTOCK STREET DISTRICT #1

City/State/Zip CRYSTAL LAKE, IL 60014

Phone 815-356-3605 Email SCARRUTHERS@CRYSTALLAKE.ORG

Please provide your thoughts on the Preferred Alternative and Environmental Assessment (EA).
Add any additional information that you feel should be considered by the Project Study Group:

WE ARE HAPPY THE STATE IS CONTEMPLATING IMPROVE ROUTE 31
BETWEEN CRYSTAL LAKE AND McHENRY
HOWEVER-

IDOT JUST IMPROVED THE ROUTE 31/176 INTERSECTION AND
NOW THE BUSINESSES ON THE NORTH LEG OF THE INTERSECTION ARE
GOING TO HAVE TO ENDURE CONSTRUCTION INTERRUPTIONS AGAIN.
WEREN'T THE IMPROVEMENTS THAT WERE MADE TO ROUTE 31
NORTH OF ROUTE 176 WITH THE INTERSECTION IMPROVEMENTS
THE ULTIMATE IMPROVEMENTS? WHY IS THIS BEING REMOVED
AND REPLACED AGAIN? IT DOESN'T MAKE SENSE TO IMPACT
THE BUSINESSES TWICE.

STEVE CARRUTHERS

If you need more space, please use the reverse side or additional forms. Please place this form in the comment box this evening, or mail it to the Illinois Department of Transportation by March 10, 2017 at the address listed below, or you may scan and email it to the project email address at: info@ILRoute31.com

Illinois Department of Transportation
Attn: Bureau of Programming, Scott Czaplicki
201 W. Center Court
Schaumburg, IL 60196-1096



Illinois Route 31

ROUTE 176 TO ROUTE 120

www.idot.illinois.gov/projects/il31

Comment Form

Please Print Clearly

Name

Michael Chambers

Organization

Adams Steel Service Inc

Mailing Address

2022 S. IL Rt. 31

City/State/Zip

McHenry IL 60050

Phone

815 276-3510
815 385-9100

Email

MIKE@ADAMSSTEELSERVICE.COM

Please provide your thoughts on the Preferred Alternative and Environmental Assessment (EA).

Add any additional information that you feel should be considered by the Project Study Group:

#1 Move my entrance to the north to accommodate left turn to go south. Which would be less costly than combine my entrance with the Building next door,

#2 Combine entrances with next door But now the septic field would be torn out with no other location available.

If you need more space, please use the reverse side or additional forms. Please place this form in the comment box this evening, or mail it to the Illinois Department of Transportation by March 10, 2017 at the address listed below, or you may scan and email it to the project email address at: info@ILRoute31.com



Illinois Route 31

ROUTE 176 TO ROUTE 120

www.idot.illinois.gov/projects/il31

Comment Form

Please Print Clearly

Name Michael Chambers
 Organization Newport Properties 14-10-452-003
 Mailing Address 1326 Old Bay Rd
 City/State/Zip Johansburg IL 60051
 Phone 815 276 3510 Email Mike@AdamsSteelService.com

Please provide your thoughts on the Preferred Alternative and Environmental Assessment (EA).

Add any additional information that you feel should be considered by the Project Study Group:

Sheet 8

If I can move my entrance
to the north or move the turn
lane to the south, This would allow
our semi Trucks to turn south
& north with out sending them north
to the industrial park to make u turns

If you need more space, please use the reverse side or additional forms. Please place this form in the comment box this evening, or mail it to the Illinois Department of Transportation by March 10, 2017 at the address listed below, or you may scan and email it to the project email address at: info@ILRoute31.com

Address	233 3rd Street
City-State-Zip	Crystal Lake, IL. 60014
Comments	<p>Type any comments you have here: I really appreciate the effort on IDOT's part to lay this plan out to stakeholders. There is more here than anyone could possibly comment on comprehensively. I am concerned with three aspects of the plan: 1. The effort at multi-modality is much appreciated. I heard some residents who lived close by 31 worrying about the possible ramifications from foot and bike traffic, but I certainly hope that their worries will prove unfounded. For me, the fact that residents of the senior living center at the Fountains could get out and connect, walking or biking, with other parts of the community including with possible new businesses located at the southern end of the project is an important manifestation of how multi-modal thinking about transportation can improve our lives, our health, and our social connectivity. As a soon-to-be senior, and someone whose mother has stayed alive in vigorous form into her mid-80s by daily walks through her neighborhood, this part of the project makes me really happy. 2. The highway medians: I heard residents who live close to the highway worrying about how changes in water flow, especially as it comes under the highway from the west to east, could impact their yards and basements. I'm confident in IDOT's assertions that it has engineered to prevent this, but having deep-rooted native grasses on the medians could help to modulate the impacts of heavy rains. These should be low-profile grasses and vegetation, which generally grow no higher than three feet tall. They would of course require control against invasive species, and the department should adopt clear performance criteria so that whomever is tasked with the maintenance of these areas understands how to get on top of it, and keep it from being taken over by a patch of giant reeds, such as we see on some other recent highway projects. I'm hardly an expert on the life-cycles of prairie vegetation, but I believe it is possible to establish a stable community of grasses with attentive management during an initial number of years, and then these medians would require much less maintenance care. The Environmental Defenders of McHenry County have expertise in the kind of burns that work to keep these kinds of prairie grass communities flourishing, and I would encourage IDOT to consider a possible partnership with this local group. I'm a member of the group, and I believe that this kind of local responsibility, especially if it is connected with concerned neighbors to the highway, can serve to connect people socially, and can educate them as to how to play a part in keeping their local habitat thriving and beautiful, in terms of aesthetics, but also again, in terms of the environmental services such native grasslands can provide. 3. We in McHenry County have worked hard to begin reversing the decimation of the flourishing oak savannah ecosystems that were the norm here before settlers streamed in from the east coast in the early 19th century. We appreciate the consideration that IDOT is giving to the tree populations in the area, and the principle of replacing trees that must be cut down with others that can compensate for their loss. We want to state emphatically for the public record (I am someone who has been part of the Quercus Project, participating with many different people but most importantly with younger people for whom replanting trees and working to establish new growth of oak and hickory is a significant act of hope and investment in their future" and so I say "we" in the spirit of all the people who have done not only the planting but the sustained follow-up care for</p>

these fledgling trees)--we want it to show in the public record that oaks are keystone species for the forests and savannah that we are working to restore, and we need replacement trees to be 1. Of similar quality, in other words, if a healthy 200 year old tree is lost, we need the number of trees replacing it to be requisite to establishing the probability that the impact of this loss on contiguous forest will be compensated for; in other words, so that there will be a high probability that the local forest will flourish in the way it could have if the highway hadnâ€™t been expanded and had to take that 200 year old oak. Basically, thereâ€™s an opportunity with this expansion to be part of the solution to the problem of McHenryâ€™s Countyâ€™s loss of oak/hickory systems, and we hope that this project will fully take advantage of it. 2. We want the trees to be replaced IN McHenry county, so if foresters judge that there arenâ€™t good spots to re-establish lost savannah footprint in the immediate area, weâ€™d like to see trees planted in such a way that the green â€œhighwayâ€ of ecosystems that flows through the county will be every bit as strong as the flow of human population and commerce that this highway will enable. 3. A bonus: given that trees provide both shade and beauty, it would be wonderful if at least some of the replacement trees could be located in places that will give pleasure and hope to pedestrians and bikers who will make use of the non-vehicle paths, as well as eventually rising to maintain the pleasure that this corridor currently gives to those who drive through this area (who will, we hope, be much safer as a result of this highway expansion). But the more beauty along the multi-modal paths, the more of a sense of thoughtful human planning, the more use these paths will get, and the more bang for the buck our citizens will get from this highway improvement. Thanks for your consideration of my comments.

E-mail	Rjdarger@gmail.com
FirstName	John
LastName	Darger
Organization	Quercus Project, the Environmental Defenders of McHenry County
subject	Submission
Submit	Submit
subscribebox	on

Address	11203 reed rd
City-State-Zip	Huntley, il 60142
Comments	Please leave the ability to turn left onto our field office and scout store.
E-mail	Melissad@att.net
FirstName	Melissa
LastName	Davis
Organization	Boy scouts
subject	Submission
Submit	Submit

2-21-17

Dear Scott Czapilicki and IL Dept of Transportation Bureau of Programming,

As a resident impacted by this plan, I have to question why we need to create another road project on Route 31. Not less than 5 years ago, a project was completed on Route 31 to upgrade turn lanes, repave, and improve certain intersections from Crystal Lake Ave up to Edgewood Rd. We have barely had time to utilize those improvements and realize the ROI before the State takes on yet another project. In addition, this new project creates significant safety issues and encroaches on the rights of residents without any benefits. In fact, by creating a "highway" that encroaches on the Oak Grove HOA land, and not improving the Ames or Edgewood intersections, it will be even more difficult to access Route 31 on an everyday basis. I've added the thoughts from one of our neighborhood representatives that attended the most recent meeting:

"Second, the plan is a huge safety concern on several fronts.

1. Ames Rd. intersection: We are not getting a stop light, so to turn left our residents, including many new drivers will have to navigate across a potential multi use path, 2 lanes of traffic and a median. Ames Rd is a 35 mph road, Rt 31 is a 55 mph highway.
2. The multi-use path is a safety concern in and of itself. It seeks to increase the flow of people, strangers, literally in the backyards of several neighbors. There is no privacy or protection from any person using the path. Has a berm or landscaping been considered to offer protection/safety? The location of the path on the East side of Route 31 is also perplexing. One would think that placing the path on the West side of the road would be advisable as this is the side of the Route 31 that the Prairie Path (bike path) runs along. Bikers would be able to get from the Prairie Path to the multi-use path without crossing Rt 31, which would be 4 lanes of traffic plus a median.

Third, I have environmental concerns about the project as a whole. The plan has a significant shift in the water flow as a result of added earth on the east side of 31. Additionally, all the western-most trees will be removed, including those on the berm at the northern most portion of the neighborhood. I have small children, it feels like an example of the Lorax come to life.

Finally, this project will certainly negatively impact the property values of our 82 home neighborhood. Property values are still ailing and have not recovered from the circumstances of 2007/2008 and this project will not help to move things in the right direction. Anything of this nature negatively impacting property values can only hurt our State's already fragile and in my opinion, abysmal, financial condition.

In closing, while I learned, from you, Scott, that there is no funding for this project at this time, I also learned this is the time to speak up. Thank you for the chance to raise my voice in opposition to this project as it stands. I hope my comments, along with the many others I hope you are receiving are thoughtfully considered."

This project should be halted until the State of Illinois addressed the concerns of residents and can truly demonstrate the benefits of this project.

Sincerely – Mike Dennis
Resident – Oak Grove H.O.A – Route 31 & Ames
815-347-9122
denfamily@comcast.net

Address	3512 Lakewood Dr
City-State-Zip	Prairie Grove
Comments	<p>As a resident of the Oak Grove subdivision, I have concerns regarding this project. I already submitted a comment regarding the unsafe conditions at the intersection of Ames Road and Route 31; the project does not adequately improve the safety of this congested and dangerous intersection. In addition, the project encroaches entirely into the Oak Grove subdivision on the east side of the project, as opposed to the vacant industrial land on the west side of the project. Instead of impacting residential homes and neighborhoods, using vacant industrial land would be safer for the families and children that live and play in Oak Grove. At a minimum, the dual-use portion of the project should be moved to the west side of the road. In addition, the plan appears to remove most of the old-growth trees in the area of our neighborhood that borders this project, as well as significantly affect the water flow. These environmental issues have not been adequately addressed in the plan for this project. Thank you for the opportunity to comment, and I hope the plan can be revised to address these concerns.</p>
E-mail	jdrozt@yahoo.com
FirstName	Jennifer
LastName	Drozt
Organization	Resident
subject	Submission
Submit	Submit

From: [Terry Dyra](#)
To: info@ilroute31.com
Subject: [External] IDOT Route 31 Project
Date: Tuesday, February 28, 2017 7:04:03 PM

I have a number of concerns with the proposed expansion of Illinois Route 31, from 176 to 120. My concerns center around the intersection of Route 31 and Ames Road which is an intersection that has sorely needed attention for years. Many intersections on Route 31 today have a right-hand exit lane, the current Ames intersection lacks this exit and adds to the current safety issues and resident concerns. That Ames Road also lacks a right hand lane to turn onto Route 31 is also a current frustration. I had hoped that the expansion of Route 31 would alleviate some of the issues at this intersection, however the proposed changes to this intersection actually appear to worsen the situation.

A few issues, along with recommendations.

1. Multi-use path. The multi-use path is a great idea, and it does belong on the east side of Route 31. However as there is nothing to walk south to from Ames, and nothing to walk north of from Gracy...much of this proposed path is a waste. What is needed is a path only from Ames to Gracy on the east side of Route 31- along with a crosswalk at Edgewood. This path would allow walkers/bikers from Ames to walk north to Edgewood and cross at a traffic light- similar with walkers/bikers coming south from Gracy. There is Prairie Trail west of Ames that is accessible from Edgewood,. Providing a way for bicyclists traveling through Prairie Grove to travel safely the east side of Route 31 to the west side, with a way to cross 31 at Edgewood would be very helpful. Of course extending bike lanes west on Edgewood to the prairie path and eastward on Ames to the park at Ames/Barreville...would be helpful as well- as these are not well lit roads which make dusk travel dangerous. Additionally Prairie Grove residents could leave their vehicles at this park and travel via Ames to Edgewood and on to the prairie path.

Moving multi-use lanes to the west side of Route 31 would be a complete waste as it would not solve anything- as it would not facilitate crossing route 31 from either direction by walkers/cyclists. Extending multi-use lanes south of Ames or North of Gracy is completely unnecessary- given there is not now, or anything built in either direction is short order. Or more simply- those would be paths to nowhere.

2. U Turn Lane at 31 and Ames. Completely useless to put this where it is proposed. There is an opportunity for north bound traffic to turn left at Halfmile Road at a traffic light, and Pingree a few blocks in- so changing direction at this Halfmile facilitates much or what a U Turn lane at Ames would. There is exactly 1 driveway on the west side of 31 between Halfmile and Ames. Was this U Turn lane intended for use by this single driveway?

Adding another lane for traffic turning left (southbound) onto 31 from Ames to cross is illogical and one more reason to move this U Turn elsewhere. If you want to facilitate southbound traffic and help local residents and not just commuters passing through- a better alternative would be to place the U Turn lane north of the Ames intersection. This would allow Prairie Grove residents who travel Ames regularly and wish to travel south...the ability to turn right on 31, then utilize a U Turn lane to change directions. You could place this U Turn between Ames and Edgewood- no reason to place it in an intersection...and you could also use this same lane to allow north bound traffic to enter west side businesses. This would be a much greater use of a U Turn lane the the one currently proposed- which again appears to serve one driveway, a complete waste of asphalt.

3. No Right Turn Lane from Ames onto 31. This is an oversight. As traffic will build upon Ames, as westbound Ames traffic waits to attempt to turn left across 2 lanes of north bound traffic, a U Turn Lane as well as two lanes of south bound traffic and an east bound turn lane.....traffic will backup on Ames more than it does today. Installing a right turn lane on Ames will help to alleviate that as it will provide a dedicated for both north and south

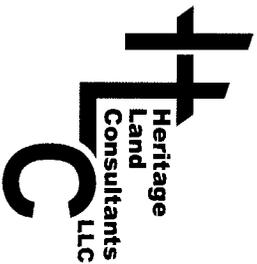
bound traffic from Ames onto route 31.

4. Water/Drainage. This part of Mc Henry County has a tremendous amount of water, both above and below ground. The area of Route 31 and Ames is no exception. The Oak Grove subdivision is directly south and east of this intersection and is home to Thunderbird Lake, an above ground creek and wetlands...and plenty of underground water as well. Multiple homes have had concrete walkways sink and need to be raised, have sump pumps that run 24/7 and flood surrounding grounds with water. Prairie Grove is well aware of this issue, however it does not appear that enough consideration was given to managing water flow in this area- simply moving water to Thunderbird lake via existing above and below ground waterways is not adequate as it risks exasperating an existing situation by bringing additional water flow to this area. Any water that IDOT needs to move- either existing or runoff, should be diverted to well south of the existing Oak Grove subdivision- not through it or under it. As Prairie Grove is aware of water issues in the region, and as IDOT has been advised as well, I expect the impact on local water table and the water flow to be given more than a cursory look by the civil engineering team.

Thank you for your time and for considering these suggestions.

Please feel free to call/email me with questions or concerns on these suggestions.

Terry Dyra
Oak Grove resident
815.404.6617
tdyra@yahoo.com



Heritage Land Consultants, LLC

HLC Surveying – HLC Engineering – HLC Septic Design

Office: 758 Ridgeway Drive, McHenry, IL 60050

Tel: 815-344-3252 **Fax:** 815-344-3257

Website: www.heritagelandconsultants.com



March 10, 2017

Re: Rt. 31 Improvements
Rt 176 (C.L.) to 120 (McHenry).

Mr. Scott Czapllicki – Project Manager
Illinois Department of Transportation,
201 West Center Road
Schaumburg, Il. 60196

Dear Mr. Czapllicki:

Upon reviewing some exhibit documents for the proposed improvements of Route 31, between Rt. 176 in Crystal Lake and Route 120 in McHenry, I have two items of concern.

The first item is the area between Ames Road and Edgewood . There are some proposals for putting a frontage road on the west side of Route 31 to collect traffic from all the driveways in that area, but I don't see any proposals for the east side of Route 31 in that same area, possibly because there are no driveways in that area. There is however considerable traffic in that area primarily during the morning and evening rush hours. In the morning there is significant traffic on Ames road and trying to turn onto Rt. 31 is problematic. Turning north, it's just the volume of traffic on Ames Road, but turning left, it's the traffic on both roads, which makes that turn extremely difficult. In my opinion there are two ways to adjust this intersection, knowing there is farm field on the northeast corner of Ames & Route 31. (1) the first approach would be to construct a frontage road on the east side of Rt. 31, between Edgewood and Ames. This would negate the intersection at Ames and convey the traffic northward on the frontage road to Edgewood where there would be a traffic signal installed., or (2) just realign Ames Road with two sweeping curves so Ames Road would align with Edgewood, and again, install a stop light at Edgewood. I realize another stop light on Route 31 may not be welcomed by drivers on Rt. 31, but it would make for safer driving conditions, and the traffic is only going to increase in the future.

The second item is the south portion of the hill on Rt. 31, between Brighton Lane and Drake Drive, where the highway makes a steady ascent for about ¼ mile; on the east side of the highway there is an abrupt change in grade from the R/W to the adjacent property; with the widening of the road this grade will become more abrupt. I do not see any retaining walls proposed, so therefore I assume the grade is going to be cut back a considerable distance to make for a standard highway slope, taking out a large forested area with large, mature trees. For this area and other areas like this along the improvement work, can you consider retaining walls.

If you have any questions concerning this transmittal, please contact this office.. Thank you.

Respectfully,

A handwritten signature in black ink, appearing to read 'Peder A. Finnberg', is written over a horizontal line. The signature is fluid and cursive.

Peder A. Finnberg - Prof. Engr



Illinois Route 31

ROUTE 176 TO ROUTE 120

www.idot.illinois.gov/projects/il31

Comment Form

Please Print Clearly

Name Joe Gottenolla

Organization _____

Mailing Address _____

City/State/Zip _____

Phone _____ Email Joe @ McHenryCountyLaw.com

Please provide your thoughts on the Preferred Alternative and Environmental Assessment (EA).
Add any additional information that you feel should be considered by the Project Study Group:

Just Get It Built you are 20 years
Behind schedule

If you need more space, please use the reverse side or additional forms. Please place this form in the comment box this evening, or mail it to the Illinois Department of Transportation by March 10, 2017 at the address listed below, or you may scan and email it to the project email address at: info@ILRoute31.com



Illinois Route 31

ROUTE 176 TO ROUTE 120

www.idot.illinois.gov/projects/il31

Comment Form

Please Print Clearly

Name Lisa Haderlein

Organization The Land Conservancy of McHenry County

Mailing Address Po Box 352

City/State/Zip Woodstock IL 60098

Phone 815-337-9502 Email lhaderlein@conserveinc.org

Please provide your thoughts on the Preferred Alternative and Environmental Assessment (EA).
Add any additional information that you feel should be considered by the Project Study Group:

- ① We would like to see all oaks and hickories that will be removed replaced with species and caliper inches equal to the total diameter of trees removed.
- ② Establish a "tree bank" in the Village of Prairie Grove where most of the trees will be removed. The bank should be permanently protected and trees planted in a way that will allow them to grow into an oak woodland (i.e. not too close together)
- ③ For every tree (oak or hickory), 2 should be planted on the property (or near to) where the tree had been growing.
- ④ Replacement trees should be locally grown and from local acorn & hickory nut material.
- ⑤ Wetland mitigation funds should be used to create new wetland habitat in the same watershed, and physically near to, the impacts. The Land Conservancy has a site north of Brighton Lane and the Fountains that offers up to 8 acres of wetland creation.

If you need more space, please use the reverse side or additional forms. Please place this form in the comment box this evening, or mail it to the Illinois Department of Transportation by March 10, 2017 at the address listed below, or you may scan and email it to the project email address at: info@ILRoute31.com



Illinois Route 31

ROUTE 176 TO ROUTE 120

www.idot.illinois.gov/projects/il31

Comment Form

Please Print Clearly

Name Jim Haister

Organization _____

Mailing Address 3206 Prairie View

City/State/Zip Prairie Grove IL

Phone 847.606.5000 Email JSHaister@gmail.com

Please provide your thoughts on the Preferred Alternative and Environmental Assessment (EA).
Add any additional information that you feel should be considered by the Project Study Group:

There seems to be no clear method for Prairie Grove residents to head south. No proposed lights at Ames, Gray or Veterans. We can't get out now with 2 lanes. How safe will it be with 4 lanes of traffic?

Need full intersection at Edgewood.

If you need more space, please use the reverse side or additional forms. Please place this form in the comment box this evening, or mail it to the Illinois Department of Transportation by March 10, 2017 at the address listed below, or you may scan and email it to the project email address at: info@ILRoute31.com

From: [Ruthie Harvey](#)
To: [Czaplicki, Scott D](#)
Subject: [External] Question about a project
Date: Tuesday, February 14, 2017 2:42:48 PM

Good afternoon,

I am hoping you can help me with some information about this project:

P-91-135-99 (IL 31 from IL 176 to IL 120)

Can you tell me the status of the ROW phase? I was also wondering if you can tell me if a preferred alternative has been chosen for the North Section yet?

Thank you in advance for your help!

Ruthie Harvey
ruthieharvey56@gmail.com



Illinois Route 31

ROUTE 176 TO ROUTE 120

www.idot.illinois.gov/projects/il31

FEB 14 2017

DISTRICT #1

Comment Form

Please Print Clearly

Name

Organization

Mailing Address

City/State/Zip

Phone

Email



Please provide your thoughts on the Preferred Alternative and Environmental Assessment (EA).
Add any additional information that you feel should be considered by the Project Study Group:

2/10/17

Dear friends,

I attended the meeting Wednesday at the
Shah Center in McHenry and made a
mistake with my new e-mail address. I got
it confused with my old Juno address.

Please correct my address to Fran.b.hicks@gmail.com.

Thank you for your patience

Fran Hicks

If you need more space, please use the reverse side or additional forms. Please place this form in the comment box this evening, or mail it to the Illinois Department of Transportation by March 10, 2017 at the address listed below, or you may scan and email it to the project email address at: info@ILRoute31.com

Illinois Department of Transportation
Attn: Bureau of Programming, Scott Czaplicki
201 W. Center Court
Schaumburg, IL 60196-1096

Address	3508 Lakewood Dr
City-State-Zip	Crystal Lake, IL 60012
Comments	Type any comments you have here: Please add me to the mailing list. Although according to exhibits I am located within the study area, I was not notified of the project or invited to any of the Public Meetings/Hearing. I am very disappointed. I think our whole neighborhood was excluded. I found out about the Public Hearing by chance. It is very sad as TC industries was coordinated with and all the impacts are being pushed into our neighborhood. Coincidence? Sincerely, Julie Hoberg
E-mail	julie.hoberg1@gmail.com
FirstName	Julie
LastName	Hoberg
Organization	resident
postal-list-box	on
subject	Submission
Submit	Submit
subscribebox	on

Address	3508 Lakewood Dr
City-State-Zip	Crystal Lake, IL 60012
Comments	<p>Comments on EA Please accept these comments as part of the Public Hearing commenting period for the Route 31 EA. I agree changes are needed but it appears the study skirted around safety issues and did not include all stakeholders that could have included information helpful to the study. It is very disappointing. Mailing list. How was I, my neighborhood not included? My house is included in one of the study area maps and I have not received any notifications of this project. I found out about the Public Hearing by chance. I had even inquired about the traffic studies and gap analysis at the Village a ways back and was not notified of the project then either. It would have been nice to be involved at the scoping meeting. What are the intersection improvements at Half mile, Ames, Edgewood? I have read the EA online and it did not detail the intersection improvements that I could find. Traffic studies, IDSs, and plan and profile drawings are not included in the EA or its appendices online. They are not included in the traffic noise report either. I wanted to see the profile drawings to see if they matched up with the berms and variable topography along Route 31 between Ames and Half mile but I could not find them in the available reports. Coordination with agencies showed intersection improvements that are not occurring (between Gracie and Ames). How is safety at the Ames Intersection going to be handled as there are roadway deficiencies and safety concerns at these locations? How do the proposed improvements meet the purpose and need? Route 31 through traffic seems well taken care of; however, additional points of conflict will be introduced along Route 31. There are no physical improvements to ped/bicycle facilities besides allowing a ROW setback. The large quantity of bikers that travel down Ames and Gracie to head to Edgewood to access the Prairie Path was not even mentioned in the ped/bike section. I would like a to request a copy of the traffic signal warrant analysis and gap analysis completed for this project. If a gap analysis was not completed, why? That is my main problem trying to turn on Route 31. I have had to routinely wait seven to ten minutes to turn left from Ames Road. Was a traffic simulation completed for the corridor? What will the spacing of the proposed traffic signals be and will that allow for enough gaps for unsignalized minor streets to access Route 31. I am also concerned for winter icy conditions with the raised median. The turning gap looks tight from the drawing I had seen for buses to turn left, let alone my car, especially when the roadway conditions are not good. I have slid multiple times turning left from Ames to Route 31 and had to use the shoulder and or median to gain control going down the hill. Especially scary when so much traffic speeding towards me. Why was the median decision made for a raised median as opposed to mountable? Bus traffic was also not well addressed besides saying that a letter was left unanswered. How about a call to the transportation department. Elementary, Jr High and High School buses access Route 31 and Half mile, Ames and Edgewood on a daily basis delivering kids to and from elementary, middle high and private schools in Crystal Lake and Prairie Grove. If I was included in a mailing list I would have gladly provided the necessary information. With improvements to date there are not enough gaps to allow cars to turn left safely, especially after school hours. I have almost been in three head on collisions while waiting in or entering into the SB LTL on 31 at Ames as people whip around the cars slowing to turn right onto Ames that</p>

are traveling NB. Where is the information on the proposal to change the speed limit from 55 to 45? I would prefer to keep the 55 mph speed limit. Traffic Noise. How were receptors selected? The two in our neighborhood (Oak Groves Subdivision) do not appear to be representative as there is a small berm in one section and none in another. There are trees in one area and none in the other. The road is higher than the adjacent neighborhood in one section and not in the other. Where is the model input and profile drawings? The maps included in the Traffic noise report do not provide contours, pictures, or any helpful information in deciphering the necessary information. The traffic noise reading seems very low, especially if the roadway profile is also being raised. Is there any way to provide a traffic noise wall or berm to help shield the additional traffic noise between Half Mile and Ames? Trucks are really loud going up the hill. Trees. Is it possible to save more of the large diameter trees? Drainage. Sometimes there is ponding at the bottom of the hill near Half Mile Road that causes hydroplaning during large storm events. Will this be addressed in the proposed improvements? ROW. Isn't it possible to take a little more ROW to the west? It appears that there is some room on the TC industries side. Thanks for allowing comment. I respectfully request a response and copies of the traffic gap and signal warrant analysis. I sure hope changes can still be made to address some safety and other concerns. Sincerely, Julie Hoberg

E-mail	julie.hoberg1@gmail.com
FirstName	Julie
LastName	Hoberg
Organization	resident
postal-list-box	on
subject	Submission
Submit	Submit
subscribebox	on

Address	2018 oak drive
City-State-Zip	Mchenry illinois 60050
Comments	Type any comments you have here: Has a plan been brought forth as to what construction would have to be done? I'm concerned for the historic buildings in mchenry.
E-mail	Will.howe96@yahoo.com
FirstName	Michael
LastName	Howe
Organization	Resist
subject	Submission
Submit	Submit

March 28, 2017

Mayor Sue Low
Administrator Derik Morefield
Alderman Victor Santi
Alderwoman Geri Condon

Re: IDOT Route 31 Widening

Ladies and Gentlemen:

I am writing regarding the proposed project with the hope that City representatives can use their influence to induce IDOT to modify their plans.

Enclosed are printouts of pictures of the portion of the project affecting the Schaid property (SW corner of Kane and 31), my property (NW corner of Kane and 31), and the Low property (SW corner of Meadow and 31). As you can see, the current plan effectively involves a taking of all the parking spaces adjacent to Route 31 on all three properties. Properties across from us are also facing a similar problem.

The taking of those spaces will result in a catastrophic economic loss to the owners of these properties. Depending on the particular property, it appears to me the effect could well be a diminution of value of 60 - 80%. This reduction in value will obviously also have a direct impact on tax revenues as the assessed valuation will be impacted by the same percentage.

It seems to me there could be a redesign which would leave that parking intact without altering the fundamental widening of Route 31 itself.

I'm sure I will not be the only owner requesting the City to use its influence to effect modification of the plan.

Yours truly,





35-152-021
N KANE AVE
PARTNERS

778-7777
ATH

33'

27'

09-35-151-014
805 FRONT ST
MCHERRY STATE
BANK

09-35-151-014

223

09-35-151-014
803 FRONT ST
WESTBANK

33'

59'

65' STORAGE

60' STORAGE

38'

60'

60'

56'

47'

42'



Dear Scott Czapilicki and IL Dept of Transportation Bureau of Programming,

I am contacting you to voice my concerns regarding the IL Route 31 (Rt 176 – Rt 120) Preferred Alternative and EA. I am a resident of the Oak Grove subdivision, which is at the corner of Ames Rd and Rt. 31. I attended the recent public hearing and was happy to be able to speak with yourself as well as many other project consultants and representatives so I really could learn more about the plan and attempt to understand aspects of it fully and in greater detail.

First, I want to express my disappointment that our neighborhood HOA was not contacted to be a part of the CAG, however our across Rt 31 neighbor, Terra Cotta Industries was invited. In reviewing the group's notes online, I can see that only a strongly worded letter from TC Industries is represented about the plan, including the sidewalk/multi use path. Essentially our direct "competition" was invited to participate in the process, where as we were not and advocated for the largest impact to be borne by us, their neighbors. What a disadvantage. No group wants the expanding road to solely encroach on their property and this is what is happening to Oak Grove.

Second, the plan is a huge safety concern on several fronts.

1. Ames Rd. intersection: We are not getting a stop light, so to turn left our residents, including many new drivers will have to navigate across a potential multi use path, 2 lanes of traffic and a median. Ames Rd is a 35 mph road, Rt 31 is a 55 mph highway.
2. The multi-use path is a safety concern in and of itself. It seeks to increase the flow of people, strangers, literally in the backyards of several neighbors. There is no privacy or protection from any person using the path. Has a berm or landscaping been considered to offer protection/safety? The location of the path on the East side of Route 31 is also perplexing. One would think that placing the path on the West side of the road would be advisable as this is the side of the Route 31 that the Prairie Path (bike path) runs along. Bikers would be able to get from the Prairie Path to the multi-use path without crossing Rt 31, which would be 4 lanes of traffic plus a median.

Third, I have environmental concerns about the project as a whole. The plan has a significant shift in the water flow as a result of added earth on the east side of 31. Additionally, all the western-most trees will be removed, including those on the berm at the northern most portion of the neighborhood. I have small children, it feels like an example of the Lorax come to life.

Finally, this project will certainly negatively impact the property values of our 82 home neighborhood. Property values are still ailing and have not recovered from the circumstances of 2007/2008 and this project will not help to move things in the right direction. Anything of this nature negatively impacting property values can only hurt our State's already fragile and in my opinion, abysmal, financial condition.

In closing, while I learned, from you, Scott, that there is no funding for this project at this time, I also learned this is the time to speak up. Thank you for the chance to raise my voice in opposition to this project as it stands. I hope my comments, along with the many others I hope you are receiving are thoughtfully considered.

Sincerely,
Emily Kalal

Address	920 Susan Ct
City-State-Zip	60102
Comments	Type any comments you have here: • every effort should be made to not take down trees that are "old growth," i.e., > 100 yrs old, because it will be impossible to have a one-to-one replacement. It is not possible to replace trees that are the same diameter as ones that are larger than even perhaps 6". Old growth oaks are at least 2 to 3 ft across and there is no inventory of trees more than a few years old I'm guessing, nor is it physically possible to replant ones a small fraction of old oak size in any case. • replacements should not be just any oak or hickory but ones native (red, white, burr oak, etc...). to the area, under advisement of local forestry experts. • what precisely are the mitigation plans for disposing of contaminated soils, and for the larger amount of construction waste? I hope that material, for instance , isn't simply dumped out of view into quarries, where they could contaminate ground water. Thanks for planning for bike and pedestrian paths and soliciting public comments. Thanks, Gary Kanner
E-mail	kannergary@gmail.com
FirstName	Gary
LastName	Kanner
Organization	Environmental Defenders of McHenry County
subject	Submission
Submit	Submit

BILL KENNELLY

773-255-0974

BKENNELLY@JAFRATE.COM

- QUESTION ABOUT TIMING OF TRAFFIC SIGNALS
ON RTE 31



Illinois Route 31

ROUTE 176 TO ROUTE 120

www.idot.illinois.gov/projects/il31

Comment Form

Please Print Clearly

Name

ERICA KLINKER

Organization

RESIDENT

Mailing Address

213 S. GREEN ST.

City/State/Zip

MCHENRY, IL 60050

Phone

Email

ERIKAWINKLER880@GMAIL.COM

Please provide your thoughts on the Preferred Alternative and Environmental Assessment (EA).
Add any additional information that you feel should be considered by the Project Study Group:

EVER SINCE BULL VALLEY ROAD / ROUTE 31 INTERSECTION WAS REDONE LAST YEAR WE HAVE NOTICED A SIGNIFICANT INCREASE IN TRAFFIC NOISE FROM OUR HOUSE ON GREEN STREET. WE ARE CONCERNED ABOUT THE AMOUNT OF NOISE THAT WE ARE GOING TO HEAR FROM OUR HOUSE. WE ARE ALSO CONCERNED ABOUT THE AMOUNT OF TRAFFIC THAT WILL MORE THAN LIKELY USE GREEN STREET DURING CONSTRUCTION. PLEASE PROVIDE US WITH A CITY OF MCHENRY CONTACT OR PLAN TO ADDRESS THIS - MORE POLICE RADAR PRESENCE, LOWERING THE ENTIRE GREEN STREET TO 25MPH, INSTALLING FLASHING PEDESTRIAN WALKWAY LIGHTS AT MAJOR POINTS OF CROSSING GREEN STREET (IN FRONT OF KNOX PARK TO TURNBERRY DRIVE). IS THERE ALSO ANY PLANS TO EVALUATE TRAFFIC NOISE IN THE RESIDENTIAL AREAS ALONG THE RTE 31 CORRIDOR? WE LIVE 3/4 OF A MILE FROM RTE 31 AND FEEL THAT THIS IS CLOSE ENOUGH TO BE EVALUATED FOR TRAFFIC NOISE, WHO DO WE WORK WITH TO DISCUSS THIS? CONCERNED ABOUT PROPERTY VALUES BEING IMPACTED BY THIS

If you need more space, please use the reverse side or additional forms. Please place this form in the comment box this evening, or mail it to the Illinois Department of Transportation by March 10, 2017 at the address listed below, or you may scan and email it to the project email address at: info@ILRoute31.com

Address	PO Box 501
City-State-Zip	Crystal Lake, IL 60039
Comments	Type any comments you have here: The Oak Grove Subdivision HOA would like the State to reconsider installation of sound barriers adjacent to our neighborhood. Not only will such barriers increase the safety of our neighbors (especially those upon whom the project will directly impact their property), a sound barrier might lessen the negative impact on our neighborhood\'s property values. We take issue with the sound level study results. We think the study was flawed and does not reflect the probable increase in traffic or the effect of moving the traffic 80 feet closer to our neighborhood. We also take umbrage with the lack of interest in safety at the intersection of Ames and Rt 31 or for the safety of our neighbors who\'s property will be directly affected by this expansion. Thank you for allowing us to subscribe to an email list. Perhaps now we will be informed regarding updates to a project that will have significant affect on our neighborhood, property values and quality of life.
E-mail	bod@oakgrovesubdivision.com
FirstName	John
LastName	Koss
Organization	Oak Grove Subdivision HOA
postal-list-box	on
subject	Submission
Submit	Submit
subscribebox	on

Address	3315 thunderbird lane
City-State-Zip	prairie grove, il. 60012
Comments	Type any comments you have here: from the oak grove subdivision...resident since 2004. ames and rt 31. morning work traffic and from 3pm to 5pm turning left from ames onto rt 31 is most dangerous time to find an opening. many school buses do hold up traffic onto rt 31 from ames. could they turn right instead is??? even slowing to make a right turn onto ames from rt 31 you need to wonder if the guy behind you won't rear end you instead. should have added a right turn lane onto ames from rt 31 when rt 31 road surface was improved with only left turn lanes. in 2004 it was said someday possible a traffic light at edgewood would at least slow down the traffic. and a possible rerouting of ames to edgewood would be a relief., ending ames road at lakewood road oak grove subdivision entrance. yet rerouting ames to edgewood would sadly eliminate many oak trees and pines in that area. too bad for them. more oak trees disappearing again and tree berms along rt31 oak grove subdivision.
E-mail	bkurth1951@gmail.com
FirstName	robert
LastName	kurth
Organization	oak grove subdivision
subject	Submission
Submit	Submit
subscribebox	on

Address	
City-State-Zip	McHenry
Comments	<p>a. Based off the D&E 18 policy, it requires that trees removed be replaced by equally functional trees...it is my hope that the IDOT replaces oak trees with oak trees and hickory trees with hickory trees as it is known that a large percentage of our oak canopy has been lost over the hundred years. I would not want progress and safety to our roads to contribute to the oak canopy loss. This is why replacing oak tree removals with oak tree replacements important</p> <p>b. Not only is the quantity of oak and hickory trees removal important to replace, but also the replacement of the trees\' caliber. It is my hope that a 12 inch diameter oak is replaces with six, two-inch caliber, or however wide the removed tree is. c. We highly encourage the implementation of the points 3.e.2.b where replacement trees are planted as practical to the removal site as possible. If citizen mobilization needs to happen, let the Environmental Defenders of McHenry know. d. How can we track the commitment and implementation of the D&E-18 Policy? e. Of those 1.53 acres of wetlands impacted, how many acres will be eliminated? Be specific about the impacts. We want to make sure that these impacts do not negatively affect Silver Creek Watershed. f. What is the difference between floodways and floodplains? g. We encourage planting native, deeply rooted grasses in the median strips. What would take to make this happen? h. When the multipurpose and sidewalks are established and become a functional attribute communities far and near, like the Fountains, can utilize these for exercise and connection to the businesses to the south</p>
E-mail	ckustral11@gmail.com
FirstName	Christine
LastName	Kustra
Organization	Environmental Defenders of McHenry County
subject	Submission
Submit	Submit

[External] RE: IL 31 Public Hearing

 DELETE
  REPLY
  REPLY ALL
  FORWARD
  ...



Kathy Martinez <kathym@tcindustries.com>

Mark as unread

Fri 2/10/2017 6:12 AM

To: Czaplicki, Scott D;

Cc: thomaszhayward@gmail.com;

[Enterprise Vault](#)

[Bing Maps](#)

[+](#) Get more apps

Good morning, Scott.

Thank you for your time Wednesday evening at the Route 31 Open House Public Hearing. As discussed, Terra Cotta Realty Co. will be submitting two (2) separate Cross Access Easement Documents for the locations identified by IDOT as depicted on the exhibits that we reviewed. I understand John Swierk has requested copies of the Exhibit relating to the access point at Ames Road that is of interest to his firm. I would request that you provide me with copies of the exhibits for both access points for Terra Cotta Realty Co.

Upon receipt, I will review with Mr. Hayward and contact you with any questions. I am interested in completing the review and returning executed Easement Documents to you as quickly as possible.

If you have any questions, please don't hesitate to contact me at 815-333-8235. Thank you.

Kathleen M. Martinez
 General Manager
 Terra Cotta Realty Co.
 3703 S. Route 31
 Crystal Lake, IL 60012
 815-333-8235 - phone

-----Original Appointment-----

From: Czaplicki, Scott D [mailto:Scott.Czaplicki@illinois.gov]

Sent: Monday, January 23, 2017 9:32 AM

To: Kathy Martinez; Rosemary Swierk; 'ewitowski@yahoo.com'; 'sellcommercial@yahoo.com';
 'vsmith@mchenry.edu'; 'ujhicks@juno.com'; 'cjones@mchenry.edu'; 'hburnap@wi.rr.com';
 'Route31auto@yahoo.com'; 'jorthowell@yahoo.com'; 'Eberhard.Veit@eisenmann.com';
 'mcdef@owc.net'; 'bebomoore@aol.com'; Doug Martin; Steve Carruthers; 'Shawn_Cirton@fws.gov';
 'Kathy.G.Chernich@usace.army.mil'; 'Soren.G.Hall@usace.army.mil'; 'Hiway@NundaTownship.com';
 'jjosborn@co.mchenry.il.us'; 'ammiller@co.mchenry.il.us'; 'bdgraham@co.mchenry.il.us';
 'cldaigle@co.mchenry.il.us'; 'w.busse@firstmchenry.com'; 'glenn.richmond@lennar.com';
 'emaxwell@crystallake.org'; 'leejennings@nundatownship.com'; 'pelloso.elizabeth@epa.gov';
 'vsiler@mccdDistrict.org'; 'super@nundatownship.com'; 'sbuchtel@me.com'; 'patrock6@comcast.net';
 'kkrueger@crystallake.org'; 'Gene Potempa'; 'clhorton@co.mchenry.il.us'; 'dthompson@cmap.gov';
 'ddreher@geosyntec.com'; 'buroak@owc.net'; 'cindy.skrukrud@sierraclub.org';
 'mccd@mccdDistrict.org'; 'lhaderlein@conservemc.org'; 'danderson@openlands.org';
 'warrenflh@aol.com'; 'sahennings@co.mchenry.il.us'; 'dthompson@cmap.illinois.gov'; 'Abigail

From: [Kim Minor](#)
To: ["Jason J. Fluhr"; Czaplicki, Scott D](#)
Cc: jsinger@prairiegrove.org; [Village President Stan Duda](#)
Subject: [External] RE: IL Route 31 widening
Date: Wednesday, March 22, 2017 11:31:16 AM
Attachments: [image003.png](#)

Good Morning All,

Dennis Marunde is the owner of Arvidson & Sons, Inc. located at 3209 S. Rt. 31, Prairie Grove, IL 60014. He can be reached at 815-459-0660 or via email dennism@arvidsons.com

He would like to discuss these concerns with our Engineer.

Thank you Jason.

Kim Minor

Kim Minor, RMC, CMC
Village Clerk



Village of Prairie Grove
3125 Barreville Road
Prairie Grove, IL 60012
PH (815) 455-1411
FX (815) 455-0783
www.prairiegrove.org

Email: kminor@prairiegrove.org

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represent those of the Village.

From: Jason J. Fluhr [mailto:jfluhr@baxterwoodman.com]

Sent: Wednesday, March 22, 2017 11:04 AM

To: scott.czaplicki@illinois.gov

Cc: kminor@prairiegrove.org; jsinger@prairiegrove.org

Subject: IL Route 31 widening

Scott – we are the Village of Prairie Grove’s engineer. A business owner along IL 31 has expressed concerns about access restrictions to his business, Arvidson Pools and Spas, as part of the IL 31 widening project. He acknowledges that he missed the public hearing and hope it’s not too late to influence the design. Mr. Arvidson’s concern is that northbound trucks making deliveries to his business will not be able to access his property very easily. Would IDOT entertain an opening in the median to make a left turn into his driveway? If not, it appears that IL 31 needs to be widened at Edgewood Road in order to accommodate northbound truck U-turns so the trucks can access his and the other businesses on the west side of IL 31.

Also, the Village is concerned about the safety of westbound vehicles on Ames Road making left turns onto IL 31. Its already difficult to make that turn because of the heavy traffic on IL 31 and sight limitations caused by the hill to the north, and widening IL 31 will make it more difficult. Did IDOT evaluate other alternatives at Ames, such as realigning it with Edgewood Road?

Thank you,
Jason



main: 815.459.1260 | direct: (815) 444-3222

email:jfluhr@baxterwoodman.com

www.baxterwoodman.com

8678 Ridgefield Rd., Crystal Lake, IL 60012

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Address	3209 S IL ROUTE 31
City-State-Zip	Crystal Lake, IL 60012
Comments	<p>To Whom it may concern: Only in the last several weeks have I become aware of the plans for the widening of IL Rt. 31 in front of our business. I am writing to raise my great concern over the plan as presented to me a short while ago. The plan as it stands today (to my knowledge) would eliminate the ability of semi-trucks to enter our property and there are no practical alternatives that exist short of a modest revision to the plan. I have met with the Village Board of Prairie Grove, my (commercial) neighbors, and my US Representative (Hon Randy Hultgren). They are all in agreement that my concerns are valid and they are willing to support the only viable and practical solution we can identify. Next I will be contacting my state representative, state senator, and my municipal authorities to bring this matter to their attention and seek their support in fixing the current plan which will absolutely threaten our company's ability to continue doing business from this facility. Please contact me asap to discuss this problem. We have already voiced our concerns to one IDOT official who was present at a public meeting earlier this spring. I was out of town on business and not able to attend that meeting on my own, but one of my partners was there and verbalized our deep and urgent concern about the plan. Looking forward to hearing from someone asap. Sincerely, Dennis Marunde President Ben Arvidson & Sons, Inc dba- Arvidson Pools & Spas 815-861-6996</p>
E-mail	DennisM@Arvidsons.com
FirstName	Dennis
LastName	Marunde
Organization	Arvidson, Inc
postal-list-box	on
subject	Submission
Submit	Submit
subscribebox	on

* Internal Tickle

#1704 *

Due = 3/22/17

A = 3/1/17



McHenry County
CONSERVATION DISTRICT

JAB
L KKM
BUREAU OF PROGRAMMING
RECEIVED

FEB 24 2017

DISTRICT #1

February 22, 2017

Scott Czaplicki, Bureau of Programming
Illinois Department of Transportation
201 West Center Court
Schaumburg, IL 60196-1096

RE: Illinois Route 31
Route 176 to Route 120

Dear Mr. Czaplicki:

Thank you for the opportunity to provide comment on the above project. The McHenry County Conservation District does not have any property directly impacted by the proposed project as presented at the February 8, 2017 public hearing. Several staff members of the District attended and have provided the following comments.

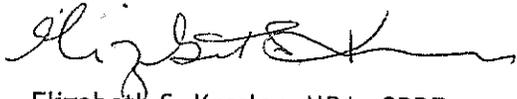
First, it should be noted that the McHenry County Conservation District's existing 26-mile linear multiuse Prairie Trail runs parallel to the proposed shared use path and is separated by a distance of only 400-600 feet within the City of McHenry and does not show a connection between the two trails. A portion of the proposed shared use trail would be a duplication of services and it is our understanding that the proposed shared use trail is not included in the state funding request. The cost of construction and ongoing maintenance would fall to the local governments. The District is not interested in participating in this part of the project at this juncture but would encourage the placement of signage and on-street routes to direct users to the existing multiuse trail to safely traverse north and south through the county and to the western businesses within the City of McHenry.

There is concern that the 84 Oak trees and 17 Hickory trees would be impacted by the current alignment and that some of these critical local species could be avoided with a shifting of the project either to the east or west. If such a move is unavoidable, several nearby Conservation Areas (Stickney Run and Silver Creek) could benefit from replacement trees which may be required to mitigate such an impact.

The District may also be able to assist in the anticipated wetland mitigation measures by providing off-site local wetland improvements within the Regional watershed. The District will continue to stay informed on this project as it moves into Phase II.

If you have any questions or we can be of further assistance, please do not hesitate to give us a call.

Sincerely,
McHENRY COUNTY CONSERVATION DISTRICT



Elizabeth S. Kessler, MBA, CPRE
Executive Director

- c:
- Bona Heinsohn, President, Board of Trustees
 - Stephen Barrett, Vice President
 - Brandon Thomas, Treasurer
 - Dave Kranz, Secretary
 - Pete Merkel, Trustee
 - Vern Scacci, Trustee
 - Dave Brandt, Trustee
 - Robert Nowak, Liaison, McHenry County Board
 - Ed Collins, Director of Land Preservation & Natural Resources
 - John Kremer, Director of Operations & Public Safety
 - Val Siler, Land Preservation Manager
 - Amy Peters, Planning Manager



McHenry County Council of Governments

BUREAU OF PROGRAMMING
RECEIVED

FEB 10 2017

DISTRICT #1

January 31, 2017

Mr. Scott Czaplicki
Illinois Department of Transportation
201 W. Center Court
Schaumburg, IL 60196-1096

Dear Mr. Czaplicki:

Thank you for providing the opportunity to comment on the IL 31 Project from IL 176 in the City of Crystal Lake to IL 120 in the City of McHenry in McHenry County. This is an important project for McHenry County and will greatly benefit the residents and commuters in McHenry County.

For the past several years, the McHenry County Council of Governments (MCCG) has included the IL 31 project on their list of transportation priority projects. The list is approved by our full membership (29 municipal and township members) in January and used in meetings with legislators, state officials and local elected officials.

We are excited to see that the project continues to move forward and that Phase I Engineering and the Environmental Assessment have been completed. In addition we are pleased that Phase II Engineering is programmed in the FY 2017-2022 Proposed Highway Improvement Program. The MCCG will continue to support this project through completion and look forward to working with IDOT and our municipal members along the corridor.

Enclosed with this letter is a copy of our 2017 Legislative and Transportation Priorities. Please feel free to contact me with any questions.

Sincerely,

Chalen Daigle
Executive Director

Executive Committee

President Rick Mack
Village of Ringwood
MCCG President

Mayor Mark Kownick
Village of Cary
MCCG Vice-President

Mayor Charles Sass
Village of Huntley
MCCG Treasurer

Mayor Donald Lockhart
City of Marengo
MCCG Secretary

Supervisor Craig Adams
McHenry Township
Chairman of the
Finance Committee

President Peter Koenig
Village of Richmond
Chairman of the
Legislative Committee

President John Schmitt
Village of Algonquin
Chairman of the
Transportation Committee

President Terry Counley
Village of McCullom Lake
Chairman of the Water Policy
Task Force

President Robert Nunamaker
Village of Fox River Grove
Chairman of the
Mayors Caucus

Chairman Jack Franks
McHenry County Board
Ex-Officio Member

Chalen Daigle
Executive Director
44 N. Virginia
Suite 2-A
Crystal Lake, IL 60014
815-477-2090 (p)
847-767-0440 (c)
cdaigle@mchenrycountycog.org
www.mchenrycountycog.org

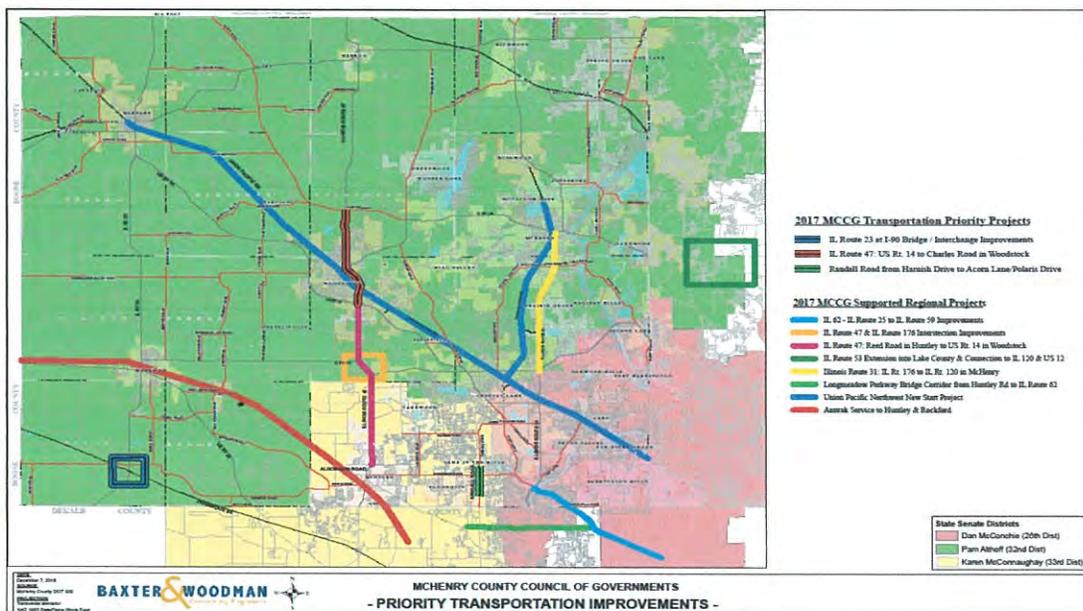
2017 MCCG Transportation Priority List

Key Corridor Projects

- *IL 47 from US 14 to Charles Road in Woodstock*
- *Randall Road from Harnish Drive to Acorn Lane/Polaris Drive*
- *IL Route 23 at I-90 Bridge/Interchange Improvements*
- *IL 47 and IL 176 Intersection Improvements*

Regional Projects for which the MCCG Supports

- *IL 31 from IL 176 in Crystal Lake to IL 120 in McHenry*
- *IL 47 from Reed Road in Huntley to US 14 in Woodstock*
- *Union Pacific Northwest Line New Start Projects*
- *Longmeadow Parkway*
- *IL 53 Extension into Lake County and connection to IL 120 and US 12 with the addition of an IL 31 Connector project that would help ease possible congestion in Lakemoor, McHenry, Ringwood, Johnsburg and Richmond.*
- *IL 62 Phase I Study in Barrington Hills*
- *Amtrak Service to Huntley and Rockford*



From: [Benjamin Redding](#)
To: [Czaplicki, Scott D](#); [Murphy, Kimberly K.](#)
Subject: [External] Illinois Route 31 - Public Hearing
Date: Friday, February 10, 2017 12:14:59 PM
Attachments: [image001.png](#)

Scott/Kimberly,

Thank you for moving the Illinois Route 31 project to the Public Hearing stage on Wednesday February 8, 2017. The County is excited to see this project move forward as it is the third highest priority project in the County, behind the two Illinois Route 47 projects.

We look forward to seeing this project progress quickly through the final stages of Phase I and so forth.

Best Regards,

Benjamin A Redding, P.E., ENV SP, LEED Green Associate
Design Manager



McHenry County Division of Transportation

16111 Nelson Road, Woodstock, IL 60098

Direct: 815.334.4980 | Cell: 815.482.0358 | Main: 815.334.4960 | Fax: 815.334.4989

baredding@co.mchenry.il.us | www.McHenryCountyDOT.org



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Address	3610 Thunderbird Ln
City-State-Zip	Prairie Grove
Comments	I live in the Oak Grove Subdivision that borders along Route 31 in Prairie Grove. I would like to see a sound barrier built between the expanded road and private property as there is a strong danger of terrible accidents happening into yards. And the noise pollution lowers the value of our homes. Plus there should be no bike/walking trail on the east side of the road. Plus, the master plan for Prairie Grove called for a cul-de-sac at Ames and 31. Having to make a U-Turn to go South is unacceptable. How are the school buses going to make that turn? Let's rethink this whole thing please??
E-mail	bill@stopngo.com
FirstName	Bill
LastName	Merriman
Organization	
postal-list-box	on
subject	Submission
Submit	Submit
subscribebox	on



Illinois Route 31

ROUTE 176 TO ROUTE 120

www.idot.illinois.gov/projects/il31

Comment Form

Please Print Clearly

Name Jon Meyer

Organization _____

Mailing Address 6020 Tomlinson Dr.

City/State/Zip Mt Henry, IL 60050

Phone 815-363-1880 Email jjmeyer77@comcast.net

Please provide your thoughts on the Preferred Alternative and Environmental Assessment (EA).
Add any additional information that you feel should be considered by the Project Study Group:

For 2 years, Mt Henry businesses on Rt 31 South, suffered due to construction. Some had to close due to medians prohibiting travelers going in one direction or another turning into the business. Now, under this proposal, we once again are going to impact Rt 31 South businesses, again + my best guess, is force some more out of business due to medians & turning restrictions. I truly believe the need to widen the road is necessary, but IDOT should work with businesses to come up with solutions that meet the traffic needs while avoiding the cause of business closure.

If you need more space, please use the reverse side or additional forms. Please place this form in the comment box this evening, or mail it to the Illinois Department of Transportation by March 10, 2017 at the address listed below, or you may scan and email it to the project email address at: info@ILRoute31.com

Illinois Department of Transportation
Attn: Bureau of Programming, Scott Czaplicki
201 W. Center Court
Schaumburg, IL 60196-1096

Address	4802 Patty Ln
City-State-Zip	Ringwood,IL 60072
Comments	I would like to make a suggestion to add a right turn lane to turn east on Ames Rd from northbound Route 31. Also a left turn lane if you are heading southbound. A concern I have about the light at Route 31 & Half Mile Rd is for southbound truck traffic and them having to stop after coming down that hill.
E-mail	gtp302@hotmail.com
FirstName	Tracy
LastName	Morey
Organization	
subject	Submission
Submit	Submit
subscribebox	on

From: [Nicole Mullins](#)
To: [Czaplicki, Scott D](#)
Subject: [External] Oak Grove
Date: Thursday, March 02, 2017 7:31:26 AM

Dear Mr. Czaplicki,

I live at 3403 Lakewood in the Oak Grove neighborhood and I share all of the same concerns my neighbors have regarding the Rt. 31 expansion and its impact on my neighborhood. Some concerns are, improper sound testing, use of HOA land, loss of 100 year old living oaks, decrease in home value, corporate influence...

The list goes on and on, but I ask you, Mr. Czaplicki, how would you have me respond when I am driving with my 4 year old every morning trying to take a left from Ames on to Rt. 31 and she looks at and occasionally asks, "Mommy, why is there ribbons and flowers tied around that tree." ?????

Think about it...
Nicole Mullins

Sent from my iPhone

Address	1702 Cashel Ln
City-State-Zip	McHenry IL 60050
Comments	<p>I just found this website after 10 years of wondering when a traffic signal would be placed at the intersection of Route 31 and Veterans Parkway. Traffic on Rt31 has been increasing over the past 10 years that I have lived off of Veterans Parkway. I have to go south on Rt31 every morning for work and it has become increasingly difficult and dangerous to do so with all the traffic. I like the proposed improvements you have for Rt 31 make it a 4 lane highway from Rt 176 to Rt 120 however with the proposed medians you might place on Rt31 at Veterans parkway it will make it more difficult and DANGEROUS to turn left onto Rt 31 to go to work. I hope that this is something that you will research before approving the project and add the traffic signal at the intersection of Rt 31 and Veterans Parkway.</p>
E-mail	krackerfamily@gmail.com
FirstName	Kimberly
LastName	Nagel
Organization	Homeowner
postal-list-box	on
subject	Submission
Submit	Submit



Illinois Route 31

ROUTE 176 TO ROUTE 120

www.idot.illinois.gov/projects/il31

Comment Form

Please Print Clearly

Name *Tina Napolitano*

Organization

Mailing Address *1373 New Haven Dr.*

City/State/Zip *Cary IL 60013*

Phone *847.997.9929* Email *tstorin@aol.com*

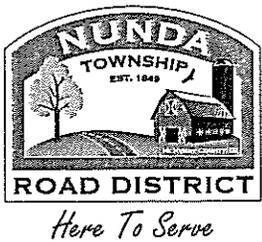
Please provide your thoughts on the Preferred Alternative and Environmental Assessment (EA).
Add any additional information that you feel should be considered by the Project Study Group:

I am very ANXIOUS & excited for this project to come to fruition! A 2-lane highway is just NOT SAFE for such a well-traveled road.

Thank you for holding this hearing for everyone to see & discuss the plans.

If you need more space, please use the reverse side or additional forms. Please place this form in the comment box this evening, or mail it to the Illinois Department of Transportation by March 10, 2017 at the address listed below, or you may scan and email it to the project email address at: info@ILRoute31.com

Address	
City-State-Zip	Crystal Lake,IL60014
Comments	1st off regarding Rt. 31 extension of road changes, I would like to say YOU IDOT NEED TO TO THIS WITHIN A SMALL WINDOW OF TIME, NOT YEARS! I HAVE BEEN TO OTHER STATES WHERE THEY HAVE PULLED UP AND LAID ROAD BACK IN 2 DAYS! AND ITS MILES OF ROAD INCLUDING STRIPPING SO I CANNOT SEE ANY EXCUSE WHY YOU CANNOT DO THE SAME! ORGANIZATION NOT LAZY IS WHAT IS NEEDED!
E-mail	michelenelson@pwainc.net
FirstName	M
LastName	Nelson
Organization	
subject	Submission
Submit	Submit



MIKE LESPERANCE
HIGHWAY COMMISSIONER

February 15, 2017

BUREAU OF PROGRAMMING
RECEIVED

Mr. Jose Rios, P.E.
Illinois Dept. of Transportation
Office of Highways Project Implementation / Region 1 / District 1
201 West Center Court
Schaumburg, Illinois 60196-1096

FEB 23 2017

DISTRICT #1

Dear Mr. Rios,

As the Nunda Township Highway Commissioner looking at the overall project improvement of Route 31 from Route 176 to Route 120, I think it is long overdue, much needed and very much appreciated.

As an elected official and concerned citizen I would like to request some changes to the plans.

First, I would like to request retaining walls / sound barriers from Drake Drive Latitude and Longitude 42.260286, -88.286761 to L.L. 42.265039, -88.286778 and from 42.275065, -88.286846 to Ames Road 42.281133, -88.286859. In these two areas there are many residents with very rural settings. On their properties there exist many very old growth and healthy Oak trees. In my opinion, rather than removing them and cutting a steep slope, we could take less land and extend the retaining walls above grade to create a sound barrier.

Second, Ames Road should be re-aligned to Edgewood Road in order to allow all of the residents of Prairie Grove to turn left at the stop light to go to Crystal Lake.

It seems to me now is the time to address this issue while we are already doing a \$75 million dollar improvement. The danger and time, especially in turning left or turning right with a U-turn, could be easily rectified either with a frontage road or an S curve on Ames Road.

I offer as much time and energy as it will take to see these suggestions come to pass. Please call on me for any assistance you may need.

Sincerely,

NUNDA TOWNSHIP ROAD DISTRICT

Mike Lesperance
Highway Commissioner

cc: Scott Czaplic



From the Bureau of : Programming

Name:	<u>Mike Lesperance</u>	Date:	<u>February 10, 2017</u>
Title:	<u>Highway Commissioner</u>		
Organization:	<u>Nunda Township Road District</u>	Project:	<u>IL 31</u>
Address:	<u>3518 Bay Road</u>		<u>IL 176 to IL 120</u>
	<u>Crystal Lake, IL 60012</u>		<u>P-91-135-99</u>
Telephone:	<u>(815) 459-4410</u>		
E-Mail:	<u>highway@nundaroaddistrict.com</u>	PTB No.:	<u>155-015 & 170-007</u>

Items Discussed:

Mr. Lesperance requested the Department consider a frontage road that connects Ames Road to a new east leg at Edgewood Road

Follow-Up:

Evaluate and respond to request.

By: Scott Czaplicki
Bureau of Programming/Consultant

From: president@oakgrovesubdivision.com
To: [Czaplicki, Scott D](#)
Cc: [Bod; Murphy, Kimberly K.](#); [Brown, Lori S.](#)
Subject: [External] Re: IL 31; Oak Grove HOA Follow-Up
Date: Wednesday, March 01, 2017 12:39:47 PM

Scott,

Thank you very much for your response. There is much concern in our neighborhood. Your intention to re-accomplish a traffic count will come as welcome news.

Three schools send busses into our neighborhood daily: Prairie Ridge High School, Hannah-Beardsley Middle School, and Hussman K-5. All three schools' bus routes use Ames road both east and west.

I would ask that the State consider re-accomplishing a noise study during the hours of 6:00 AM to 9:00 AM and 3:00 PM to 6:00 PM. Empirically speaking, these are the worst times of day along our stretch of Rt 31. I think the previous study was accomplished between the hours of 10:00 AM and 4:00 PM. Traffic is very light during that period.

Thank you for reaching out to me. I will let the neighbors know their concerns are not falling on deaf ears.

Respectfully,

Mary Koss

Mary Koss
President
Oak Grove Subdivision HOA

On 2017-03-01 12:18, Czaplicki, Scott D wrote:

Mary,

Thank you for all the comments submitted by the Oak Grove subdivision. The comment period is still open, but we want to give you an update that we are starting to evaluate the subdivision's concerns noted below. This is not an all-inclusive list and we intend to address all comments received. We'll keep you posted on our findings. In regards to Item #3, we will perform updated traffic counts at Ames Road. Several residents questioned if the previous traffic counts were taken on school days and included buses along Ames Road. Could you let me know which schools the subdivision is concerned with so we can plan the traffic count accordingly? Thank you.

. Reduce right-of-way needs and impacts to HOA property

. Evaluate a traffic noise abatement wall

. Evaluate warrants for a traffic signal at Ames Road

Scott Czaplicki, P.E.

From: president@oakgrovesubdivision.com
To: [Czaplicki, Scott D](#)
Cc: [Bod; Murphy, Kimberly K.; Brown, Lori S.](#)
Subject: RE: [External] Re: IL 31; Oak Grove HOA Follow-Up
Date: Wednesday, March 01, 2017 3:47:41 PM

Thanks very much for scheduling that so quickly. I also checked the spring break calendars. I mis-read your email so I was going to tell you exactly what you sent.

Any chance you can order another sound study? If I recall correctly, the location of the sound monitors were not adjacent to our neighborhood - one was north and one was south. I seem to recall the monitor nearest our neighborhood was inoperative. Also, I seem to recall the times of day of the study as somewhat onerous. Can you verify that data? I read through the big book weeks ago, I wish I had taken better notes.

Thank you,

Mary

Mary Koss
President
Oak Grove Subdivision HOA

On 2017-03-01 14:57, Czaplicki, Scott D wrote:

Mary,

Thank you for the quick reply. I checked the school calendars and all have spring break the last week of March. We've requested the traffic counts be performed before then. I didn't see any other scheduled days off in March.

Scott Czaplicki, P.E.

(847) 705-4678

scott.czaplicki@illinois.gov

.

 Please consider the environment before printing this message or attachments

From: president@oakgrovesubdivision.com [mailto:president@oakgrovesubdivision.com]
Sent: Wednesday, March 01, 2017 2:43 PM
To: Czaplicki, Scott D
Cc: Bod; Murphy, Kimberly K.; Brown, Lori S.
Subject: [External] Re: IL 31; Oak Grove HOA Follow-Up

Scott,

A neighbor brought to my attention there are two other schools that use Ames on a regular basis: Prairie Grove Elementary and St Thomas. Our neighborhood uses Crystal Lake schools, so I was only aware of the three I mentioned previously.

Regards,

Mary Koss

Mary Koss
President
Oak Grove Subdivision HOA

On 2017-03-01 12:18, Czaplicki, Scott D wrote:

Mary,

Thank you for all the comments submitted by the Oak Grove subdivision. The comment period is still open, but we want to give you an update that we are starting to evaluate the subdivision's concerns noted below. This is not an all-inclusive list and we intend to address all comments received. We'll keep you posted on our findings. In regards to Item #3, we will perform updated traffic counts at Ames Road. Several residents questioned if the previous traffic counts were taken on school days and included buses along Ames Road. Could you let me know which schools the subdivision is concerned with so we can plan the traffic count accordingly? Thank you.

- 1 Reduce right-of-way needs and impacts to HOA property
- 2 Evaluate a traffic noise abatement wall
- 3 Evaluate warrants for a traffic signal at Ames Road

Scott Czaplicki, P.E.

(847) 705-4678

scott.czaplicki@illinois.gov



Please consider the environment before printing this message or attachments

From: president@oakgrovesubdivision.com [<mailto:president@oakgrovesubdivision.com>]

Sent: Saturday, February 11, 2017 12:00 AM
To: Czaplicki, Scott D
Cc: Bod
Subject: Re: [External] Rt 31 Construction

Hello Scott,

I have attached my prepared comments.

I must tell you, from the Home Owners Association perspective, the failure to include us in the Community Awareness Group allowed TC Industries to unfairly sway the decision regarding the "preferred design" (I'm referencing the strongly worded letter written by Ms. Martinez in 2014). Had we been given a voice, we would have fervently argued for a different plan. The plan as presented encroaches on HOA deeded property along the entire length of the neighborhood. Our visual and sound barriers are gone. Our neighbors' back yards are shamefully bared to Rt 31 traffic. Property values in the entire subdivision will be negatively affected - not just those looking over Rt 31.

All that vice juniper trees on TCI's commercial/industrial property.

Not surprisingly, there have been many calls for action, both on an individual basis as well as formal response from the Association. I imagine you will be hearing a lot from the Oak Grove neighborhood.

Respectfully,

Mary Koss

Mary Koss
President
Oak Grove Subdivision HOA

On 2017-02-10 16:50, Czaplicki, Scott D wrote:

Mary,

I wasn't able to attend the public forum, but I believe you may have read a prepared statement. The statements were recorded by a court reporter, but we won't receive the transcripts for a couple weeks. If possible, could you forward a copy of the prepared statement? Thank you.

Scott Czaplicki, P.E.

(847) 705-4107

scott.czaplicki@illinois.gov



Please consider the environment before printing this message or attachments

From: president@oakgrovesubdivision.com <president@oakgrovesubdivision.com>

Sent: Thursday, February 9, 2017 8:05 PM

To: Czaplicki, Scott D

Cc: Bod

Subject: [External] Rt 31 Construction

Scott,

It was a pleasure to meet you last night at the hearing.

Please add not only this email address but also BOD@oakgrovesubdivision.com to your database for future correspondence regarding the Rt 31 construction.

Clearly the neighborhood is rattled. Any further information you can provide to calm fears or raise awareness would be appreciated.

Regards,

Mary

--

Mary Koss
President
Oak Grove Subdivision HOA

State of Illinois - CONFIDENTIALITY NOTICE: The information contained in this communication is confidential, may be attorney-client privileged or attorney work product, may constitute inside information or internal deliberative staff communication, and is intended only for the use of the addressee. Unauthorized use, disclosure or copying of this communication or any part thereof is strictly prohibited and may be unlawful. If you have received this communication in error, please notify the sender immediately by return e-mail and destroy this communication and all copies thereof, including all attachments. Receipt by an unintended recipient does not waive attorney-client privilege, attorney work product privilege, or any other exemption from disclosure.

Address	4520 Carthage Ct
City-State-Zip	Crystal Lake, IL 60012
Comments	<p>Two Comments to voice the two major concerns I have with this project; Increased Noise Pollution and mitigating the negative Environmental impact from Drake Dr. to Half Mile Road...The North facing Hill. As you know, this area has many heritage oaks that will need to be removed in order to widen RT 31. As a licensed Landscape Architect in the State of Illinois, I would welcome the opportunity to meet with your Landscape Architects to review the plans for restoring this area as part of your project. I understand the need to widen the road... (though if "safety" start by lowering the speed limit to 45 on the hill)...but am hopeful that your team will see the need to replace the inches lost at a minimum of 1 for 1. In addition, the understory and seed matrix should support the wildlife that rely on this plant community. We have Fox, Coyote, Deer, Owls, Tree Frogs, Toads, Skunks, Opossum, Raccoons, Hawks, Hummingbird and Bats to name a few. Please plan and plant to support this diverse habitat.</p> <p>Secondly; Noise. I spoke with an Engineer on 2-8-17 that stated you conducted a decibel level survey and determined a sound wall was not necessary. I believe he stated a 69 reading was recorded (averaged?), and the minimum requirement for a wall is 70. As a resident, I can tell you the road is already very loud. Especially In the winter months or when the pavement is wet. I downloaded a free meter and it peaks at 87 dB nearly every time I stand at the corner of my drive @ 220 ft. away. Higher on the hill, where the road falls below the embankment, I would imagine it may be @ 70, but when the grade falls off, and the road is higher than the surrounding grade (@ my property) it is, as stated, very loud. The roadway improvements will add cars and increase noise. I ask that you reconsider a sound wall where the road is higher than the surrounding grade. Thank You Jon Olmsted, PLA</p>
E-mail	j_olmsted@comcast.net
FirstName	Jon
LastName	Olmsted
Organization	Homeowner
postal-list-box	on
subject	Submission
Submit	Submit
subscribebox	on

[External] RE: Route 31 Planning - Pacini / Ebel Farm

✖ DELETE ← REPLY ⇐ REPLY ALL → FORWARD ⋮



PAUL PACINI <pacinigroup@yahoo.com>

Mark as unread

Fri 2/10/2017 11:32 AM

To: info@ilroute31.com;

Cc: Doug Martin <dmartin@ci.mchenry.il.us>;

📎 1 attachment

Ebel Site
P~.PDF

Il Dept of Transportation,

I received documentation regarding plans to widen and make changes to Route 31 from Rt 176 to Rt 120. I would like to submit the attached documents and my contact information. If there is any detail that can be provided regarding changes to the roadway effecting this parcel, I would greatly appreciate it. There had been several requests in the past from both the County and the City with regards to maintaining the opportunity to access this site for a future East /West Route. I would like to know if there is still any interest in maintaining that option. It is also important to clarify the preservation of the current access to this site from Route 31.

Thank You

Paul Pacini
312-810-8894
pacinigroup@yahoo.com



Illinois Route 31

ROUTE 176 TO ROUTE 120

www.idot.illinois.gov/projects/il31

Comment Form

Please Print Clearly

Name Jeff Pittman

Organization The Chapel

Mailing Address 1809 S. IL 31

City/State/Zip McHenry IL 60050

Phone 262-442-0362 Email jpittman@chapel.org

Please provide your thoughts on the Preferred Alternative and Environmental Assessment (EA).
Add any additional information that you feel should be considered by the Project Study Group:

1) Our property is at 1809 S. IL 31 in McHenry. We are interested in gaining cross access with the adjoining property north of us owned by Waste Management. Most of our congregation is from communities north of the property. Specifically on Sunday mornings, having access to go north from our property is critical.

2) The current plans call for eliminating two of our three access points. Since we will be adding a driveway on the south side of our building, we'd like to keep the southern most access for a right only exit. Our plans include a one-way drive on the north of our building, the majority of parking in the back or west of building, and a one-way drive on the south of the building. With two access points, we can have dedicated entrances and exits. Having two access points will provided safer conditions for our congregations to the building.

If you need more space, please use the reverse side or additional forms. Please place this form in the comment box this evening, or mail it to the Illinois Department of Transportation by March 10, 2017 at the address listed below, or you may scan and email it to the project email address at: info@ILRoute31.com

Address	3515 Thunderbird Ln.
City-State-Zip	Prairie Grove
Comments	<p>I am deeply concerned with the Mchenry County Rte. 31 expansion plan. The DANGER of getting out of our neighborhood from Ames onto 31 with no plan for a light is my biggest concern! Here are the obvious dangers: 1. Young drivers- Prairie Ridge high school students use Ames/31/Edgewood as a main drag to and from school. 2. Bad weather-foggy, rainy, snowy, icy....all of this makes it even more difficult to get out now. I cannot imagine what four lanes of traffic will cause. 3. School busses- busses leave Ames rd. onto 31 about 14 times per day. (3 public schools busses , 2 private school busses, and 2 band busses X 2= 14) I have been behind them numerous times. They take chances turning left because during those times it is very busy. They will never be able to get out with four lanes of traffic. How will our children get to school? 4. Night driving- it will be very difficult to judge what lane a vehicle is in. It is poorly lit and even now we have a very hard time at night. 5. Increased traffic -adding two lanes of traffic will surely result in increased traffic. This will obviously make it even more impossible for the residents of Prairie Grove and non residents using these roads. 6. Turning right onto Rte. 31 ILO turning left- Even if we turned right onto Rte. 31 from Ames and took Edgewood Rd. west, there will now be two lanes of traffic to watch for. Also, we would have to get into the left lane quickly to turn left onto Edgewood. This creates a very dangerous situation. These dangers listed can easily be solved by adding a traffic light at Edgewood and Rte. 31 and extend Edgewood east of 31 to Ames. Right now it is very stressful when leaving our subdivision at certain times of the day. I anticipate accidents, injuries and worse. I read in a previous document that a traffic light is not \"fiscally necessary\". How many injuries and/or lives need to be lost in order for it to be fiscally necessary? In addition to the danger....as if I need to add more concerns... The loss of privacy and noise conditions for the homes that back up to the well planted tree line along 31. Please, please do not remove those trees or burm! I can not imagine having my home back up to a four lane highway, can you? I appreciate your time and do hope these concerns are heard and resolved. Chris and Terri Regione</p>
E-mail	reg9900@aol.com
FirstName	Terri
LastName	Regione
Organization	
subject	Submission
Submit	Submit

Address	3519 Oakleaf Lane
City-State-Zip	Prairie Grove, Illinois 60012
Comments	<p>As a concerned citizen and resident of Prairie Grove, I have a vested interest in the Route 31 improvement project. 1. Firstly, the planned initiative will directly impact my property as the back of my house faces Route 31 just south of Ames Road. As I understand the project plan, natural sound barriers are not intended which would directly impact the value and salability of my home. As a senior citizen, this is obviously of great personal concern. 2. I have issues with storm water runoff which directly affects my property in a heavy rain. I understand this topographical area has had a history of storm water run off issues which has triggered remedial actions and has improved, but is still not ideal and continues to result in standing water on the low-level areas of the property and rushing water from neighboring properties in a heavy rain. I don't see where this issue has been acknowledged or addressed in the Route 31 improvement project, which is an additional concern. 3. Being an environment, health and safety professional, I am concerned with the removal of trees, impact to rural properties and wildlife in the area. I have deer, wild turkeys and coyotes on my property indicating habitats in the immediate area. 4. The ability to turn onto Route 31 from Ames in any direction is an on-going safety concern and should be strongly evaluated to promote the best and safest alternative with the road improvement effort. Please do not hesitate to contact me with any questions or additional information. Kind Regards, Susan M. Reinhardt, C.S.P.</p>
E-mail	smr4nexus@gmail.com
FirstName	Susan
LastName	Reinhardt
Organization	
postal-list-box	on
subject	Submission
Submit	Submit
subscribebox	on

[External] Comments on the route 31 expansion - from 1903 S. Route 31 (McHenry Heating & Excavating)

✖ DELETE ← REPLY ⇐ REPLY ALL → FORWARD ⋮



Mike Rogulic <Mike@mchenryheating.com>

Mark as unread

Fri 3/10/2017 4:36 PM

To: info@ilroute31.com;

Cc: Czaplicki, Scott D; Matt Rogulic <Matt@mchenryheating.com>;

[Enterprise Vault](#)

[Bing Maps](#)

+ Get more apps

We will be losing up to 10 of our parking spots

We have a digital sign which will need to be moved which the base consists of 30 yards of concrete as well as a couple thousand dollar steel reinforcement cage.

The right away will be very close to our well.

The drawing we saw at the Public Forum at the Shah Center has a Google Earth view of our property which is from before 2011. The current site is over 6 acres which is 50% developed and am planning on expanding in the near future. There is a now an 14,400 square foot building which houses among other things, an excavating company. We typically run 6 trucks daily in and out and perform emergency water main repair at all hours of the day/night. We also own large permit required equipment that comes and goes on a regular basis. Not having access from or to the northbound lane will be extremely detrimental to our business. On top of our own trucks we also run a recycling facility where approximately 20-50 tractor trailers come and go dropping off materials to be processed or pick up materials that have been recycled. Including our trucks we had approximately 3,000 trips coming and going last year alone. Not allowing truck to access this property will create a tremendous amount of congestion because they will be forced to turn around in our neighbors parking lots, Hi-Point Rd, Veterans Parkway or Prime Parkway. We feel it will be detrimental to our company and the flow of traffic through that area if we do not have a turn lane or mountable median to allow North and south access to our property.

I appreciate your time in this matter.

Regards,

Mike Rogulic
McHenry Heating & Air, Inc.
4561 Prime Parkway
McHenry, IL 60050
815-444-9900
www.mchenryheating.com
www.mchenryplumbing.com
www.mchenryexcavating.com



From: [Gerhard Rosenberg](#)
To: info@ilroute31.com
Cc: [Pat Wirtz](#); [LC-Donald Schmidt](#); [LC-George Lamm](#); [LC-James Johnson](#); [LC-Zelinda Paluch](#); [LC-Kaaren Gies](#); [LC-Nick Bennett](#); [AAAA-Jeff Varda](#); [LC-Pat Schafer](#); [LC-Robin Schmidt](#); [LCb-Marya Dixon](#)
Subject: [External] Review comments / recommendations for proposed IL 31 widening
Date: Wednesday, March 08, 2017 4:30:08 PM
Attachments: [IL_31.doc](#)

3/8/17

Scott,

I have reviewed your 31 120 to 176 proposed widening project and determined widening to 4 lanes from McHenry to Crystal Lake is not in the best interests of residents living in McHenry or helping to preserve the small town and historical characteristics of our small city.

Prior IDOT projects proposing 4 lanes going west to Woodstock fortunately died and hopefully proposals to widen rt 47 through Woodstock with 5 roundabouts will also never be built.

IDOT's planning is to push as many vehicles down a highway according to over estimated population predictions when our populations are actually declining.

Has anyone from IDOT contacted our City of McHenry as to how our citizens and historic preservationists will approve or object to your project.

As a concerned resident of McHenry and a historic preservationist 9 items describe in more details other major concerns with your IDOT proposal are attached as a word document.

I am looking forward for IDOT's response to my 3/8/17 email and attached 9 word.doc review comments.

Thank you,

Gerhard Rosenberg

REVIEW COMMENTS PROPOSED IL 31 WIDENING

1. Recently RT 120 east of RT 31 and RT 31 north of RT 120 were made into 4 lane roads resulting in speeding traffic exceeding speed limits up to Walmart in Johnsburg where roads decrease to 2 lanes. **There is no need to continue this 4 lane speedway through McHenry south to RT 176**
2. Existing traffic approaching the RT 120 interchange with closely spaced cross streets slows down, and aligns itself in proper lanes to access RT 120. Existing traffic lanes at some cross streets directly south of RT 120 could be modified to right turn in and right turn out.
3. Both sides of Rt 31 directly south of RT 120 have historical buildings adjacent the street which should not be demolished. Waukegan Rd. at RT 31 should be made into a culdesac
4. Proposed 18' to 28' wide medians become maintenance problems taking up more road space whether concrete or planted with grass or vegetation.
5. Consider additional concrete for 2 additional lanes, turning lanes, new impervious surfaces requiring drainage and issues with environmental and wetland areas.
6. Proposed 4 lane roadway has to jog 5 to 6 times east and west to avoid wet lands, Terracotta buildings, cemetery, residential property and natural gas electrical sub station
7. Table 2-1 Traffic volume indicates negligible increases/decreases 2005 to 2009 . Increases in 2040 are debatable / questionable considering loss of population as is occurring now.
8. Safety concerning most rear end crashes is due to drugs, cell phones, texting and inexperienced drivers. Adding 2 more lanes will provide more speed and less time and more space for accidents to occur.
9. Forget about IL 31 widening to 4 lanes to RT 176. Consider additional right turn access lanes and single left turn lanes along the existing 2 lane roadway. to prevent cars from backing up during rush hour to make turns.

Gerhard Rosenberg
3716 Waukegan RD.
McHenry IL 60050

2/8/17

[External] Rte 31 expansion

 DELETE  REPLY  REPLY ALL  FORWARD 



Gurbax saini <gurbaxsaini@yahoo.com>

Mark as unread

Thu 2/9/2017 4:47 PM

To: Czaplicki, Scott D;

[Enterprise Vault](#)

[Action Items](#)

[+](#) Get more apps

Hi Scott,

It was nice meeting you last night in Mchenry. I had talked to you about getting right in/out to our plaza at southwest intersection of Rte 31 and Shamrock drive in Mchenry (across the street from where we were last night).

We would really appreciate you looking into it and giving us the guidance.

Respectfully,

Gurbax Saini
847-341-0571

Sent from my iPhone



Illinois Route 31

ROUTE 176 TO ROUTE 120

www.idot.illinois.gov/projects/il31

Comment Form

Please Print Clearly

Name Ed Salisbury

Organization _____

Mailing Address 2016 S. Rte 31

City/State/Zip Mchenry IL 60550

Phone 815-363-9466 Email blue tool pro@aol

Please provide your thoughts on the Preferred Alternative and Environmental Assessment (EA).
Add any additional information that you feel should be considered by the Project Study Group:

Proposed changes to direct
Access to Property ARE NOT
Acceptable. We need
Direct Access to North And
South Rte 31.

If you need more space, please use the reverse side or additional forms. Please place this form in the comment box this evening, or mail it to the Illinois Department of Transportation by March 10, 2017 at the address listed below, or you may scan and email it to the project email address at: info@ILRoute31.com

MIDTOWN WAREHOUSE AND STORAGE
2016 S. IL ROUTE 31
MCHENRY, IL 60050
815-363-9466

244 STORAGE SPACES

26' MOVING TRUCKS, SEMI TRUCKS AND TRAILERS

25% COMMERCIAL TENNANTS:

MIDTOWN WAREHOUSE AND STORAGE – 26' MOVING TRUCKS DAILY

MCHENRY LIMOUSINE – 24' LIMOS

ILLINOIS MAILING SYSTEMS - DAILY SEMI LOADS

WAUCONDA MOTOR SPORTS – CAR HAULERS, TRAILERS, CAR TEST RIDES

CONCRETE CONTRACTORS – 4

PLUMBING CONTRACTORS – 6

CARPENTERS/RE-MODELERS- 12

FLOORING CONTRACTORS – 2

FOOD SERVICE – 5

SERVICE INDUSTRY – 25

HEATING & A/C – 2

WE NEED DIRECT NORTH AND SOUTH ACCESS TO ROUTE 31
FRONTAGE IS LEASED LONG TERM TO A USED CAR OPERATION

MIDTOWN WAREHOUSE & STORAGE CONTACT:

ED SALISBURY

815-363-9466 OR 815-378-9309



Illinois Route 31

ROUTE 176 TO ROUTE 120

www.idot.illinois.gov/projects/il31

Comment Form

Please Print Clearly

Name Randall Schietzelt

Organization The Land Conservancy of McHenry County

Mailing Address 4419 Walkup Road

City/State/Zip Crystal Lake, IL 60012-1846

Phone 815-355-1710 Email randall.schietzelt@gmail.com

Please provide your thoughts on the Preferred Alternative and Environmental Assessment (EA).
Add any additional information that you feel should be considered by the Project Study Group:

1. All wetland mitigation funds should go to local projects within the affected watershed, particularly any that create wetlands (IE. compensatory flood storage). Wetland banks in other watersheds will not help local flooding.

2. A number of oak and hickory trees will need to be removed with this project. The plan calls for planting natives, but this should be further specialized to include many more planted oaks and hickories than were removed. McHenry County is known for its oak forests on the glacial terrain. The benefits of this project should enhance the Counties ecosystems. Please include this specific planting request in the plan.

If you need more space, please use the reverse side or additional forms. Please place this form in the comment box this evening, or mail it to the Illinois Department of Transportation by March 10, 2017 at the address listed below, or you may scan and email it to the project email address at: info@ILRoute31.com

Illinois Department of Transportation
Attn: Bureau of Programming, Scott Czaplicki
201 W. Center Court
Schaumburg, IL 60196-1096



From the Bureau of : Programming

Name:	<u>Jim Scholz</u>	Date:	<u>February 8, 2017</u>
Title:	<u>Executive Director</u>		
Organization:	<u>McHenry Public Library</u>	Project:	<u>IL 31</u>
Address:	<u>809 Front Street</u>		<u>IL 176 to IL 120</u>
	<u>McHenry, IL</u>		<u>P-91-135-99</u>
Telephone:	<u>(815) 385-0036</u>		
E-Mail:	<u>jscholtz@mchenrylibrary.org</u>	PTB No.:	<u>155-015 & 170-007</u>

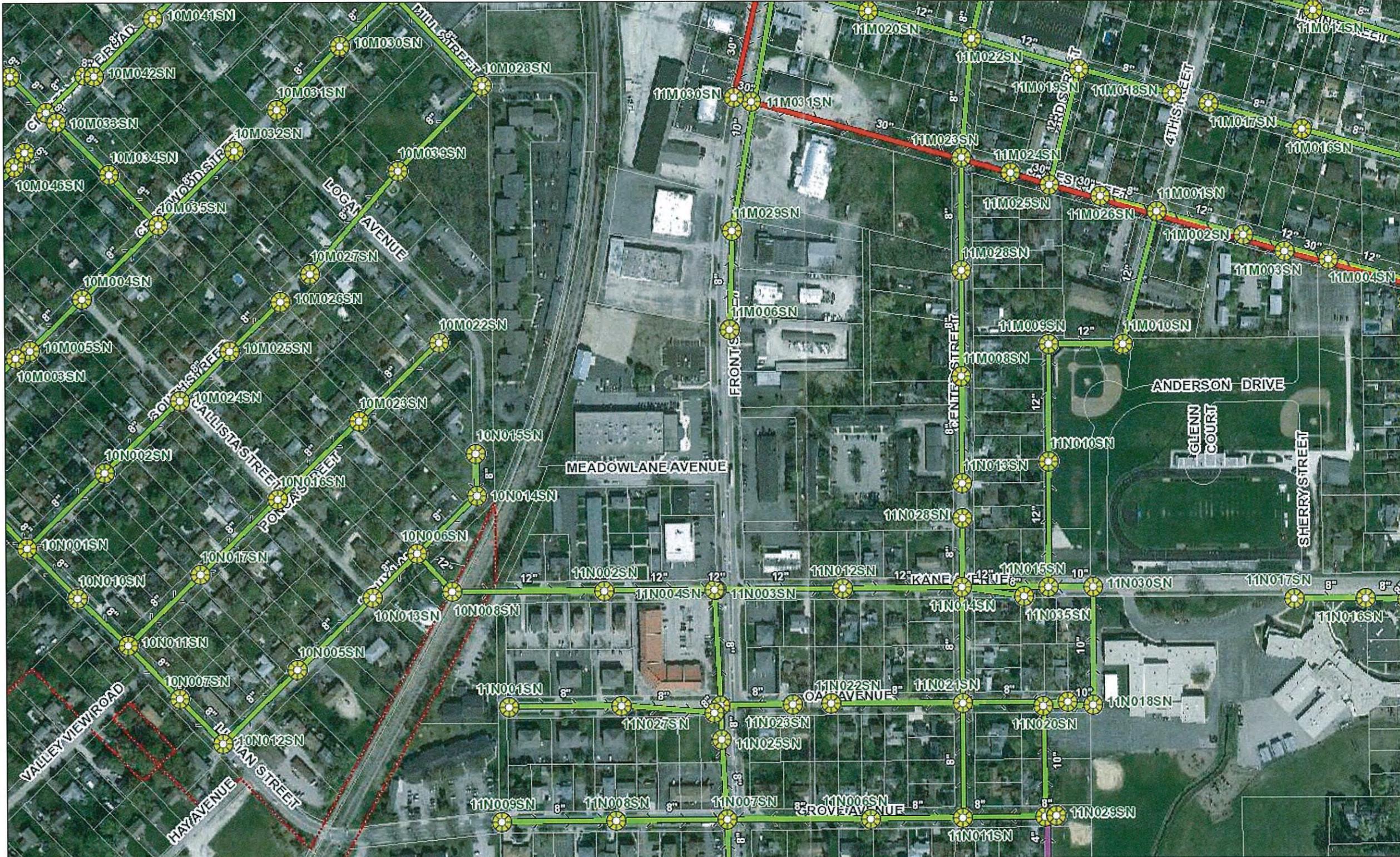
Items Discussed:

The library is looking to construct a new facility in NW corner of property and demolish the current building. Erickson Engineering is their consultant. They, along with the two businesses to the south, share a private 6" cast iron sanitary sewer. The sewer is undersized and shallow (~5'). They are considering a lift station in the NE corner of their property as part of their redevelopment and will connect to the City sanitary sewer. The private sewer is not shown in the EDP, but they believe it will be in the Pr ROW. They may be looking to change the entrance location along IL 31.

Follow-Up:

Send the EDP, PDP and P&P to markup their sanitary sewer and potential new entrance location.

By: Scott Czaplicki
Bureau of Programming/Consultant



LEGEND

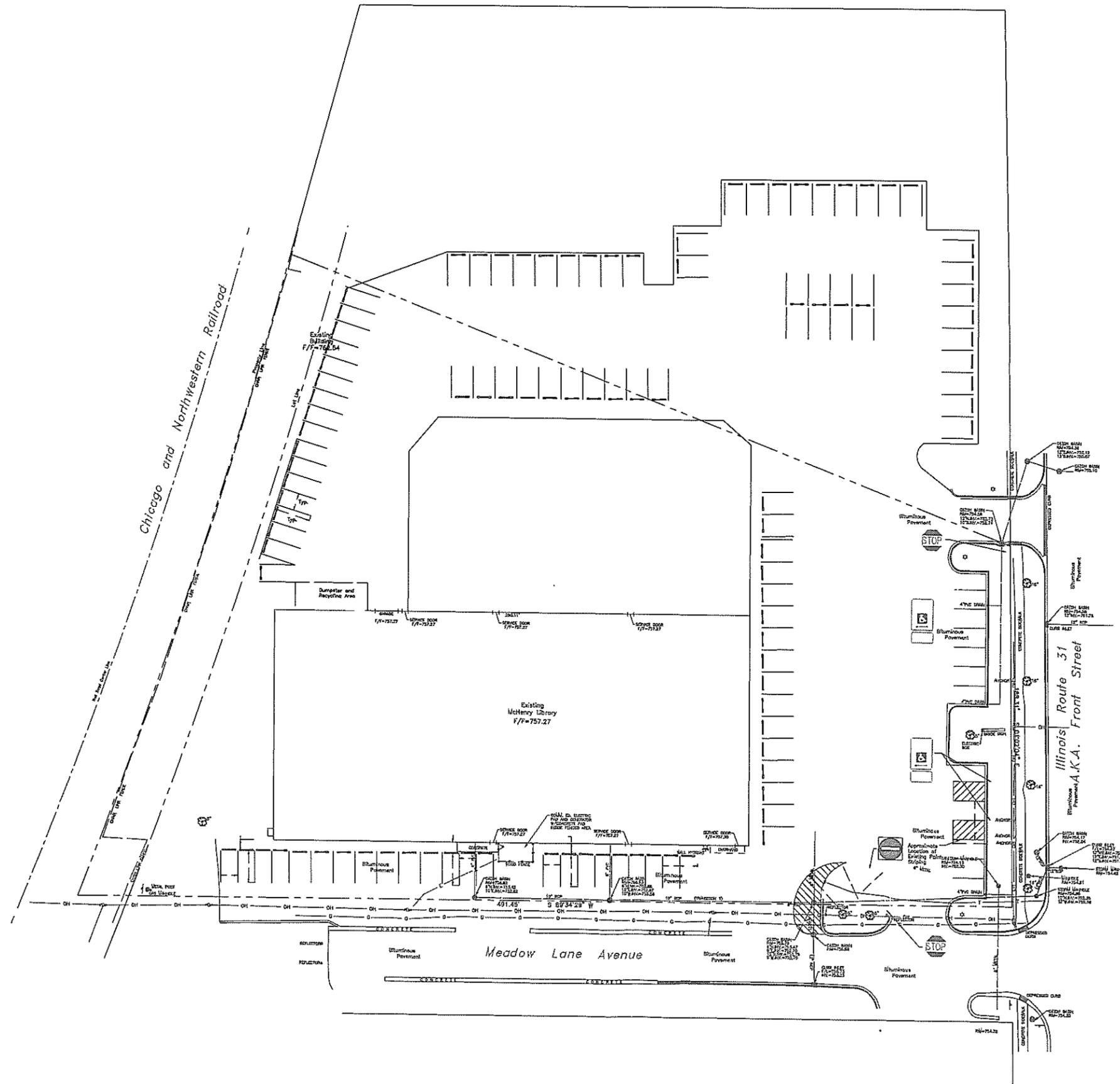
- Lift Station
- Sanitary Manhole**
 - North Model Manholes
 - South Model Manholes
- Sewer Air Release/Clean-Out**
 - Air Release
 - ◆ Cleanout
- Sewer Stubs**
- Sewer Pipe**
 - Conduit 6" - 15"
 - Conduit 18"+
 - Force Mains
- Municipal Limits**
- McHenry County Lot Lines**
- McHenryCity2012.sid
 - Red: Band_1
 - Green: Band_2
 - Blue: Band_3

Date Printed: 4/4/2016



NOTES:

This map was generated using the City of McHenry's GIS Web Mapping Application.
 This map is a user generated static output from an Internet Mapping Site and is for reference only.





Illinois Route 31

ROUTE 176 TO ROUTE 120

www.idot.illinois.gov/projects/il31

Comment Form

Please Print Clearly

Name STEVEN SCHWARZ, SE, PE

Organization HR Green

Mailing Address 420 N Front St

City/State/Zip McHenry IL 60050

Phone 815-479-8392 Email sschwarz@hrgreen.com

Please provide your thoughts on the Preferred Alternative and Environmental Assessment (EA).
Add any additional information that you feel should be considered by the Project Study Group:

This project is long overdue & greatly needed. I hope this moves along to Phase 2 & 3 quickly. A local design firm intimate with the area should perform Phase 2 (HR Green)

If you need more space, please use the reverse side or additional forms. Please place this form in the comment box this evening, or mail it to the Illinois Department of Transportation by March 10, 2017 at the address listed below, or you may scan and email it to the project email address at: info@ILRoute31.com

Illinois Department of Transportation
Attn: Bureau of Programming, Scott Czaplicki
201 W. Center Court
Schaumburg, IL 60196-1096

Address	600 NORTH ROUTE 31
City-State-Zip	Crystal Lake, IL
Comments	I am very concerned about this project having no left turn into our Scout Office. We recently invested in this area to be convenient for our members to get in and out of quickly. If this project goes forward, we would look at relocating.
E-mail	scott.seibert@scouting.org
FirstName	Scott
LastName	Seibert
Organization	Boy Scouts of America
subject	Submission
Submit	Submit
subscribebox	on

Address	28955 RT 173
City-State-Zip	Antioch Il 60002
Comments	I was told at the open house last week that the detailed plan documents shown in multiple sections would be available on this web site this week. However I do not see those.
E-mail	shepardd@thelensg.com
FirstName	Dan
LastName	Shepard
Organization	Thelen Sand & Gravel Inc.
subject	Submission
Submit	Submit

From: jeanlsmith2@yahoo.com
To: [Czaplicki, Scott D](#)
Subject: [External] Route 31 expansion
Date: Thursday, March 02, 2017 8:17:36 PM

Scott

Hi Scott, my name is Jean Smith and I am a resident of Oak Grove subdivision. I work for a construction company (Berger Excavating Contractors) which has worked on many IDOT jobs so I am very familiar with all the work that goes into a major road project. I do appreciate anything you can do to assist our subdivision with our concerns, including safety, noise and placement. In addition to these concerns, I have an additional concern and that is with water runoff.

In 2004, we worked very hard with the Village of Prairie Grove and Hank Braley of Wynwood Builders to fix a major water runoff issue that effected many lots in the subdivision. Most of the people who were effected have moved away and current residents are unaware of the issues we faced. There is a culvert running beneath 31 about 1/2 mile south of Ames. Water from the farm fields west of 31 drains through this culvert. After coming through the culvert, the water use to flow through the backyards of many our residents. The amount and velocity of this water was truly amazing. An independent engineer was hired to evaluate this flow. The engineer determined that the water coming through the culvert was flowing at 46 cubic feet per second (May 17, 2004... date studied) as it flowed through the yards the rate increased due to the slope. On lot 30, the flow was determined to be 83 cubic feet per second.

As a result of this study, Wynwood Builders agreed to install a drainage easement which is located adjacent to route 31 running from the culvert south to an underground drain which goes into the retention area. This drainage easement has worked tremendously since being installed. Several times every year you can see where the grass in the easement has been knocked down due to the water runoff.

I can see that a box culvert is on the Route 31 expansion plan in what appears to be the same location. I can not tell from what I have seen if any plan has been developed to direct the water coming out of the culvert to somewhere other than the back yards.

Please let me know if this water run off issue has been addressed in the Route 31 expansion plan. I can provide copies of letters from engineers, the Village, the Builder and pictures of channel erosion. I know the village of Prairie Grove also has documents on file if needed.

Thanks for your assistance.

Jean Smith
815-814-7556

Sent from my iPad



Illinois Route 31

ROUTE 176 TO ROUTE 120

www.idot.illinois.gov/projects/il31

Comment Form

Please Print Clearly

Name **Denise Strom, Property Manager**

Organization **McDonald's Corporation**

Mailing Address **711 Jorie Boulevard, Third Floor**

City/State/Zip **Oak Brook, IL 60523**

Phone **630-234-9775**

Email **denise.strom@us.mcd.com**

Please provide your thoughts on the Preferred Alternative and Environmental Assessment (EA).

Add any additional information that you feel should be considered by the Project Study Group:

I had an opportunity to attend the public hearing on February 8, 2017. I also had the opportunity to review the proposed taking of the McDonald's restaurant parcel. I would like to propose that the right of way taking be moved to the West far enough so that, together with the proposed temporary construction easement ("TCE"), McDonald's parking spaces and retaining wall would not be adversely affected by the taking and the TCE. The restoration costs to our site would be considerable if the retaining wall is affected. Thank you.

If you need more space, please use the reverse side or additional forms. Please place this form in the comment box this evening, or mail it to the Illinois Department of Transportation by March 10, 2017 at the address listed below, or you may scan and email it to the project email address at: info@ILRoute31.com

Illinois Department of Transportation
Attn: Bureau of Programming, Scott Czaplicki
201 W. Center Court
Schaumburg, IL 60196-1096

[External] Route 31 - Planning

John Swierk <jswierk@ddcaarchitects.com>

Thu 2/9/2017 1:06 PM

To: Czaplicki, Scott D <Scott.Czaplicki@illinois.gov>;

Scott.

We met yesterday at the Public Hearing for Rt 31 in McHenry.

Please forward a pdf of the proposed cross easement plan for Route 31 at Ames Road. I own the property at 3321 S. Route 31 and will be working with Terra Cotta to provide the easement Agreements required for IDOT to design & Construct the driveway.

As discussed, we will review the proposed plan and provide comments to you.

Please contact me if you have any questions.

Thanks

John Swierk, AIA

President, NCARB

LEED AP BD+C



Direct Design Ltd.

Carroll Associates

3321 S. Route 31

Prairie Grove, IL 60012

Address	3804 Main
City-State-Zip	McHenry, IL 60050
Comments	We need a stoplight at the Main and Front intersection
E-mail	gretchen48@sbcglobal.net
FirstName	Gretchen
LastName	Thomas
Organization	
postal-list-box	on
subject	Submission
Submit	Submit

2012 last public meeting

PUBLIC HEARING



Illinois Route 31

ROUTE 176 TO ROUTE 120

www.idot.illinois.gov/projects/il31

Comment Form

Please Print Clearly

BUREAU OF PROGRAMMING
RECEIVED

Name

Nancy A. Tober

FEB 22 2017

Organization

DISTRICT #1

Mailing Address

4614 Shady Oaks Lane

City/State/Zip

Crystal Lake, Illinois 60012-1921

Phone

815-459-2982

Email

nancytober@AOL.com

Please provide your thoughts on the Preferred Alternative and Environmental Assessment (EA).

Add any additional information that you feel should be considered by the Project Study Group:

I have lived along Route 31 for 56 years. Our subdivision (Deering Oaks Estates) is a quiet, peaceful community where the residents enjoy nature and its benefits. I am very concerned with the proposed widening project of this highway, and the sidewalk and shared use path. There is presently a bike path that goes to McHenry and beyond. It is located 1/2 mile west of Route 31. A much safer path than along a 4 lane highway. Why is another one needed? Another very large concern of mine is noise. Are you planning a noise barrier? I certainly hope so. Traffic noise would increase with 4 lanes and my house would be closer to that noise. A noise barrier would be needed. I certainly hope my peaceful living would not be a thing of the past. Thank you for your consideration in these matters.

Nancy A Tober

If you need more space, please use the reverse side or additional forms. Please place this form in the comment box this evening, or mail it to the Illinois Department of Transportation by March 10, 2017 at the address listed below, or you may scan and email it to the project email address at: info@ILRoute31.com

Illinois Department of Transportation
Attn: Bureau of Programming, Scott Czaplicki
201 W. Center Court
Schaumburg, IL 60196-1096

Address	3615 Oakleaf Lane
City-State-Zip	Crystal Lake, IL 60012
Comments	Dear Mr. Czaplicki, As a resident of the Oak Grove Subdivision located at the corner of Route 31 and Ames Road, I am very concerned about the impact of widening 31 to the East near that intersection. As reported in the Northwest Herald on January 31, 2017- \"Report concludes 1 Illinois home in 5 \"seriously\" underwater.\" (http://www.nwherald.com/2017/01/30/report-concludes-1-illinois-home-in-5-seriously-underwater/ajdl3t9/) Although I understand that the property is owned by TC Industries, the impact on Oak Grove home values would be devastating. Most homes in the neighborhood have already been hit hard by the economic recession, the real estate crash, and the high tax rate in McHenry County making it nearly impossible to sell our homes much less without taking a loss of equity. I respectfully ask that you consider widening Route 31 to the west on industrial land where the financial consequences to the neighborhood and its residents would be lessened. Thank you for your consideration- Leslie Tremblay 847-431-4294
E-mail	latremblay@comcast.net
FirstName	Leslie
LastName	Tremblay
Organization	Oak Grove HOA
subject	Submission
Submit	Submit
subscribebox	on

Address	4613 oakcrest rd
City-State-Zip	crystal lake illinois 60012
Comments	1. Traffic slows down in this section (1/2 mile road and College Hill) due to no left turn lanes and becomes backed up. In this stretch of 31,adding left turn lane can handle the volume in lieu of 4 lanes. With just the left turn lane Safety would increase because idiots won't use the shoulder to pass on the right doing 50-55 mph. Cars behind the idiot taking the shoulder do not have time to react to this action and will rear end the car turning left. The other fatal collision at oakcrest and 31 was due to another idiot who wanted to go faster and pass the slower car on the left going uphill in a no passing zone. this was the last thing he did. It is about obeying rules of the road and speed limits. 2. Installing \"No Engine Braking\" signs will help in noise reduction. 3.Need to have noise or sound barrier. Cars with bad exhausts, high rpm, loud music,and vibration rattles the house. It is also large volume truck traffic, especially early mornings stating at 4:30 am. Need to add No Engine Braking signs.
E-mail	marktropinski@aol.com
FirstName	Mark
LastName	Tropinski
Organization	
subject	Submission
Submit	Submit

Address	3510 Washington St
City-State-Zip	McHenry
Comments	Could you please tell me which buildings will be demolished in the City of McHenry? Was this discussed at the 2/4/17 meeting? It's not clear on the project site. Thank you.
E-mail	sassandahalf@yahoo.com
FirstName	Michelle
LastName	Trost
Organization	
subject	Submission
Submit	Submit



UNITED STATES ENVIRONMENTAL PROTECTION AGENCY
REGION 5
77 WEST JACKSON BOULEVARD
CHICAGO, IL 60604-3590

FEB 27 2017

REPLY TO THE ATTENTION OF:

E-19J

Matt Fuller
Federal Highway Administration – Illinois Division
3250 Executive Park Drive
Springfield, Illinois 62703

Re: Draft Environmental Assessment for the IL Route 31 (From IL Route 176 to IL 120) Project, McHenry County, Illinois

Dear Mr. Fuller:

The U.S. Environmental Protection Agency (EPA) has reviewed the referenced document dated January 18, 2017, which was produced by the Illinois Department of Transportation (IDOT), in coordination with the Federal Highway Administration (FHWA). We are providing comments pursuant to our authorities under the National Environmental Policy Act (NEPA), Council on Environmental Quality regulations (40 CFR Parts 1500-1508), Section 309 of the Clean Air Act, Section 404 of the Clean Water Act, and in accordance with EPA's role in the NEPA-CWA Section 404 Merger Process.

The proposed project involves addition of roadway infrastructure on IL Route 31, between IL Route 176 and IL Route 120. The proposed project is broken into three separate sections: South Section, North Section, and IL Route 120 Intersection.

Alternatives carried forward for the South Section:

- Alternative S-5 (Preferred Alternative for the South Section). Widen to 4 lanes with 30-foot raised median; and
- Alternative S-6. Widen to 4 lanes with 30-foot depressed median and 10-foot shoulders.

Alternatives carried forward for the North Section:

- Alternative N-2 (preferred alternative for the North Section). Widen to 4 lanes with an 18-foot raised curb median.

Alternatives carried forward for the IL Route 120 Intersection:

- Alternative 120INT-1 (preferred alternative for the IL Route 120 Intersection). Construct a five-lane cross section (two lanes in each direction with a two-way left turn lane); and

- Alternative 120INT-3. Widen the IL Route 120 intersection for additional capacity. A 30-foot raised median with two 12-foot through lanes would be constructed in each direction.

Other Alternatives carried forward for consideration:

- No Action Alternative. The project does not proceed; and
- Congestion Management Process Alternative. Perform safety and infrastructure alterations without adding any new infrastructure.

We do not object to any of the three preferred alternatives. Additionally, we have comments that we urge FHWA and IDOT to consider before producing a Final EA (FEA) and Finding of No Significant Impact (FONSI), as stated below.

Stormwater Management

We support the stormwater management features and erosion control best management practices (BMPs) incorporated into this project. We do, however, urge FHWA and IDOT to consider constructing enough stormwater storage capacity for the volume of stormwater associated with a 500-year flood event.

Air Quality

We recommend FHWA and IDOT commit to reducing air emissions from internal combustion vehicles during the construction phase of this project. A list of BMPs is found in the enclosed document *U.S. Environmental Protection Agency Diesel Emission Reduction Checklist*.

Pollinator Habitat

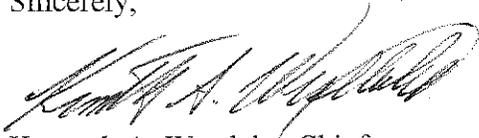
The 2014 Presidential Memorandum entitled, *Creating a Federal Strategy to Promote the Health of Honey Bees and Other Pollinators*, responds to evidence of steep declines in certain pollinator populations. Pollinators are critical contributors to our nation's economy, food system, and environmental health. Vegetation within the project area can provide much needed habitat for pollinators, providing food, shelter, and connections to other patches of habitat. Maintenance staff and landscape designers can all take steps to improve the quality of vegetation to benefit pollinators, steps that can also reduce costs, maintain public safety, and improve public good will. The FEA and FONSI should include specific commitments by FHWA and IDOT to incorporate pollinator habitat into the project where practical.

Transportation Linkages

We applaud FHWA and IDOT for committing to constructing sidewalks and providing linkages between sidewalks and local trails, consistent with IDOT's Complete Streets Policy. We recommend FHWA and IDOT identify potential locations where constructing sidewalk or trail crossings over or under IL 31 and IL 120 would be beneficial for the safety of pedestrians and bicyclists. This would include (but not limited to) sidewalk routes that lead from residential areas to schools and/or shopping centers that would require crossing IL 31 or IL 120. FHWA and IDOT should also consider the proposed wildlife crossings as potential trail and sidewalk crossings.

We are available to discuss these comments at your convenience. Please feel free to contact Mike Sedlacek of my staff at 312-886-1765, or by email at sedlacek.michael@epa.gov.

Sincerely,



Kenneth A. Westlake, Chief
NEPA Implementation Section
Office of Enforcement and Compliance Assurance

Enclosure: U.S. Environmental Protection Agency Diesel Emission Reduction Checklist

cc: Scott Czaplicki, Illinois Department of Transportation
Shawn Cirton, U.S. Fish and Wildlife Service
Soren Hall, U.S. Army Corps of Engineers – Chicago District

U.S. Environmental Protection Agency
Diesel Emission Reduction Checklist

- Use low-sulfur diesel fuel (15 ppm sulfur maximum) in construction vehicles and equipment.
- Retrofit engines with an exhaust filtration device to capture diesel particulate matter before it enters the construction site.
- Position the exhaust pipe so that diesel fumes are directed away from the operator and nearby workers, reducing the fume concentration to which personnel are exposed.
- Use catalytic converters to reduce carbon monoxide, aldehydes, and hydrocarbons in diesel fumes. These devices must be used with low sulfur fuels.
- Use enclosed, climate-controlled cabs pressurized and equipped with high efficiency particulate air (HEPA) filters to reduce the operators' exposure to diesel fumes. Pressurization ensures that air moves from inside to outside. HEPA filters ensure that any incoming air is filtered first.
- Regularly maintain diesel engines, which is essential to keep exhaust emissions low. Follow the manufacturer's recommended maintenance schedule and procedures. Smoke color can signal the need for maintenance. For example, blue/black smoke indicates that an engine requires servicing or tuning.
- Reduce exposure through work practices and training, such as turning off engines when vehicles are stopped for more than a few minutes, training diesel-equipment operators to perform routine inspection, and maintaining filtration devices.
- Repower older vehicles and/or equipment with diesel- or alternatively-fueled engines certified to meet newer, more stringent emissions standards. Purchase new vehicles that are equipped with the most advanced emission control systems available.
- Use electric starting aids such as block heaters with older vehicles to warm the engine reduces diesel emissions.
- Use respirators, which are only an interim measure to control exposure to diesel emissions. In most cases, an N95 respirator is adequate. Workers must be trained and fit-tested before they wear respirators. Depending on work being conducted, and if oil is present, concentrations of particulates present will determine the efficiency and type of mask and respirator. Personnel familiar with the selection, care, and use of respirators must perform the fit testing. Respirators must bear a NIOSH approval number.
- Per Executive Order 13045 on Children's Health¹, EPA recommends operators and workers pay particular attention to worksite proximity to places where children live, learn, and play, such as homes, schools, daycare centers, and playgrounds. Diesel emission reduction measures should be strictly implemented near these locations in order to be protective of children's health.

¹ Children may be more highly exposed to contaminants because they generally eat more food, drink more water, and have higher inhalation rates relative to their size. Also, children's normal activities, such as putting their hands in their mouths or playing on the ground, can result in higher exposures to contaminants as compared with adults. Children may be more vulnerable to the toxic effects of contaminants because their bodies and systems are not fully developed and their growing organs are more easily harmed. EPA views childhood as a sequence of lifestages, from conception through fetal development, infancy, and adolescence.



REPLY TO
ATTENTION OF:

DEPARTMENT OF THE ARMY
CHICAGO DISTRICT, CORPS OF ENGINEERS
231 SOUTH LA SALLE STREET
CHICAGO, ILLINOIS 60604-1437

March 15, 2017

Technical Services Division
Regulatory Branch
LRC-2011-336

SUBJECT: NEPA/404 Merger Process Comments on the Draft Environmental Assessment for the IL Route 31 (from IL Route 176 to IL 120) Project, McHenry County, Illinois

Catherine Batey
Federal Highway Administration
3250 Executive Park Drive
Springfield, Illinois 62703

Dear Ms. Batey:

This letter is in response to your request that the Department of the Army (Corps) provide comments on the Draft Environmental Assessment for the IL Route 31 (from IL Route 176 to IL 120) Project, McHenry County, Illinois (EA). Various Federal and state agencies are providing a concurrent review of the project under the terms and conditions as set forth in the "Statewide Implementation Agreement National Environmental Policy Act And Clean Water Act Section 404 Concurrent NEPA/404 Processes For Transportation Projects in Illinois".

Following a review of the EA, the Corps provides the following comments:

1. Best Management Practices (BMP) for the Preferred Alternative

The Water Quality Volume does not meet the requirement around Sleepy Hollow Creek, a perennial stream where two pollution intolerant fish species were identified. The permit application will need to provide a detailed BMP proposal that includes information on what efforts were made to meet the 1.25" recommended storage.

In addition, Exhibit 5 appears to show outfalls discharging directly into wetlands. Stormwater outfalls should be located as far from the resource as possible. This information should be made available in a detailed BMP proposal. Finally, please make sure to label all wetlands and streams in the BMP proposal.

2. Water Resources Impacts

In addition to indicating whether proposed impacts are permanent or temporary, please make sure to qualify each proposed impact. Avoidance and minimization should also be discussed. For example, Sheet 13 in Exhibit 3 shows temporary impacts proposed in W6. It appears that a detention basin is proposed at this location, but the particular impacts are unclear.

If so, what avoidance and minimization measures were considered? It should be noted that detention in-line with a stream is generally not permissible. This information could be presented in the form of a table.

The information presented for culverts does not contain any information on the stream channel width. For culverts spanning a stream, this information should be provided in the application. Culverts should be designed to prevent the restriction of expected high water flows, and should be designed so as not to impede low water flows or the movement of aquatic organisms. Typically, a stream channel cross section taken upstream, at and downstream of the culvert should help determine if proposed culvert is meeting this requirement.

3. Proposed Mitigation

The proposed mitigation for impacts to Squaw Creek is on-site, in-kind replacement with meanders. The proposal will need to include existing and proposed cross sections of the stream. A monitoring and management plan will be required.

The EA states that a mitigation ratio of 1.5:1 will be applied to all non-ADID wetlands and a mitigation ratio of 3:1 will be applied to HQAR wetlands. There are instances where mitigation ratio of up to 6:1 are applied for ADID wetlands depending on the type of ADID classification. Note that stream mitigation will be required for areas that are considered stream loss.

Thank you for the opportunity to comment. If you have any questions, please contact Julie Rimbault of my staff by telephone at 312-846-5542, or email at Julie.C.Rimbault@usace.army.mil.

Sincerely,

Keith L. Wozniak
Chief, West Section
Regulatory Branch

Copy Furnished:

U.S. Environmental Protection Agency (Ken Westlake)
U.S. Fish and Wildlife Service (Shawn Cirton)
Illinois Department of Natural Resources (Sheldon Fairfield)
Illinois Department of Transportation (Anthony Quigley)



Illinois Route 31

ROUTE 176 TO ROUTE 120

www.idot.illinois.gov/projects/il31

FEB 14 2017

DISTRICT #1

Comment Form

Please Print Clearly

Name MARTIN AND NANCY VAZZANO

Organization property owners

Mailing Address P.O. Box 2304

City/State/Zip Crystal Lake, IL 60039

Phone 815-477-5300 Email _____

Please provide your thoughts on the Preferred Alternative and Environmental Assessment (EA).
Add any additional information that you feel should be considered by the Project Study Group:

If IDOT takes the vacant land on the west side of the creek in McHenry, just south of 120, what will happen to the City of McHenry's plans to develop the Riverwalk? This is hardly the "highest and best use" of the property. The Riverwalk was intended to attract tourists and provide enjoyment to the community and revitalize the downtown area. Water management and an access road hardly rival those worthy goals.

NANCY VAZZANO

If you need more space, please use the reverse side or additional forms. Please place this form in the comment box this evening, or mail it to the Illinois Department of Transportation by March 10, 2017 at the address listed below, or you may scan and email it to the project email address at: info@ILRoute31.com

NORTHWEST HERALD

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Letter: Route 31 widening concerns

Published: Monday, Feb. 20, 2017 11:47 a.m. CST

To the Editor:

IDOT hosted an open house to get public comments on its plan to widen Route 31 all the way up to Route 120 and east to the 31 turn.

In addition, they plan to acquire the vacant land along the west side of Boone Creek south of 120, some of which we own, for water management and an access road.

McHenry does not seem aware of IDOT's interest in this western section of the undeveloped Riverwalk. When I spoke to the city manager, he thought the project was just widening 120 at that point.

The Riverwalk will revitalize the downtown area, the property IDOT plans to acquire has C5, the highest zoning, and could accommodate a multifamily housing project.

Water management is not "highest and best" use and defeats some of the efforts made by Mayor Lowe, the Smith family and so many others to create a beautiful extensive Riverwalk for generations to come.

Nancy Vazzano

Address	15445 Innovation Drive
City-State-Zip	92128
Comments	I am the Asset Manager for McHenry, a 20 MW battery plant located on 302 North Front Street, McHenry, Illinois 60050 I would like to talk with someone about this project and its impact on our property, can someone please contact me? Thank you!
E-mail	bryan.villano@edf-re.com
FirstName	Bryan
LastName	Villano
Organization	EDF RE
subject	Submission
Submit	Submit
subscribebox	on

Address	15445 Innovation Drive
City-State-Zip	92128
Comments	Hello, I tried to contact your organization a few months ago but I lost response the email. I am the representative of a battery plant that is located on 302 N. Front Street in Mchenry, Illinois. I'd like to talk about the plans to expand the highway, please email or call me at the contact information above. 858-521-3568 Thank you, Bryan Villano
E-mail	bryan.villano@edf-re.com
FirstName	Bryan
LastName	Villano
Organization	EDF RE
subject	Submission
Submit	Submit

Address	6415 s il route 31
City-State-Zip	Crystal Lake
Comments	Type any comments you have here: yes! When will it be done?
E-mail	Johnkwill@comcast.net
FirstName	John
LastName	Williams
Organization	
subject	Submission
Submit	Submit

NORTHWEST HERALD

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Letter: Route 31 widening concerns

Published: Monday, Feb. 20, 2017 11:47 a.m. CST

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Water management is not "highest and best" use and defeats some of the efforts made by Mayor Lowe, the Smith family and so many others to create a beautiful extensive Riverwalk for generations to come.

Nancy Vazzano

United States Senate
Washington, DC 20510-1304

February 3, 2017

BUREAU OF PROGRAMMING
RECEIVED

FEB 07 2017

DISTRICT #1

Scott Czaplicki, P.E.
Project Manager
Illinois Department of Transportation
201 W. Center Court
Schaumburg, IL 60196

Dear Mr. Czaplicki:

Thank you very much for your invitation to Senator Durbin. Unfortunately, due to the legislative session, the Senator will be in Washington D.C. and will not be able to attend the Illinois Department of Transportation's Public Hearing on February 8th, 2017.

The Senator truly appreciates being kept in mind and hopes that in the future you will continue to keep him abreast of any special occasions that your organization hosts.

If you have any further questions or if I can be of any assistance in the future, please contact me at (312) 353-4952.

Sincerely,



Abigail Watkins
Scheduling Department
U.S. Senator Richard J. Durbin

Public Hearing Attendees and Comments

February 8, 2017 - March 10, 2017

Comment No.	First_Name	Last_Name	Representing	Attended Hearing ?	Provided Comment?	How Addressed?
1	Janess	Abraham		No	Yes	2/16/17-Forwarded website link
2	Cat	Barnes		No	Yes	Newsletter
3	Margaret	Barranco	self	Yes	Yes	Newsletter
4	Kevin	Bauer	Sno-Bugs/Lakers Snomobile Club	No	Yes	Noted
5	Juliette	Beaulieu		No	Yes	Newsletter
6	Fred	Bencriscutto		No	Yes	Newsletter
7	Sally	Bender		No	Yes	Newsletter
8	Melissa	Blackwell	Boy Scout Troop 657	No	Yes	Newsletter
9	Ronald	Branum		No	Yes	Support
10	Dan	Browne		No	Yes	Newsletter
11	Wendy	Browne		No	Yes	Newsletter
12	Linda and Ron	Bykowski	Self	Yes	Yes	2/5/18-Emailed response to McHenry letter. 11/17/17 Meeting w/ City of McHenry to remove cul-de-sac, reduce ROW.
13	Erica	Carlson	Digital Pix Composites, LLC	Yes	Yes	11/20/17- Phone call discussion with McGuire
14	Steven	Carruthers	City of Crystal Lake	Yes	Yes	4/19/17 - Replied via e-mail
15	Michael	Chambers	Adams Steel	Yes	Yes	2/22/17 -Sent P&P. Received revised driveway drawing.
16	Sharon and Paul	Christenson		Yes	Yes	Newsletter
17	John	Darger	Environmental Defenders of McHenry County	No	Yes	Newsletter
18	Tim	Daum	self	Yes	Yes	Newsletter
19	Melissa	Davis	Boy Scouts	No	Yes	Newsletter
20	Mike	Dennis		No	Yes	Newsletter
21	Jennifer	Drozt		No	Yes	Newsletter
22	Barb	Dworak	Shah Center	No	Yes	Support
23	Terry	Dyra	Self	Yes	Yes	Newsletter
24	Peder	Finnberg	Heritage Land Consultants	No	Yes	Newsletter
25	Joe	Gottenolle	County Board	Yes	Yes	Support
26	Lisa	Haderlein	The Land Conservancy	Yes	Yes	Newsletter
27	Jim	Haisler	Hertland Realtor	Yes	Yes	Newsletter
28	Ruthie	Harvey		No	Yes	3/10/17- Emailed reply
29	Jim	Hicks	Resident Edgewood Rd Region	Yes	Yes	1/24/18-Email with updated plan sheet.
30	Julie	Hoberg	resident	Yes	Yes	Newsletter
31	Michael	Howell	Resist	No	Yes	Newsletter
32	Bert	Irslinger Jr	Second Amendment Sports	Yes	Yes	2/21/18-Emailed reply. 1/25/18-Two options to avoid new building being evaluated.
33	Dr. Bertram	Irslinger Sr	AA Club	Yes	Yes	2/15/18-Emailed updated plans with wider drive and flush median.
34	Iver	Johnson	Self, 803 W Front St	Yes	Yes	2/21/18-Emailed reply, need to send revised drawings once drainage studies updated.
35	Anthony	Jones	Homeowner	Yes	Yes	Newsletter
36	Wiliam	Jones	Self	Yes	Yes	Newsletter
37	Emily	Kalal	Oak Grove Subdivision	Yes	Yes	Newsletter
38	Gary	Kanner	Environmental Defenders of McHenry County	No	Yes	Newsletter
39	Bill	Kennelly	Ja Frate	Yes	Yes	11/16/17-Called and discuss project status and no new signal proposed between Albany and Shamrock.
40	Brian & Erica	Kilinski		Yes	Yes	11/20/17-Replied via email.
41	Patricia	Knapik		Yes	Yes	11/17/17-Mailed Sheet 14
42	John	Koss	Oak Grove Subdivision	Yes	Yes	Newsletter

Public Hearing Attendees and Comments

February 8, 2017 - March 10, 2017

Comment No.	First_Name	Last_Name	Representing	Attended Hearing ?	Provided Comment?	How Addressed?
43	Mary	Koss	Oak Grove Subdivision	Yes	Yes	Newsletter
44	Robert	Kurth		No	Yes	Newsletter
45	Christine	Kustra	Environmental Defenders of MC	Yes	Yes	Newsletter
46	Jamie	Lee		No	Yes	Newsletter
47	Kathleen	Martinez	Terra Cotta Realty Co.	Yes	Yes	2/5/18-Emailed updated plans with new cross access locations
48	Doug	Marunde	Arvidson Pools and Spas	Yes	Yes	8/24/17 Meeting and 11/17/17 Tickle Reponse
49	Dennis	Marunde	Arvidson & Sons, Inc.	No	Yes	8/24/17 Meeting and 11/17/17 Tickle Reponse
50	Bill and Bonnie	Merriman		Yes	Yes	Newsletter
51	Jon	Meyer	McHenry	Yes	Yes	Newsletter
52	Tracy	Morey		No	Yes	Newsletter
53	Nicole	Mullins		No	Yes	Newsletter
54	Tina	Napolitano	Self	Yes	Yes	Support
55	M	Nelson		No	Yes	Newsletter
56	Jon	Olmsted		Yes	Yes	Newsletter
57	Gary	Overbay	Self	Yes	Yes	Newsletter
58	Paul	Pacini		No	Yes	2/23/17-Directed to Permits
59	Jeff	Pittman	The Chapel	Yes	Yes	12//7/17-Spoke with H. Brewer. 11/22/17-Called and left a message.
60	Gene	Potempa	The Greenhouse of Crystal Lake	Yes	Yes	6/14/17-Mailed/emailed plans for him to markup. Reduce Pr ROW to avoid septic, replace ditch with SS, PE ok for SS.
61	Ben	Redding	MCDOT	Yes	Yes	Support
62	Terri	Regione		No	Yes	Newsletter
63	Susan	Reinhardt		No	Yes	Newsletter
64	Mike	Rogulic	McHenry Heating	Yes	Yes	11/22/17-Called and discussed cross access with adjacent property owners.
65	Gerhard	Rosenberg	Self	Yes	Yes	Newsletter
66	Gurbax	Saini		No	Yes	2/23/17-Directed to Permits
67	Ed	Salisbury	Midwest Storage 2016 S RTE 31	Yes	Yes	2/22/17-Emailed info for cross access.
68	Meredith	Sarkees	self	Yes	Yes	Newsletter
69	Randy	Schietzelt	Land Conservatory of McHenry County	Yes	Yes	Newsletter
70	Jim	Scholtz	McHenry Public Library	Yes	Yes	2/22/17-Forwarded plans
71	Steven	Schwarz	HR Green	Yes	Yes	Support
72	Scott	Seibert		No	Yes	Newsletter
73	Dan	Shepard	Thelen Sand & Gravel, Inc	Yes	Yes	2/16/17-Forwarded website
74	Jean	Smith		No	Yes	Newsletter
75	David	Soskin	PO Box 1297, McHenry, IL 60051	No	Yes	Newsletter
76	Denise	Strom	McDonalds	Yes	Yes	2/20/18-Emailed revised plans.
77	John	Swierk	Direct Design LTD	Yes	Yes	2/5/18-Emailed updated plans with cross access. 2/22/17-Emailed info for cross access.
78	Gretchen	Thomas		No	Yes	Newsletter
79	Nancy	Tober		No	Yes	Newsletter
80	Leslie	Tremblay		No	Yes	Newsletter
81	Mark and Fran	Tropinski		Yes	Yes	Newsletter
82	Michelle	Trost		No	Yes	3/3/17-Emailed response
83	Martin and Nancy	Vazzano	prop owners, ourselves	Yes	Yes	Newsletter
84	Bryan	Villano	EDF RE	No	Yes	2/21/18-Emailed revised plan. 6/16/17-Phone call to revise plans and ROW. 3/6/17-Emailed my contact info.

IL 31
 IL 176 to IL 120
 McHenry County

Public Hearing Attendees and Comments

February 8, 2017 - March 10, 2017

Comment No.	First_Name	Last_Name	Representing	Attended Hearing ?	Provided Comment?	How Addressed?
85	Kelly	Weaver	ALCA	Yes	Yes	Newsletter
86	John	Williams		No	Yes	Support
87	Michael	Wolff	Self	Yes	Yes	1/24/18-Emailed revised plan. 12/29/17-Email with revised plan
88			McHenry County Conservation District	No	Yes	5/24/17-Response letter sent.
89			McHenry County Council of Governments	No	Yes	Support
90	Mike	Lesperance	Nunda Township Road District	Yes	Yes	3/17/17-Response letter sent.
91			USEPA	No	Yes	12/19/17-Response letter mailed.
92			USACE	No	Yes	12/19/17-Response letter mailed.

S:\WP\p&es\CONSULT\Projects - Active\IL 31 (IL 176 to IL 120)\Public Involvement\2017-02-08 Public Hearing\5-Responses\[Response.xlsx]Sheet1



Illinois Route 31

ROUTE 176 TO ROUTE 120

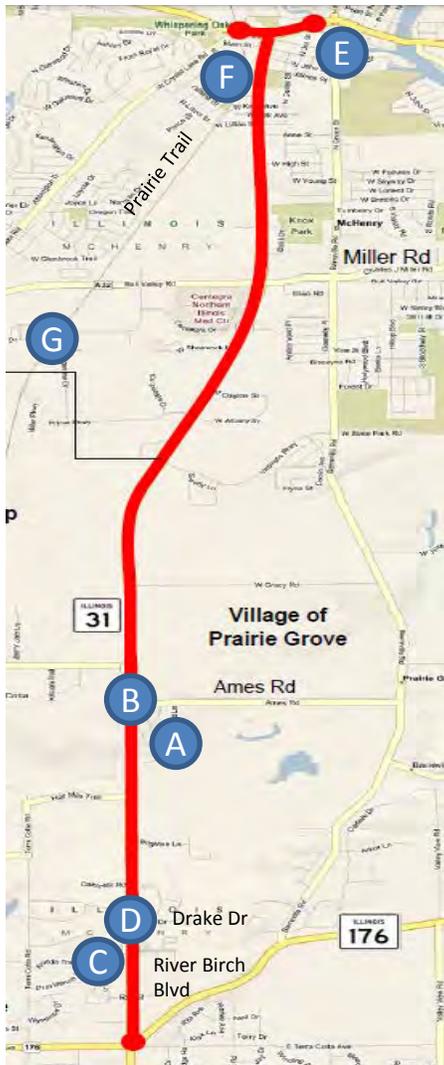
www.idot.illinois.gov/projects/il31



Public Hearing Newsletter

January 2018

Thank you for your attendance at the public hearing held on February 8, 2017 for the Illinois Department of Transportation's (Department) proposed improvement of IL 31 from IL 176 to IL 120 in McHenry County. Your comments have become part of the official public hearing record and are included in the Combined Design Report and Environmental Assessment Errata. There were 169 people that attended the hearing. A total of 92 comments were received during the comment period that lasted through March 10, 2017. The comments were evaluated and the plans modified where feasible. As many comments concerned similar issues, this newsletter has been prepared to address your concerns as well as provide you with an overview of topics. Please visit the project website at www.idot.illinois.gov/projects/il31 for future project updates.



Construction

Comments – When will construction start and how long will it take?

Unfortunately a specific or even general timeline is not available. Funding for land acquisition and construction is **not** included in the Department's FY 2018-2023 Proposed Highway Improvement Program (referred to as the multi-year program). Each fall, the Department solicits feedback on the upcoming multi-year program which is taken into consideration in developing the next year's program. In years past, the Department's outreach efforts included meetings, displays at fairs, traditional town hall meetings, and webinars. Details regarding outreach efforts will be updated on the Department's website, www.idot.illinois.gov as soon they are finalized. The new multi-year program is then published every spring.

The duration of construction depends on several factors, many of which are not known at this time. The Department advertises construction projects (lettings) seven times a year which determines when a project will start. Other factors include private utility relocations, and the number of construction contracts. Due to the project length and cost, the project may be divided into smaller contracts. In addition, advance contracts for items such as tree removal or stream restoration may be let separately to expedite construction.

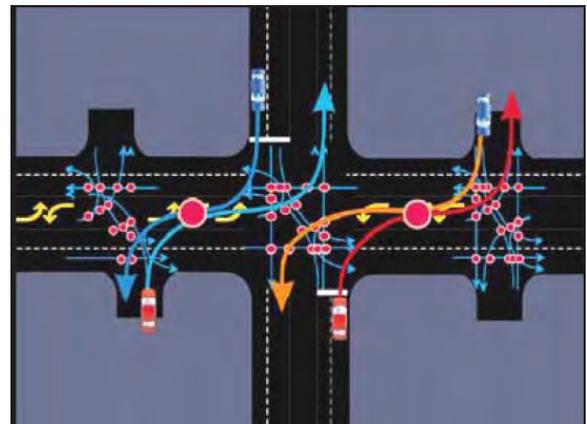
Barrier Median and U-Turns

Comments – A flush median should be provided instead; why such a wide median between IL 176 and Bull Valley/Miller Road; how do trucks make U-turns or access properties

Most of IL 31 within the project limits is a top “5% percent” location meaning that it experiences higher severity and frequency of crashes than similar roadways statewide. Access management, such as raised medians, is a method to control access to highways and is a proven safety countermeasure. Access management principles are applicable to roadways of all types, but are especially important on Strategic Regional Arterials such as IL 31. The benefits of access management include improved movement of traffic, reduced crashes, and fewer vehicle conflicts. Raised medians improve safety by managing access points so that turning and crossing movements occur at fewer locations, and allow drivers to predict where other drivers will turn and cross. Median openings are generally spaced at quarter-mile intervals to provide frequent U-turn opportunities for passenger vehicles and locations for police enforcement. Trucks would need to alter their route to access properties on the opposite side of the roadway.

A wide median is proposed to accommodate dual left turn lanes into some existing side streets and for the potential growth of large undeveloped parcels along IL 31. A flush two-way left-turn lane was considered, but was dismissed since the raised median is expected to provide a higher reduction of crashes as analyzed using methodologies in the Highway Safety Manual. In the northern section of the project there was not enough right-of-way available to provide full access to properties via U-turns so a flush median is proposed. Near IL 120 the right-of-way was so constrained that any median could not be proposed without substantial building impacts so turning restrictions on streets near the intersection will be implemented.

The FHWA considers left turns into and out of a driveway less safe than a U-turn, as they comprise the majority of driveway crashes. Studies have shown that providing a U-turn at a median opening to get to the opposite side of a busy highway rather than a direct left-turn from a side street or driveway can result in a 25% reduction in crashes.



Conflict points with a flush median

Cross Access

Comments – Need direct left-in and left-out. The Department should work with businesses to mitigate raised median impacts

The Department encourages adjacent properties to work together to form a combined entrance at a currently proposed median opening. Combined entrances reduce potential conflict points and are a public safety benefit. The Department can construct a combined entrance as part of the project if a signed cross access agreement between the properties is provided. Additional information on cross access, including an example agreement, is available on the [project website](#).

Additional Lanes

Comments - Projected traffic doesn't warrant additional through lanes; bus service should be expanded instead; why are existing right turn lanes not being replaced.

The existing average daily traffic on IL 31 varies between 17,100 and 24,600 vehicles per day (vpd) and is projected to increase up to 39,000 vpd in year 2040. A roadway with one lane in each direction, like IL 31 today, functions appropriately with 15,000 to 18,000 vpd. The Department works closely with Pace and local municipalities to provide multi-modal transportation opportunities. Bus user pads are currently proposed along northbound IL 31 at Gracy Road and southbound IL 31 at Edgewood Road. The proposed improvements are compatible with future bus service expansion. Right turn lanes require additional right-of-way and increase impacts to adjacent properties and the environment, and are only proposed where warranted. With the addition of a second through lane, right turning vehicles can utilize the outside through lane to make turns.

Oak Grove Subdivision



Comments - Avoid impacting the landscaped berm near Ames Road; the shared-use path should be on the west side of IL 31; a traffic noise abatement wall should be constructed; verify the project is compatible with the subdivision's drainage features; the subdivision was not informed of the project.

The proposed plan has been revised to maintain the landscaped berm south of Ames Road by reducing the width of the path from ten to eight feet, reducing the depth and width of the adjacent drainage swale, and steepening slopes to the berm. Shifting IL 31 to the west was considered, but is constrained by buildings located on the west side and would not provide a noticeable benefit.



When the subdivision was platted a 40 foot strip of land was dedicated for roadway purposes. The proposed shared-use path is located within this dedication. The path is a benefit to the subdivision by providing direct access to multi-modal accommodations along IL 31 and connections to signalized intersections to cross IL 31.

A noise abatement wall was evaluated along the subdivision; however it did not meet the criteria to be included in the project. Please note the construction of a noise abatement wall would have required additional right-of-way and potentially increase impacts to adjacent properties. The updated traffic noise analysis can be viewed on the [project website](#).

The proposed drainage plan is compatible with the modifications made by the subdivision to the exiting flow routes. The Department resurveyed the swale and berms between the existing culvert (Outfall 12) and the existing detention basin near Thunderbird Lane to ensure the roadway widening would not alter current flow paths.

This study has been following the Context Sensitive Solutions process which strives to involve stakeholders to ensure that the social, economic and environmental concerns of the surrounding community are considered. Membership to the Community Advisory Group (CAG) was open to all individuals and solicited at the first public meeting in 2012. Property owners and occupants adjacent to IL 31 were notified of the public meetings via post card. To increase involvement, ads were placed in the Northwest Herald newspaper and municipalities were coordinated with to expand notification lists through email lists and websites. The Department will continue to work with the subdivision throughout the project development.

Property Impacts

Comments – The proposed plan will impact my septic field, parking lot, or business sign

The Department is working with several property owners to address specific property issues. Revisions to the proposed plans have been made at many of these locations to minimize or avoid the impact. In other locations, additional coordination will occur during the land acquisition process when property impacts will be evaluated and potential mitigation measures, such as parking lot modifications or septic field relocation, are more closely evaluated.

Ames Road Intersection

Comments – Realign Ames Road to Edgewood Road or add a traffic signal at Ames Road because it is/will be dangerous to turn left onto IL 31; add a northbound right turn lane at Ames Road

Ames Road is under the jurisdiction of the Village of Prairie Grove. The realignment of Ames Road to Edgewood Road at IL 31 is identified in the Village's Comprehensive Plan. The Village could initiate engineering and construction for the realignment through either the Department's Bureau of Local Roads and Streets or the Bureau of Traffic Operations Permit Section. As requested, the Department performed traffic counts after the public hearing at the intersection of IL 31 and Ames Road to determine if a traffic signal is warranted. The results determined that a traffic signal is not warranted. Likewise, traffic volumes do not warrant an exclusive right turn lane. The right turn lane would impact the landscaped berm that has been requested to be maintained. It should be noted that Ames Road connects to Barreville Road which runs north-south between IL 176 and IL 120 and provides access to other east-west roadways that are signalized at IL 31.

North Ridge Professional Center (600-630 N. IL Route 31)

C

Comments – Need direct left-in and left-out.

The current entrance to the office complex is in such close proximity to the proposed median opening at River Birch Boulevard that another opening cannot be provided. In situations such as this, the Department encourages adjacent properties to work together to form a combined entrance at a currently proposed median opening. Please refer to the cross access discussion on Page 2 for additional information.

Drake Drive Intersection

D

Comments - There are many crashes; no gaps to make turns; add traffic signal

After the public hearing, the Department reviewed crash data from 2010 through 2015 at the intersection of IL 31 and Drake Drive, and determined a near-term improvement is not merited. In addition, the Department performed traffic counts and a traffic signal analysis after the public hearing. The results show a traffic signal is not warranted at the intersection. A traffic signal is proposed at the IL 31 and River Birch Boulevard intersection by the development along the east side of IL 31. Access to River Birch Boulevard will eventually be allowed to Drake Drive via Purdue Place.



Boone Creek Riverwalk

E

Comments – The proposed detention basin near Boone Creek will prohibit the City of McHenry's future Riverwalk

The proposed detention basin was coordinated with the City of McHenry and is compatible with the future riverwalk plans. The basin can include enhanced native landscaping to compliment the riverwalk. Landscaping plans will be developed during contract plan preparation. Depending on the enhancements, funding and maintenance may be required by the City of McHenry.

Historic Buildings in McHenry and On-Street Parking

F

Comments – Concerns of historic buildings being impacted and removal of on-street parking

Building displacements and impacts were minimized to the extent possible. The building located at the northwest corner of IL 31 and Main Street (right) was identified as a structure to avoid because of its local importance. As a result, the lanes along IL 31 were reduced to 10 feet wide, no median was proposed, and the on-street parking along IL 31 was removed. The removal of on-street parking also has safety benefits by removing vehicle conflict points. On-street parking is not recommended along Strategic Regional Arterials such as IL 31 and IL 120.



Landscaping & Tree Replacement

Comments – Landscaping should include native grasses; impacts to oak and hickory trees should be minimized; retaining walls should be considered to save trees; tree replacement should be in accordance with the species and caliper removed, and be locally grown.

Landscaping plans will be developed during contract plan preparation. The intent is to utilize the Department’s native seed mix to match the area and minimize maintenance needs. Measures to further minimize oak and hickory tree impacts were evaluated after the public hearing. Adjusting ditches and sidewalk /path were effective methods that were implemented. Retaining walls were evaluated and could save approximately 40 trees between Drake Drive and Shady Oaks Lane; however their high construction and long term maintenance costs were not reasonable. In lieu of retaining walls, the Department will work with adjacent municipalities and the McHenry County Conservation District to identify replacement areas near the project. Mitigation for tree removal will follow the Department’s Preservation and Replacement of Trees Policy, which is available on the [project website](#).

In locations where the raised median is wide enough, grass will be planted unless a local agency requests trees or enhanced landscaping. Depending on the treatment, the local agency may be required to help fund the additional cost, and will be required to provide maintenance.

Drainage Design

Comment - The proposed plan will change stormwater flow.

The drainage design has taken into account existing drainage patterns and analyzed impacts of the proposed improvements on the surrounding properties. Runoff from the widened roadway will be conveyed through oversized pipes to new detention basins. Furthermore, nine acres of additional right-of-way is proposed to retain runoff created by the new impervious surfaces within the project. This is accomplished with retention basins, widened ditches and an infiltration basin.



Pedestrian and Bicyclist Accommodations



Comments – The shared-use path is a duplication of the Prairie Trail, improve accommodations on east-west streets such as Ames Road and Edgewood Road.

While the proposed path and Prairie Trail are parallel, they support different uses. The proposed path will provide opportunities for both short and long distance trips along the corridor to connect to residential, commercial and industrial areas along IL 31. In addition, the proposed path will collect users from the east side of IL 31 and provide crossing opportunities to the west at the eight signalized intersections along the project. Streets under local jurisdiction such as Edgewood Road, Bull Valley Road, and Lillian Street provide a direct connection to the Prairie Trail. The Prairie Trail is a regional trail that connects McHenry County communities and other regional trails such as the Fox River Trail.

Wetland and Special Waste Mitigation

Comments - Wetland mitigation should be in watershed, what are the special waste disposal procedures

Wetland mitigation will be coordinated during contract plan preparation, and is expected to be attained at a wetland bank within the watershed. The Department also intends to work with agencies such as the McHenry County Conservation District to identify other potential wetland mitigation sites. Regarding special waste procedures, the Department conducts extensive soil testing during the design process and manages excavated soil in accordance with applicable federal and state laws and regulations in a manner that protects human health and the environment. Requirements for disposing excavated materials can be found in Title 35, Part 1100 of the Illinois Administrative Code.

Construction Concerns

Comments – Businesses will be impacted during construction, minimize construction duration

The Department will continue to work with adjacent businesses and property owners prior to and during construction. Representatives from local municipalities will be invited to attend the contract progress meetings where they will be given information that they can share with their communities about the current and upcoming activities.

Access to each property will be provided during construction. Driveways will receive a “Driveway Entrance” sign with an arrow directing traffic from both directions. If a property has two driveways, the contractor will provide access through one of the driveways while the other is being constructed. If a property has only one driveway, it will be temporarily widened during construction while the new driveway is being built or it will be built ½ at a time. Two-way traffic will be maintained along IL 31 at all times.

Property Values

Comments – My property value will decrease as a result of the project

Any property acquired or easements secured will result in compensation by the Department to the owner of that property. With respect to potential property value changes, the value of property is based on numerous factors and is a highly subjective matter, which is not easily quantifiable, either in a positive or negative sense.



Traffic and Construction Noise

No substantial changes in traffic-generated noise levels are expected as a result of this proposed improvement. Trucks and machinery used for construction will produce noise that may affect some residents and residential area activities during the construction period. Impacts resulting from construction noise are likely to occur during daylight hours and be temporary. The construction documents will include noise specifications, and the contractor will need to comply with any local noise ordinances.

Other Recent Improvements

Comments - Why do this since other projects were just completed

This is a long-range improvement designed to meet current and year 2040 needs. Other recent improvements such as roadway resurfacing and adding the bi-directional median between Ames Road and Edgewood Road were maintenance and near-term safety improvements. Please note that the recent intersection improvements at the IL 31 intersections of IL 176, Bull Valley Road, and IL 120/Richmond Road are compatible with the proposed improvements.

Next Steps

The Department is nearing completion of the Phase I study as we incorporate comments from the public hearing and refine the engineering studies, right-of-way requirements, and environmental documents.

* Funding for contract plan preparation is included in the Department’s FY 2018 - 2023 Proposed Highway Improvement Program. The remaining elements of the project (land acquisition and construction) are not included, but will be considered for future funding among similar improvement needs throughout the region.

From: Czaplicki, Scott D
To: "janness@forevergardens.com"
Cc: [Murphy, Kimberly K.](#); [Brown, Lori S.](#)
Subject: IL 31; Plans at Abraham Property
Date: Wednesday, February 22, 2017 1:49:00 PM
Attachments: [Abraham-Pages from \(11X17\) Proposed Drainage Plan 20170202.pdf](#)
[Abraham-Pages \(11x17\) IL31-Plan Profile-111016.pdf](#)

Janness,

Sorry you were not able to attend the public hearing. All the materials that were presented are available for viewing at www.ilroute31.com/publicmeetings.html. The preferred alternative plan at your residence can be viewed at http://www.ilroute31.com/Downloads/PublicHearing/PH_Preferred_Alternative_Aerial-Sheet3.pdf. In addition, attached are preliminary roadway and drainage plans at your property. Please contact me with any questions.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov

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PRELIMINARY PRINT

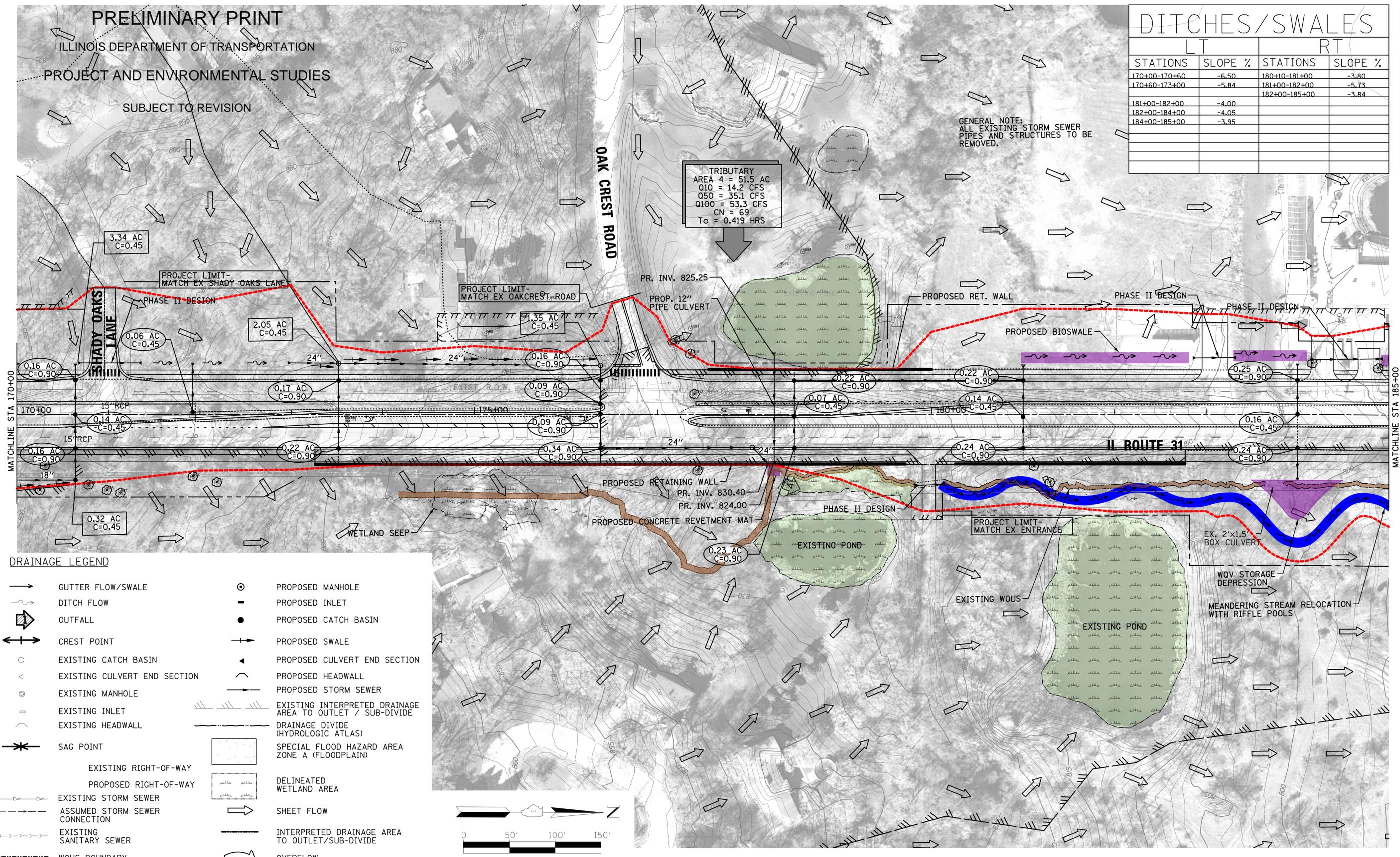
ILLINOIS DEPARTMENT OF TRANSPORTATION
PROJECT AND ENVIRONMENTAL STUDIES

SUBJECT TO REVISION

DITCHES/SWALES			
LT		RT	
STATIONS	SLOPE %	STATIONS	SLOPE %
170+00-170+60	-6.50	180+10-181+00	-3.80
170+60-173+00	-5.84	181+00-182+00	-5.73
		182+00-185+00	-3.84
181+00-182+00	-4.00		
182+00-184+00	-4.05		
184+00-185+00	-3.95		

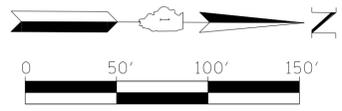
GENERAL NOTE:
ALL EXISTING STORM SEWER
PIPES AND STRUCTURES TO BE
REMOVED.

TRIBUTARY
AREA 4 = 51.5 AC
Q10 = 14.2 CFS
Q50 = 35.1 CFS
Q100 = 53.3 CFS
CN = 69
Tc = 0.419 HRS



DRAINAGE LEGEND

	GUTTER FLOW/SWALE		PROPOSED MANHOLE
	DITCH FLOW		PROPOSED INLET
	OUTFALL		PROPOSED CATCH BASIN
	CREST POINT		PROPOSED SWALE
	EXISTING CATCH BASIN		PROPOSED CULVERT END SECTION
	EXISTING CULVERT END SECTION		PROPOSED HEADWALL
	EXISTING MANHOLE		PROPOSED STORM SEWER
	EXISTING INLET		EXISTING INTERPRETED DRAINAGE AREA TO OUTLET / SUB-DIVIDE
	EXISTING HEADWALL		DRAINAGE DIVIDE (HYDROLOGIC ATLAS)
	SAG POINT		SPECIAL FLOOD HAZARD AREA ZONE A (FLOODPLAIN)
	EXISTING RIGHT-OF-WAY		DELINEATED WETLAND AREA
	PROPOSED RIGHT-OF-WAY		SHEET FLOW
	EXISTING STORM SEWER		INTERPRETED DRAINAGE AREA TO OUTLET/SUB-DIVIDE
	ASSUMED STORM SEWER CONNECTION		OVERFLOW
	EXISTING SANITARY SEWER		
	WOUS BOUNDARY		



From: [Czaplicki, Scott D](#)
To: [Doug Martin](#)
Cc: [Wayne Jett](#); [Jon Schmitt](#); [RON BYKOWSKI](#); [Troy Strange](#); [Derik Morefield](#); [Brown, Lori S.](#)
Subject: IL 31; RE: Letter Waukegan and Front
Date: Monday, February 05, 2018 2:25:00 PM
Attachments: [image001.png](#)
[image002.png](#)

Doug,

Thank you for the letter. We will have the consultant revise the plans so the Department can review the change. Please note we just mailed the letter of intent for this project that includes a CD of the proposed geometry (with the Waukegan Road cul-de-sac). We'll forward you an updated plan after the Department review is complete.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov

-
 Please consider the environment before printing this message or attachments

From: Doug Martin [mailto:dmartin@ci.mchenry.il.us]
Sent: Friday, February 02, 2018 3:23 PM
To: Czaplicki, Scott D <Scott.Czaplicki@illinois.gov>
Cc: Wayne Jett <wjett@ci.mchenry.il.us>; Jon Schmitt <jschmitt@ci.mchenry.il.us>; RON BYKOWSKI <rmbykowski@comcast.net>; Troy Strange <tstrange@ci.mchenry.il.us>; Derik Morefield <dmorefield@ci.mchenry.il.us>
Subject: [External] Letter Waukegan and Front

Hard copy will be mailed.
Take Care Scott and thanks.
Doug

Douglas P. Martin
Director of Economic Development
City of McHenry
333 S Green Street
McHenry, IL 60050
815.363.2110 (d)
815.363.2173 (f)
815.790.4752 (c)
dmartin@ci.mchenry.il.us
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[McHenry Market Pulse](#)
[@mchmarketpulse](#)



Office of the Mayor
333 S. Green Street
McHenry, IL 60050
www.ci.mchenry.il.us

February 1, 2018

Scott Czaplicki, P.E.
Illinois Department of Transportation
201 Center Court
Schaumburg, IL 60196

Re: Letter of Support for Right-In Access off of Front Street to Waukegan Road and Retention of Usable Developable Land at the Southeast Corner of Front Street and Elm Street in the City of McHenry

Dear Mr. Czaplicki:

The purpose of this letter is to express my complete support for Mr. Bykowski's right-in from Illinois Route 31/Front Street at Waukegan Road. This is paramount for the business located at the southeast corner of Front Street and Waukegan Road (1112 Front Street) to deliver cars. There are no alternative routes for car carriers if Waukegan Road is transformed into a cul-de-sac.

Waukegan Road, in its current configuration, is operating well and safe and is enforced by police. Mr. Bykowski owns most of the southeast corner of Illinois Route 31 and Illinois Route 120 (Elm Street) and has plans to redevelop it. Any future development will be prohibited from accessing westbound Waukegan Road if the right-in is maintained.

Additionally, Mr. Bykowski would like to retain as much land as possible in the parcels which are identified as displacements for redevelopment purposes, at the southeast corner of Illinois Route 120 and Illinois Route 31.

I greatly appreciate your time and consideration of my requests. If you have any questions regarding this letter, please do not hesitate to contact me at 815-363-2108.

Sincerely,

Wayne Jett, Mayor
City of McHenry

The City of McHenry is dedicated to providing the citizens, businesses and visitors of McHenry with the highest quality of programs and services in a customer-oriented, efficient and fiscally responsible manner.

From: [Czaplicki, Scott D](#)
To: scarruthers@crystallake.org
Cc: [Murphy, Kimberly K.](#); [Brown, Lori S.](#)
Subject: IL 31; Public Hearing Comment
Date: Wednesday, April 19, 2017 11:38:09 AM
Attachments: [Carruthers \(Crystal Lake\).pdf](#)

Steve,

Thank you for the comments you provided at the public hearing for the proposed improvement of IL 31 from IL 176 to IL 120 in McHenry County (attached). The Department appreciates your support for the project. You questioned if the north leg of the recently improved IL 31 and IL 176 intersection will have to be removed and replaced which will impact the adjacent businesses again. The north leg through the dual left turns lanes was constructed for the ultimate improvement (two through lanes in each direction, dual left turn lanes and a raised curb median) and will not need to be removed and replaced. From that point north through Ray Street the improvement tapered down to the existing flush median and one/two lanes in each direction. Widening IL 31 to the ultimate width in this section appears to be feasible since the existing and proposed profiles are similar; however, the decision to widen or reconstruct will be determined during Phase II contract plan preparation. A Special Design/Construction Consideration will be added to the Combined Design Report to consider construction duration and business impacts in the decision to widen or reconstruct this section. Please note partial funding for Phase II engineering is included in the Department's FY 2017-2022 Proposed Highway Improvement Program. The remaining elements of the project, land acquisition and construction, will be included in our priorities for future funding consideration among similar improvement needs throughout the region.

Please contact me with any questions.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov



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From: Czaplicki, Scott D
To: mike@adamssteelservice.com
Cc: "sales@adamssteelservice.com"
Subject: RE: [External] RE: [FWD: RE: IL 31; Cross Access at Adams Steel and Midtown Storage]
Date: Wednesday, March 01, 2017 8:29:00 AM

Mike,

The link below contains MicroStation files for your reference. Your location is about in the middle of the project near Station 307+00. In addition to sending us a CADD file, please also provide a PDF plot in the area of your property. Thank you.

<https://fileT.illinois.gov/filet/download.asp?key=eCuEl1LgBZh9wN3ViU1cRHoetma40sTc>

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov



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From: sales@adamssteelservice.com [mailto:sales@adamssteelservice.com]
Sent: Tuesday, February 28, 2017 11:11 AM
To: Czaplicki, Scott D
Cc: mike@adamssteelservice.com
Subject: [External] RE: [FWD: RE: IL 31; Cross Access at Adams Steel and Midtown Storage]

Scott: I've attached the signed disclaimer.

If you have any questions, please contact Mike at 815-385-9100 or by email:
mike@adamssteelservice.com

Thanks. Heidi

Adams Steel Service Inc.
2022 S IL Route 31 (we are 1 mile south of Gary Lang Chevy)
McHenry, IL 60050-8211
815-385-9100 phone
815-385-8382 fax

----- Original Message -----

Subject: [FWD: RE: IL 31; Cross Access at Adams Steel and Midtown Storage]

From: <mike@adamssteelservice.com>

Date: Tue, February 28, 2017 10:13 am

To: "Adams Steel" <sales@adamssteelservice.com>

----- Original Message -----

Subject: RE: IL 31; Cross Access at Adams Steel and Midtown Storage
From: "Czaplicki, Scott D" <Scott.Czaplicki@illinois.gov>
Date: Tue, February 28, 2017 8:04 am
To: "mike@adamssteelservice.com" <mike@adamssteelservice.com>

Mike,

It was a pleasure speaking with you this morning. Please sign and complete the attached CADD disclaimer form and I'll forward the files for you to draw the location of your desired entrance. Thank you.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov

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From: Czaplicki, Scott D
Sent: Wednesday, February 22, 2017 2:35 PM
To: 'mike@adamssteelservice.com'; 'bluetoolpro@aol.com'
Cc: Murphy, Kimberly K.; Brown, Lori S.
Subject: IL 31; Cross Access at Adams Steel and Midtown Storage

Mr. Chambers and Mr. Salisbury,

Thank you for providing comments at the February 8, 2017 public hearing concerning the proposed improvement of IL 31 in McHenry County. Your comments requested changes to the potential cross access location that was presented at the hearing. The exhibit can be viewed at http://www.ilroute31.com/Downloads/PublicHearing/PH_12-Cross_Access_Locations.pdf. Attached for your use is the preliminary plan & profile, cross access fact sheet, and example cross access agreement for further coordination between your properties. Please sketch the location of the septic field and the layout of the desired combined entrance on the attached plan. With that information we can work with you on refining the design. Please contact me with any questions.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov

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From: Czaplicki, Scott D
To: ["Ruthie Harvey"](#)
Cc: [Murphy, Kimberly K.](#); [Brown, Lori S.](#)
Subject: IL 31; Public Comment (Harvey)
Date: Friday, March 10, 2017 1:31:00 PM
Attachments: [Harvey.pdf](#)

Ms. Harvey,

Thank you for your comments regarding the proposed improvement of IL 31 from IL 176 to IL 120 in McHenry County. You questioned the status of land acquisition and if a preferred alternative has been selected for the north section. Partial funding for Phase II engineering is included in the Department's FY 2017-2022 Proposed Highway Improvement Program. The remaining elements of the project, Phase II land acquisition and Phase III construction, will be included in our priorities for future funding consideration among similar improvement needs throughout the region. The preferred alternative for the north section has been selected and is shown on the Preferred Alternative Aerial exhibits from the public hearing. The exhibits are available on the [project website](#). The north section begins on sheet 11.

If you have any questions or need additional information, please contact me.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov

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From: [Czaplicki, Scott D](#)
To: "[UJHicks2015@Gmail.com](#)"; "[fran.b.hicks@gmail.com](#)"
Cc: [Brown, Lori S.](#)
Subject: IL 31; Public Hearing Comments (Hicks)
Date: Wednesday, January 24, 2018 12:19:00 PM
Attachments: [Hicks-Court Reporter-Comments 22-23.pdf](#)
[Hicks.pdf](#)
[2018-01-24 Hicks \(Revised Plan Sheet\).pdf](#)

Mr. & Mrs. Hicks,

Thank you for the comments you provided at the public hearing for the Illinois Department of Transportation's (Department) proposed improvement of IL 31 from IL 176 to IL 120 in McHenry County (attached). Your comments are part of the official public hearing record. The Department is nearing completion of the Phase I study as we incorporate comments from the public hearing and refine the engineering studies, right-of-way requirements, and environmental documents. Funding for contract plan preparation is included in the Department's 2018-2023 Proposed Highway Improvement Program. The remaining elements of the project (land acquisition and construction) are not included, but will be considered for future funding among similar improvement needs throughout the region.

You requested that the oak trees along the south side of Edgewood Road not be impacted. As discussed during the Community Advisory Group meetings, the design was modified to avoid these oak trees. Attached is an updated plan sheet with a note to avoid the oak trees.

Please visit the project website at www.idot.illinois.gov/projects/il31 for project updates. If you have any questions or need additional information, please contact me.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov

-
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From: Czaplicki, Scott D
To: ["bert@secondamendsports.com"](mailto:bert@secondamendsports.com)
Subject: IL 31; Public Hearing Comment (Second Amendment Sports)
Date: Friday, March 10, 2017 2:36:00 PM
Attachments: [Irslinger Jr-Court Reporter-Comments 9-11.pdf](#)
[Second Amendment Sports Pages from \(11X17\) Proposed Drainage Plan 20170202.pdf](#)

Mr. Irslinger,

Thank you for the comments you provided at the public hearing for the proposed improvement of IL 31 from IL 176 to IL 120 in McHenry County (attached). To help us review your requests, could you provide a plan or sketch where your proposed accessory building will be located? Attached is the preliminary proposed drainage plan for your use if needed.

If you have any questions or need additional information, please contact me.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov



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From: [Czaplicki, Scott D](#)
To: ["DRBPI@Mc.net"](mailto:DRBPI@Mc.net)
Cc: [Brown, Lori S.](#)
Subject: IL 31; Public Hearing Comment (Dr. Irslinger)
Date: Thursday, February 15, 2018 2:41:00 PM
Attachments: [Irslinger Sr-Court Reporter-Comments 8.pdf](#)
[Irslinger Sr Revised Plan 20180215 .pdf](#)

Dr. Irslinger,

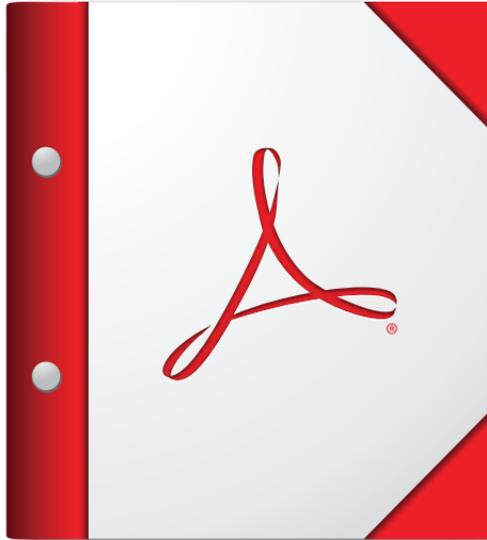
Thank you for your comment (attached) regarding the February 8, 2017 public hearing for the proposed improvement of IL 31 from IL 176 to IL 120 in McHenry County. As requested, the proposed plan has been revised to include a 24-foot wide entrance and a flush median at that entrance. The entrance is highlighted yellow in the attached plan. Since your driveway was located at the transition area between the raised and flush median, the flush median was able to be extended south to High Street. Regarding coordination of work in temporary easements, the Department will work with you during construction to maintain access to your building. Please note that only funding for contract plan preparation is included in the Department's 2018-2023 Proposed Highway Improvement Program. The remaining elements of the project (land acquisition and construction) are not included, but will be considered for future funding among similar improvement needs throughout the region.

If you have any questions, please contact me.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov

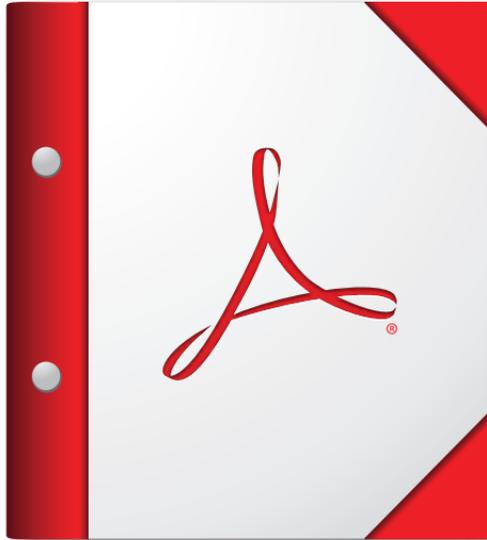
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From the Bureau of : Programming

To: Ms. Patricia Knapik Date: November 17, 2017
3711 Freund Avenue
McHenry, IL 60050 Project: IL 31
IL 176 to IL 120
P-91-135-99

PTB No.: 155-015/170-007

We are sending you:

Plans Copy of Letter Other See below

Date	Description
	Public Hearing Exhibit Sheet 14 of 15

- For your information Approved as submitted Resubmit
 For your review/comment Approved as noted Included in report
 Returned for revisions As requested

Remarks:

Ms. Knapik,

As requested in your public hearing comment (attached), enclosed is Public Hearing Exhibit Sheet 14 of 15. Please contact me with any questions or concerns. Thank you.

Signed  Phone Number (847) 705-4678
Scott Czaplicki, P.E. scott.czaplicki@illinois.gov

From: [Czaplicki, Scott D](#)
To: ["Kathy Martinez"](#)
Cc: [Brown, Lori S.](#); ["Thomas Z Hayward Jr"](#)
Subject: RE: IL 31; Cross Access at Terra Cotta - Location #1
Date: Monday, February 05, 2018 12:35:00 PM
Attachments: [2017-08-10 Cross Access Agreement \(Location #1\).pdf](#)
[2018-01-30 - Cross Access Location#1 P&P-8.pdf](#)

Ms. Martinez,

The proposed IL 31 plans have been updated in accordance with the signed cross access agreement in your letter dated August 10, 2017. Please see the yellow highlighted combined entrance in the attached plan & profile sheet. Additional coordination will occur during the land acquisition process regarding the temporary easements to be donated, shown outlined in blue. Please note that only funding for contract plan preparation is included in the Department's 2018-2023 Proposed Highway Improvement Program. The remaining elements of the project (land acquisition and construction) are not included, but will be considered for future funding among similar improvement needs throughout the region.

If you have any questions, please contact me.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov

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From: Kathy Martinez [mailto:kathym@tcindustries.com]
Sent: Friday, March 10, 2017 3:37 PM
To: Czaplicki, Scott D <Scott.Czaplicki@illinois.gov>
Cc: Murphy, Kimberly K. <Kimberly.Murphy@Illinois.gov>; Brown, Lori S. <Lori.S.Brown@Illinois.gov>; 'Thomas Z Hayward Jr' <thomashayward@gmail.com>
Subject: [External] RE: IL 31; Cross Access at Terra Cotta - Location #1

Scott,

I confirm receipt of the documents provided to me via your e-mail of February 23, 2017 relating to the Cross Access Agreement and related sketches and exhibits. As we discussed yesterday, I am working on finalizing the Cross Access Easement document and exhibits required to be returned to IDOT. I anticipate getting drafts to you for review next week. Thank you for your assistance and cooperation in this matter.

Kathleen M. Martinez
General Manager
Terra Cotta Realty Co.
3703 S. Route 31
Crystal Lake, IL 60012
815-333-8235

From: Czaplicki, Scott D [<mailto:Scott.Czaplicki@illinois.gov>]
Sent: Thursday, February 23, 2017 9:00 AM
To: Kathy Martinez <kathym@tcindustries.com>
Cc: Murphy, Kimberly K. <Kimberly.Murphy@Illinois.gov>; Brown, Lori S. <Lori.S.Brown@Illinois.gov>
Subject: FW: IL 31; Cross Access at Terra Cotta - Location #1

Ms. Martinez,

Attached are the preliminary plan and profile sheets along TCI's property between Half- Mile Trail and Ames Road for your use in documenting the combined entrance north of the engineering office building. The conceptual combined entrance was presented at the public hearing as Location #1 on the [Potential Cross Access exhibit](#). Please sketch the layout of the desired combined entrance on the attached plan so we can work with you on refining the design. Your previous letters requesting this access are attached for reference. Please contact me with any questions.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov



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From: Czaplicki, Scott D
Sent: Wednesday, February 22, 2017 2:47 PM
To: 'Kathy Martinez'; 'jswierk@ddcaarchitects.com'
Cc: Murphy, Kimberly K.; Brown, Lori S.
Subject: IL 31; Cross Access at Terra Cotta and Direct Design

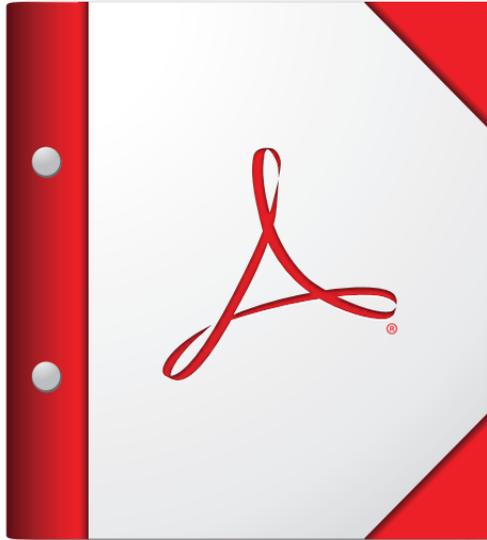
Ms. Martinez and Mr. Swierk,

Thank you for providing comments at the February 8, 2017 public hearing concerning the proposed improvement of IL 31 in McHenry County. Your comments requested changes to the potential cross access location that was presented at the hearing. The exhibit can be viewed at http://www.ilroute31.com/Downloads/PublicHearing/PH_12-Cross_Access_Locations.pdf. Attached for your use is a preliminary plan & profile sheet, cross access fact sheet, and example cross access agreement for further coordination between your properties. Please sketch the layout of the desired combined entrance on the attached plan. With that information we can work with you on refining the design. Please contact me with any questions.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov



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MEETING SUMMARY

Senator Althoff and Arvidson Pools
August 24, 2017

The purpose of the meeting was to discuss access options for Arvidson Pools and adjacent properties.

1. Senator Althoff explained the funding status for the project and need for the proposed improvement.
2. Median openings are typically proposed every quarter mile to facilitate U-turns and police enforcement. This is in accordance with design criteria for Strategic Regional Arterials (SRA) like IL 31. The properties are located between Ames Road and Edgewood Road. These roads are spaced approximately one-quarter mile apart and each have median openings. A traffic signal is proposed at Edgewood Road.
3. A break in the median needs to serve multiple properties, otherwise it is considered a private benefit and the property owner needs to pay the additional cost for the left turn lane and median opening's full depth pavement.
4. Arvidson Pools provided the following information:
 - A median opening is needed for truck access. There are limited alternative routes for trucks in the area. The Department suggested Edgewood Road-Bay Road to Half Mile Trail may be an option since both Edgewood Road and Half Mile Trail have proposed traffic signals, and Nunda Township Road District is located along Bay Road.
 - Their building is proposed to be expanded to the back (west) property line so cross access behind and within the properties is not feasible. There is a grade difference in the rear at the property to the north (Remsing). There is also a cell tower on the property that is currently under lease that may affect any back access.
 - The IL 31 (Ames Road to Edgewood Road) project constructed a flush bi-directional median and seems to have reduced the number of crashes.
 - Questioned why the raised median was required when another property they own along Palatine Road has a flush median. The safety benefits and access control requirements of a SRA were noted.
5. Arvidson's current driveway is approximately half-way between the adjacent median openings at Ames Road and Edgewood Road. It appears to be spaced far enough from Ames Road to provide the required deceleration distance if a median opening is added.
6. Remsing Construction (north of Arvidson Pools) does not have an immediate need for direct access, but the property may be sold and access will be a concern.
7. The property owners may consider a combined access in front of Arvidson building; however, this is where their septic field is located.
8. An access drive behind Arvidson and Remsing within the property to the west (Jim Hicks) was discussed. This would need to be initiated by the Village of Prairie Grove. They also have concerns what the stormwater detention requirements would be for additional impervious area. Baxter & Woodman will review with the Village of Prairie Grove and report back to the attendees.

IL 31
IL 176 to IL 120
McHenry County

9. The Department requested the property owners to provide a proposed median break location and combined access location as soon as possible to be included in the Phase I study. The study is anticipated to be completed by the end of the year.

By: Scott Czaplicki, IDOT/Programming-Consultant

Attachments:

- Attendance Sheet

S:\WP\p&es\CONSULT\Projects - Active\IL 31 (IL 176 to IL 120)\Meetings\Property Owner\2017-08-24 Arvidson Pools\2017-08-24 Meeting Summary (Arvidson Pools) DRAFT.docx



Illinois Department of Transportation

Attendance Roster

Bureau: Programming

Section: Project & Environmental Studies

Project/Topic: IL 31; Arvidson Pools

Date: 8/24/17

Time: 10:30 a.m.

Location: Arvidson Pools

	Attendees	Representing	Phone Number	Email Address
1.	Scott Czaplinski	IDOT/consultant	(847) 705-4678	scott.czaplinski@illinois.gov
2.	Kimberly Muegg	IDOT-DI	847-705-4711	Kimberly.Muegg@illinois.gov
3.	Jason Fluhr	Prairie Grove/ Baxter & Woodman	815-444-3222	jfluhr@baxterwoodman.com
4.	John Baczek	IDOT	847 705 4119	john.baczek@illinois.gov
5.	Pamela Althoff	senator	(815) 455-6330	pamela@pamelaalthoff.net
6.	Dennis Marandi	ARVIDSON, INC	815-861-6916	Dennis.M@arvidsons.com
7.	John J. Lemsing	Keltic Properties	315-356-7400	john@lemsing.com
8.				
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1/24/17

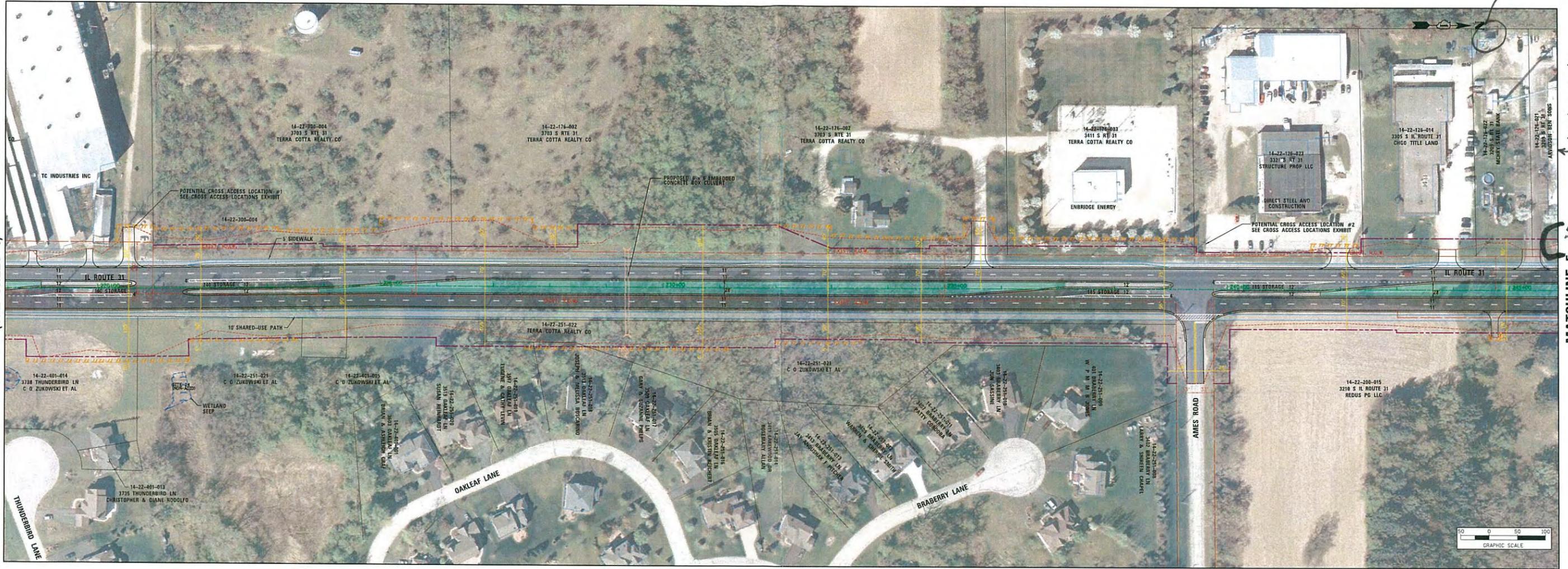
cell tower

Arvidson Pools

ATC #1

MATCHLINE 5 (SEE SHEET 6)

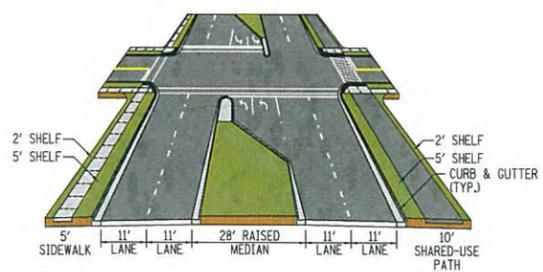
MATCHLINE 4 (SEE SHEET 4)



LEGEND

- | | | | | | | |
|-----|-------------------------------------|-----|---------------------------------|-----|-----------------------------------|----------------------|
| — | EX. BUILDING OR STRUCTURE | — | EX. FLOODPLAIN | ●●● | EX. TRAFFIC SIGNAL TO REMAIN | |
| --- | EX. MUNICIPAL BOUNDARY | — | EX. WATERS OF THE U.S. (WOUS) | ●●● | EX. TRAFFIC SIGNAL TO BE REPLACED | |
| --- | EX. PROPERTY LINE | --- | EX. WETLANDS | ●●● | ●●● | PROP. TRAFFIC SIGNAL |
| --- | EX. RIGHT OF WAY LINE | --- | PROP. GRASS MEDIAN | ●●● | | |
| --- | PROP. CENTERLINE | --- | PROP. MEANDERING STREAM / BASIN | | | |
| --- | PROP. LIMIT OF CONST. (APPROXIMATE) | --- | PROP. PAVEMENT SURFACE | | | |
| --- | PROP. RIGHT OF WAY LINE | --- | PROP. RAISED CURB MEDIAN | | | |
| --- | PROP. TEMP. EASEMENT LINE | --- | COMMERCIAL DISPLACEMENT | | | |
| | | --- | RESIDENTIAL DISPLACEMENT | | | |
| | | --- | DRAINAGE OUTFALL | | | |

PROPOSED TYPICAL SECTION



PRELIMINARY

SUBJECT TO CHANGE
PUBLIC HEARING
FEBRUARY 8, 2017

Illinois Route 31
ROUTE 176 TO ROUTE 120
www.idot.illinois.gov/projects/il31



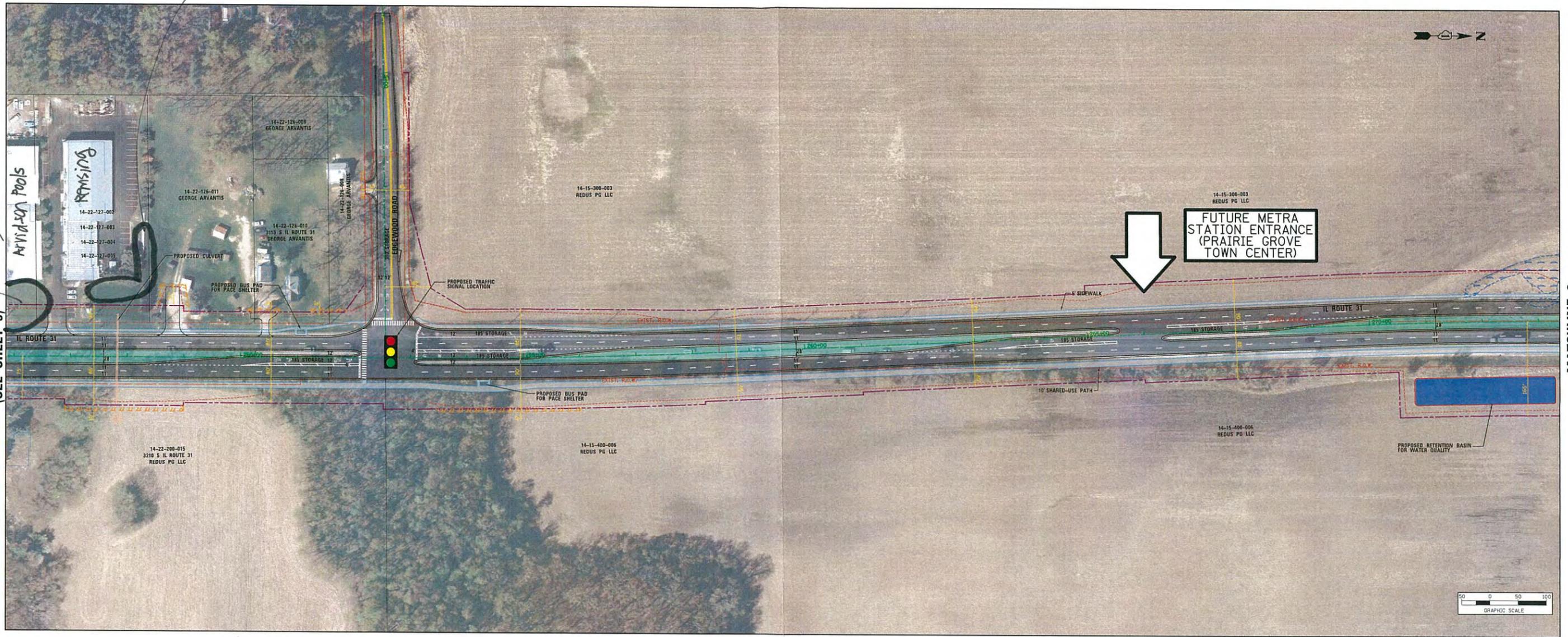
ILLINOIS ROUTE 31
SOUTH SECTION: ILLINOIS ROUTE 176 TO SOUTH OF BULL VALLEY ROAD

PREFERRED ALTERNATIVE
SHEET 5 OF 15

SPOTIC #2

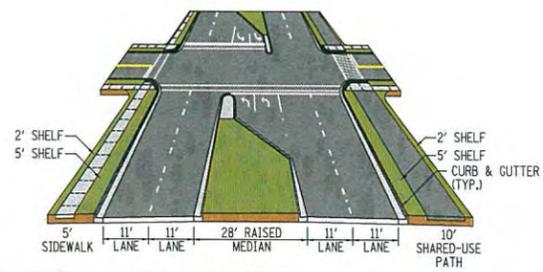
MATCHLINE 5
(SEE SHEET 5)

MATCHLINE 6
(SEE SHEET 7)



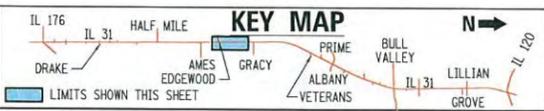
LEGEND					
	EX. BUILDING OR STRUCTURE		EX. FLOODPLAIN		EX. TRAFFIC SIGNAL TO REMAIN
	EX. MUNICIPAL BOUNDARY		EX. WATERS OF THE U.S. (WOUS)		EX. TRAFFIC SIGNAL TO BE REPLACED
	EX. PROPERTY LINE		EX. WETLANDS		PROP. TRAFFIC SIGNAL
	EX. RIGHT OF WAY LINE		PROP. GRASS MEDIAN		
	PROP. CENTERLINE		PROP. MEANDERING STREAM / BASIN		
	PROP. LIMIT OF CONST. (APPROXIMATE)		PROP. PAVEMENT SURFACE		
	PROP. RIGHT OF WAY LINE		PROP. RAISED CURB MEDIAN		
	PROP. TEMP. EASEMENT LINE		COMMERCIAL DISPLACEMENT		
			RESIDENTIAL DISPLACEMENT		
			DRAINAGE OUTFALL		

PROPOSED TYPICAL SECTION



PRELIMINARY

SUBJECT TO CHANGE
PUBLIC HEARING
FEBRUARY 8, 2017



ILLINOIS ROUTE 31
SOUTH SECTION: ILLINOIS ROUTE 176 TO SOUTH OF BULL VALLEY ROAD

PREFERRED ALTERNATIVE
SHEET 6 OF 15



Illinois Department of Transportation

Office of Highways Project Implementation / Region 1 / District 1
201 West Center Court / Schaumburg, Illinois 60196-1096

May 24, 2017

Ms. Elizabeth S. Kessler, MBA, CPRE
Executive Director
McHenry County Conservation District
18410 US Highway 14
Woodstock, IL 60098

Dear Ms. Kessler:

Thank you for your February 22, 2017 letter regarding the Illinois Department of Transportation's (Department) proposed improvement of IL 31 from IL 176 to IL 120 in McHenry County. Partial funding for Phase II engineering is included in the Department's FY 2017-2022 Proposed Highway Improvement Program. The remaining elements of the project will be included in our priorities for future funding consideration among similar improvement needs throughout the region. The general scope of work for this improvement is anticipated to consist of roadway widening and reconstruction to provide two lanes in each direction separated by a median, bicycle and pedestrian accommodations, and intersection and drainage improvements.

In your letter, concerns were noted that the proposed shared-use path is a duplication of the Prairie Trail, and its construction cost and maintenance would be entirely a local agency responsibility. While the proposed path and Prairie Trail are parallel, they would provide different uses. The proposed path will provide opportunities for both short and long distance trips along the corridor to connect to residential, commercial and industrial areas along IL 31. In addition, the proposed path will collect users from the east side of IL 31 and provide crossing opportunities to the west at the eight signalized intersections along the project. Streets under local jurisdiction such as Edgewood Road, Bull Valley Road, and Lillian Street provide a direct connection to the Prairie Trail. Regarding local agency participation, the local cost share for new pedestrian and bicyclist facilities is 20% of the construction cost, plus a 15% engineering fee. The local agency, as you state in your letter, must agree to accept long-term responsibility for the administration, control, reconstruction and maintenance of the shared-use path and/or sidewalk.

Regarding impacts to oak and hickory trees, the preservation of trees has been a Department concern throughout the study. We are currently evaluating the proposed design to further reduce impacts to oak and hickory trees. The McHenry County Conservation District (MCCD) and public will be notified of the results of this analysis through a project newsletter this summer. Tree replacement at nearby conservation areas can be considered, as well as at areas along the project, as outlined in Department Policy D&E-18, Preservation and Replacement of Trees. A link to the policy is available on the project website at www.idot.illinois.gov/projects/il31 under Info Center/Newsletters.

Ms. Elizabeth S. Kessler, MBA, CPRE
May 24, 2017
Page 2

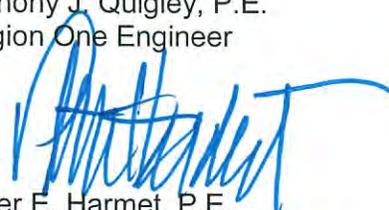
A Special Design/Construction Consideration (SDCC) will be added to the Phase I Combined Design Report to work with the MCCD during Phase II, contract plan preparation, to identify the location, type and number of trees to be planted. The MCCD would need to donate any temporary easements required for this work.

A SDCC will also be added to the Phase I report for the Department to work with MCCD during Phase II, contract plan preparation, to identify potential wetland mitigation sites.

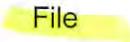
If you have any questions or need additional information, please contact Scott Czaplicki, Project Manager, at (847) 705-4678.

Very truly yours,

Anthony J. Quigley, P.E.
Region One Engineer

By: 
Peter E. Harmet, P.E.
Bureau Chief of Programming

Attachments

bcc:  File

S:\Gen\WP\p&es\CONSULT\Projects - Active\IL 31 (IL 176 to IL 120)\Correspondence\Tickles\Internal Tickle 1704 Response .docx



BUREAU OF PROGRAMMING
RECEIVED

FEB 24 2017

DISTRICT #1

February 22, 2017

Scott Czaplicki, Bureau of Programming
Illinois Department of Transportation
201 West Center Court
Schaumburg, IL 60196-1096

RE: *Illinois Route 31
Route 176 to Route 120*

Dear Mr. Czaplicki:

Thank you for the opportunity to provide comment on the above project. The McHenry County Conservation District does not have any property directly impacted by the proposed project as presented at the February 8, 2017 public hearing. Several staff members of the District attended and have provided the following comments.

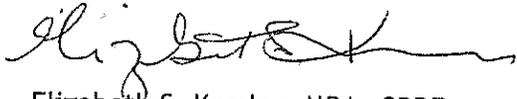
First, it should be noted that the McHenry County Conservation District's existing 26-mile linear multiuse Prairie Trail runs parallel to the proposed shared use path and is separated by a distance of only 400-600 feet within the City of McHenry and does not show a connection between the two trails. A portion of the proposed shared use trail would be a duplication of services and it is our understanding that the proposed shared use trail is not included in the state funding request. The cost of construction and ongoing maintenance would fall to the local governments. The District is not interested in participating in this part of the project at this juncture but would encourage the placement of signage and on-street routes to direct users to the existing multiuse trail to safely traverse north and south through the county and to the western businesses within the City of McHenry.

There is concern that the 84 Oak trees and 17 Hickory trees would be impacted by the current alignment and that some of these critical local species could be avoided with a shifting of the project either to the east or west. If such a move is unavoidable, several nearby Conservation Areas (Stickney Run and Silver Creek) could benefit from replacement trees which may be required to mitigate such an impact.

The District may also be able to assist in the anticipated wetland mitigation measures by providing off-site local wetland improvements within the Regional watershed. The District will continue to stay informed on this project as it moves into Phase II.

If you have any questions or we can be of further assistance, please do not hesitate to give us a call.

Sincerely,
McHENRY COUNTY CONSERVATION DISTRICT



Elizabeth S. Kessler, MBA, CPRE
Executive Director

- c:
- Bona Heinsohn, President, Board of Trustees
 - Stephen Barrett, Vice President
 - Brandon Thomas, Treasurer
 - Dave Kranz, Secretary
 - Pete Merkel, Trustee
 - Vern Scacci, Trustee
 - Dave Brandt, Trustee
 - Robert Nowak, Liaison, McHenry County Board
 - Ed Collins, Director of Land Preservation & Natural Resources
 - John Kremer, Director of Operations & Public Safety
 - Val Siler, Land Preservation Manager
 - Amy Peters, Planning Manager



From the Bureau of : Programming

Name:	<u>Ryan McGuire</u>	Date:	<u>November 20, 2017</u>
Title:	<u>Property Manager</u>		
Organization:	<u>Digital Pix and Composites</u>	Project:	<u>IL 31</u>
Address:	<u>1811 South Route 31</u>		<u>IL 176 to IL 120</u>
	<u>McHenry, IL 60050</u>		<u>P-91-135-99</u>
Telephone:	<u>(815) 363-2800</u>		
E-Mail:	<u>Ryan@DPCPix.com</u>	PTB No.:	<u>155-015 & 170-007</u>

Items Discussed:

I called Digital Pix to follow-up regarding their public hearing comments and to convey information learned from the City of McHenry (City). The City noted that The Chapel and Waste Management recently entered into a cross access agreement and a sanitary sewer was extended to The Chapel and stubbed at Digital Pix. Digital Pix is aware of the sanitary stub and cross access agreement. Digital Pix has concerns of truck access from IL 31 during and after construction, parking loss, dock access, and well and septic impacts. Digital Pix is interested in cross access to the north and south. They have spoken with McHenry Heating (Mike Rogulic) to the south, but they are not ready to commit to cross access. The land acquisition process was discussed and noted that a parking layout study and circulation study could be included as part of the appraisal process. Funding for land acquisition or construction is not included in the Department's FY 2018-2023 multi-year program.

Follow-Up:

Send email to Mr. McQuire with my contact information.

By: Scott Czaplicki
Bureau of Programming/Consultant

From: [Czaplicki, Scott D](#)
To: [Ryan McGuire](#)
Subject: IL 31; Phone Call Follow-Up (Digital Pix)
Date: Monday, November 20, 2017 12:00:00 PM

Ryan,

It was good speaking with you this morning regarding the proposed improvement of IL 31 between IL 176 and IL 120. My contact information is below for your forwarding. Thank you.

Scott Czaplicki, P.E.
(847) 705-4678 Office
(630) 291-0869 Cell
scott.czaplicki@illinois.gov



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Illinois Department of Transportation

Office of Highways Project Implementation / Region 1 / District 1
201 West Center Court / Schaumburg, Illinois 60196-1096

March 17, 2017

Mr. Mike Lesperance
Highway Commissioner
Nunda Township
3518 Bay Road
Crystal Lake, IL 60012

Dear Mr. Lesperance:

Thank you for your February 15, 2017 letter regarding the Illinois Department of Transportation's (Department) proposed improvement of IL 31 from IL 176 to IL 120 in McHenry County. Partial funding for Phase II engineering is included in the Department's FY 2017-2022 Proposed Highway Improvement Program. The remaining elements of the project will be included in our priorities for future funding consideration among similar improvement needs throughout the region. The general scope of work for this improvement is anticipated to consist of roadway widening and reconstruction to provide two lanes in each direction separated by a median, bicycle and pedestrian accommodations, and intersection and drainage improvements.

The Department appreciates your support for the project. In your letter, the Township requested retaining walls along IL 31 between Drake Drive and Ames Road to reduce tree impacts and provide a noise barrier. The preservation of trees has been a concern throughout the study. Retaining walls will be evaluated to assess their effectiveness in reducing tree impacts.

A traffic noise analysis was performed using projected year 2040 noise levels and concluded that four of the seven receptors in this area (R11, R12, R13, and R15) merited an evaluation of traffic noise abatement walls. However, none of the walls met the criteria to implement them as part of this project. Due to public comments received after the February 8, 2017 public hearing for this project, the Department is evaluating a traffic noise abatement wall for the Oak Grove subdivision located along the east side of IL 31 south of Ames Road (R14 and R16). See attached Noise Receptor Maps for receptor locations. The original noise analysis is part of the Environmental Assessment and can be found on the project website at www.idot.illinois.gov/projects/il31.

Mr. Mike Lesperance
March 17, 2017
Page 2

In addition, Ames Road was requested to be realigned with Edgewood Road so Prairie Grove residents can make a left turn onto IL 31 at a signalized intersection to go south. Ames Road is under the jurisdiction of the Village of Prairie Grove (Village) and this realignment is identified in their Town Center & Transit-Oriented Development Plan dated June 2010. The Village could initiate engineering and construction for an improvement on Ames Road through either the Department's Bureau of Local Roads and Streets or the Bureau of Traffic Operations Permit Section. More information on the funds available to local governments for infrastructure improvements can be found at <http://www.idot.illinois.gov/transportation-system/local-transportation-partners/county-engineers-and-local-public-agencies/index>.

As a result of comments from the public hearing, new traffic counts will be performed and a traffic signal warrant performed at the IL 31 and Ames Road intersection. Regardless of additional improvements, we would like to note that Ames Road connects to Barreville Road which runs north-south between IL 176 and IL 120 and provides access to other east-west roadways that are signalized at IL 31.

If you have any questions or need additional information, please contact Kimberly Murphy, Consultant Studies Unit Head, at (847) 705-4791.

Very truly yours,

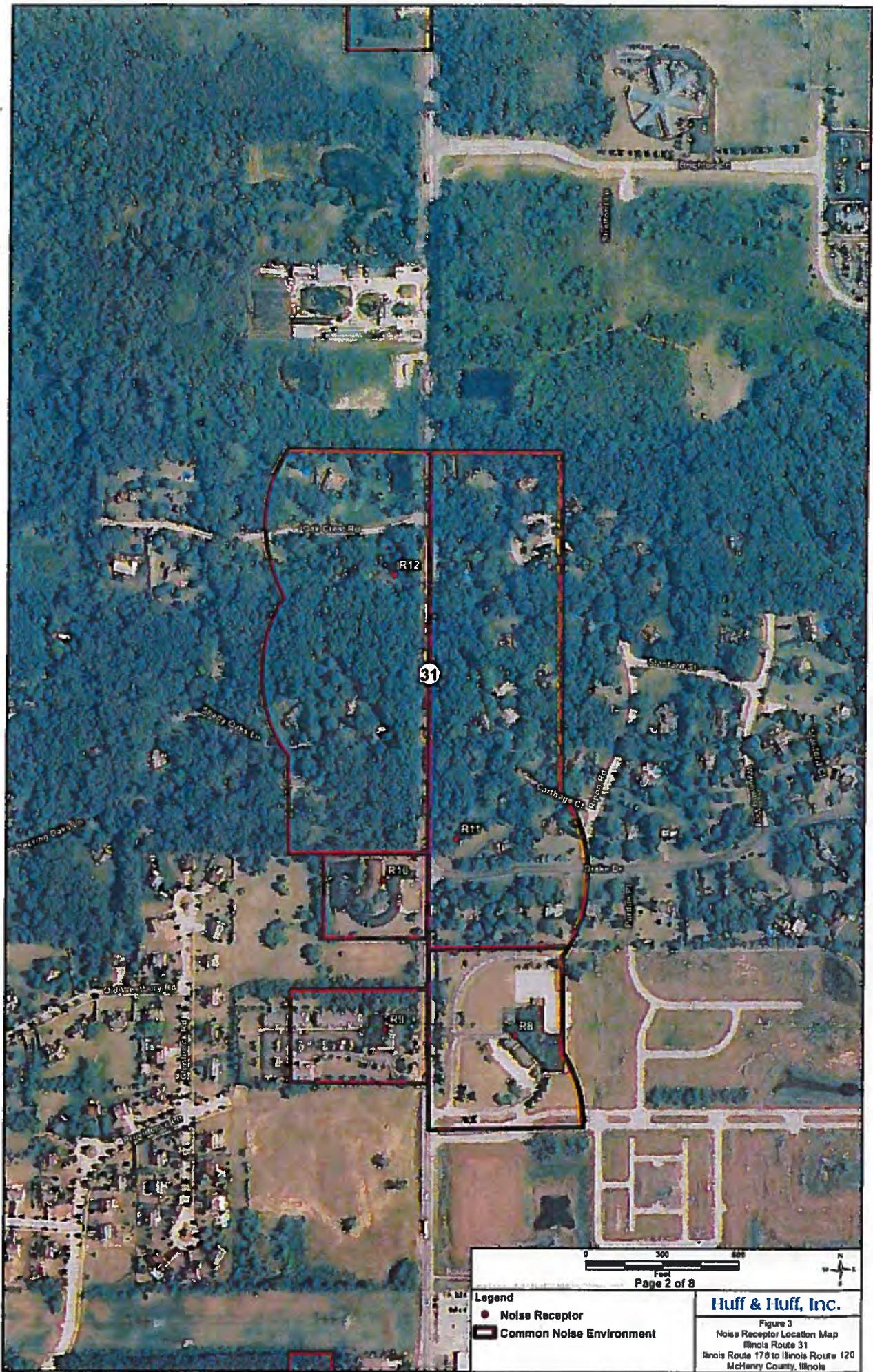
Anthony J. Quigley, P.E.
Region One Engineer

By: 
Jose Rios, P.E.
Engineer of Program Development

Attachments

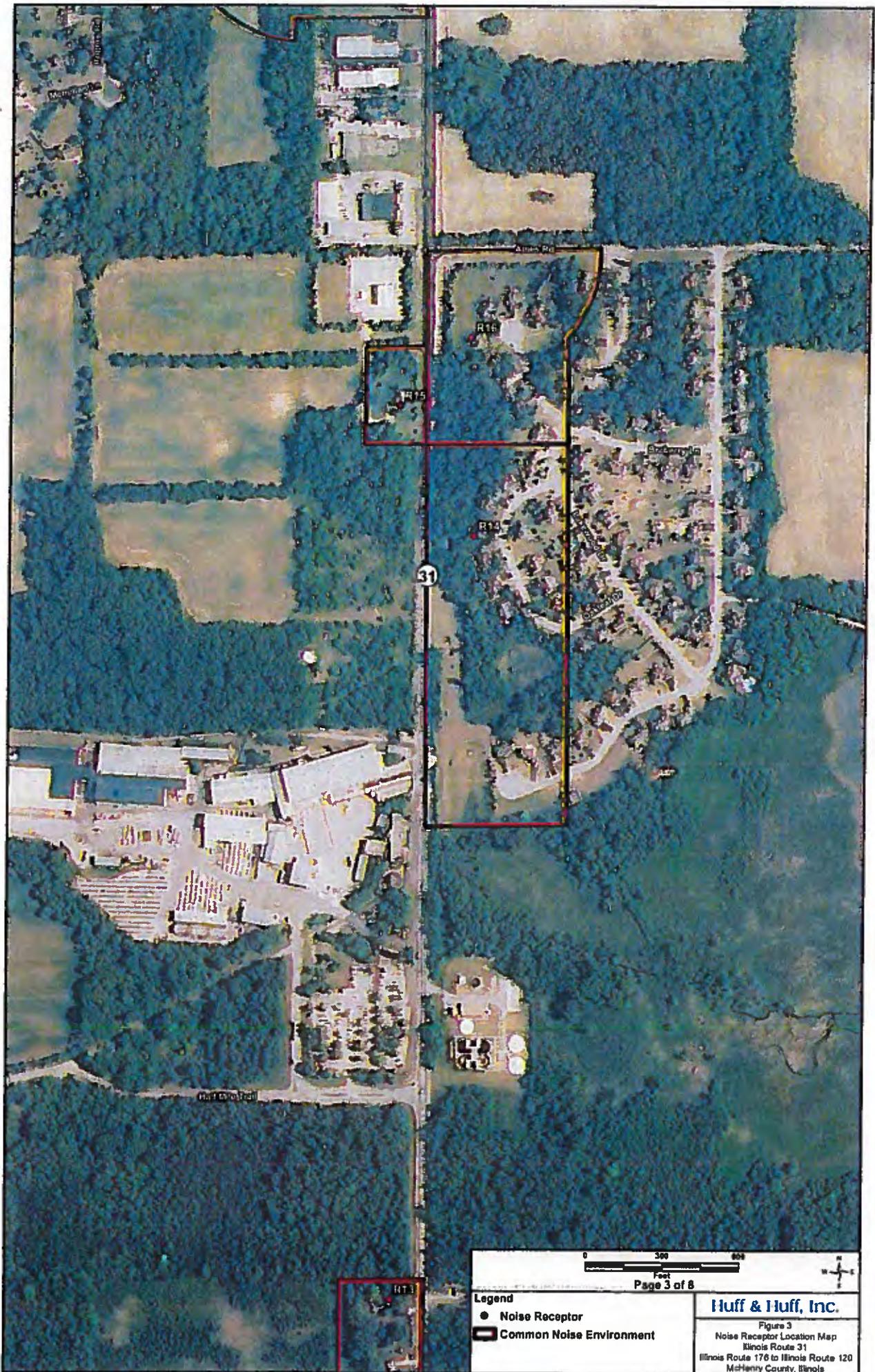
bcc: Jose Rios
Brian Carlson
File

Prepared By: Scott Czaplicki, Ext. 4678
Bureau of Programming



Legend
 ● Noise Receptor
 [Red Outline] Common Noise Environment

Huff & Huff, Inc.
 Figure 3
 Noise Receptor Location Map
 Illinois Route 31
 Illinois Route 178 to Illinois Route 120
 McHenry County, Illinois



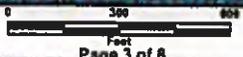
31

R15

R16

R14

R13



Page 3 of 8

- Legend**
- Noise Receptor
 - ▭ Common Noise Environment

Huff & Huff, Inc.
 Figure 3
 Noise Receptor Location Map
 Illinois Route 31
 Illinois Route 176 to Illinois Route 120
 McHenry County, Illinois



31

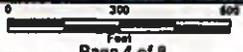
R19

R16

R17

Ridgeview Rd

Rogers Ave



Page 4 of 8

- Legend**
- Noise Receptor
 - ▭ Common Noise Environment

Huff & Huff, Inc.

Figure 3
 Noise Receptor Location Map
 Illinois Route 31
 Illinois Route 178 to Illinois Route 120
 McHenry County, Illinois



MIKE LESPERANCE
HIGHWAY COMMISSIONER

February 15, 2017

Mr. Jose Rios, P.E.
Illinois Dept. of Transportation
Office of Highways Project Implementation / Region 1 / District 1
201 West Center Court
Schaumburg, Illinois 60196-1096

Dear Mr. Rios,

As the Nunda Township Highway Commissioner looking at the overall project improvement of Route 31 from Route 176 to Route 120, I think it is long overdue, much needed and very much appreciated.

As an elected official and concerned citizen I would like to request some changes to the plans.

First, I would like to request retaining walls / sound barriers from Drake Drive Latitude and Longitude 42.260286, -88.286761 to L.L. 42.265039, -88.286778 and from 42.275065, -88.286846 to Ames Road 42.281133, -88.286859. In these two areas there are many residents with very rural settings. On their properties there exist many very old growth and healthy Oak trees. In my opinion, rather than removing them and cutting a steep slope, we could take less land and extend the retaining walls above grade to create a sound barrier.

Second, Ames Road should be re-aligned to Edgewood Road in order to allow all of the residents of Prairie Grove to turn left at the stop light to go to Crystal Lake.

It seems to me now is the time to address this issue while we are already doing a \$75 million dollar improvement. The danger and time, especially in turning left or turning right with a U-turn, could be easily rectified either with a frontage road or an S curve on Ames Road.

I offer as much time and energy as it will take to see these suggestions come to pass. Please call on me for any assistance you may need.

Sincerely,

NUNDA TOWNSHIP ROAD DISTRICT

Mike Lesperance
Highway Commissioner

cc: Scott Czaplac

From: [Czaplicki, Scott D](#)
To: highway@nundaroaddistrict.com
Cc: [Brown, Lori S.](#); [Murphy, Kimberly K.](#)
Subject: IL 31; Path Maintenance on State ROW
Date: Wednesday, April 19, 2017 8:11:33 AM

Mike,

At the IL 31 public hearing you asked if the Township would need to obtain a permit from IDOT to maintain the proposed path. Below is a response from the District's ADA Coordinator in coordination with the District's Permit Engineer. Please let me know if you have any questions.

Any entity working on ped/bike facilities in the public ROW owned by the State (i.e. reconstructing/adding) needs a permit to do the work unless it's done as part of a Local Roads or State Highway improvement. Maintenance work such as sidewalk vertical discontinuity beveling and grinding, patching, etc. would not require a permit in my opinion. Snow removal is a maintenance item as such it would also not require a permit.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov



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From: [Czaplicki, Scott D](#)
To: ["Potempa8@Gmail.com"](mailto:Potempa8@Gmail.com)
Cc: [Brown, Lori S.](#)
Subject: IL 31; The Greenhouse of Crystal Lake
Date: Wednesday, June 14, 2017 12:23:00 PM
Attachments: [Potempa-Court Reporter-Comments 24-25.pdf](#)
[Cross Access FAQs.pdf](#)
[Cross Access Easement Agreement \(Example\).docx](#)
[Potempa - PH Preferred Alternative Aerial-Sheet3.pdf](#)

Mr. Potempa,

In response to your public hearing comment (attached) and our discussion last week, please sketch the desired location of your entrances on the attached preferred alternative plan sheet and return it to me. I will mail copies of this plan with a self-addressed stamped envelope. You can also scan the sketch and email it to me. We will evaluate your request to add a northbound left turn lane into your business once we receive your sketch. The Department encourages combining and reducing entrances along state highways to improve safety. For the left turn lane to be considered a public benefit it will need to serve multiple properties and a cross access agreement needs to be signed by you and your neighbor(s). A typical combined entrance would be located on the property line (shown as a thick black line on the plan). An example agreement and cross access FAQ sheet is attached for your use. Other potential cross access locations along IL 31 can be viewed [here](#).

Please note we are also refining the proposed design to reduce the proposed right-of-way needs within your property (shown as a thicker purple line on the plan). Please contact me with any questions. Thank you.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov

-
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From the Bureau of : Programming

To: Mr. Eugene Potempa Date: June 14, 2017
Greenhouse of Crystal Lake
4317 State Route 31 Project: IL 31
Crystal Lake, IL 60012 IL 176 to IL 120
P-91-135-99

PTB No.: 155-015/170-007

We are sending you:

Plans Copy of Letter Other See below

Date	Description
	Preferred Alternative Plan
	Cross Access FAQ Sheet
	Example Cross Access Agreement
	Self-addressed Stamped Envelope

- For your information Approved as submitted Resubmit
 For your review/comment Approved as noted Included in report
 Returned for revisions As requested

Remarks:

Please sketch the desired location of your entrances on the attached preferred alternative plan sheet and return it to me. We will evaluate your request to add a northbound left turn lane into your business once we receive your sketch. The Department encourages combining and reducing entrances along state highways to improve safety. For the left turn lane to be considered a public benefit it will need to serve multiple properties and a cross access agreement needs to be signed by you and your neighbor(s). A typical combined entrance would be located on the property line. Please contact me with any questions.

Signed  Phone Number (847) 705-4678
Scott Czaplicki, P.E. scott.czaplicki@illinois.gov



From the Bureau of : Programming

Name:	<u>Mike Rogulic</u>	Date:	<u>November 22, 2017</u>
Title:	<u>Owner</u>		
Organization:	<u>McHenry Heating & Air, Inc.</u>	Project:	<u>IL 31</u>
Address:	<u>4561 Prime Parkway</u>		<u>IL 176 to IL 120</u>
	<u>McHenry, IL 60050</u>		<u>P-91-135-99</u>
Telephone:	<u>(815) 444-9900</u>		
E-Mail:	<u>Mike@MchenryHeating.com</u>	PTB No.:	<u>155-015 & 170-007</u>

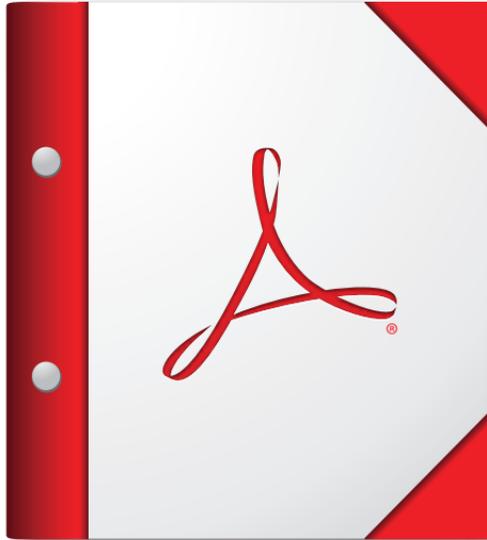
Items Discussed:

I called McHenry Heating to follow-up regarding their public hearing comments and to convey information learned from the City of McHenry (City). The City noted that The Chapel and Waste Management recently entered into a cross access agreement and a sanitary sewer was extended to The Chapel and stubbed at Digital Pix. McHenry is aware of the sanitary stub and cross access agreement. McHenry Heating has concerns of truck access from IL 31 due to their expanding excavating business. McHenry Heating is interested in a frontage road to Veterans Parkway, and would like a traffic signal installed at that intersection with IL 31. Funding for land acquisition or construction is not included in the Department's FY 2018-2023 multi-year program.

Follow-Up:

Recontact with any new developments.

By: Scott Czaplicki
Bureau of Programming/Consultant



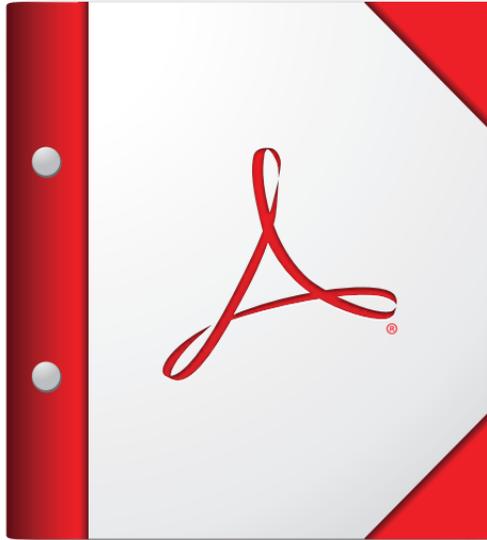
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From: Czaplicki, Scott D
To: ["shepardd@thelensg.com"](mailto:shepardd@thelensg.com)
Cc: info@ilroute31.com
Subject: RE: [External] Form Submission Digest
Date: Thursday, February 16, 2017 12:30:00 PM

Mr. Shepard,

The materials presented at the public hearing are available at the link below. We'll revise the website to make them easier to find.

<http://www.ilroute31.com/publicmeetings.html>

Scott Czaplicki, P.E.

(847) 705-4678 **New Phone Number**

scott.czaplicki@illinois.gov

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From: formmailer@secureserver.net [mailto:formmailer@secureserver.net]
Sent: Tuesday, February 14, 2017 3:30 PM
To: info@ilroute31.com
Subject: [External] Form Submission Digest

MEETING SUMMARY

Snelton Construction
November 2, 2017

The purpose of the meeting was to discuss future access to the Snelton/Remsing Construction building located at 3119 IL Route 31. The meeting was requested by the property owner. The following items were discussed:

1. Representatives for Snelton conveyed the following concerns:
 - a. There are many large trucks that use this building and prohibiting left turn into and out will affect the business.
 - b. There are no short-distance alternative routes due to roadway load restrictions.
 - c. The property owner the north located at the southwest corner of IL 31 and Edgewood Road is not interested in allowing cross access.
 - d. The property owner to the south (Arvidson Pools) is not interested in cross access because their septic field is in the front of their building and they are planning to expand their building to their back property line.
2. The Department noted that funding for contract plan preparation is included in the Department's FY 2018-2023 Proposed Highway Improvement Program; however, land acquisition and construction is not. The improvement may be divided into more than one construction project due to the project length and cost.
3. A median opening has been discussed at the existing Arvidson Pools entrance for cross access with the property to the south. The Department requires a cross access agreement be provided in order for the median opening to be proposed. The Department offered to extend the left turn lane from this median opening north to the Snelton/Remsing entrance for a left-in only movement. A left-out of this entrance to northbound IL 31 will not be allowed. The property owner will be responsible for the additional cost to extend the left turn lane since it is benefitting only one property. If the median opening for Arvidson Pools does not happen, then the property owner will be responsible for the entire cost of the median opening and left turn lane taper and storage bay. Correspondence from the property owner agreeing to these conditions will need to be provided to the Department in order for this to be included with the proposed improvement.

By: Scott Czaplicki, IDOT/Programming-Consultant

Attachments:

- Attendance Sheet

S:\WP\p&es\CONSULT\Projects - Active\IL 31 (IL 176 to IL 120)\Meetings\Property Owner\2017-11-02 Snelton\2017-11-02 Meeting Summary (Snelton).docx



Illinois Department of Transportation

Attendance Roster

Bureau: Programming

Section: Project & Environmental Studies

Project/Topic: IL 31

Date: 11/2/17

Time: 9:30

Location: IDOT / Executive Conference Room

	Attendees	Representing	Phone Number	Email Address
1.	Scott Czapliski	IDOT / Prog-consultant	(647) 705-4678	scott.czapliski@illinois.gov
2.	MARC PAULOS	III FFL	815-600-1652	mpoulos@iiffl.org
3.	Matt Hughes	MRL Solutions LLC	217.836.0225	Hughesmatttr@gmail.com
4.	John Baczek	IDOT	847 705 4119	john.baczek@illinois.gov
5.	Kimberly Murphy	IDOT	847-705-4791	Kimberly.Murphy@illinois.gov
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From the Bureau of : Programming

Name:	<u>Vorgias Stanton</u>	Date:	<u>February 22, 2017</u>
Title:	<u>Owner</u>		
Organization:	<u>518 Front Street – Residential D</u>	Project:	<u>IL 31</u>
Address:	<u>10005 Fair Lane</u>		<u>IL 176 to IL 120</u>
	<u>Union, IL 60180</u>		<u>P-91-135-99</u>
Telephone:	<u>(815) 923-2521</u>		
E-Mail:	<u></u>	PTB No.:	<u>155-015 & 170-007</u>

Items Discussed:

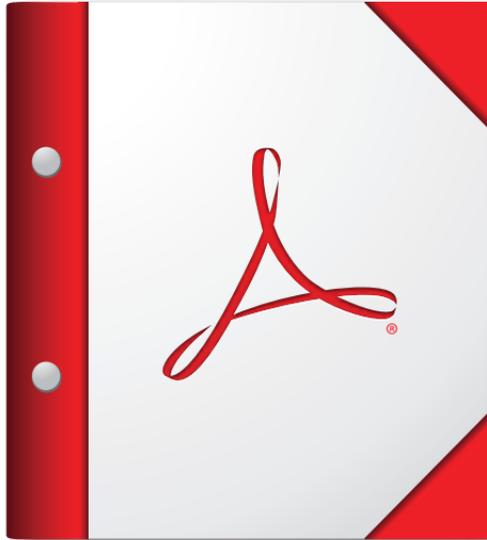
Mr. Stanton is the owner of 518 Front Street, a residential property in the City of McHenry which is one of the proposed displacements. A certified letter was sent to Mr. Stanton as an invitation to the public hearing and to provide land acquisition/relocation information. The certified letter was returned to IDOT as “Unclaimed”. Mr. Stanton was called and a message left regarding this property.

Denise called back and gave Mr. Stanton’s cell number (815) 245-6098. She said they attended the public hearing and spoke with several project team members included Mike Cullian from the Bureau of Land Acquisition. I informed her to contact me if they need any additional information.

Follow-Up:

Resend the letter non-certified.

By: Scott Czaplicki
Bureau of Programming/Consultant



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From: [Czaplicki, Scott D](#)
To: [Kathy Martinez](#); [John Swierk](#)
Cc: [Brown, Lori S.](#); ["Thomas Z Hayward Jr"](#)
Subject: RE: IL 31; Cross Access at Terra Cotta and Direct Design
Date: Monday, February 05, 2018 12:35:00 PM
Attachments: [2017-08-07 Cross Access Agreement \(Location #2\).pdf](#)
[2018-01-31 - Cross Access-Location#2 \(P&P-9\).pdf](#)

Ms. Martinez/Mr. Swierk,

The proposed IL 31 plans have been updated in accordance with the signed cross access agreement in your letter dated August 7, 2017. Please see the yellow highlighted combined entrance in the attached plan & profile sheet. Additional coordination will occur during the land acquisition process regarding the temporary easements to be donated, shown outlined in blue. Please note that only funding for contract plan preparation is included in the Department's 2018-2023 Proposed Highway Improvement Program. The remaining elements of the project (land acquisition and construction) are not included, but will be considered for future funding among similar improvement needs throughout the region.

If you have any questions, please contact me.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov

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From: Kathy Martinez [mailto:kathym@tcindustries.com]
Sent: Friday, March 10, 2017 3:25 PM
To: Czaplicki, Scott D <Scott.Czaplicki@illinois.gov>
Cc: Murphy, Kimberly K. <Kimberly.Murphy@Illinois.gov>; Brown, Lori S. <Lori.S.Brown@Illinois.gov>; John Swierk <jswierk@ddcaarchitects.com>; 'Thomas Z Hayward Jr' <thomaszhayward@gmail.com>
Subject: [External] RE: IL 31; Cross Access at Terra Cotta and Direct Design

Scott,
I confirm receipt of the documents provided to Mr. Swierk and I via your e-mail of February 22, 2017 relating to the Cross Access Agreement and related sketches and exhibits. As we discussed yesterday, John Swierk and I are working on finalizing the Cross Access Easement document and exhibits required to be returned to IDOT. I anticipate getting drafts to you for review next week. Thank you for your assistance and cooperation in this matter.

Kathleen M. Martinez
General Manager
Terra Cotta Realty Co.
3703 S. Route 31

Crystal Lake, IL 60012
815-333-8235

From: Czaplicki, Scott D [<mailto:Scott.Czaplicki@illinois.gov>]
Sent: Wednesday, February 22, 2017 2:47 PM
To: Kathy Martinez <kathym@tcindustries.com>; jswierk@ddcaarchitects.com
Cc: Murphy, Kimberly K. <Kimberly.Murphy@Illinois.gov>; Brown, Lori S. <Lori.S.Brown@Illinois.gov>
Subject: IL 31; Cross Access at Terra Cotta and Direct Design

Ms. Martinez and Mr. Swierk,

Thank you for providing comments at the February 8, 2017 public hearing concerning the proposed improvement of IL 31 in McHenry County. Your comments requested changes to the potential cross access location that was presented at the hearing. The exhibit can be viewed at http://www.ilroute31.com/Downloads/PublicHearing/PH_12-Cross_Access_Locations.pdf. Attached for your use is a preliminary plan & profile sheet, cross access fact sheet, and example cross access agreement for further coordination between your properties. Please sketch the layout of the desired combined entrance on the attached plan. With that information we can work with you on refining the design. Please contact me with any questions.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov

-
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From the Bureau of : Programming

Name:	<u>H. Brewer</u>	Date:	<u>December 7, 2017</u>
Title:	<u>Director of Facilities</u>		
Organization:	<u>The Chapel</u>	Project:	<u>IL 31</u>
Address:	<u>1809 State Rte 31</u>		<u>IL 176 to IL 120</u>
	<u>McHenry, IL 60050</u>		<u>P-91-135-99</u>
Telephone:	<u>(847) 347-4635</u>		
E-Mail:	<u>Hbrewer@chapel.org</u>	PTB No.:	<u>155-015 & 170-007</u>

Items Discussed:

Mr. Brewer called in response to previous calls to Jeff Pittman, whom made a comment at the public hearing. The Chapel has redevelopment plans to extend their driveway around the building. They currently have three driveways along IL 31 and two are proposed to be closed due to inadequate space to regrade them since IL 31 is being raised. They would like two entrances. There may be an opportunity to have a second driveway to the south if it is shifted south to align with the driveway extension. The Chapel will provide plans.

Cross access with their neighbors to the south and north was discussed. It was recommended that the property owners propose a plan and approach IDOT.

Follow-Up:

Add redevelopment plans and correspondence to CDR.

By: Scott Czaplicki
Bureau of Programming/Consultant

From: [Czaplicki, Scott D](#)
To: sassandahalf@yahoo.com
Cc: [Murphy, Kimberly K.](#); [Brown, Lori S.](#)
Subject: IL 31; Response to Public Comment (Trost)
Date: Friday, March 03, 2017 12:37:53 PM
Attachments: [Trost.pdf](#)

Ms. Trost,

Thank you for your comment regarding the February 8, 2017 public hearing for the proposed improvement of IL 31 in McHenry County (attached). The Phase I study identified three potential building displacements, all of which are located in the City of McHenry. The addresses and links to the applicable preferred alternative exhibits are listed below. Please note property impacts and building displacements will be further evaluated during the contract plan preparation and land acquisition phase (Phase II).

- 518 Front Street -
http://www.ilroute31.com/Downloads/PublicHearing/PH_PREFERRED_Alternative_Aerial-Sheet13.pdf
- 3815 W. Elm Street and 3817 W. Elm Street -
http://www.ilroute31.com/Downloads/PublicHearing/PH_PREFERRED_Alternative_Aerial-Sheet14.pdf

Please contact me with any questions.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov

 Please consider the environment before printing this message or attachments



Illinois Department of Transportation

Office of Highways Project Implementation / Region 1 / District 1
201 West Center Court / Schaumburg, Illinois 60196-1096

Project and Environmental Studies
Illinois Route 31
Illinois Route 176 to Illinois Route 120
McHenry County

December 19, 2017

Mr. Keith Wozniak
Chief, CELRC-TS-RW
U.S. Army Corps of Engineers
Chicago District - Regulatory Branch
231 South LaSalle Street
Suite 1500
Chicago, IL 60604

Dear Mr. Wozniak:

Thank you for your March 15, 2017 letter regarding the Illinois Department of Transportation's (Department) proposed improvement of Illinois Route 31 (IL 31) from Illinois Route 176, to Illinois Route 120 in McHenry County. Funding for Phase II engineering is included in the Department's FY 2018-2023 Proposed Highway Improvement Program. The remaining elements of the project will be included in our priorities for future funding consideration among similar improvement needs throughout the region. The general scope of work for this improvement is anticipated to consist of roadway widening and reconstruction to provide two lanes in each direction separated by a median, bicycle and pedestrian accommodations, and intersection and drainage improvements.

In your letter, the US Army Corps of Engineers (USACE) provided comments on the IL 31 Environmental Assessment. Responses to your comments are provided below.

Best Management Practices

The drainage plan was redesigned to meet the USACE's Stormwater Performance Standards Guidance for water quality volume (WQV) retention. As a result, nine acres of additional right-way is proposed to meet the guidance. The guidance was met for two of the three watersheds and for the project as a whole. The guidance could not be met for the Sleepy Hollow Creek watershed due to the existing terrain and environmental resources. The watershed is located in a 150-foot deep valley along IL 31 that has steep longitudinal grades which do not allow for effective storm water storage. In addition, environmental resources such as wetlands, waters, and oak and hickory trees are concentrated at the bottom of the valley. Even though the guidance was subsequently rescinded, the proposed retention measures are still included in the project.

Discharging storm water directly into wetlands and waters has been avoided where feasible. Locations that could not be avoided include culverts at existing low points. BMPs are proposed upstream of these locations to the greatest extent practicable. A detailed BMP plan will be included in the Section 404 permit application.

Water Resource Impacts

Avoidance and minimization measures were discussed during the four NEPA/404 Merger meetings as well as five USACE meetings for the project. Measures include alignment shifts, retaining walls, and lane and median width reduction. Impacts are anticipated to be further reduced during Phase II, contract plan preparation. Impacts to wetlands and waters are generally a result of roadway widening. As you noted, there are some locations where the impacts are not apparent and are discussed below:

- W6 (Sheet 13) – Potential temporary impacts due to the construction of the adjacent detention basin
- W1 (Sheets 14 and 15) – The existing box culvert is proposed to be replaced with a 90-foot span bridge. As a result, the Unnamed Tributary to the Fox River (W1) will be regraded upstream and downstream of the bridge and will include a low-flow channel
- W5 (Sheet 16) – Temporary impacts for the construction of outlet pipes from proposed detention basin

There are three waters identified as permanent streams that cross IL 31. The Section 404 permit application will include upstream and downstream channel cross sections.

Proposed Mitigation

The detailed design for the proposed meandering of Squaw Creek will be performed during Phase II, contract plan preparation. The proposed cross sections and monitoring and management plan will be provided as part of the Section 404 permit application. The Department acknowledges that the actual mitigation ratios will be determined as part of the application process.

Mr. Keith Wozniak
December 19, 2017
Page three

If you have any questions or need additional information, please contact me or Kimberly Murphy, Consultant Studies Unit Head, at (847) 705-4791.

Very truly yours,

Anthony J. Quigley, P.E.
Region One Engineer



By:
Issam Rayyan, P.E.
Acting Bureau Chief of Programming

cc: Robin Helmerichs, FHWA
John Sherrill, BDE

bcc: Sam Mead
Perry Masouridis
File

S:\WP\p&es\CONSULT\Projects - Active\IL 31 (IL 176 to IL 120)\Correspondence\Letters\2017-07-13
USACE Response.docx



REPLY TO
ATTENTION OF:

DEPARTMENT OF THE ARMY
CHICAGO DISTRICT, CORPS OF ENGINEERS
231 SOUTH LA SALLE STREET
CHICAGO, ILLINOIS 60604-1437

March 15, 2017

Technical Services Division
Regulatory Branch
LRC-2011-336

SUBJECT: NEPA/404 Merger Process Comments on the Draft Environmental Assessment for the IL Route 31 (from IL Route 176 to IL 120) Project, McHenry County, Illinois

Catherine Batey
Federal Highway Administration
3250 Executive Park Drive
Springfield, Illinois 62703

Dear Ms. Batey:

This letter is in response to your request that the Department of the Army (Corps) provide comments on the Draft Environmental Assessment for the IL Route 31 (from IL Route 176 to IL 120) Project, McHenry County, Illinois (EA). Various Federal and state agencies are providing a concurrent review of the project under the terms and conditions as set forth in the "Statewide Implementation Agreement National Environmental Policy Act And Clean Water Act Section 404 Concurrent NEPA/404 Processes For Transportation Projects in Illinois".

Following a review of the EA, the Corps provides the following comments:

1. Best Management Practices (BMP) for the Preferred Alternative

The Water Quality Volume does not meet the requirement around Sleepy Hollow Creek, a perennial stream where two pollution intolerant fish species were identified. The permit application will need to provide a detailed BMP proposal that includes information on what efforts were made to meet the 1.25" recommended storage.

In addition, Exhibit 5 appears to show outfalls discharging directly into wetlands. Stormwater outfalls should be located as far from the resource as possible. This information should be made available in a detailed BMP proposal. Finally, please make sure to label all wetlands and streams in the BMP proposal.

2. Water Resources Impacts

In addition to indicating whether proposed impacts are permanent or temporary, please make sure to qualify each proposed impact. Avoidance and minimization should also be discussed. For example, Sheet 13 in Exhibit 3 shows temporary impacts proposed in W6. It appears that a detention basin is proposed at this location, but the particular impacts are unclear.

If so, what avoidance and minimization measures were considered? It should be noted that detention in-line with a stream is generally not permissible. This information could be presented in the form of a table.

The information presented for culverts does not contain any information on the stream channel width. For culverts spanning a stream, this information should be provided in the application. Culverts should be designed to prevent the restriction of expected high water flows, and should be designed so as not to impede low water flows or the movement of aquatic organisms. Typically, a stream channel cross section taken upstream, at and downstream of the culvert should help determine if proposed culvert is meeting this requirement.

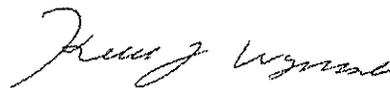
3. Proposed Mitigation

The proposed mitigation for impacts to Squaw Creek is on-site, in-kind replacement with meanders. The proposal will need to include existing and proposed cross sections of the stream. A monitoring and management plan will be required.

The EA states that a mitigation ratio of 1.5:1 will be applied to all non-ADID wetlands and a mitigation ratio of 3:1 will be applied to HQAR wetlands. There are instances where mitigation ratio of up to 6:1 are applied for ADID wetlands depending on the type of ADID classification. Note that stream mitigation will be required for areas that are considered stream loss.

Thank you for the opportunity to comment. If you have any questions, please contact Julie Rimbault of my staff by telephone at 312-846-5542, or email at Julie.C.Rimbault@usace.army.mil.

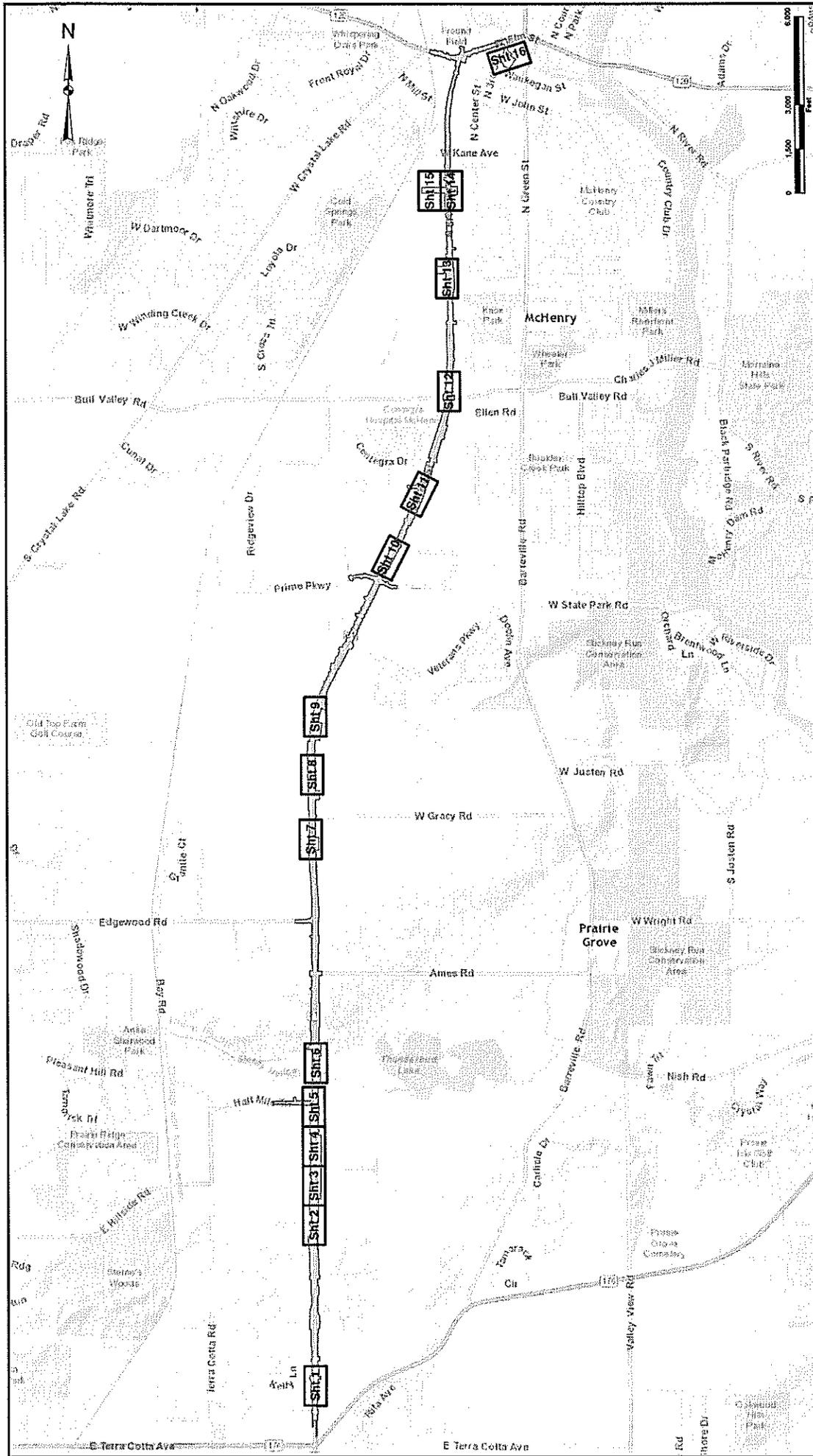
Sincerely,



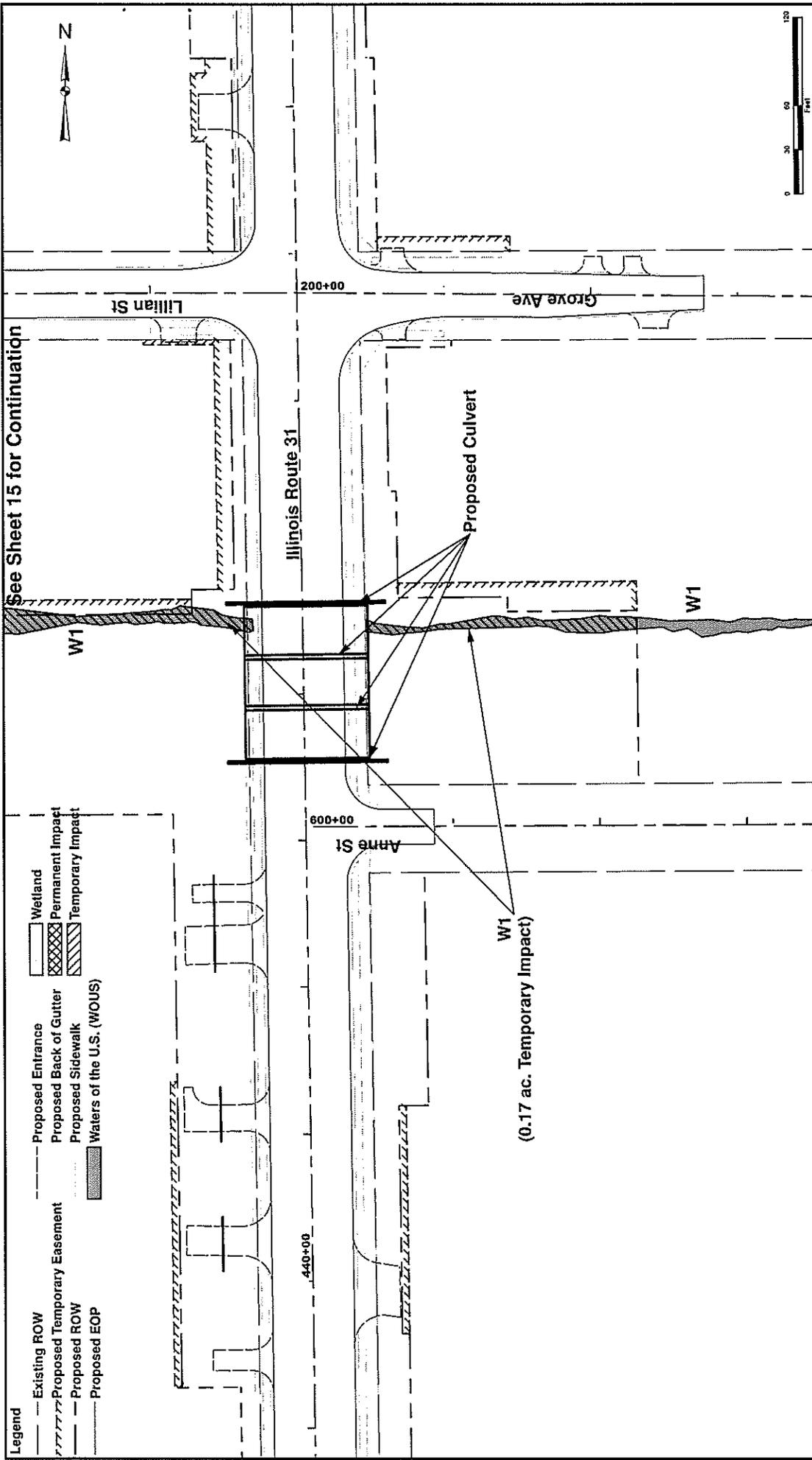
Keith L. Wozniak
Chief, West Section
Regulatory Branch

Copy Furnished:

U.S. Environmental Protection Agency (Ken Westlake)
U.S. Fish and Wildlife Service (Shawn Cirton)
Illinois Department of Natural Resources (Sheldon Fairfield)
Illinois Department of Transportation (Anthony Quigley)



Illinois Route 31 Wetlands Impact Exhibit Key Map		Total Sheet No.
State of Illinois Department of Transportation		Scale: 1" = 3000'
Date:	Designed:	Revised:
Plot Scale:	Drawn:	Revised:
Plate Date:	Checked:	Revised:
	Date:	Revised:



- Legend**
- Existing ROW
 - - - Proposed Temporary Easement
 - - - Proposed ROW
 - Proposed EOP
 - Proposed Entrance
 - Proposed Back of Gutter
 - Proposed Sidewalk
 - Waters of the U.S. (WOUS)
 - Wetland
 - Permanent Impact
 - Temporary Impact

W1
(0.17 ac. Temporary Impact)

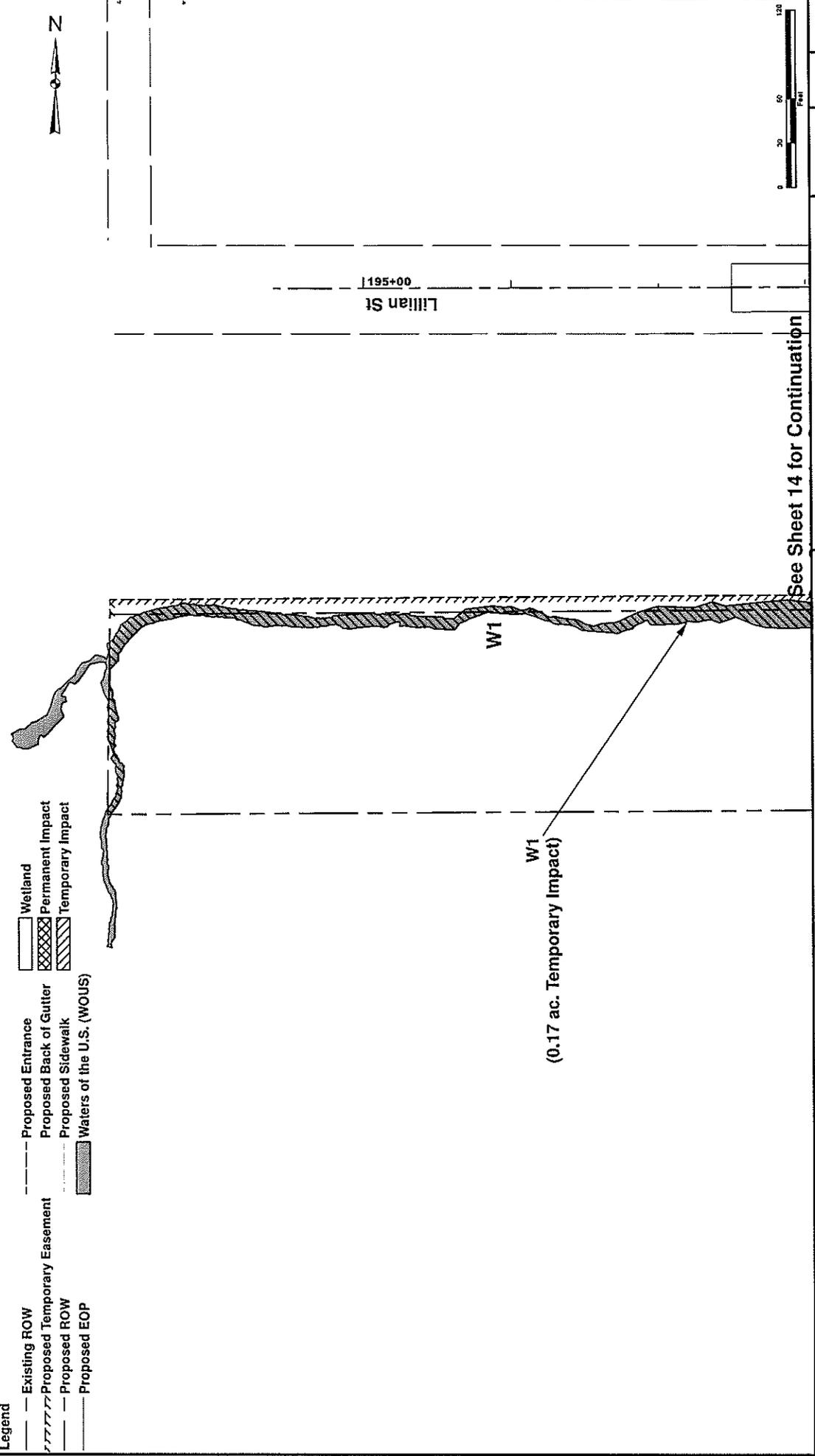
See Sheet 15 for Continuation

Date:	Designed:	Revised:	Illinois Route 31 Wetlands Impact Exhibit WOUS W1	Total Sheets 16	Sheet No. 14
Plot Scale:	Drawn:	Revised:			
Plot Date:	Checked:	Revised:	State of Illinois Department of Transportation		
	Date:	Revised:			

TEMPORARY IMPACT SHEET 14 OF 16

Legend

- Existing ROW
- Proposed Temporary Easement
- Proposed ROW
- Proposed EOP
- Proposed Entrance
- Proposed Back of Gutter
- Proposed Sidewalk
- Waters of the U.S. (WOUS)
- Wetland
- ▨ Permanent Impact
- ▧ Temporary Impact



W1
(0.17 ac. Temporary Impact)

See Sheet 14 for Continuation

Date:	Designed:	Revised:	Total Sheet No.
	Drawn:	Revised:	16
Plot Scale:	Checked:	Revised:	15
Plate Date:	Date:	Revised:	Scale: 1" = 60'

**Illinois Route 31
Wetlands Impact Exhibit
WOUS W1**

**State of Illinois
Department of Transportation**



Illinois Department of Transportation

Office of Highways Project Implementation / Region 1 / District 1
201 West Center Court / Schaumburg, Illinois 60196-1096

FILE COPY

Project and Environmental Studies
Illinois Route 31
Illinois Route 176 to Illinois Route 120
McHenry County

December 19, 2017

Mr. Kenneth A. Westlake
Chief, NEPA Implementation Section
Office of Enforcement and Compliance Assurance
US Environmental Protection Agency, Region 5
77 West Jackson Boulevard
Chicago, IL 60604-3590

Dear Mr. Westlake:

Thank you for your February 27, 2017 letter regarding the Illinois Department of Transportation's (Department) proposed improvement of Illinois Route 31 (IL 31) from Illinois Route 176 to Illinois Route 120 in McHenry County. Funding for Phase II engineering is included in the Department's FY 2018-2023 Proposed Highway Improvement Program. The remaining elements of the project will be included in our priorities for future funding consideration among similar improvement needs throughout the region. The general scope of work for this improvement is anticipated to consist of roadway widening and reconstruction to provide two lanes in each direction separated by a median, bicycle and pedestrian accommodations, and intersection and drainage improvements.

In your letter, the US Environmental Protection Agency (USEPA) provided comments on the IL 31 Environmental Assessment. Responses to your comments are provided below:

Stormwater Management

The project has been designed to provide stormwater detention to the greatest extent practicable. Additional detention storage would result in increased impacts to adjacent property and environmental resources, and would require additional right-of-way. It should be noted that the drainage plan was redesigned after receiving concurrence on the preferred alternative to meet the US Army Corps of Engineer's Stormwater Performance Standards Guidance for water quality volume retention which resulted in nine additional acres of right-of-way.

Air Quality

Attached is a table correlating the BMPs listed in the USEPA's *Diesel Emission Reduction Checklist* to the Department's *Standard Specifications for Road and Bridge Construction* and other specifications. In addition, the following Special Design/Construction Consideration will be added to the Combined Design Report.

During Phase II, contract plan preparation, and Phase III, construction, the Department will develop construction special provisions to address possible environmental effects related to construction activities conducted within close proximity to residential areas, schools, parks, childcare locations and other sensitive receptors. The special provisions will address the following areas of concern:

Communication Plan - Notify local communities about construction activities that may contribute to temporary noise and air quality effects. The special provision will include requirements for developing notification procedures regarding regional air quality, local construction activities, and emergency situations.

Dust Control Plan – This would augment the existing dust control methods currently in place in Department's Standard Specifications. The Dust Control Plan special provision will require development of methods and procedures that the contractor will employ during construction activities to reduce or contain construction dust in urban environments. These may be best practices from other similarly sized and located projects that are applicable to this project at the time that the special provision is drafted in Phase II, contract plan preparation.

Contractor's Site Health and Safety Plan - Although contractors guarantee the Department a safe work place for their employees, this special provision will consider the application of additional best practices for worker protection that may be applicable at that time the special provision is drafted in Phase II, contract plan preparation. This special provision will improve contractor awareness of worker protection best practices as identified by the USEPA and other environmental agencies.

Pollinator Habitat

The following commitment will be added to the Environmental Assessment and Combined Design Report.

Pollinator habitat shall be incorporated into the project where practical. The Phase II landscaping and signing plans shall be developed in accordance with the current version of the FHWA publication *Pollinators and Roadsides: Best Management Practices for Managers and Decision Makers*, and coordinated through the Department's Bureau of Programming/ Environmental Studies Unit. (USEPA letter dated February 27, 2017).

Mr. Kenneth A. Westlake
December 19, 2017
Page three

Transportation Linkages

Please note that grade separated structures for pedestrians and bicyclists would be entirely a local agency cost for design, construction, and land acquisition. At this time, there are no local agency sponsors for grade separated structures. It should be noted that there are eight existing or new signalized intersections along the project that include at-grade pedestrian crossing accommodations such as ADA ramps, pedestrian signals, and crosswalks.

The proposed culverts identified for wildlife crossings are only appropriate for small to medium sized mammals as large as foxes or coyotes. To accommodate pedestrians and bicyclists, the roadway profile would need to be raised several feet to provide appropriate vertical clearance while keeping the path above normal water levels. For instance, the tallest culvert identified as a wildlife crossing is six feet high. The IL 31 profile would need to be raised four feet to meet design standard for a path crossing. This would have compounding impacts to adjacent areas. The wildlife crossings are typically located in low areas connecting wetlands and waters. Raising the roadway profile at these locations would result in additional impacts to these resources.

If you have any questions or need additional information, please contact me or Kimberly Murphy, Consultant Studies Unit Head, at (847) 705-4791.

Very truly yours,

Anthony J. Quigley, P.E.
Region One Engineer



By:
Issam Rayyan, P.E.
Acting Bureau Chief of Programming

cc: Robin Helmerichs, FHWA
John Sherrill, IDOT-BDE

bcc: Sam Mead
Perry Masouridis
File

S:\WP\p&es\CONSULT\Projects - Active\IL 31 (IL 176 to IL 120)\Correspondence\Letters\2017-06-xx
USEPA Response.docx

Table
Air Quality Best Management Practice Reference

USEPA Diesel Emission Reduction Checklist	Illinois Department of Transportation Specification ^{1 2}
Use low-sulfur diesel fuel (15 ppm sulfur maximum) in construction vehicles and equipment.	SSRBC Article 107.41 (b) Construction Air Quality
Retrofit engines with an exhaust filtration device to capture diesel particulate matter before it enters the construction site.	BDE Special Provision – Construction Air Quality – Diesel Retrofit
Position the exhaust pipe so that diesel fumes are directed away from the operator and nearby workers, reducing the fume concentration to which personnel are exposed.	Special Design/Construction Consideration for project
Use catalytic converters to reduce carbon monoxide, aldehydes, and hydrocarbons in diesel fumes. These devices must be used with low sulfur fuels.	SSRBC Article 107.41 (b) Construction Air Quality
Use enclosed, climate-controlled cabs pressurized and equipped with high efficiency particulate air (HEPA) filters to reduce the operators' exposure to diesel fumes. Pressurization ensures that air moves from inside to outside. HEPA filters ensure that any incoming air is filtered first.	Special Design/Construction Consideration for project
Regularly maintain diesel engines, which is essential to keep exhaust emissions low. Follow the manufacturer's recommended maintenance schedule and procedures. Smoke color can signal the need for maintenance. For example, blue/black smoke indicates that an engine requires servicing or tuning.	SSRBC Article 105.03 (d) Diesel Vehicle Emissions Control Deduction
Reduce exposure through work practices and training, such as turning off engines when vehicles are stopped for more than a few minutes, training diesel-equipment operators to perform routine inspection, and maintaining filtration devices.	SSRBC Article 107.41 (a) Construction Air Quality, and Article 105.03 (c) Idling Restriction Deficiency Deduction
Repower older vehicles and/or equipment with diesel- or alternatively-fueled engines certified to meet newer, more stringent emissions standards. Purchase new vehicles that are equipped with the most advanced emission control systems available.	Special Design/Construction Consideration for project
Use electric starting aids such as block heaters with older vehicles to warm the engine reduces diesel emissions.	Special Design/Construction Consideration for project

¹ Standard Specification for Road and Bridge Construction, 2016 (SSRBC)

² Bureau of Design and Environment Special Provision (BDE Special Provision)

Table (continued)
Air Quality Best Management Practice Reference

USEPA Diesel Emission Reduction Checklist	Illinois Department of Transportation Specification
<p>Use respirators, which are only an interim measure to control exposure to diesel emissions. In most cases, an N95 respirator is adequate. Workers must be trained and fit-tested before they wear respirators. Depending on work being conducted, and if oil is present, concentrations of particulates present will determine the efficiency and type of mask and respirator. Personnel familiar with the selection care and use of respirators must perform the fit testing. Respirators must bear a NIOSH approval number.</p>	<p>Special Design/Construction Consideration for project</p>
<p>Per Executive Order 13045 on Children's Health, EPA recommends operators and workers' pay particular attention to worksite proximity to places where children live, learn, and play, such as homes, schools, daycare centers, and playgrounds. Diesel emission reduction measures should be strictly implemented near these locations in order to be protective of children's health.</p>	<p>SSRBC Article 107.41 (b) Construction Air Quality (staging in sensitive areas) also covered under Special Design and Construction Considerations (Communication Plan)</p>



UNITED STATES ENVIRONMENTAL PROTECTION AGENCY
REGION 5
77 WEST JACKSON BOULEVARD
CHICAGO, IL 60604-3590

FEB 27 2017

REPLY TO THE ATTENTION OF:

E-19J

Matt Fuller
Federal Highway Administration – Illinois Division
3250 Executive Park Drive
Springfield, Illinois 62703

**Re: Draft Environmental Assessment for the IL Route 31 (From IL Route 176 to IL 120)
Project, McHenry County, Illinois**

Dear Mr. Fuller:

The U.S. Environmental Protection Agency (EPA) has reviewed the referenced document dated January 18, 2017, which was produced by the Illinois Department of Transportation (IDOT), in coordination with the Federal Highway Administration (FHWA). We are providing comments pursuant to our authorities under the National Environmental Policy Act (NEPA), Council on Environmental Quality regulations (40 CFR Parts 1500-1508), Section 309 of the Clean Air Act, Section 404 of the Clean Water Act, and in accordance with EPA's role in the NEPA-CWA Section 404 Merger Process.

The proposed project involves addition of roadway infrastructure on IL Route 31, between IL Route 176 and IL Route 120. The proposed project is broken into three separate sections: South Section, North Section, and IL Route 120 Intersection.

Alternatives carried forward for the South Section:

- Alternative S-5 (Preferred Alternative for the South Section). Widen to 4 lanes with 30-foot raised median; and
- Alternative S-6. Widen to 4 lanes with 30-foot depressed median and 10-foot shoulders.

Alternatives carried forward for the North Section:

- Alternative N-2 (preferred alternative for the North Section). Widen to 4 lanes with an 18-foot raised curb median.

Alternatives carried forward for the IL Route 120 Intersection:

- Alternative 120INT-1 (preferred alternative for the IL Route 120 Intersection). Construct a five-lane cross section (two lanes in each direction with a two-way left turn lane); and

- Alternative 120INT-3. Widen the IL Route 120 intersection for additional capacity. A 30-foot raised median with two 12-foot through lanes would be constructed in each direction.

Other Alternatives carried forward for consideration:

- No Action Alternative. The project does not proceed; and
- Congestion Management Process Alternative. Perform safety and infrastructure alterations without adding any new infrastructure.

We do not object to any of the three preferred alternatives. Additionally, we have comments that we urge FHWA and IDOT to consider before producing a Final EA (FEA) and Finding of No Significant Impact (FONSI), as stated below.

Stormwater Management

We support the stormwater management features and erosion control best management practices (BMPs) incorporated into this project. We do, however, urge FHWA and IDOT to consider constructing enough stormwater storage capacity for the volume of stormwater associated with a 500-year flood event.

Air Quality

We recommend FHWA and IDOT commit to reducing air emissions from internal combustion vehicles during the construction phase of this project. A list of BMPs is found in the enclosed document *U.S. Environmental Protection Agency Diesel Emission Reduction Checklist*.

Pollinator Habitat

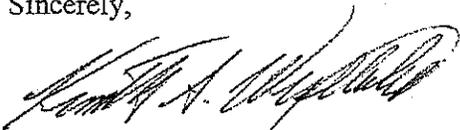
The 2014 Presidential Memorandum entitled, *Creating a Federal Strategy to Promote the Health of Honey Bees and Other Pollinators*, responds to evidence of steep declines in certain pollinator populations. Pollinators are critical contributors to our nation's economy, food system, and environmental health. Vegetation within the project area can provide much needed habitat for pollinators, providing food, shelter, and connections to other patches of habitat. Maintenance staff and landscape designers can all take steps to improve the quality of vegetation to benefit pollinators, steps that can also reduce costs, maintain public safety, and improve public good will. The FEA and FONSI should include specific commitments by FHWA and IDOT to incorporate pollinator habitat into the project where practical.

Transportation Linkages

We applaud FHWA and IDOT for committing to constructing sidewalks and providing linkages between sidewalks and local trails, consistent with IDOT's Complete Streets Policy. We recommend FHWA and IDOT identify potential locations where constructing sidewalk or trail crossings over or under IL 31 and IL 120 would be beneficial for the safety of pedestrians and bicyclists. This would include (but not limited to) sidewalk routes that lead from residential areas to schools and/or shopping centers that would require crossing IL 31 or IL 120. FHWA and IDOT should also consider the proposed wildlife crossings as potential trail and sidewalk crossings.

We are available to discuss these comments at your convenience. Please feel free to contact Mike Sedlacek of my staff at 312-886-1765, or by email at sedlacek.michael@epa.gov.

Sincerely,



Kenneth A. Westlake, Chief
NEPA Implementation Section
Office of Enforcement and Compliance Assurance

Enclosure: U.S. Environmental Protection Agency Diesel Emission Reduction Checklist

cc: Scott Czaplicki, Illinois Department of Transportation
Shawn Cirton, U.S. Fish and Wildlife Service
Soren Hall, U.S. Army Corps of Engineers – Chicago District

U.S. Environmental Protection Agency
Diesel Emission Reduction Checklist

- Use low-sulfur diesel fuel (15 ppm sulfur maximum) in construction vehicles and equipment.
- Retrofit engines with an exhaust filtration device to capture diesel particulate matter before it enters the construction site.
- Position the exhaust pipe so that diesel fumes are directed away from the operator and nearby workers, reducing the fume concentration to which personnel are exposed.
- Use catalytic converters to reduce carbon monoxide, aldehydes, and hydrocarbons in diesel fumes. These devices must be used with low sulfur fuels.
- Use enclosed, climate-controlled cabs pressurized and equipped with high efficiency particulate air (HEPA) filters to reduce the operators' exposure to diesel fumes. Pressurization ensures that air moves from inside to outside. HEPA filters ensure that any incoming air is filtered first.
- Regularly maintain diesel engines, which is essential to keep exhaust emissions low. Follow the manufacturer's recommended maintenance schedule and procedures. Smoke color can signal the need for maintenance. For example, blue/black smoke indicates that an engine requires servicing or tuning.
- Reduce exposure through work practices and training, such as turning off engines when vehicles are stopped for more than a few minutes, training diesel-equipment operators to perform routine inspection, and maintaining filtration devices.
- Repower older vehicles and/or equipment with diesel- or alternatively-fueled engines certified to meet newer, more stringent emissions standards. Purchase new vehicles that are equipped with the most advanced emission control systems available.
- Use electric starting aids such as block heaters with older vehicles to warm the engine reduces diesel emissions.
- Use respirators, which are only an interim measure to control exposure to diesel emissions. In most cases, an N95 respirator is adequate. Workers must be trained and fit-tested before they wear respirators. Depending on work being conducted, and if oil is present, concentrations of particulates present will determine the efficiency and type of mask and respirator. Personnel familiar with the selection, care, and use of respirators must perform the fit testing. Respirators must bear a NIOSH approval number.
- Per Executive Order 13045 on Children's Health¹, EPA recommends operators and workers pay particular attention to worksite proximity to places where children live, learn, and play, such as homes, schools, daycare centers, and playgrounds. Diesel emission reduction measures should be strictly implemented near these locations in order to be protective of children's health.

¹ Children may be more highly exposed to contaminants because they generally eat more food, drink more water, and have higher inhalation rates relative to their size. Also, children's normal activities, such as putting their hands in their mouths or playing on the ground, can result in higher exposures to contaminants as compared with adults. Children may be more vulnerable to the toxic effects of contaminants because their bodies and systems are not fully developed and their growing organs are more easily harmed. EPA views childhood as a sequence of lifestages, from conception through fetal development, infancy, and adolescence.

From: [Czaplicki, Scott D](#)
To: ["Bryan Villano"](#)
Cc: [Brown, Lori S.](#)
Subject: IL 31; 302 Front Street (Battery Farm)
Date: Friday, June 16, 2017 1:12:00 PM
Attachments: [Villano-Public Hearing Exhibit-12r_email.pdf](#)
[Villano.pdf](#)
[Villano - Pages from \(11x17\) IL31-Cross Sections-020317.pdf](#)

Bryan,

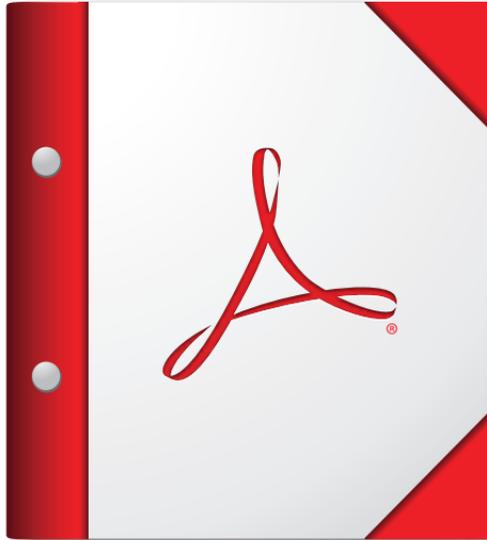
As requested, we updated our aerial image at your property to assess impacts to your recent battery farm development. As shown on the attached preliminary plan, the currently proposed right-of-way (purple line) encompasses your three communication boxes. To avoid this, we intend to shift the proposed right-of-way line (Pr ROW) approximately 20 feet west. A 10-foot temporary easement may be required during construction for grading beyond the Pr ROW. The attached cross section illustrates the proposed changes. We will send you a revised plan once the changes have been made by the consultant. If you have any questions, please contact me.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov

-



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From: Czaplicki, Scott D
To: ["bryan.villano@edf-re.com"](mailto:bryan.villano@edf-re.com)
Subject: IL 31; Public Hearing Comment
Date: Monday, March 06, 2017 11:22:00 AM

Mr. Villano,

Thank you for the comment you submitted regarding the proposed improvement of IL 31 from IL 176 to IL 120 in McHenry County. You requested someone contact you. I am available all this week except Wednesday to speak with you. Please suggest a date and time and I'll give you a call.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov

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From: formmailer@secureserver.net [mailto:formmailer@secureserver.net]
Sent: Thursday, March 02, 2017 7:45 PM
To: info@ilroute31.com
Subject: [External] Form Submission Digest

From: [Czaplicki, Scott D](#)
To: M_Wolff@LCGC.com
Cc: [Brown, Lori S.](#)
Subject: IL 31; Public Hearing Comment (Michael Wolff)
Date: Friday, December 29, 2017 1:05:49 PM
Attachments: [Wolff-Court Reporter-Comments 5.pdf](#)
[Revised Plan Sheet - Michael Wolff \(Dec 2017\).pdf](#)

Mr. Wolff,

Thank you for the comment you provided at the public hearing for the Illinois Department of Transportation's (Department) proposed improvement of IL 31 from IL 176 to IL 120 in McHenry County (attached). Your comment is part of the official public hearing record. The Department is nearing completion of the Phase I study as we incorporate comments from the public hearing and refine the engineering studies, right-of-way requirements, and environmental documents. Funding for contract plan preparation is included in the Department's 2018-2023 Proposed Highway Improvement Program. The remaining elements of the project (land acquisition and construction) are not included, but will be considered for future funding among similar improvement needs throughout the region.

You requested wider driveways and the ability to turn left and right for the property located at 2207 South IL Route 31. We revised the proposed driveway width at the house entrance (near Station 300+00 Lt) from 12 to 35 feet. This entrance is highlighted in yellow on the second page of the attached plans. Thirty-five feet is the maximum width for a commercial entrance according to the Illinois Administrative Code.

Regarding the ability to turn right and left, your south entrance can be realigned approximately 100 feet south to the median opening for Gracy Road. See the first page of the attached plans. If you would like the entrance realigned, please respond by making the request. In addition, since the entrance would be located on a property line, a cross access agreement would need to be provided to the Department. Information regarding cross access agreements can be found on the project website at www.ilroute31.com/publicmeetings.html. This entrance can also be widened to a maximum of 35 feet. Please let me know how you would like to proceed and contact me with any questions. Thank you.

Scott Czaplicki, P.E.
(847) 705-4678
scott.czaplicki@illinois.gov

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From the Bureau of : Programming

Name:	<u>Michael Wolff</u>	Date:	<u>January 9, 2018</u>
Title:	<u></u>		
Organization:	<u>McHenry Heating & Air, Inc.</u>	Project:	<u>IL 31</u>
Address:	<u>2207 South IL Route 31</u>		<u>IL 176 to IL 120</u>
	<u>McHenry, IL 60050</u>		<u>P-91-135-99</u>
Telephone:	<u></u>		
E-Mail:	<u>M_Wolff@LCGC.com</u>	PTB No.:	<u>155-015 & 170-007</u>

Items Discussed:

Mr. Wolff called in follow-up to my December 29, 2017 email which responded to his public hearing comments. We discussed the possibility of creating a combined entrance at the property line across from Gracy Road. Mr. Wolff will consider the combined entrance, but requested his existing driveway located 100 feet north of Gracy Road along the west side of IL 31 be widened to 35 feet (by holding the north edge of his driveway and widening south).

Follow-Up:

Send revised plan sheet.

By: Scott Czaplicki
Bureau of Programming/Consultant



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