

INDEX OF SHEETS

	Sh. #/Fig.#
EXISTING GEOMETRY -	2 THRU 6
PROPOSED GEOMETRY -	7 THRU 11
EXISTING RIGHT-OF-WAY-	12 THRU 15
PROPOSED PLAN SHEETS-	16 THRU 39
PROPOSED PROFILE SHEETS-	40 THRU 53
STAGE CONSTRUCTION PLANS-	54 THRU 70
STAGE CONSTRUCTION FIGURES-	1 THRU 16
INTERCHANGE & INTERSECTION DESIGN STUDIES-	NOT NUMBERED
TYPE, SIZE, LOCATION (TSL) STUDIES-	NOT NUMBERED

VOLUME II

FAIRTE 39 {1-39} AND FAP RTE 301 (US 20)
SECTION {201-3}K AND {4-1,5}R
WINNEBAGO COUNTY, IL

EXISTING GEOMETRY

I-39 C-1
 PI STA. = 2523+70.41
 $\Delta = 48^{\circ}02'45''$ (LT)
 D = 2°00'00"
 R = 2,864.85'
 T = 1,276.89'
 L = 2,402.34'
 E = 271.68'
 P.C. STA. = 2510+93.52
 P.T. STA. = 2534+95.86

I-39 C-4
 PI STA. = 2482+69.55
 $\Delta = 50^{\circ}10'11''$ (RT)
 D = 1°30'00"
 R = 3,819.80'
 T = 1,788.09'
 L = 3,344.71'
 E = 397.80'
 P.C. STA. = 2464+81.46
 P.T. STA. = 2498+26.17

RAMP DB AUX C-1
 PI STA. = 3+01.05
 $\Delta = 11^{\circ}41'36''$ (LT)
 D = 1°56'56"
 R = 2,939.98'
 T = 301.05'
 L = 600.01'
 E = 15.37'
 P.C. STA. = 0+00.00
 P.T. STA. = 6+00.01

RAMP DB AUX C-2
 PI STA. = 14+23.59
 $\Delta = 31^{\circ}14'58''$ (LT)
 D = 1°56'44"
 R = 2,944.86'
 T = 823.58'
 L = 1,606.13'
 E = 113.00'
 P.C. STA. = 6+00.01
 P.T. STA. = 22+06.14

I-39 C1		
POINT	NORTHING	EASTING
P.I.	2,019,933.450	2,609,454.939
P.C.	2,019,093.894	2,608,492.867
P.T.	2,021,210.196	2,609,473.760
C.C.	2,021,252.423	2,606,609.219

I-39 C4		
POINT	NORTHING	EASTING
P.I.	2,017,084.937	2,606,190.744
P.C.	2,015,297.292	2,606,230.665
P.T.	2,018,260.612	2,607,537.985
C.C.	2,015,382.573	2,610,049.516

RAMP DB AUX C-1		
POINT	NORTHING	EASTING
P.I.	2,019,435.721	2,608,939.452
P.C.	2,019,222.231	2,608,727.194
P.T.	2,019,687.798	2,609,104.036
C.C.	2,021,295.092	2,606,642.306

RAMP DB AUX C-2		
POINT	NORTHING	EASTING
P.I.	2,020,385.522	2,609,541.614
P.C.	2,019,687.798	2,609,104.036
P.T.	2,021,209.017	2,609,553.753
C.C.	2,021,252.423	2,606,609.220

RAMP ABD C-1
 PI STA. = 114+11.71
 $\Delta = 11^{\circ}56'48''$ (RT)
 D = 2°03'27"
 R = 2,784.84'
 T = 291.39'
 L = 580.66'
 E = 15.20'
 P.C. STA. = 111+20.32
 P.T. STA. = 117+00.98

RAMP ABD C-2
 PI STA. = 106+77.82
 $\Delta = 19^{\circ}11'57''$ (RT)
 D = 2°08'56"
 R = 2,666.30'
 T = 450.95'
 L = 893.45'
 E = 37.87'
 P.C. STA. = 102+26.87
 P.T. STA. = 111+20.32

RAMP ABD C-1		
POINT	NORTHING	EASTING
P.I.	2,019,345.760	2,608,659.813
P.C.	2,019,578.645	2,608,834.946
P.T.	2,019,154.172	2,608,440.267
C.C.	2,021,252.417	2,606,609.226

RAMP ABD C-2		
POINT	NORTHING	EASTING
P.I.	2,019,942.624	2,609,101.171
P.C.	2,020,373.908	2,609,232.894
P.T.	2,019,578.645	2,608,834.946
C.C.	2,021,152.734	2,606,682.878

RAMP ABD C-3
 PI STA. = 101+76.88
 $\Delta = 2^{\circ}09'38''$ (RT)
 D = 2°09'39"
 R = 2,651.46'
 T = 50.00'
 L = 99.99'
 E = 0.47'
 P.C. STA. = 101+26.88
 P.T. STA. = 102+26.86

RAMP ABD C-4
 PI STA. = 98+46.27
 $\Delta = 28^{\circ}40'01''$ (RT)
 D = 4°59'55"
 R = 1,146.21'
 T = 292.88'
 L = 573.49'
 E = 36.83'
 P.C. STA. = 95+53.39
 P.T. STA. = 101+26.88

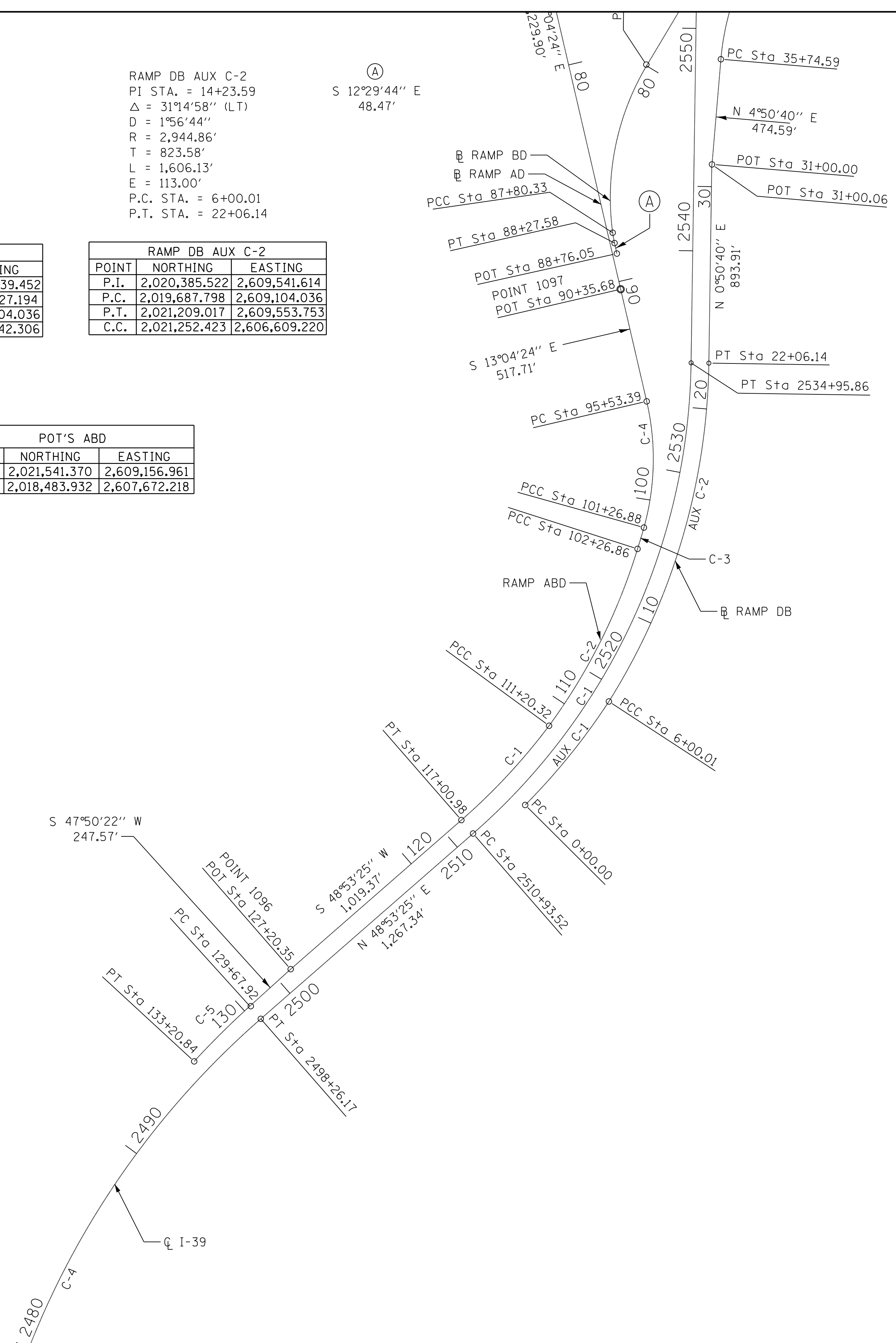
RAMP ABD C-3		
POINT	NORTHING	EASTING
P.I.	2,020,421.534	2,609,248.143
P.C.	2,020,469.692	2,609,261.584
P.T.	2,020,373.916	2,609,232.897
C.C.	2,021,182.444	2,606,707.715

RAMP ABD C-4		
POINT	NORTHING	EASTING
P.I.	2,020,751.790	2,609,340.313
P.C.	2,021,037.077	2,609,274.065
P.T.	2,020,469.692	2,609,261.584
C.C.	2,020,777.809	2,608,157.566

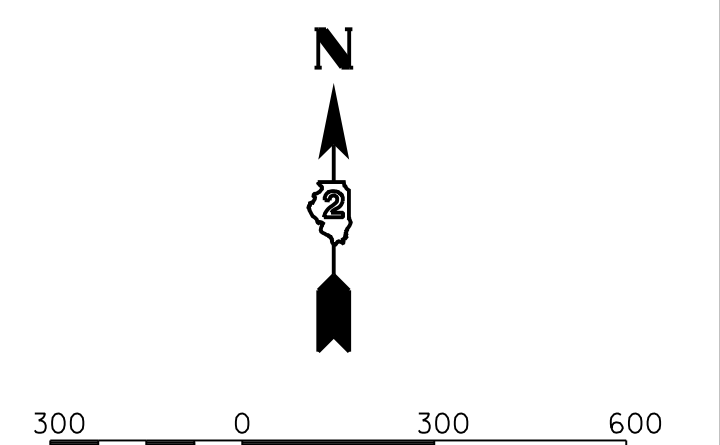
RAMP ABD C-5
 PI STA. = 131+44.50
 $\Delta = 5^{\circ}11'54''$ (LT)
 D = 1°28'23"
 R = 3,889.91'
 T = 176.58'
 L = 352.92'
 E = 4.01'
 P.C. STA. = 129+67.92
 P.T. STA. = 133+20.84

RAMP ABD C-5		
POINT	NORTHING	EASTING
P.I.	2,018,199.265	2,607,357.788
P.C.	2,018,317.762	2,607,488.704
P.T.	2,018,069.395	2,607,238.147
C.C.	2,015,433.796	2,610,099.090

POT'S ABD		
POINT	NORTHING	EASTING
1097	2,021,541.370	2,609,156.961
1096	2,018,483.932	2,607,672.218



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FILE NAME = I:\06\jobs\0652055\CADD\Road\Sheet\centerline geometric 1.dgn	USER NAME = Johns00944	DESIGNED - J.M.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20) EXISTING GEOMETRY - 1 JOB NO. P-92-111-06	F.A.I. RTE. = 39	SECTION = (201-31K & (4-1,5)R)	COUNTY = WINNEBAGO	TOTAL SHEETS = xx	SHEET NO. = 2	
PLOT SCALE = 300.0000' / in.	CHECKED - S.M.	REVISED -	REVISED -			SCALE: 1"=300'	SHEET NO. 1 OF 5 SHEETS	STA. TO STA.	CONTRACT NO.		
PLOT DATE = 3/30/2017	DATE -	REVISED -	REVISED -					FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT	

I-39 C-2
 PI STA. = 2638+44.91
 $\Delta = 50^{\circ}41'00''$ (LT)
 D = 1'00'00"
 R = 5,729.70'
 T = 2,713.52'
 L = 5,068.45'
 E = 610.07'
 P.C. STA. = 2611+31.39
 P.T. STA. = 2661+99.84

RAMP DB C-1
 PI STA. = 38+39.16
 $\Delta = 26^{\circ}00'00''$ (RT)
 D = 4'59'59"
 R = 1,145.96'
 T = 264.57'
 L = 520.02'
 E = 30.14'
 P.C. STA. = 35+74.59
 P.T. STA. = 40+94.61

RAMP DB C-2
 PI STA. = 61+34.74
 $\Delta = 57^{\circ}21'47''$ (RT)
 D = 5'00'00"
 R = 1,145.94'
 T = 626.90'
 L = 1,147.29'
 E = 160.27'
 P.C. STA. = 55+07.83
 P.T. STA. = 66+55.12

RAMP DB AUX C-3
 PI STA. = 91+15.13
 $\Delta = 5^{\circ}56'46''$ (LT)
 D = 0'59'28"
 R = 5,781.75'
 T = 300.28'
 L = 600.02'
 E = 7.79'
 P.C. STA. = 88+14.85
 P.T. STA. = 94+14.86

RAMP BD C-1
 PI STA. = 51+23.86
 $\Delta = 62^{\circ}30'23''$ (LT)
 D = 4'55'59"
 R = 1,161.48'
 T = 704.90'
 L = 1,267.11'
 E = 197.16'
 P.C. STA. = 44+18.96
 P.T. STA. = 56+86.08

RAMP BD C-2
 PI STA. = 84+04.47
 $\Delta = 38^{\circ}36'45''$ (LT)
 D = 4'55'58"
 R = 1,161.52'
 T = 406.90'
 L = 782.76'
 E = 69.21'
 P.C. STA. = 79+97.57
 P.T. STA. = 87+80.33

I-39 C2		
POINT	NORTHING	EASTING
P.I.	2,024,959.002	2,616,203.233
P.C.	2,024,928.146	2,613,489.890
P.T.	2,027,077.747	2,617,898.551
C.C.	2,030,657.480	2,613,424.736

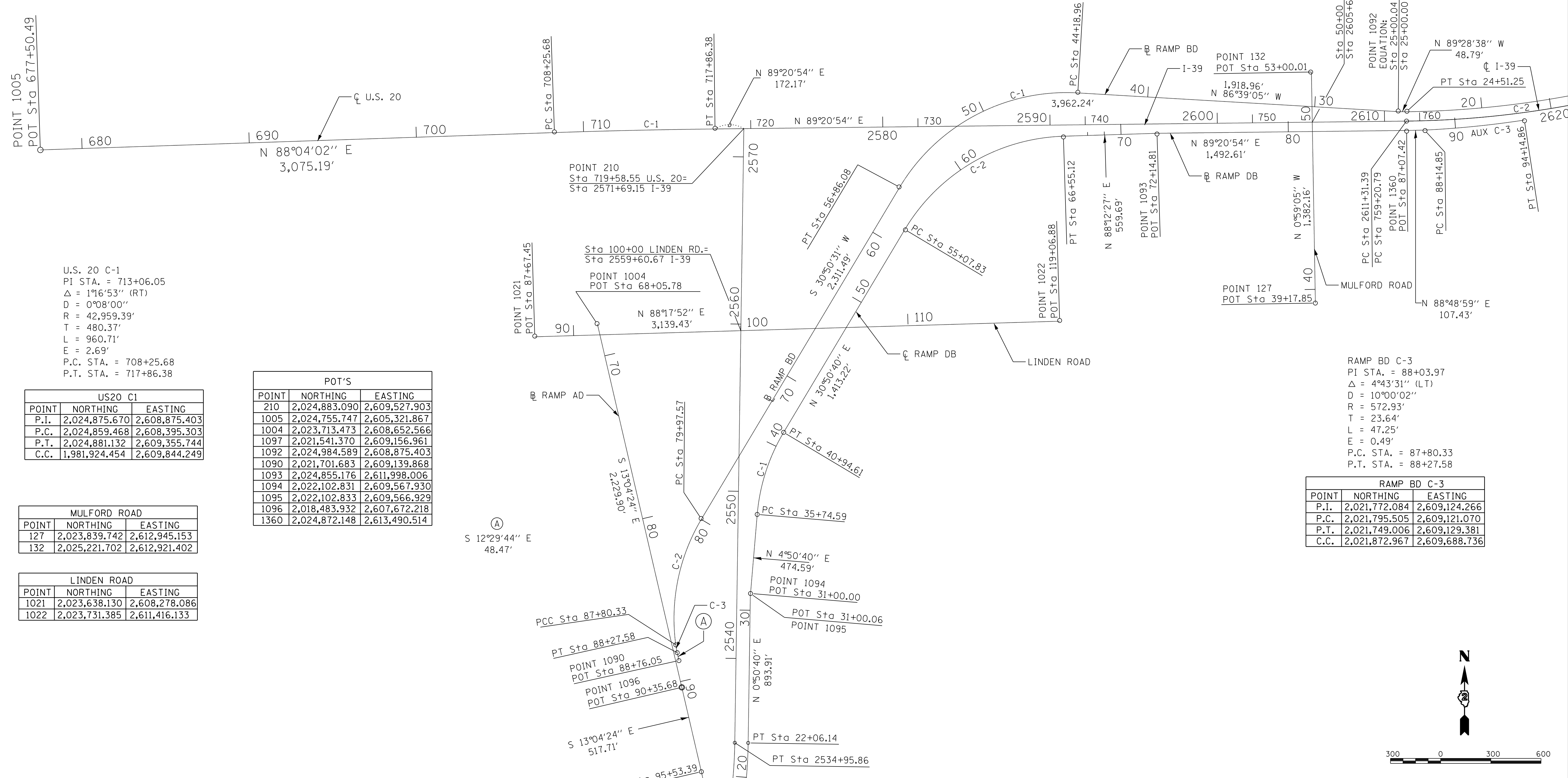
RAMP DB C-1		
POINT	NORTHING	EASTING
P.I.	2,022,839.344	2,609,630.353
P.C.	2,022,575.724	2,609,608.010
P.T.	2,023,066.490	2,609,765.999
C.C.	2,022,478.945	2,610,749.877

RAMP DB C-2		
POINT	NORTHING	EASTING
P.I.	2,024,818.060	2,610,811.989
P.C.	2,024,279.824	2,610,490.569
P.T.	2,024,837.669	2,611,438.586
C.C.	2,023,692.289	2,611,474.429

RAMP DB AUX C-3		
POINT	NORTHING	EASTING
P.I.	2,024,888.854	2,613,897.858
P.C.	2,024,874.367	2,613,597.931
P.T.	2,024,934.332	2,614,194.671
C.C.	2,030,649.382	2,613,318.998

RAMP BD C-1		
POINT	NORTHING	EASTING
P.I.	2,025,137.847	2,610,821.080
P.C.	2,025,096.675	2,611,524.772
P.T.	2,024,532.635	2,610,459.700
C.C.	2,023,937.175	2,611,456.931

RAMP BD C-2		
POINT	NORTHING	EASTING
P.I.	2,022,198.668	2,609,066.056
P.C.	2,022,548.025	2,609,274.662
P.T.	2,021,795.505	2,609,121.070
C.C.	2,021,952.547	2,610,271.924



U.S. 20 C-1
 PI STA. = 713+06.05
 $\Delta = 1^{\circ}16'53''$ (RT)
 D = 0'08'00"
 R = 42,959.39'
 T = 480.37'
 L = 960.71'
 E = 2.69'
 P.C. STA. = 708+25.68
 P.T. STA. = 717+86.38

US20 C1		
POINT	NORTHING	EASTING
P.I.	2,024,875.670	2,608,875.403
P.C.	2,024,859.468	2,608,395.303
P.T.	2,024,881.132	2,609,355.744
C.C.	1,981,924.454	2,609,844.249

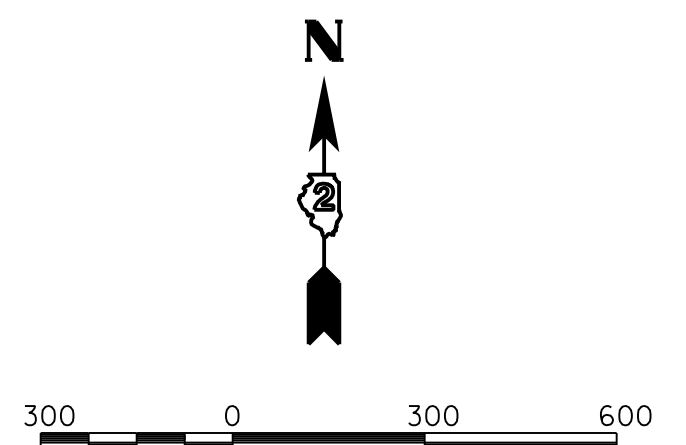
MULFORD ROAD		
POINT	NORTHING	EASTING
127	2,023,839.742	2,612,945.153
132	2,025,221.702	2,612,921.402

LINDEN ROAD		
POINT	NORTHING	EASTING
1021	2,023,638.130	2,608,278.086
1022	2,023,731.385	2,611,416.133

POT'S		
POINT	NORTHING	EASTING
210	2,024,883.090	2,609,527.903
1005	2,024,755.747	2,605,321.867
1004	2,023,713.473	2,608,652.566
1097	2,021,541.370	2,609,156.961
1092	2,024,984.589	2,608,875.403
1090	2,021,701.683	2,609,139.868
1093	2,024,855.176	2,611,998.006
1094	2,022,102.831	2,609,567.930
1095	2,022,102.833	2,609,566.929
1096	2,018,483.932	2,607,672.218
1360	2,024,872.148	2,613,490.514

RAMP BD C-3
 PI STA. = 88+03.97
 $\Delta = 4^{\circ}43'31''$ (LT)
 D = 10'00'02"
 R = 572.93'
 T = 23.64'
 L = 47.25'
 E = 0.49'
 P.C. STA. = 87+80.33
 P.T. STA. = 88+27.58

RAMP BD C-3		
POINT	NORTHING	EASTING
P.I.	2,021,772.084	2,609,124.266
P.C.	2,021,795.505	2,609,121.070
P.T.	2,021,749.006	2,609,129.381
C.C.	2,021,872.967	2,609,688.736



RAMP DB
PI STA. = 3+01.05
Δ = 11°41'36" (LT)
D = 1°56'56"
R = 2,939.98'
T = 301.05'
L = 600.01'
E = 15.37'
P.C. STA. = 0+00.00
P.T. STA. = 6+00.01

RAMP BCD AUX C-1
PI STA. = 3+00.28
Δ = 6°03'12" (RT)
D = 1°00'32"
R = 5,679.23'
T = 300.28'
L = 600.01'
E = 7.93'
P.C. STA. = 0+00.00
P.T. STA. = 6+00.01

RAMP BCD AUX C-2
PI STA. = 12+45.05
Δ = 24°45'14" (RT)
D = 1°00'35"
R = 5,673.70'
T = 1,245.05'
L = 2,451.25'
E = 135.00'
P.C. STA. = 0+00.00
P.T. STA. = 24+51.25

U.S. 20 C-2
PI STA. = 786+34.31
Δ = 50°41'00" (LT)
D = 1°00'00"
R = 5,729.70'
T = 2,713.52'
L = 5,068.45'
E = 610.07'
P.C. STA. = 759+20.79
P.T. STA. = 809+89.24

RAMP DB		
POINT	NORTHING	EASTING
P.I.	2,019,435.721	2,608,939.452
P.C.	2,019,222.231	2,608,727.194
P.T.	2,019,687.798	2,609,104.036
C.C.	2,021,295.092	2,606,642.306

RAMP BCD AUX C-1		
POINT	NORTHING	EASTING
P.I.	2,025,656.289	2,616,132.430
P.C.	2,025,808.282	2,616,391.406
P.T.	2,025,532.453	2,615,858.870
C.C.	2,030,706.256	2,613,516.783

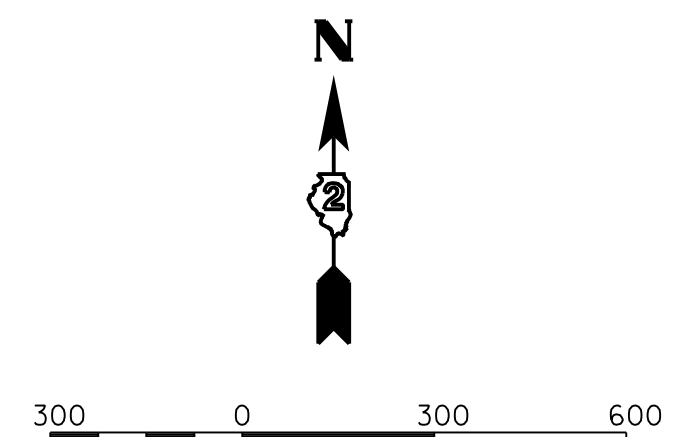
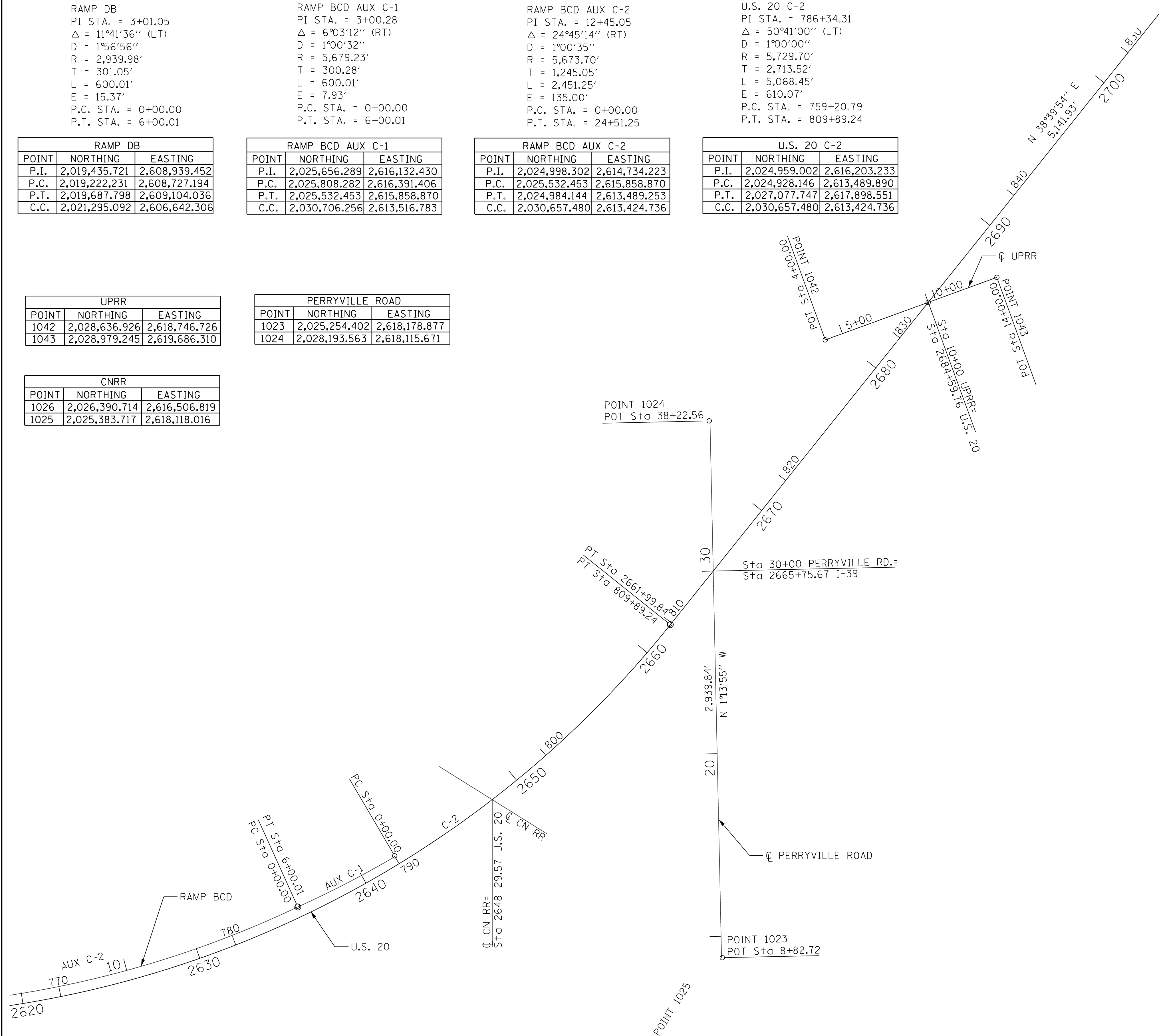
RAMP BCD AUX C-2		
POINT	NORTHING	EASTING
P.I.	2,024,998.302	2,614,734.223
P.C.	2,025,532.453	2,615,858.870
P.T.	2,024,984.144	2,613,489.253
C.C.	2,030,657.480	2,613,424.736

U.S. 20 C-2		
POINT	NORTHING	EASTING
P.I.	2,024,959.002	2,616,203.233
P.C.	2,024,928.146	2,613,489.890
P.T.	2,027,077.747	2,617,898.551
C.C.	2,030,657.480	2,613,424.736

UPRR		
POINT	NORTHING	EASTING
1042	2,028,636.926	2,618,746.726
1043	2,028,979.245	2,619,686.310

PERRYVILLE ROAD		
POINT	NORTHING	EASTING
1023	2,025,254.402	2,618,178.877
1024	2,028,193.563	2,618,115.671

CNRR		
POINT	NORTHING	EASTING
1026	2,026,390.714	2,616,506.819
1025	2,025,383.717	2,618,118.016



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		CHECKED - S.M.	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
EXISTING GEOMETRY - 3
JOB NO. P-92-111-06**

SCALE: 1"=300' SHEET NO. 3 OF 5 SHEETS STA. TO STA.

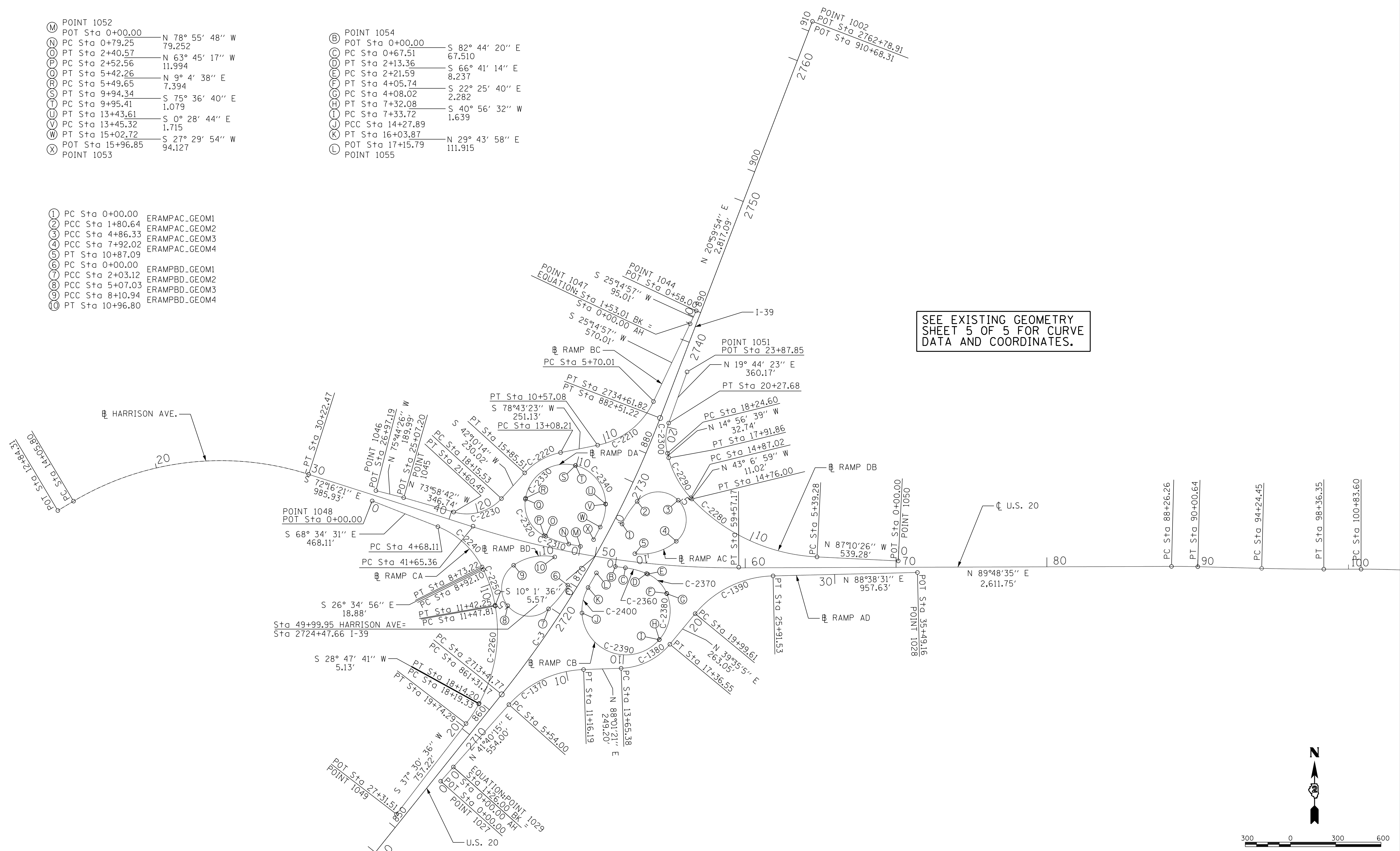
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1,5)R	WINNEBAGO	xx	4
06S2055			CONTRACT NO.	
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

- (M) POINT 1052
- POT Sta 0+00.00
- (N) PC Sta 0+79.25 N 78° 55' 48" W 79.252
- (O) PT Sta 2+40.57 N 63° 45' 17" W 11.994
- (P) PC Sta 2+52.56 N 9° 4' 38" E 7.394
- (R) PT Sta 5+42.26 N 9° 4' 38" E 7.394
- (S) PC Sta 5+49.65 S 75° 36' 40" E 1.079
- (T) PT Sta 9+94.34 S 0° 28' 44" E 1.715
- (U) PC Sta 13+45.32 S 27° 29' 54" W 94.127
- (V) PT Sta 15+02.72 S 27° 29' 54" W 94.127
- (W) POT Sta 15+96.85
- (X) POINT 1053

- (B) POINT 1054
- POT Sta 0+00.00 S 82° 44' 20" E 67.510
- (C) PC Sta 0+67.51 S 66° 41' 14" E 8.237
- (D) PT Sta 2+13.36 S 22° 25' 40" E 2.282
- (E) PC Sta 4+05.74 S 40° 56' 32" W 1.639
- (F) PT Sta 7+32.08 S 40° 56' 32" W 1.639
- (G) PCC Sta 14+27.89
- (H) PT Sta 16+03.87 N 29° 43' 58" E 111.915
- (I) POT Sta 17+15.79
- (L) POINT 1055

- (1) PC Sta 0+00.00 ERAMPAC_GEOM1
- (2) PCC Sta 1+80.64 ERAMPAC_GEOM2
- (3) PCC Sta 4+86.33 ERAMPAC_GEOM3
- (4) PCC Sta 7+92.02 ERAMPAC_GEOM4
- (5) PT Sta 10+87.09
- (6) PC Sta 0+00.00 ERAMPBD_GEOM1
- (7) PCC Sta 2+03.12 ERAMPBD_GEOM2
- (8) PCC Sta 5+07.03 ERAMPBD_GEOM3
- (9) PCC Sta 8+10.94 ERAMPBD_GEOM4
- (10) PT Sta 10+96.80

SEE EXISTING GEOMETRY SHEET 5 OF 5 FOR CURVE DATA AND COORDINATES.



FILE NAME = I:\06\jobs\0652055\CADD\Road\Sheet\centerline geometric 4.dgn	USER NAME = Johns00944	DESIGNED - J.M.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20) EXISTING GEOMETRY - 4 JOB NO. P-92-111-06			F.A.I. RTE. 39	SECTION (201-3K & (4-1,5)R)	COUNTY WINNEBAGO	TOTAL SHEETS xx	SHEET NO. 5
PLOT SCALE = 300.0000' / in.	CHECKED - S.M.	REVISOR -	REVISOR -		SCALE: 1"=300'	SHEET NO. 4	OF 5 SHEETS	STA.	TO STA.	CONTRACT NO.		
PLOT DATE = 3/30/2017	DATE -	REVISOR -	REVISOR -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							
SHEET 4												

RAMP CA C-2240
 PI STA. = 6+80.24
 Δ = 41°59'34" (RT)
 D = 10'21'57"
 R = 522.74'
 T = 212.14'
 L = 405.11'
 E = 39.31'
 P.C. STA. = 4+68.11
 P.T. STA. = 8+73.22

RAMP CA C-2250
 PI STA. = 10+18.05
 Δ = 16°33'20" (RT)
 D = 6°37'06"
 R = 865.70'
 T = 125.95'
 L = 250.14'
 E = 9.11'
 P.C. STA. = 8+92.10
 P.T. STA. = 11+42.25

RAMP CA C-2260
 PI STA. = 14+94.37
 Δ = 38°49'17" (RT)
 D = 5°49'32"
 R = 983.52'
 T = 346.56'
 L = 666.39'
 E = 59.27'
 P.C. STA. = 11+47.81
 P.T. STA. = 18+14.20

RAMP CA C-2270
 PI STA. = 18+96.96
 Δ = 8°42'56" (RT)
 D = 5°37'27"
 R = 1,018.72'
 T = 77.63'
 L = 154.96'
 E = 2.95'
 P.C. STA. = 18+19.33
 P.T. STA. = 19+74.29

RAMP AD C-1370
 PI STA. = 8+51.50
 Δ = 46°21'05" (RT)
 D = 8°14'41"
 R = 649.93'
 T = 297.50'
 L = 562.19'
 E = 61.00'
 P.C. STA. = 5+54.00
 P.T. STA. = 11+16.19

RAMP AD C-1380
 PI STA. = 15+62.87
 Δ = 48°26'16" (LT)
 D = 13°03'00"
 R = 439.05'
 T = 197.49'
 L = 371.17'
 E = 42.37'
 P.C. STA. = 13+65.38
 P.T. STA. = 17+36.55

RAMP AD C-1390
 PI STA. = 23+15.08
 Δ = 49°03'27" (RT)
 D = 8°17'16"
 R = 691.33'
 T = 315.48'
 L = 593.93'
 E = 68.58'
 P.C. STA. = 19+99.61
 P.T. STA. = 25+91.53

RAMP CB C-2360
 PI STA. = 1+40.91
 Δ = 16°03'06" (RT)
 D = 11°00'21"
 R = 520.60'
 T = 73.40'
 L = 145.85'
 E = 5.15'
 P.C. STA. = 0+67.51
 P.T. STA. = 2+13.36

POINT	NORTHING	EASTING
P.I.	2,032,126.529	2,620,885.194
P.C.	2,032,204.019	2,620,687.714
P.T.	2,031,936.816	2,620,980.122
C.C.	2,031,689.473	2,620,485.809

POINT	NORTHING	EASTING
P.I.	2,031,807.292	2,621,044.933
P.C.	2,031,919.928	2,620,988.572
P.T.	2,031,683.266	2,621,066.861
C.C.	2,031,532.542	2,620,214.383

POINT	NORTHING	EASTING
P.I.	2,031,336.522	2,621,128.168
P.C.	2,031,677.785	2,621,067.830
P.T.	2,031,032.816	2,620,961.242
C.C.	2,031,506.548	2,620,099.337

POINT	NORTHING	EASTING
P.I.	2,030,960.294	2,620,921.381
P.C.	2,031,028.325	2,620,958.773
P.T.	2,030,898.714	2,620,874.112
C.C.	2,031,519.014	2,620,066.016

POINT	NORTHING	EASTING
P.I.	2,031,246.636	2,621,356.296
P.C.	2,031,024.410	2,621,158.503
P.T.	2,031,256.902	2,621,653.619
C.C.	2,030,562.384	2,621,677.601

POINT	NORTHING	EASTING
P.I.	2,031,272.317	2,622,100.038
P.C.	2,031,265.502	2,621,902.667
P.T.	2,031,424.519	2,622,225.882
C.C.	2,031,704.286	2,621,887.516

POINT	NORTHING	EASTING
P.I.	2,031,870.382	2,622,594.530
P.C.	2,031,627.249	2,622,393.503
P.T.	2,031,877.858	2,622,909.918
C.C.	2,031,186.720	2,622,926.302

POINT	NORTHING	EASTING
P.I.	2,031,922.210	2,622,003.789
P.C.	2,031,931.488	2,621,930.973
P.T.	2,031,893.161	2,622,071.200
C.C.	2,031,415.065	2,621,865.174

RAMP CB C-2370
 PI STA. = 3+18.54
 Δ = 44°15'34" (RT)
 D = 24°02'06"
 R = 238.39'
 T = 96.94'
 L = 184.15'
 E = 18.96'
 P.C. STA. = 2+21.59
 P.T. STA. = 4+05.74

RAMP CB C-2380
 PI STA. = 5+88.88
 Δ = 63°22'13" (RT)
 D = 19°33'18"
 R = 293.00'
 T = 180.85'
 L = 324.06'
 E = 51.32'
 P.C. STA. = 4+08.02
 P.T. STA. = 7+32.08

RAMP CB C-2390
 PI STA. = 14+60.93
 Δ = 136°15'38" (RT)
 D = 19°37'45"
 R = 291.89'
 T = 727.21'
 L = 694.17'
 E = 491.71'
 P.C. STA. = 7+33.72
 P.T. STA. = 14+27.89

RAMP CB C-2400
 PI STA. = 15+18.32
 Δ = 32°31'48" (RT)
 D = 18°29'07"
 R = 309.95'
 T = 90.43'
 L = 175.98'
 E = 12.92'
 P.C. STA. = 14+27.89
 P.T. STA. = 16+03.87

RAMP DA C-2310
 PI STA. = 1+60.39
 Δ = 15°10'31" (RT)
 D = 9°24'25"
 R = 609.07'
 T = 81.13'
 L = 161.32'
 E = 5.38'
 P.C. STA. = 0+79.25
 P.T. STA. = 2+40.57

RAMP DA C-2320
 PI STA. = 4+20.68
 Δ = 72°49'56" (RT)
 D = 25°08'27"
 R = 227.90'
 T = 168.12'
 L = 289.70'
 E = 55.30'
 P.C. STA. = 2+52.56
 P.T. STA. = 5+42.26

RAMP DA C-2330
 PI STA. = 8+42.98
 Δ = 95°18'42" (RT)
 D = 21°26'01"
 R = 267.32'
 T = 293.32'
 L = 444.68'
 E = 129.54'
 P.C. STA. = 5+49.65
 P.T. STA. = 9+94.34

RAMP DA C-2340
 PI STA. = 11+99.65
 Δ = 75°07'56" (RT)
 D = 21°34'40"
 R = 265.53'
 T = 204.24'
 L = 348.19'
 E = 69.46'
 P.C. STA. = 9+95.41
 P.T. STA. = 13+43.61

POINT	NORTHING	EASTING
P.I.	2,031,851.536	2,622,167.792
P.C.	2,031,889.901	2,622,078.764
P.T.	2,031,761.926	2,622,204.778
C.C.	2,031,670.977	2,621,984.423

POINT	NORTHING	EASTING
P.I.	2,031,592.643	2,622,274.648
P.C.	2,031,759.817	2,622,205.648
P.T.	2,031,456.032	2,622,156.135
C.C.	2,031,648.033	2,621,934.814

POINT	NORTHING	EASTING
P.I.	2,030,905.484	2,621,678.522
P.C.	2,031,454.795	2,622,155.061
P.T.	2,031,631.825	2,621,643.033
C.C.	2,031,646.070	2,621,934.576

POINT	NORTHING	EASTING
P.I.	2,031,722.149	2,621,638.620
P.C.	2,031,631.826	2,621,643.033
P.T.	2,031,800.674	2,621,683.470
C.C.	2,031,646.952	2,621,952.616

POINT	NORTHING	EASTING
P.I.	2,032,104.499	2,621,475.768
P.C.	2,032,088.921	2,621,555.392
P.T.	2,032,140.378	2,621,402.997
C.C.	2,032,686.661	2,621,672.338

POINT	NORTHING	EASTING
P.I.	2,032,220.027	2,621,241.452
P.C.	2,032,145.682	2,621,392.240
P.T.	2,032,386.041	2,621,267.976
C.C.	2,032,350.086	2,621,493.020

POINT	NORTHING	EASTING
P.I.	2,032,682.991	2,621,315.419
P.C.	2,032,393.342	2,621,269.142
P.T.	2,032,610.099	2,621,599.540
C.C.	2,032,351.168	2,621,533.111

POINT	NORTHING	EASTING
P.I.	2,032,559.078	2,621,798.415
P.C.	2,032,609.821	2,621,600.585
P.T.	2,032,354.849	2,621,800.122
C.C.	2,032,352.629	2,621,534.600

RAMP DA C-2350
 PI STA. = 14+25.62
 Δ = 27°58'38" (RT)
 D = 17°46'30"
 R = 322.34'
 T = 80.30'
 L = 157.40'
 E = 9.85'
 P.C. STA. = 13+45.32
 P.T. STA. = 15+02.72

RAMP DB C-2280
 PI STA. = 10+30.17
 Δ = 44°03'27" (RT)
 D = 4°42'12"
 R = 1,218.18'
 T = 492.89'
 L = 936.72'
 E = 95.94'
 P.C. STA. = 5+39.28
 P.T. STA. = 14+76.00

RAMP DB C-2290
 PI STA. = 16+42.59
 Δ = 28°10'20" (RT)
 D = 9°14'30"
 R = 619.98'
 T = 155.57'
 L = 304.84'
 E = 19.22'
 P.C. STA. = 14+87.02
 P.T. STA. = 17+91.86

RAMP DB C-2300
 PI STA. = 19+29.36
 Δ = 34°41'01" (RT)
 D = 17°04'45"
 R = 335.47'
 T = 104.76'
 L = 203.08'
 E = 15.98'
 P.C. STA. = 18+24.60
 P.T. STA. = 20+27.68

RAMP BC C-2210
 PI STA. = 8+23.91
 Δ = 53°28'26" (RT)
 D = 10°58'43"
 R = 521.89'
 T = 262.90'
 L = 487.07'
 E = 62.48'
 P.C. STA. = 5+70.01
 P.T. STA. = 10+57.08

RAMP BC C-2220
 PI STA. = 14+51.76
 Δ = 36°33'09" (LT)
 D = 13°10'54"
 R = 434.67'
 T = 143.55'
 L = 277.30'
 E = 23.09'
 P.C. STA. = 13+08.21
 P.T. STA. = 15+85.51

RAMP BC C-2230
 PI STA. = 20+08.38
 Δ = 63°51'04" (RT)
 D = 18°30'43"
 R = 309.51'
 T = 192.84'
 L = 344.92'
 E = 55.16'
 P.C. STA. = 18+15.53
 P.T. STA. = 21+60.45

RAMP AC_GEOM1
 PI STA. = 0+90.84
 Δ = 15°00'00" (RT)
 D = 8°18'13"
 R = 990.00'
 T = 90.84'
 L = 180.64'
 E = 5.95'
 P.C. STA. = 0+00.00
 P.T. STA. = 1+80.64

POINT	NORTHING	EASTING
P.I.	2,032,272.836	2,621,800.808
P.C.	2,032,353.134	2,621,800.137
P.T.	2,032,201.608	2,621,763.731
C.C.	2,032,350.440	2,621,477.806

POINT	NORTHING	EASTING
P.I.	2,032,028.004	2,622,709.142
P.C.	2,032,003.701	2,623,201.432
P.T.	2,032,387.797	2,622,372.261
C.C.	2,033,220.404	2,623,261.496

POINT	NORTHING	EASTING
P.I.	2,032,509.399	2,622,258.402
P.C.	2,032,395.839	2,622,364.731
P.T.	2,032,659.706	2,622,218.285
C.C.	2,032,819.585	2,622,817.297

POINT	NORTHING	EASTING
P.I.	2,032,792.553	2,622,182.828
P.C.	2,032,691.340	2,622,209.842
P.T.	2,032,891.153	2,622,218.209
C.C.	2,032,777.850	2,622,533.965

POINT	NORTHING	EASTING
P.I.	2,032,795.893	2,622,044.831
P.C.	2,033,033.679	2,622,116.974
P.T.	2,032,744.482	2,621,747.003
C.C.	2,033,256.293	2,621,644.947

POINT	NORTHING	EASTING
P.I.	2,032,667.302	2,621,359.945
P.C.	2,032,695.374	2,621,500.725
P.T.	2,032,560.909	2,621,263.573
C.C.	2,032,269.100	2,621,585.725

POINT	NORTHING	EASTING
P.I.	2,032,247.502	2,620,979.686
P.C.	2,032,390.428	2,621,109.149
P.T.	2,032,300.727	2,620,794.333
C.C.	2,032,598.213	2,620,879.757

POINT	NORTHING	EASTING
P.I.	2,032,303.297	2,621,948.877
P.C.	2,032,221.801	2,621,908.749
P.T.	2,302,371.630	2,622,008.730
C.C.	2,031,916.993	2,622,527.774

RAMP AC_GEOM2
 PI STA. = 3+80.87
 Δ = 94°18'08" (RT)
 D = 30°50'56"
 R = 185.73'
 T = 200.23'
 L = 305.69'
 E = 87.38'
 P.C. STA. = 1+80.64
 P.T. STA. = 4+86.33

RAMP AC_GEOM3
 PI STA. = 6+86.56
 Δ = 94°18'08" (RT)
 D = 30°50'56"
 R = 185.73'
 T = 200.23'
 L = 305.69'
 E = 87.38'
 P.C. STA. = 4+86.33
 P.T. STA. = 7+92.02

RAMP AC_GEOM4
 PI STA. = 9+48.41
 Δ = 46°57'43" (RT)
 D = 15°54'56"
 R = 360.00'
 T = 156.39'
 L = 295.07'
 E = 32.50'
 P.C. STA. = 7+92.02
 P.T. STA. = 10+87.09

RAMP BD_GEOM1
 PI STA. = 1+02.30
 Δ = 16°52'00" (RT)
 D = 8°18'13"
 R = 690.00'
 T = 102.30'
 L = 203.12'
 E = 7.54'
 P.C. STA. = 0+00.00
 P.T. STA. = 2+03.12

RAMP BD_GEOM2
 PI STA. = 4+01.56
 Δ = 93°50'39" (RT)
 D = 30°52'44"
 R = 185.55'
 T = 198.44'
 L = 303.91'
 E = 86.12'
 P.C. STA. = 2+03.12
 P.T. STA. = 5+07.03

RAMP BD_GEOM3
 PI STA. = 7+05.47
 Δ = 93°50'39" (RT)
 D = 30°52'44"
 R = 185.55'
 T = 198.44'
 L = 303.91'
 E = 86.12'
 P.C. STA. = 5+07.03
 P.T. STA. = 8+10.94

RAMP BD_GEOM4
 PI STA. = 9+62.38
 Δ = 46°47'45" (RT)
 D = 16°22'13"
 R = 350.00'
 T = 151.44'
 L = 285.86'
 E = 31.36'
 P.C. STA. = 8+10.94
 P.T. STA. = 10+96.80

POINT

PROPOSED GEOMETRY

PRAMPDB-2
 PI STA. = 14+23.59
 $\Delta = 31^{\circ}14'58''$ (LT)
 $D = 1^{\circ}56'44''$
 $R = 2,944.85$
 $T = 823.58'$
 $L = 1,606.13'$
 $E = 113.00'$
 P.C. STA. = 6+00.01
 P.T. STA. = 22+06.14
 DESIGN SPEED = 70 MPH
 S.E. = S.E. FOR CURVE I39C1

PRAMPDB-2		
POINT	NORTHING	EASTING
P.I.	2,020,385.521	2,609,541.613
P.C.	2,019,687.797	2,609,104.035
P.T.	2,021,209.016	2,609,553.752
C.C.	2,021,252.422	2,606,609.219

125001200
 PI STA. = 2482+69.55
 $\Delta = 50^{\circ}10'11''$ (RT)
 $D = 1^{\circ}30'00''$
 $R = 3,819.80'$
 $T = 1,788.09'$
 $L = 3,344.71'$
 $E = 397.80'$
 P.C. STA. = 2464+81.46
 P.T. STA. = 2498+26.17
 DESIGN SPEED = 70 MPH

125001200		
POINT	NORTHING	EASTING
P.I.	2,017,084.937	2,606,190.744
P.C.	2,015,297.292	2,606,230.665
P.T.	2,018,260.612	2,607,537.985
C.C.	2,015,382.573	2,610,049.516

PRAMPBD-4
 PI STA. = 202+05.68
 $\Delta = 61^{\circ}57'49''$ (RT)
 $D = 2^{\circ}23'14''$
 $R = 2,400.00'$
 $T = 1,441.03'$
 $L = 2,595.52'$
 $E = 399.39'$
 P.C. STA. = 187+64.65
 P.T. STA. = 213+60.17
 DESIGN SPEED = 70 MPH
 S.E. = 5.8%
 S.E. RUNOFF = 225'
 TR = 39'

STA 185+65 N.C. - STA 188+68 MATCH EX. S.E.
 STA 212+72 5.8% - STA 215+75 MATCH EX. S.E.

PRAMPBD-4		
POINT	NORTHING	EASTING
P.I.	2,020,081.086	2,609,520.698
P.C.	2,021,484.764	2,609,194.743
P.T.	2,019,133.607	2,608,434.954
C.C.	2,020,941.892	2,606,856.947

PRAMPBD-3
 PI STA. = 169+10.06
 $\Delta = 107^{\circ}00'23''$ (LT)
 $D = 2^{\circ}14'49''$
 $R = 2,550.00'$
 $T = 3,446.54'$
 $L = 4,762.42'$
 $E = 1,737.32'$
 P.C. STA. = 134+63.52
 P.T. STA. = 182+25.95
 DESIGN SPEED = 70 MPH
 S.E. = 5.7%
 S.E. RUNOFF = 221'
 TR = 40'

STA 132+50 N.C. - STA 135+50 5.8%
 STA 181+39 5.8% - STA 184+40 N.C.

PRAMPBD-3		
POINT	NORTHING	EASTING
P.I.	2,025,366.723	2,608,293.295
P.C.	2,025,130.305	2,611,731.718
P.T.	2,022,009.510	2,609,072.890
C.C.	2,022,586.311	2,611,556.799

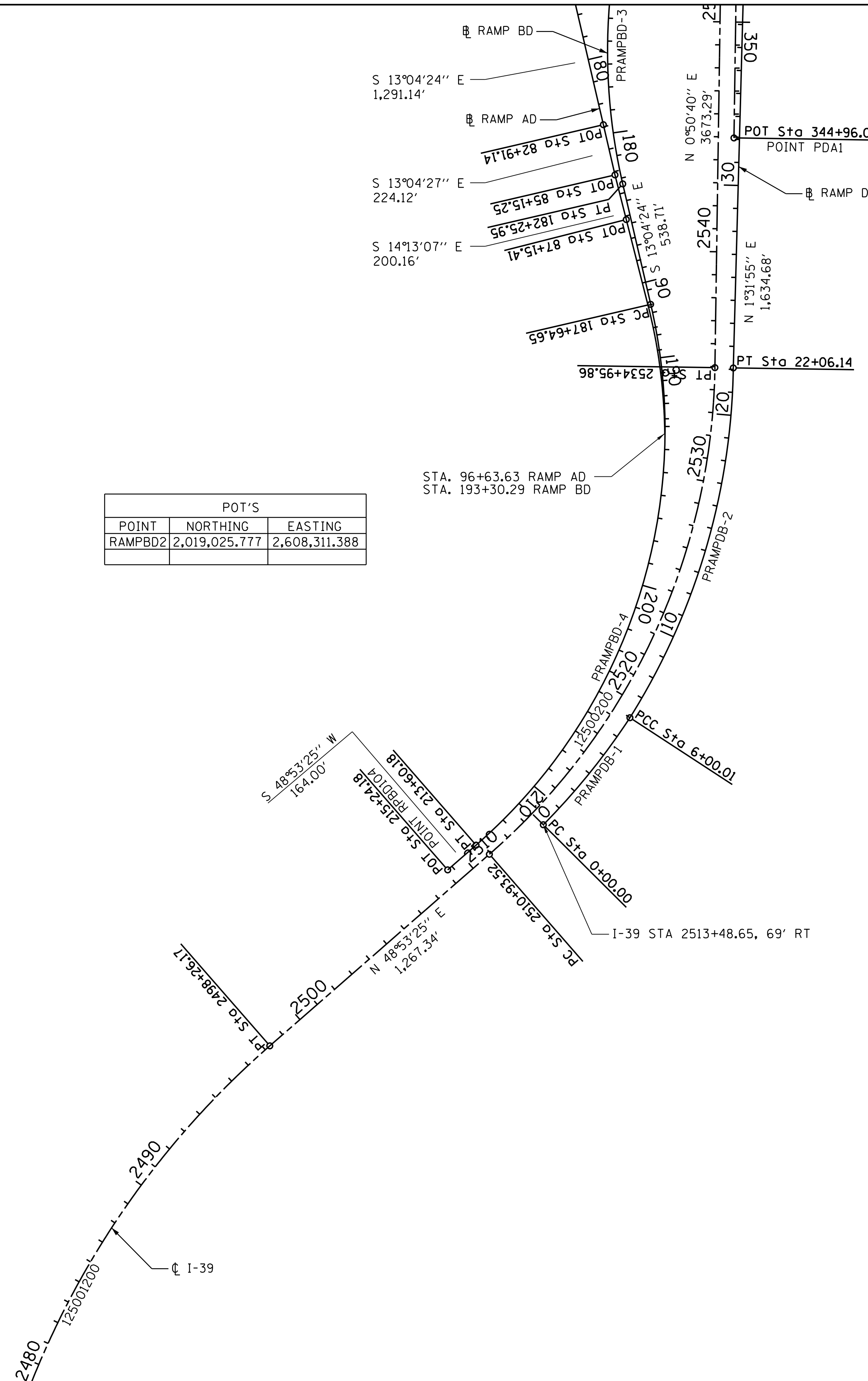
PRAMPDB-1
 PI STA. = 3+01.05
 $\Delta = 11^{\circ}41'36''$ (LT)
 $D = 1^{\circ}56'56''$
 $R = 2,939.98$
 $T = 301.05'$
 $L = 600.01'$
 $E = 15.37'$
 P.C. STA. = 0+00.00
 P.T. STA. = 6+00.01

PRAMPDB-1		
POINT	NORTHING	EASTING
P.I.	2,019,435.720	2,608,939.451
P.C.	2,019,222.230	2,608,727.193
P.T.	2,019,687.797	2,609,104.035
C.C.	2,021,295.091	2,626,642.305

12500200
 PI STA. = 2523+70.40
 $\Delta = 48^{\circ}02'45''$ (LT)
 $D = 2^{\circ}00'00''$
 $R = 2,864.85'$
 $T = 1,276.89'$
 $L = 2,402.34'$
 $E = 271.68'$
 P.C. STA. = 2510+93.52
 P.T. STA. = 2534+95.86
 DESIGN SPEED = 70 MPH
 S.E. = 6.0%
 S.E. RUNOFF = 285'
 TR = 75'

12500200		
POINT	NORTHING	EASTING
P.I.	2,019,933.450	2,609,454.939
P.C.	2,019,093.894	2,608,492.867
P.T.	2,021,210.196	2,609,473.760
C.C.	2,021,252.423	2,606,609.219

POT'S		
POINT	NORTHING	EASTING
RAMPBD2	2,019,025.777	2,608,311.388



PROPOSED RAMP DA		
POINT	NORTHING	EASTING
PDA1	2,022,209.302	2,609,556.495
PDA2	2,024,100.782	2,609,584.377
PDA3	2,024,940.359	2,609,598.919

FILE NAME = I:\06jobs\0652055\CADD\Road\Sheet\centerline geometric 6.dgn	USER NAME = Johns00944	DESIGNED - J.M.	REVISED -
PLOT SCALE = 300.0000' / in.	CHECKED - S.M.	DRAWN - R.S.J.	REVISED -
PLOT DATE = 3/30/2017	DATE -		REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
PROPOSED GEOMETRY - 1
JOB NO. P-92-111-06

SCALE: 1"=300' SHEET NO. 1 OF 5 SHEETS STA. TO STA.

F.A.I. RTE. 39	SECTION (201-3K & (4-1,5)R)	COUNTY WINNEBAGO	TOTAL SHEETS xx	SHEET NO. 7
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

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B.M. 316 - N-2024939.81, E-2612940.03 ELEV - 805.75
 MCCLURE B.M. 2" ALUMINUM DISK ON APPROX. ϕ OF U.S. 20
 ON PIER BASE UNDER MULFORD ROAD

PRAMPDB-3
 PI STA. = 58+02.34
 $\Delta = 87^\circ 28' 22''$ (RT)
 D = 2,474.42'
 R = 2,050.00'
 T = 1,961.51'
 L = 3,129.71'
 E = 787.26'
 P.C. STA. = 38+40.82
 P.T. STA. = 69+70.53
 DESIGN SPEED = 70 MPH
 S.E. = 6.0%
 S.E. RUNOFF = 230'
 TR = 40'

PRAMPDB-3		
POINT	NORTHING	EASTING
P.I.	2,024,803.923	2,609,649.894
P.C.	2,022,843.110	2,609,597.454
P.T.	2,024,837.995	2,611,611.112
C.C.	2,022,788.304	2,611,646.721

PRAMPBD-3
 PI STA. = 169+10.06
 $\Delta = 107^\circ 00' 23''$ (LT)
 D = 2° 14' 49"
 R = 2,550.00'
 T = 3,446.54'
 L = 4,762.42'
 E = 1,737.32'
 P.C. STA. = 134+63.52
 P.T. STA. = 182+25.95
 DESIGN SPEED = 70 MPH
 S.E. = 5.7%
 S.E. RUNOFF = 221'
 TR = 40'

PRAMPBD-3		
POINT	NORTHING	EASTING
P.I.	2,025,366.723	2,608,293.295
P.C.	2,025,130.305	2,611,731.718
P.T.	2,022,009.510	2,609,072.890
C.C.	2,022,586.311	2,611,556.799

PRAMPBD-2
 PI STA. = 115+13.45
 $\Delta = 10^\circ 17' 25''$ (RT)
 D = 1° 00' 50"
 R = 5,651.60'
 T = 508.89'
 L = 1,015.03'
 E = 22.86'
 P.C.C. STA. = 110+04.56
 P.T. STA. = 120+19.60
 DESIGN SPEED = 70 MPH
 S.E. = 3.5%
 S.E. RUNOFF = 118'
 TR = 41'
 STA 119+67 3.5% - STA 121+67 N.C.

PRAMPBD-2		
POINT	NORTHING	EASTING
P.I.	2,024,996.350	2,613,679.929
P.C.	2,025,052.696	2,614,185.686
P.T.	2,025,031.258	2,613,172.243
C.C.	2,030,669.550	2,613,559.919

PRAMPBD-1
 PI STA. = 105+11.67
 $\Delta = 10^\circ 00' 06''$ (RT)
 D = 1° 00' 43"
 R = 5,661.50'
 T = 495.40'
 L = 998.29'
 E = 21.63'
 P.C. STA. = 100+16.27
 P.C.C. STA. = 110+04.56
 DESIGN SPEED = 70 MPH
 S.E. = 3.5% MATCH US20C2

PRAMPBD-1		
POINT	NORTHING	EASTING
P.I.	2,025,113.386	2,614,677.359
P.C.	2,025,258.546	2,615,151.020
P.T.	2,025,052.696	2,614,185.686
C.C.	2,030,671.553	2,613,492.121

12500240
 PI STA. = 1151+92.59
 $\Delta = 1^\circ 16' 53''$ (RT)
 D = 0° 08' 00"
 R = 42,959.39'
 T = 480.37'
 L = 960.71'
 E = 2.69'
 P.C. STA. = 1147+12.22
 P.T. STA. = 1156+72.93
 DESIGN SPEED = 70 MPH
 S.E. = N.C.

12500240		
POINT	NORTHING	EASTING
P.I.	2,024,875.670	2,608,875.403
P.C.	2,024,859.468	2,608,395.303
P.T.	2,024,881.132	2,609,355.744
C.C.	1,981,924.454	2,609,844.249

12500220
 PI STA. = 2638+44.91
 $\Delta = 50^\circ 41' 00''$ (LT)
 D = 1° 00' 00"
 R = 5,729.70'
 T = 2,713.52'
 L = 5,068.45'
 E = 610.07'
 P.C. STA. = 2611+31.39
 P.T. STA. = 2661+99.84
 DESIGN SPEED = 70 MPH
 S.E. = 3.5%
 S.E. RUNOFF = 128'
 TR = 82'

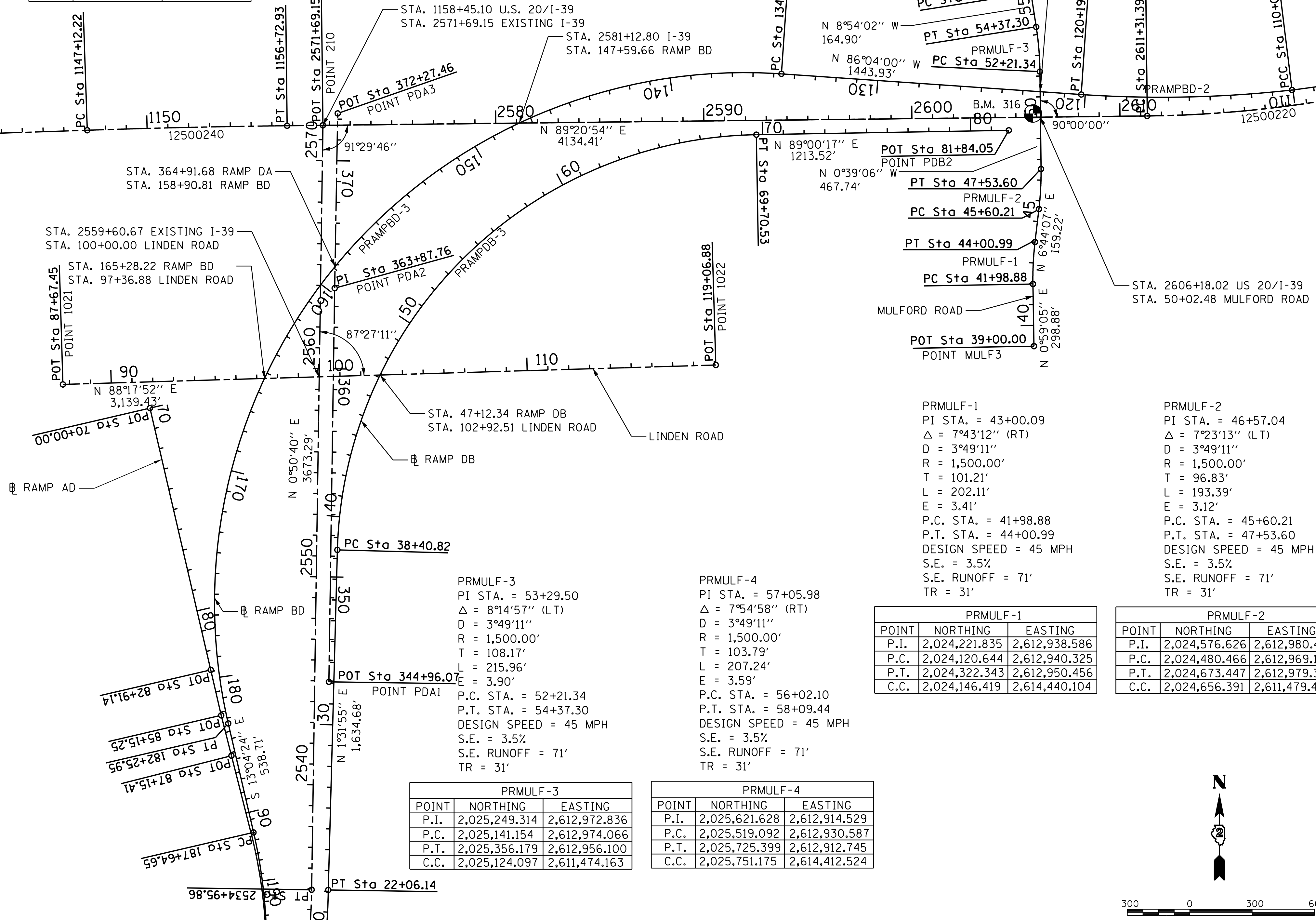
12500220		
POINT	NORTHING	EASTING
P.I.	2,024,959.002	2,616,203.233
P.C.	2,024,928.146	2,613,489.890
P.T.	2,027,077.747	2,617,898.551
C.C.	2,030,657.480	2,613,424.736

MULFORD ROAD		
POINT	NORTHING	EASTING
MULF3	2,023,821.804	2,612,945.461
MULF4	2,025,935.932	2,612,909.127

LINDEN ROAD		
POINT	NORTHING	EASTING
1021	2,023,638.130	2,608,278.086
1022	2,023,731.385	2,611,416.133

PROPOSED RAMP DA		
POINT	NORTHING	EASTING
PDA1	2,022,209.302	2,609,556.495
PDA2	2,024,100.782	2,609,584.377
PDA3	2,024,940.359	2,609,598.919

POT'S		
POINT	NORTHING	EASTING
210	2,024,883.090	2,609,527.903
PDB2	2,024,859.074	2,612,824.451



UPRR		
POINT	NORTHING	EASTING
1042	2,028,636.926	2,618,746.726
1043	2,028,979.245	2,619,686.309

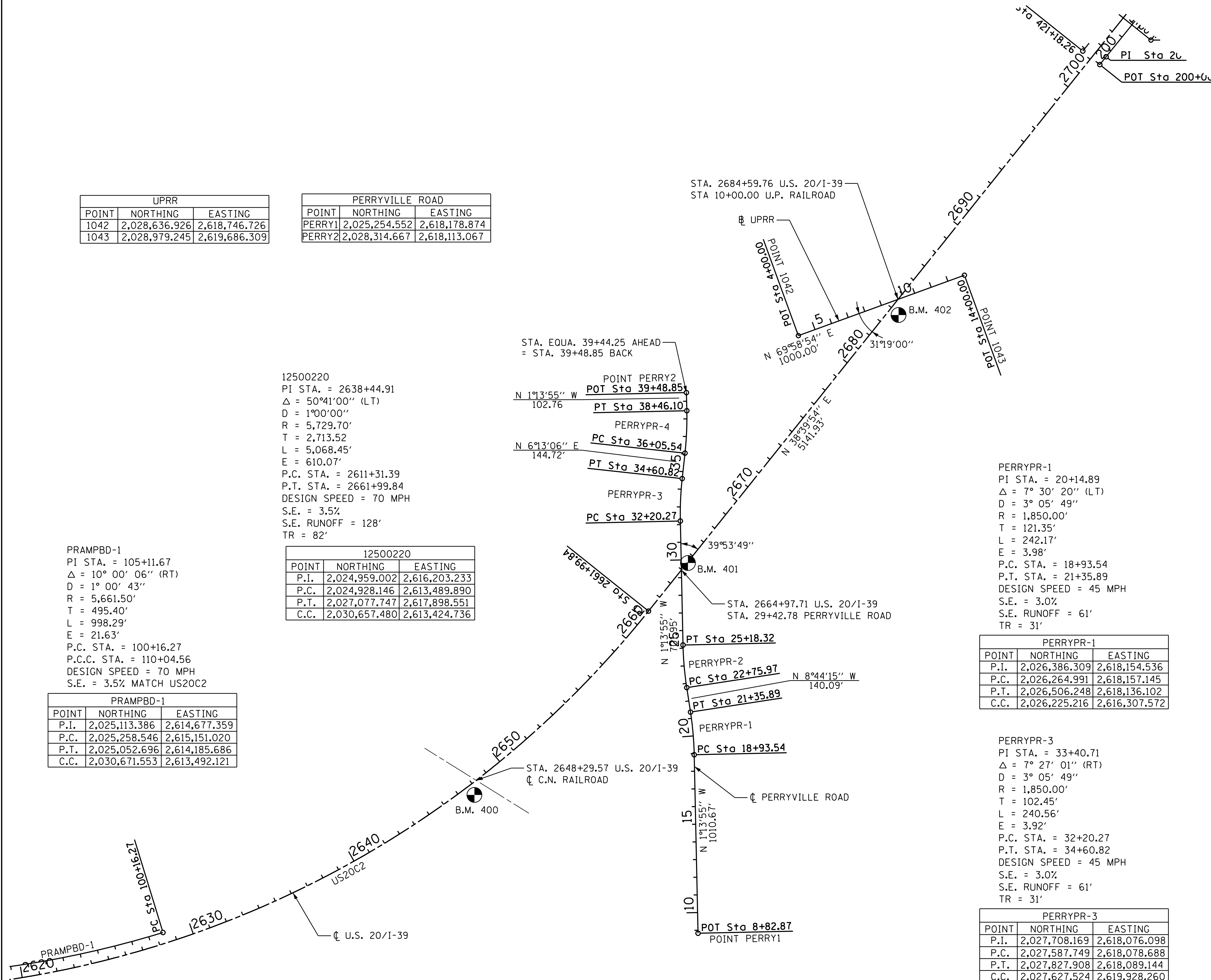
PERRYVILLE ROAD		
POINT	NORTHING	EASTING
PERRY1	2,025,254.552	2,618,178.874
PERRY2	2,028,314.667	2,618,113.067

12500220
 PI STA. = 2638+44.91
 $\Delta = 50^{\circ}41'00''$ (LT)
 $D = 1^{\circ}00'00''$
 $R = 5,729.70'$
 $T = 2,713.52$
 $L = 5,068.45'$
 $E = 610.07'$
 P.C. STA. = 2611+31.39
 P.T. STA. = 2661+99.84
 DESIGN SPEED = 70 MPH
 S.E. = 3.5%
 S.E. RUNOFF = 128'
 TR = 82'

12500220		
POINT	NORTHING	EASTING
P.I.	2,024,959.002	2,616,203.233
P.C.	2,024,928.146	2,613,489.890
P.T.	2,027,077.747	2,617,898.551
C.C.	2,030,657.480	2,613,424.736

PRAMPBD-1
 PI STA. = 105+11.67
 $\Delta = 10^{\circ}00'06''$ (RT)
 $D = 1^{\circ}00'43''$
 $R = 5,661.50'$
 $T = 495.40'$
 $L = 998.29'$
 $E = 21.63'$
 P.C. STA. = 100+16.27
 P.C.C. STA. = 110+04.56
 DESIGN SPEED = 70 MPH
 S.E. = 3.5% MATCH US20C2

PRAMPBD-1		
POINT	NORTHING	EASTING
P.I.	2,025,113.386	2,614,677.359
P.C.	2,025,258.546	2,615,151.020
P.T.	2,025,052.696	2,614,185.686
C.C.	2,030,671.553	2,613,492.121



- B.M. 400 - N-2026037.41, E-2616913.38 ELEV - 800.52
CUT SQUARE ON S.E. CORNER OF N.B. BRIDGE OF I-39 OVER C.N. RAILROAD
- B.M. 401 - N-2027350.15, E-2618121.72 ELEV - 788.53
CUT SQUARE ON SOUTHERLY END OF CENTER CRASH WALL OF BRIDGE UNDER PERRYVILLE ROAD
- B.M. 402 - N-2028753.85, E-2619313.60 ELEV - 796.69
CUT SQUARE ON S.E. CORNER OF N.B. I-39 OVER U.P. RAILROAD

PERRYPR-1
 PI STA. = 20+14.89
 $\Delta = 7^{\circ}30'20''$ (LT)
 $D = 3^{\circ}05'49''$
 $R = 1,850.00'$
 $T = 121.35'$
 $L = 242.17'$
 $E = 3.98'$
 P.C. STA. = 18+93.54
 P.T. STA. = 21+35.89
 DESIGN SPEED = 45 MPH
 S.E. = 3.0%
 S.E. RUNOFF = 61'
 TR = 31'

PERRYPR-2
 PI STA. = 23+97.32
 $\Delta = 7^{\circ}30'20''$ (RT)
 $D = 3^{\circ}05'49''$
 $R = 1,850.00'$
 $T = 121.35'$
 $L = 242.35'$
 $E = 3.98'$
 P.C. STA. = 22+75.97
 P.T. STA. = 25+18.32
 DESIGN SPEED = 45 MPH
 S.E. = 3.0%
 S.E. RUNOFF = 61'
 TR = 31'

PERRYPR-1		
POINT	NORTHING	EASTING
P.I.	2,026,386.309	2,618,154.536
P.C.	2,026,264.991	2,618,157.145
P.T.	2,026,506.248	2,618,136.102
C.C.	2,026,225.216	2,616,307.572

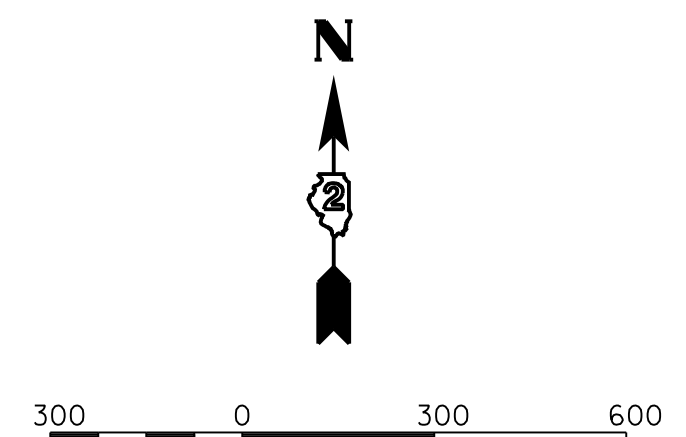
PERRYPR-2		
POINT	NORTHING	EASTING
P.I.	2,026,764.646	2,618,096.388
P.C.	2,026,644.708	2,618,114.822
P.T.	2,026,885.965	2,618,093.779
C.C.	2,026,925.740	2,619,943.352

PERRYPR-3
 PI STA. = 33+40.71
 $\Delta = 7^{\circ}27'01''$ (RT)
 $D = 3^{\circ}05'49''$
 $R = 1,850.00'$
 $T = 102.45'$
 $L = 240.56'$
 $E = 3.92'$
 P.C. STA. = 32+20.27
 P.T. STA. = 34+60.82
 DESIGN SPEED = 45 MPH
 S.E. = 3.0%
 S.E. RUNOFF = 61'
 TR = 31'

PERRYPR-4
 PI STA. = 37+25.99
 $\Delta = 7^{\circ}27'01''$ (LT)
 $D = 3^{\circ}05'49''$
 $R = 1,850.00'$
 $T = 120.45'$
 $L = 250.56'$
 $E = 3.92'$
 P.C. STA. = 36+05.54
 P.T. STA. = 38+46.10
 DESIGN SPEED = 45 MPH
 S.E. = 3.0%
 S.E. RUNOFF = 61'
 TR = 31'

PERRYPR-3		
POINT	NORTHING	EASTING
P.I.	2,027,708.169	2,618,076.098
P.C.	2,027,587.749	2,618,078.688
P.T.	2,027,827.908	2,618,089.144
C.C.	2,027,627.524	2,619,928.260

PERRYPR-4		
POINT	NORTHING	EASTING
P.I.	2,028,091.512	2,618,117.866
P.C.	2,027,971.773	2,618,104.820
P.T.	2,028,211.932	2,618,115.276
C.C.	2,028,172.157	2,616,265.704



FILE NAME = I:\06jobs\0652055\CADD\Road\Sheet\centerline geometric 8.dgn	USER NAME = Johns00944	DESIGNED - J.M.	REVISED -
PLOT SCALE = 300.0000' / in.	CHECKED - S.M.	DRAWN - R.S.J.	REVISED -
PLOT DATE = 3/30/2017	DATE -		REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 PROPOSED GEOMETRY - 3
 JOB NO. P-92-111-06**

SCALE: 1"=300' SHEET NO. 3 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1,5)R	WINNEBAGO	xx	9
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

I39NB-1
PI STA. = 4+17.53
Δ = 6° 56' 11" (LT)
D = 0° 49' 54"
R = 6,889.15'
T = 417.53'
L = 834.03'
E = 12.64'
P.C. STA. = 0+00.00
P.T. STA. = 8+34.03
DESIGN SPEED = 70 MPH
S.E. = 3.0%
S.E. RUNOFF = 175'
TR = N/A

I39NB-2
PI STA. = 17+55.89
Δ = 7° 37' 58" (RT)
D = 1° 08' 45"
R = 5,000.00'
T = 333.54'
L = 666.09'
E = 11.11'
P.C. STA. = 14+22.36
P.T. STA. = 20+88.44
DESIGN SPEED = 50 MPH
S.E. = 3.7%
S.E. RUNOFF = 191' & 223'
TR = N/A

I39NB-3
PI STA. = 25+95.82
Δ = 10° 47' 58" (LT)
D = 3° 33' 55"
R = 1,607.02'
T = 151.90'
L = 302.90'
E = 7.16'
P.C. STA. = 24+43.92
P.T. STA. = 27+46.82
DESIGN SPEED = 50 MPH
S.E. = 5.9%
S.E. RUNOFF = 311'
TR = N/A

PRAMPG-1
PI STA. = 1103+02.65
Δ = 20° 52' 28" (LT)
D = 3° 29' 14"
R = 1,643.01'
T = 302.65'
L = 598.59'
E = 27.64'
P.C. STA. = 1100+00.00
P.T. STA. = 1105+98.59
DESIGN SPEED = 50 MPH
S.E. = 5.9%
S.E. RUNOFF = N/A
TR = N/A

12500230
PI STA. = 2724+10.28
Δ = 17° 40' 00" (LT)
D = 0° 50' 00"
R = 6,875.65'
T = 1,068.50'
L = 2,120.05'
E = 82.53'
P.C. STA. = 2713+41.77
P.T. STA. = 2734+61.82
DESIGN SPEED = 70 MPH
S.E. = 3.0%
S.E. RUNOFF = 175'
TR = 82'

70200
PI STA. = 501+36.99
Δ = 75° 54' 26" (LT)
D = 52° 05' 13"
R = 110.00'
T = 85.80'
L = 145.73'
E = 29.50'
P.C. STA. = 500+51.20
P.T. STA. = 501+96.93
DESIGN SPEED = N/A
S.E. = N.C.
S.E. RUNOFF = N/A

70210
PI STA. = 505+77.33
Δ = 86° 36' 35" (RT)
D = 81° 51' 04"
R = 70'
T = 65.98'
L = 105.81'
E = 26.19'
P.C. STA. = 505+11.35
P.T. STA. = 506+17.16
DESIGN SPEED = N/A
S.E. = N.C.
S.E. RUNOFF = N/A

POINT	NORTHING	EASTING
P.I.	2,032,152.707	2,621,838.842
P.C.	2,031,796.421	2,621,621.151
P.T.	2,032,532.675	2,622,011.912
C.C.	2,035,388.305	2,615,742.483

POINT	NORTHING	EASTING
P.I.	2,033,371.608	2,622,394.033
P.C.	2,033,068.075	2,622,255.778
P.T.	2,033,654.088	2,622,571.379
C.C.	2,030,995.519	2,626,805.999

POINT	NORTHING	EASTING
P.I.	2,034,083.797	2,622,841.158
P.C.	2,033,955.148	2,622,760.390
P.T.	2,034,225.301	2,622,896.390
C.C.	2,034,809.625	2,621,399.363

POINT	NORTHING	EASTING
P.I.	2,034,494.151	2,623,039.958
P.C.	2,034,212.216	2,622,929.913
P.T.	2,034,796.793	2,623,042.322
C.C.	2,034,809.625	2,621,399.363

POINT	NORTHING	EASTING
P.I.	2,031,926.923	2,621,778.630
P.C.	2,031,092.624	2,621,111.064
P.T.	2,032,924.468	2,622,161.521
C.C.	2,035,388.304	2,615,742.486

POINT	NORTHING	EASTING
P.I.	2,032,098.560	2,624,242.906
P.C.	2,032,092.672	2,624,157.310
P.T.	2,032,183.013	2,624,258.037
C.C.	2,032,202.413	2,624,149.762

POINT	NORTHING	EASTING
P.I.	2,032,557.448	2,624,325.127
P.C.	2,032,492.507	2,624,313.491
P.T.	2,032,549.673	2,624,390.643
C.C.	2,032,480.161	2,624,382.394

POINT	NORTHING	EASTING
BELL1	2,032,362.490	2,619,133.178
BELL2	2,033,007.615	2,619,109.701
70001	2,032,085.060	2,624,046.635
70220	2,032,539.146	2,624,479.352
PRMILL1	2,031,310.105	2,624,451.517
PRMILL2	2,032,981.848	2,624,506.771
PRAMPA1	2,031,947.95	2,622,226.02
HARR12	2,031,922.599	2,626,822.584
1010	2,032,503.480	2,619,979.981
BIKE1	2,030,682.564	2,620,229.298

POINT	NORTHING	EASTING
PRAMPB1	2,030,170.97	2,620,452.36
PRAMPB2	2,030,215.75	2,620,488.19
PRAMPB3	2,031,947.95	2,622,226.02
PRAMPC1	2,033,588.70	2,622,374.76
PRAMPC2	2,032,110.74	2,621,240.65
PRAMPD1	2,032,110.74	2,621,240.65
PRAMPD2	2,030,546.66	2,620,595.44
PRAMPD3	2,030,247.82	2,620,356.32
SB2	2,033,995.27	2,622,577.56

HARRISON2-2
PI STA. = 1316+03.33
Δ = 9° 34' 19" (LT)
D = 1° 00' 00"
R = 5,729.71'
T = 479.73'
L = 957.22'
E = 20.05'
P.C. STA. = 1311+23.61
P.T. STA. = 1320+80.83
DESIGN SPEED = 45 MPH
S.E. = N/A

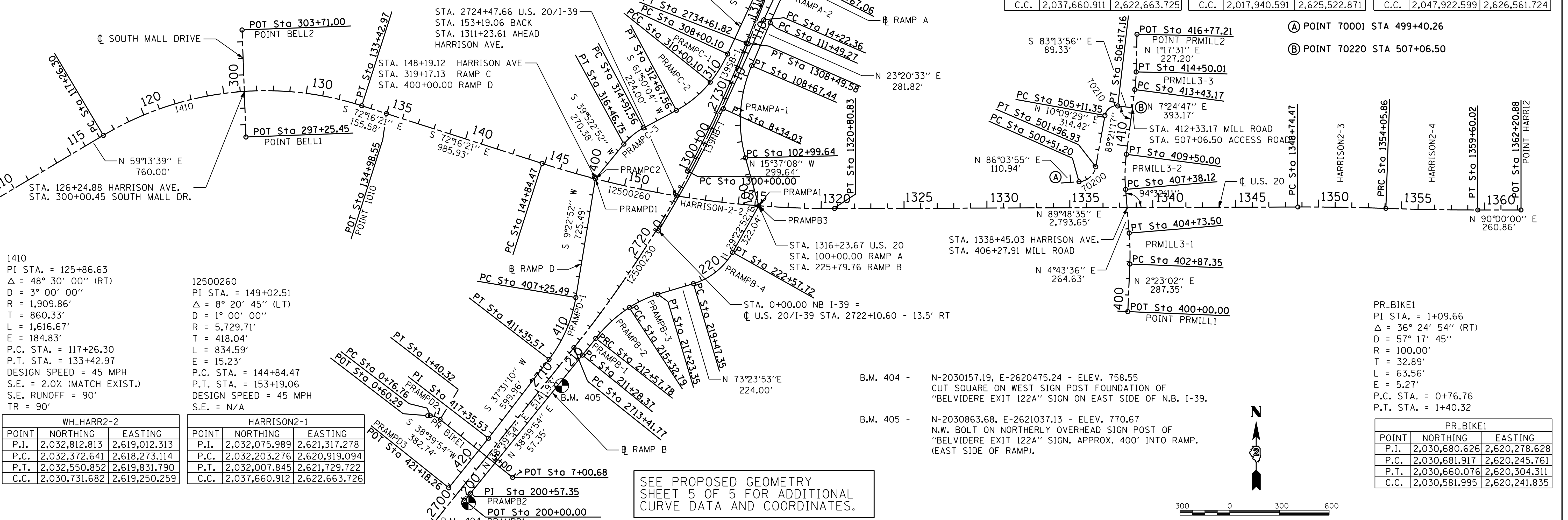
HARRISON2-3
PI STA. = 1351+40.20
Δ = 2° 10' 29" (RT)
D = 0° 24' 33"
R = 14,000.00'
T = 265.73'
L = 531.38'
E = 5.25'
P.C. STA. = 1348+74.47
P.T. STA. = 1354+05.86
DESIGN SPEED = 65 MPH
S.E. = N.C.
S.E. RUNOFF = N/A

HARRISON2-4
PI STA. = 1356+82.97
Δ = 1° 59' 04" (LT)
D = 0° 21' 29"
R = 16,000.00'
T = 277.11'
L = 554.17'
E = 2.40'
P.C. STA. = 1354+05.86
P.T. STA. = 1359+60.02
DESIGN SPEED = 65 MPH
S.E. = N.C.
S.E. RUNOFF = N/A

POINT	NORTHING	EASTING
P.I.	2,031,929.644	2,622,203.028
P.C.	2,032,007.844	2,621,729.721
P.T.	2,031,931.236	2,622,682.750
C.C.	2,037,660.911	2,622,663.725

POINT	NORTHING	EASTING
P.I.	2,031,941.396	2,625,742.106
P.C.	2,031,940.514	2,625,476.384
P.T.	2,031,932.194	2,626,007.669
C.C.	2,017,940.591	2,625,522.871

POINT	NORTHING	EASTING
P.I.	2,031,922.599	2,626,284.613
P.C.	2,031,932.194	2,626,007.669
P.T.	2,031,922.599	2,626,561.724
C.C.	2,047,922.599	2,626,561.724



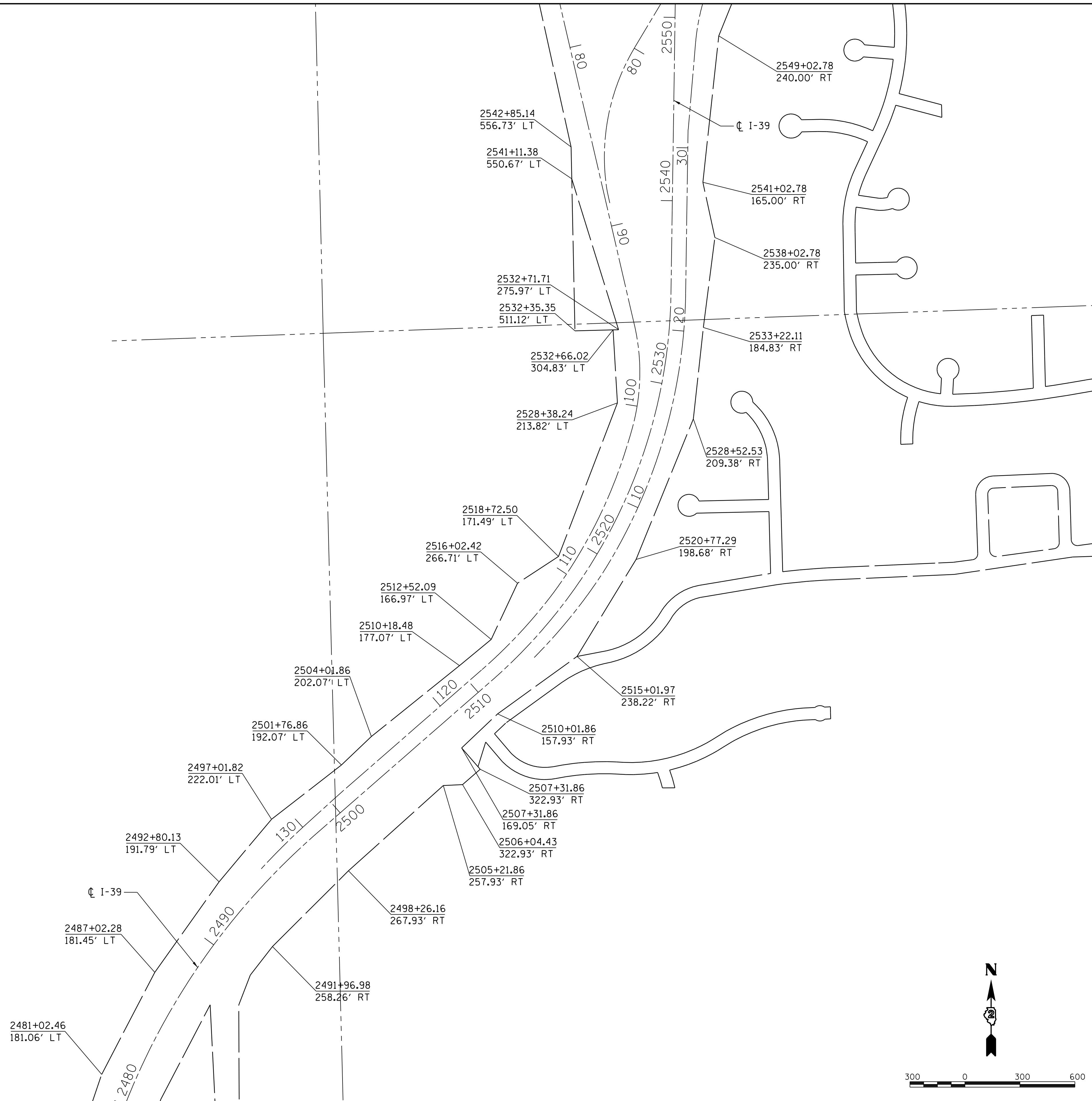
1410
PI STA. = 125+86.63
Δ = 48° 30' 00" (RT)
D = 3° 00' 00"
R = 1,909.86'
T = 860.33'
L = 1,616.67'
E = 184.83'
P.C. STA. = 117+26.30
P.T. STA. = 133+42.97
DESIGN SPEED = 45 MPH
S.E. = 2.0% (MATCH EXIST.)
S.E. RUNOFF = 90'
TR = 90'

12500260
PI STA. = 149+02.51
Δ = 8° 20' 45" (LT)
D = 1° 00' 00"
R = 5,729.71'
T = 418.04'
L = 834.59'
E = 15.23'
P.C. STA. = 144+84.47
P.T. STA. = 153+19.06
DESIGN SPEED = 45 MPH
S.E. = N/A

POINT	NORTHING	EASTING
P.I.	2,032,812.813	2,619,012.313
P.C.	2,032,372.641	2,618,273.114
P.T.	2,032,550.852	2,619,831.790
C.C.	2,030,731.682	2,619,250.259

POINT	NORTHING	EASTING
P.I.	2,032,075.989	2,621,317.278
P.C.	2,032,203.276	2,620,919.094
P.T.	2,032,007.845	2,621,729.722
C.C.	2,037,660.912	2,622,663.726

EXISTING RIGHT-OF-WAY



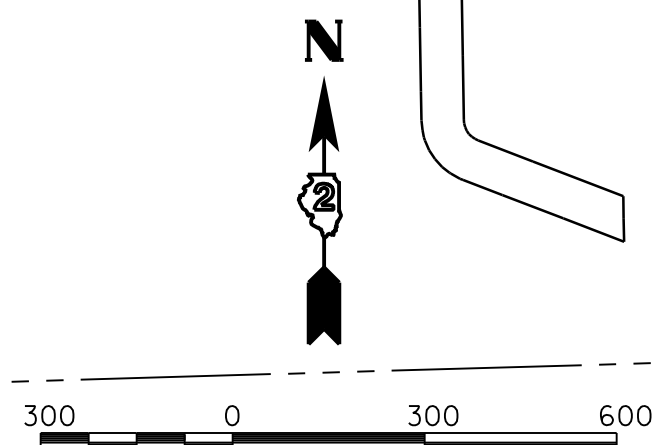
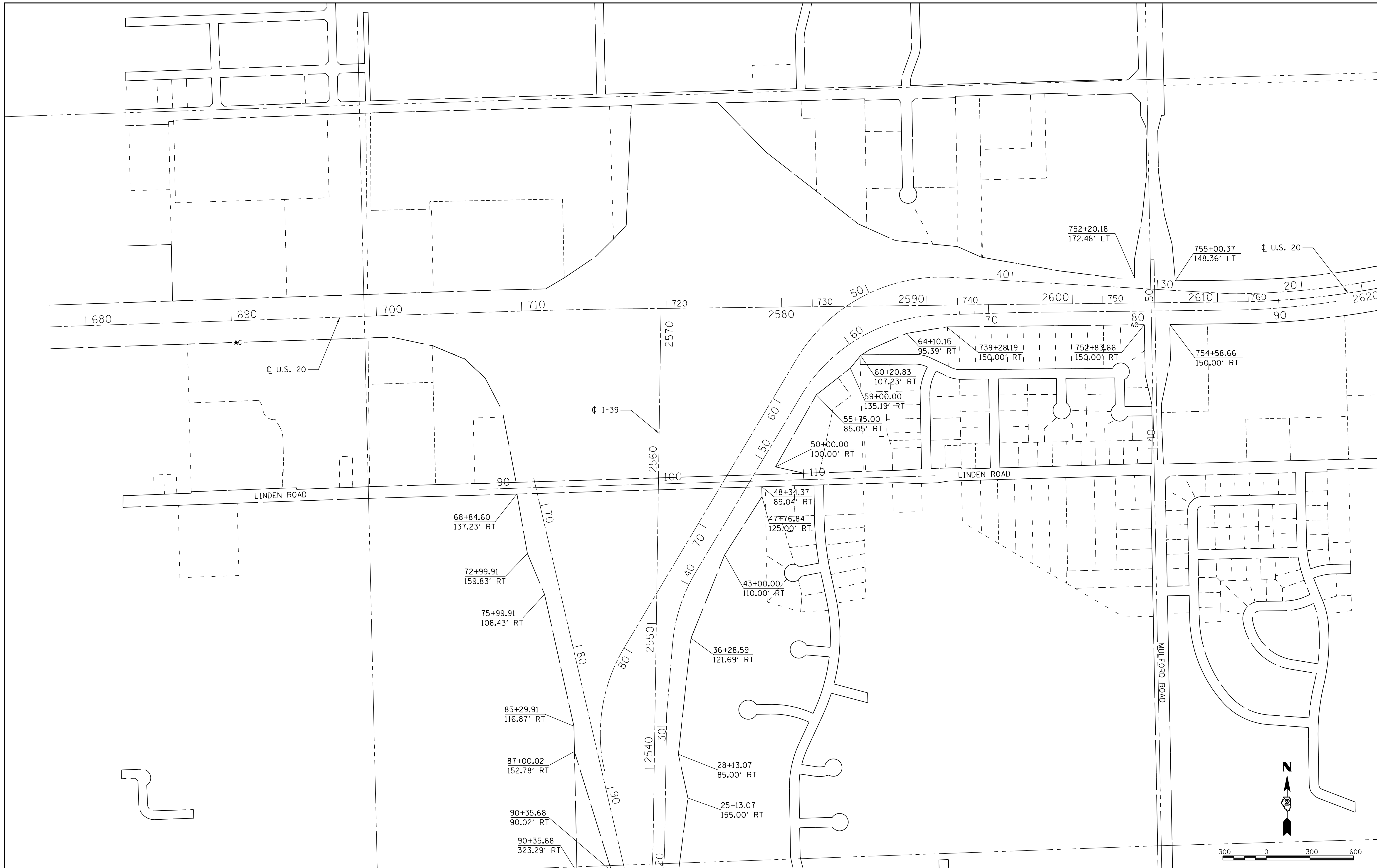
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	PLOT SCALE = 300.0000' / in.	CHECKED - S.M.	REVISED -
SHEET 1	PLOT DATE = 3/30/2017	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 EXISTING RIGHT OF WAY - 1
 JOB NO. P-92-111-06**

SCALE: 1"=300' SHEET NO. 1 OF 4 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1,5)R	WINNEBAGO	xx	12
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

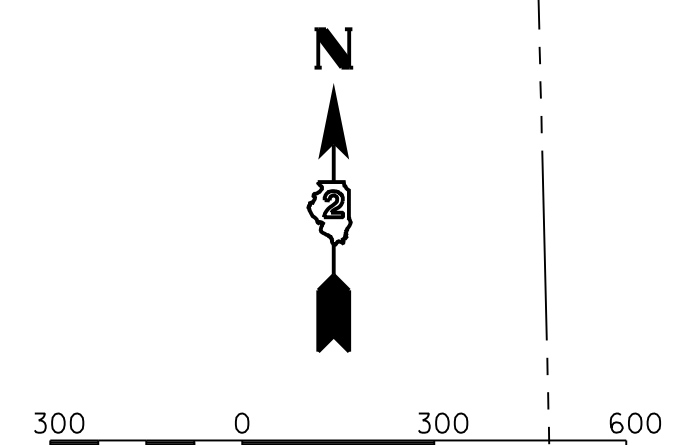
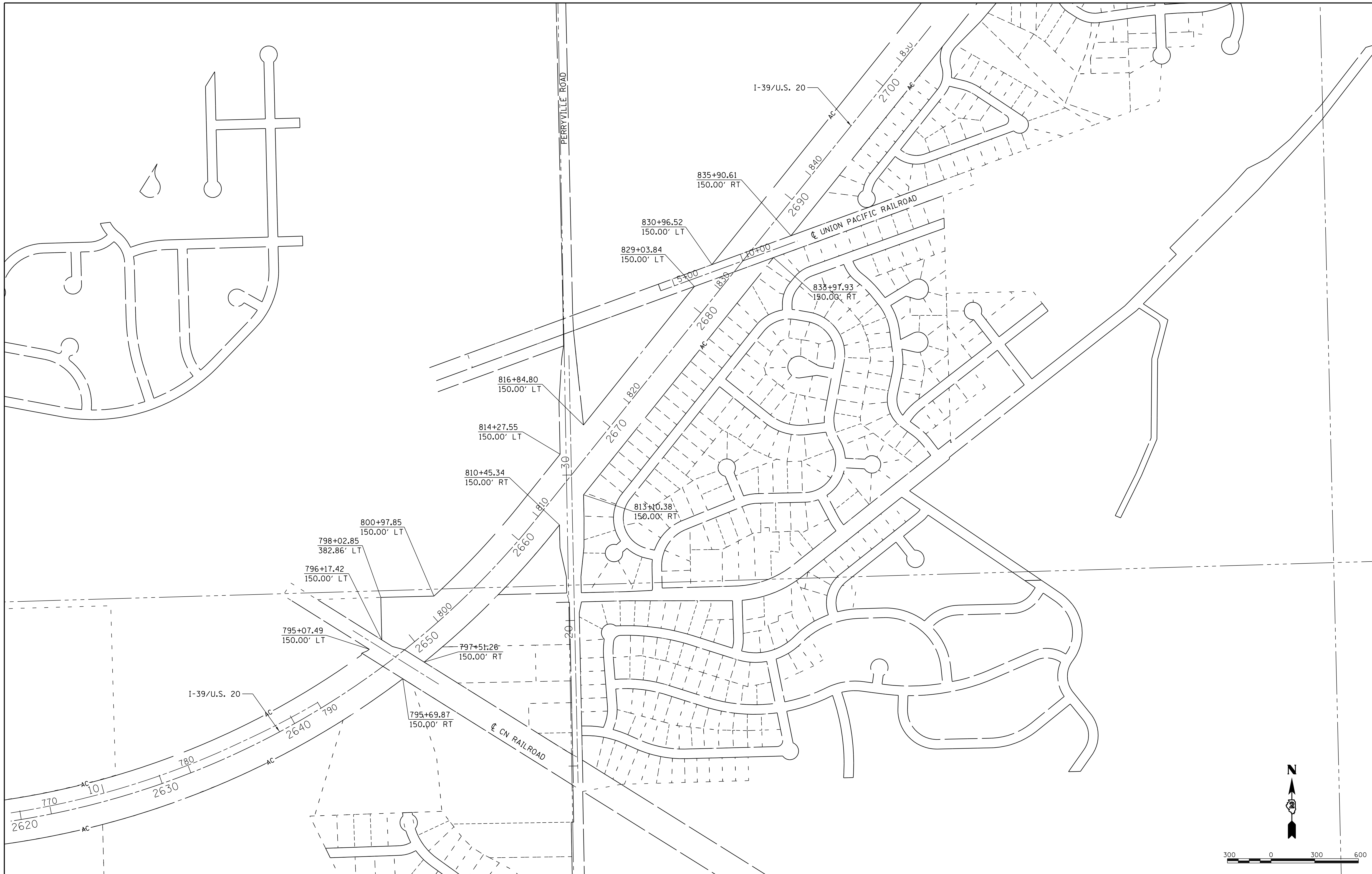


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		DRAWN - R.S.J.	REVISED -
		CHECKED - S.M.	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)			
EXISTING RIGHT OF WAY - 2			
JOB NO. P-92-111-06			
SCALE: 1"=300'	SHEET NO. 2 OF 4 SHEETS	STA.	TO STA.

F.A.I. RTE. 39	SECTION (201-31K & (4-1,5)R)	COUNTY WINNEBAGO	TOTAL SHEETS xx	SHEET NO. 13
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



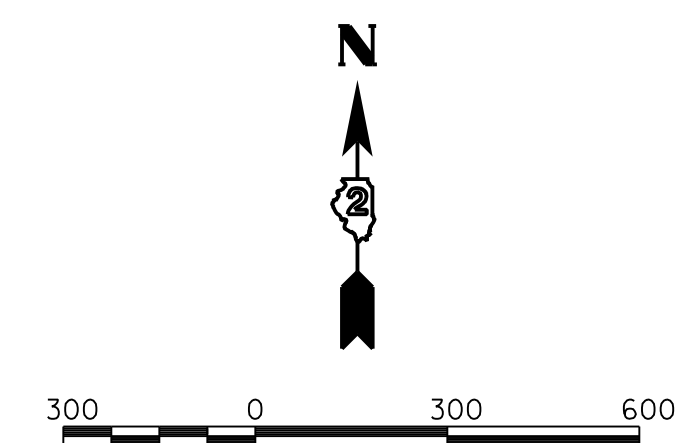
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		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 EXISTING RIGHT OF WAY - 3
 JOB NO. P-92-111-06**

SCALE: 1"=300' SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1,5)R	WINNEBAGO	xx	14
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



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USER NAME = Johns00944
 PLOT SCALE = 300.0000' / in.
 PLOT DATE = 3/30/2017

DESIGNED - J.M.
 DRAWN - R.S.J.
 CHECKED - S.M.
 DATE -

REVISED -
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 EXISTING RIGHT OF WAY - 4
 JOB NO. P-92-111-06**

SCALE: 1"=300' SHEET NO. 4 OF 4 SHEETS STA. TO STA.

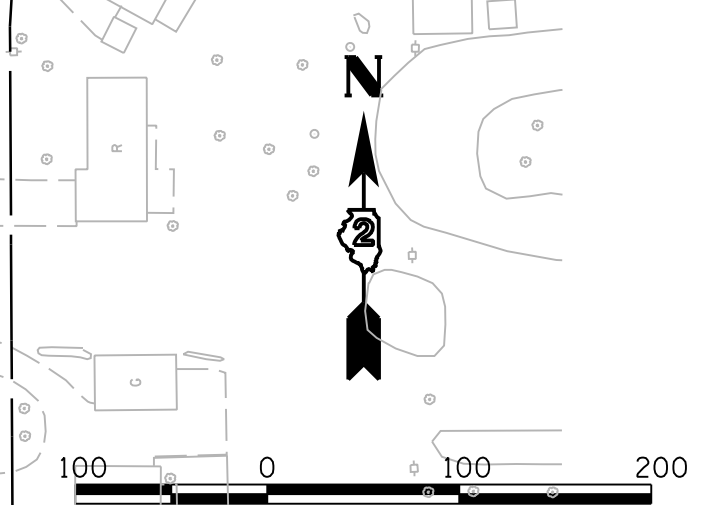
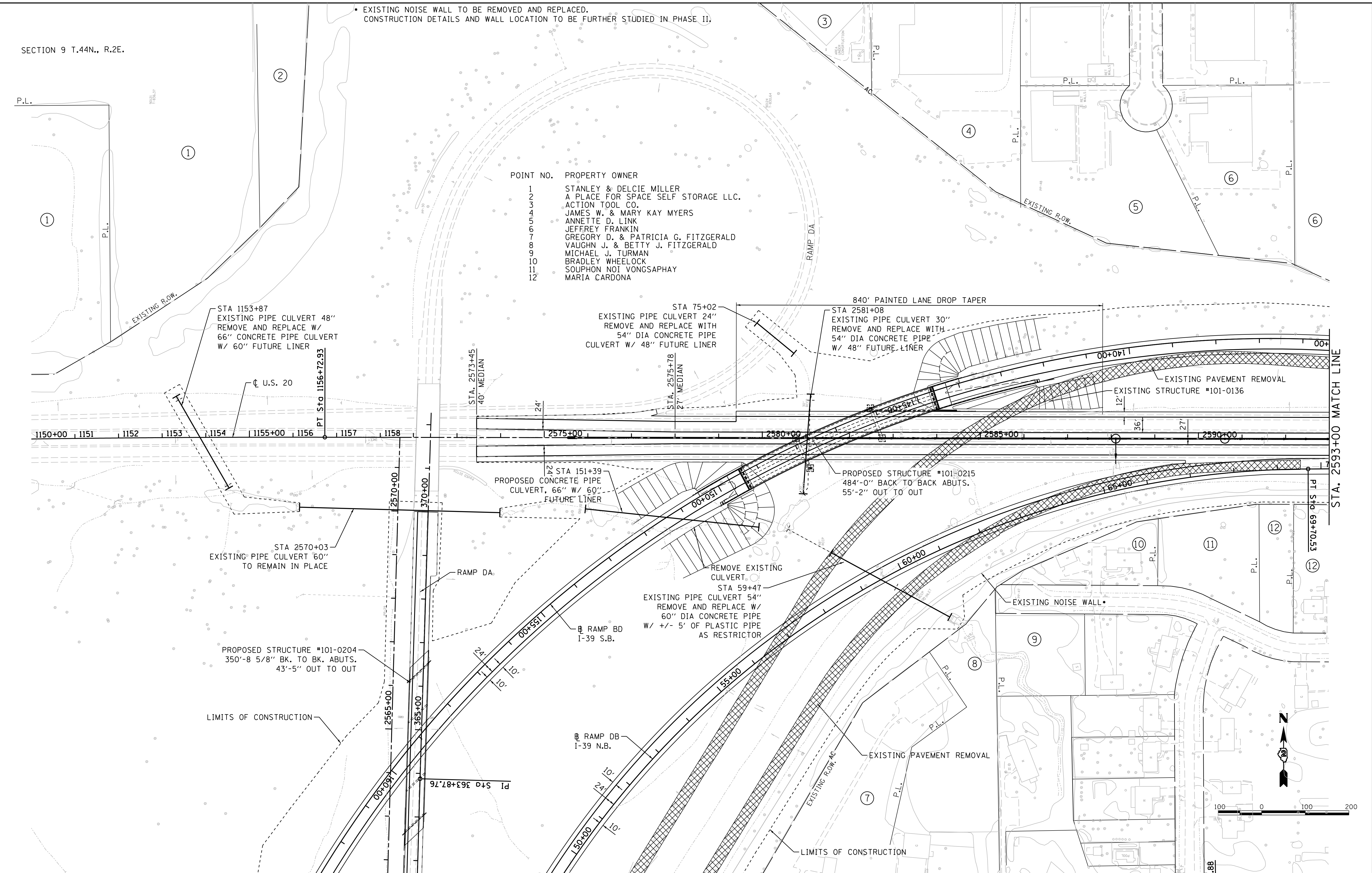
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1,5)R	WINNEBAGO	xx	15
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

PROPOSED PLAN SHEETS

SECTION 9 T.44N., R.2E.

• EXISTING NOISE WALL TO BE REMOVED AND REPLACED.
 CONSTRUCTION DETAILS AND WALL LOCATION TO BE FURTHER STUDIED IN PHASE II.

POINT NO.	PROPERTY OWNER
1	STANLEY & DELCIE MILLER
2	A PLACE FOR SPACE SELF STORAGE LLC.
3	ACTION TOOL CO.
4	JAMES W. & MARY KAY MYERS
5	ANNETTE D. LINK
6	JEFFREY FRANKIN
7	GREGORY D. & PATRICIA G. FITZGERALD
8	VAUGHN J. & BETTY J. FITZGERALD
9	MICHAEL J. TURMAN
10	BRADLEY WHELOCK
11	SOUPHON NOI VONGSAPHAY
12	MARIA CARDONA



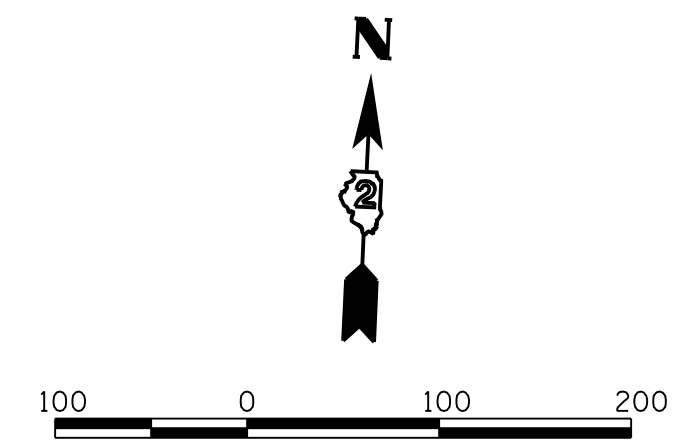
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		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

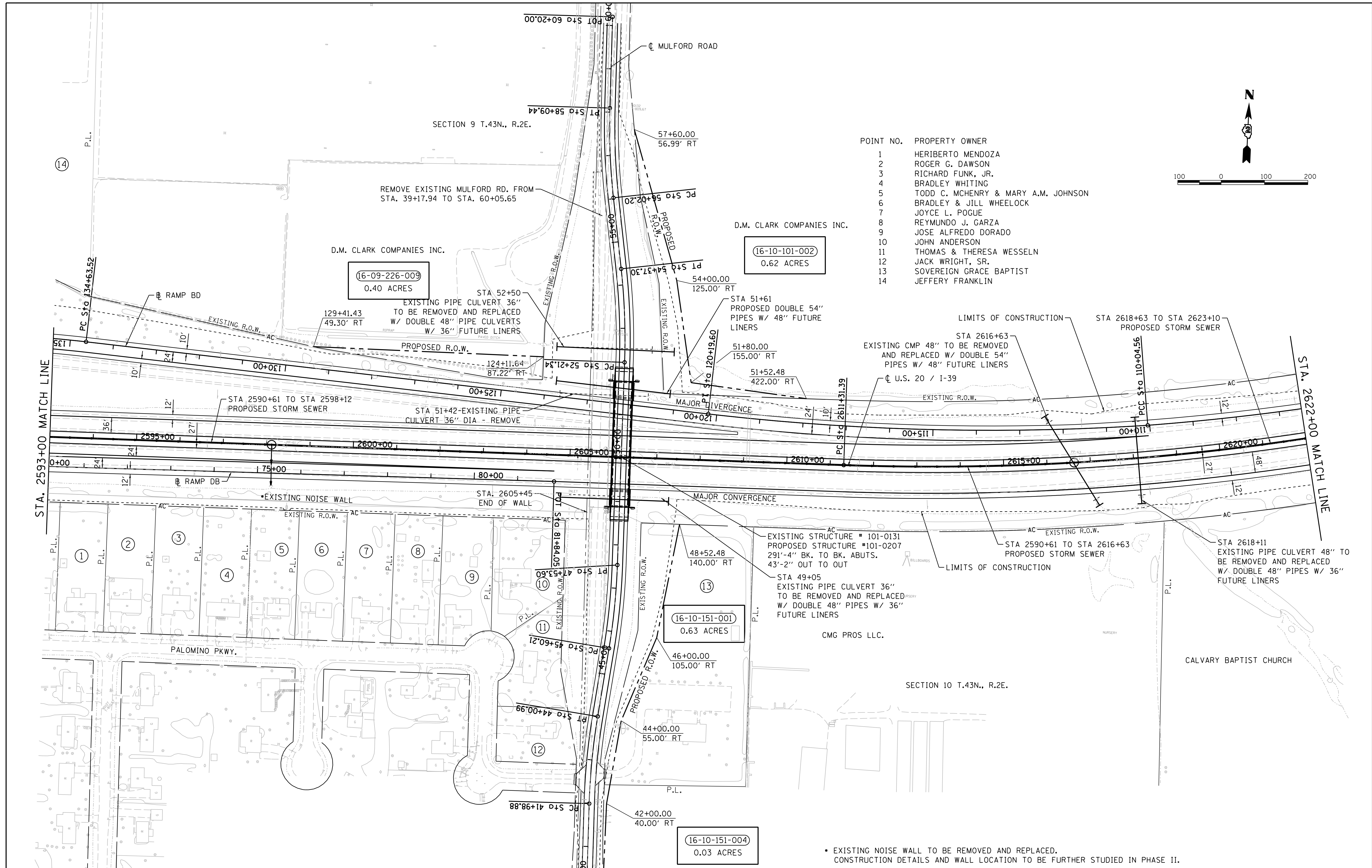
**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 U.S. 20 PLAN SHEET
 JOB NO. P-92-111-06**

SCALE: 1"=100' SHEET NO. 1 OF 7 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3IK & (4-1,5)R 06S2055	WINNEBAGO	xx	16
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



POINT NO.	PROPERTY OWNER
1	HERIBERTO MENDOZA
2	ROGER G. DAWSON
3	RICHARD FUNK, JR.
4	BRADLEY WHITING
5	TODD C. MCHENRY & MARY A.M. JOHNSON
6	BRADLEY & JILL WHEELOCK
7	JOYCE L. POGUE
8	REYMUNDO J. GARZA
9	JOSE ALFREDO DORADO
10	JOHN ANDERSON
11	THOMAS & THERESA WESSELN
12	JACK WRIGHT, SR.
13	SOVEREIGN GRACE BAPTIST
14	JEFFERY FRANKLIN



* EXISTING NOISE WALL TO BE REMOVED AND REPLACED.
 CONSTRUCTION DETAILS AND WALL LOCATION TO BE FURTHER STUDIED IN PHASE II.

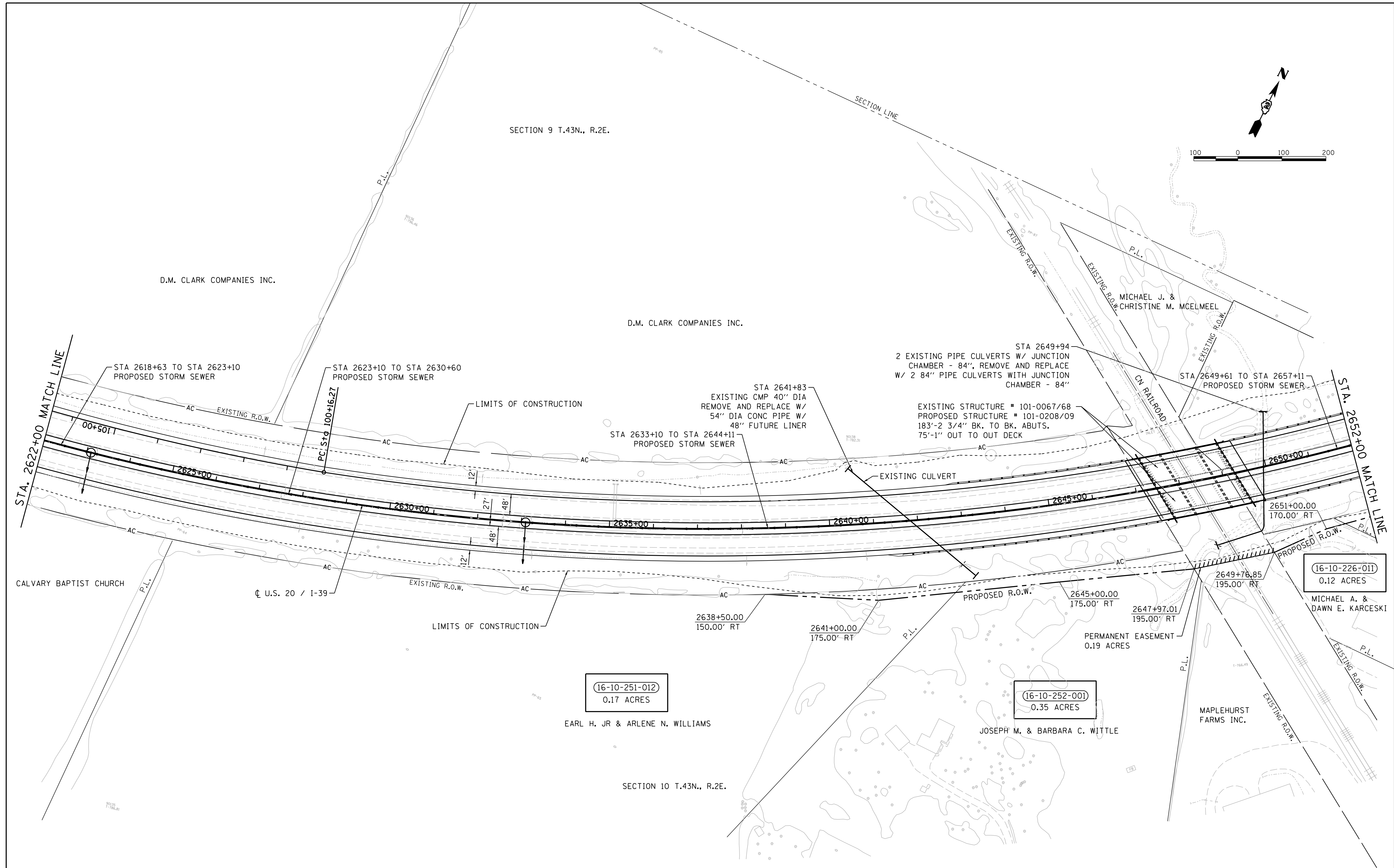
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		CHECKED - S.M.	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 U.S. 20 PLAN SHEET
 JOB NO. P-92-111-06**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3K & (4-1,5)R	WINNEBAGO	xx	17
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

SCALE: 1"=100' SHEET NO. 2 OF 7 SHEETS STA. TO STA.



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	PLOT DATE = 3/30/2017	DATE -	REVISED -

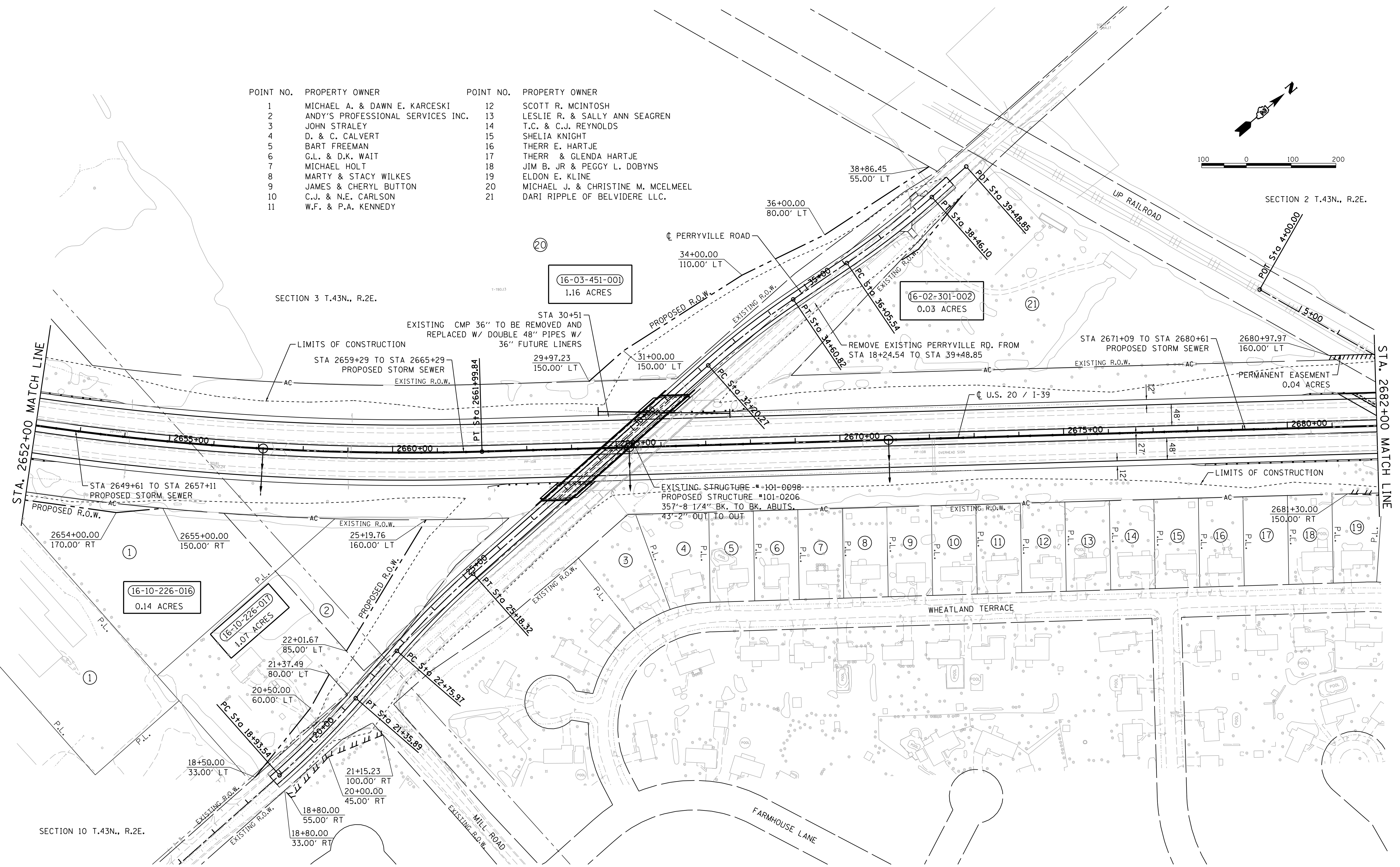
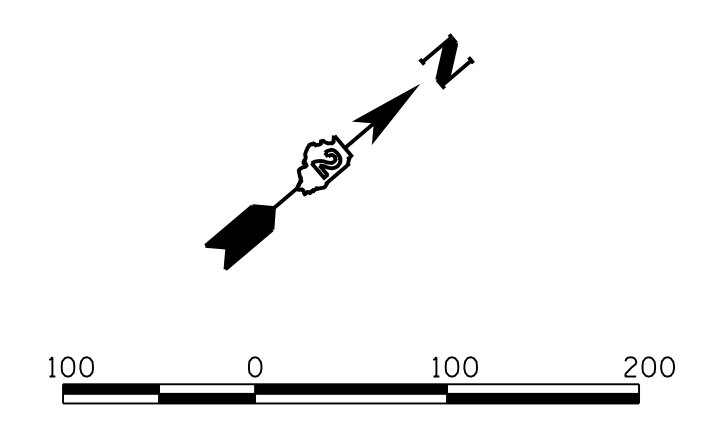
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 U.S. 20 PLAN SHEET
 JOB NO. P-92-111-06**

SCALE: 1"=100' SHEET NO. 3 OF 7 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3JK & (4-1,5)R)	WINNEBAGO	xx	18
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

POINT NO.	PROPERTY OWNER	POINT NO.	PROPERTY OWNER
1	MICHAEL A. & DAWN E. KARCESKI	12	SCOTT R. MCINTOSH
2	ANDY'S PROFESSIONAL SERVICES INC.	13	LESLIE R. & SALLY ANN SEAGREN
3	JOHN STRALEY	14	T.C. & C.J. REYNOLDS
4	D. & C. CALVERT	15	SHELIA KNIGHT
5	BART FREEMAN	16	THERR E. HARTJE
6	G.L. & D.K. WAIT	17	THERR & GLENDA HARTJE
7	MICHAEL HOLT	18	JIM B. JR & PEGGY L. DOBYNS
8	MARTY & STACY WILKES	19	ELDON E. KLINE
9	JAMES & CHERYL BUTTON	20	MICHAEL J. & CHRISTINE M. MCELMEEL
10	C.J. & N.E. CARLSON	21	DARI RIPPLE OF BELVIDERE LLC.
11	W.F. & P.A. KENNEDY		



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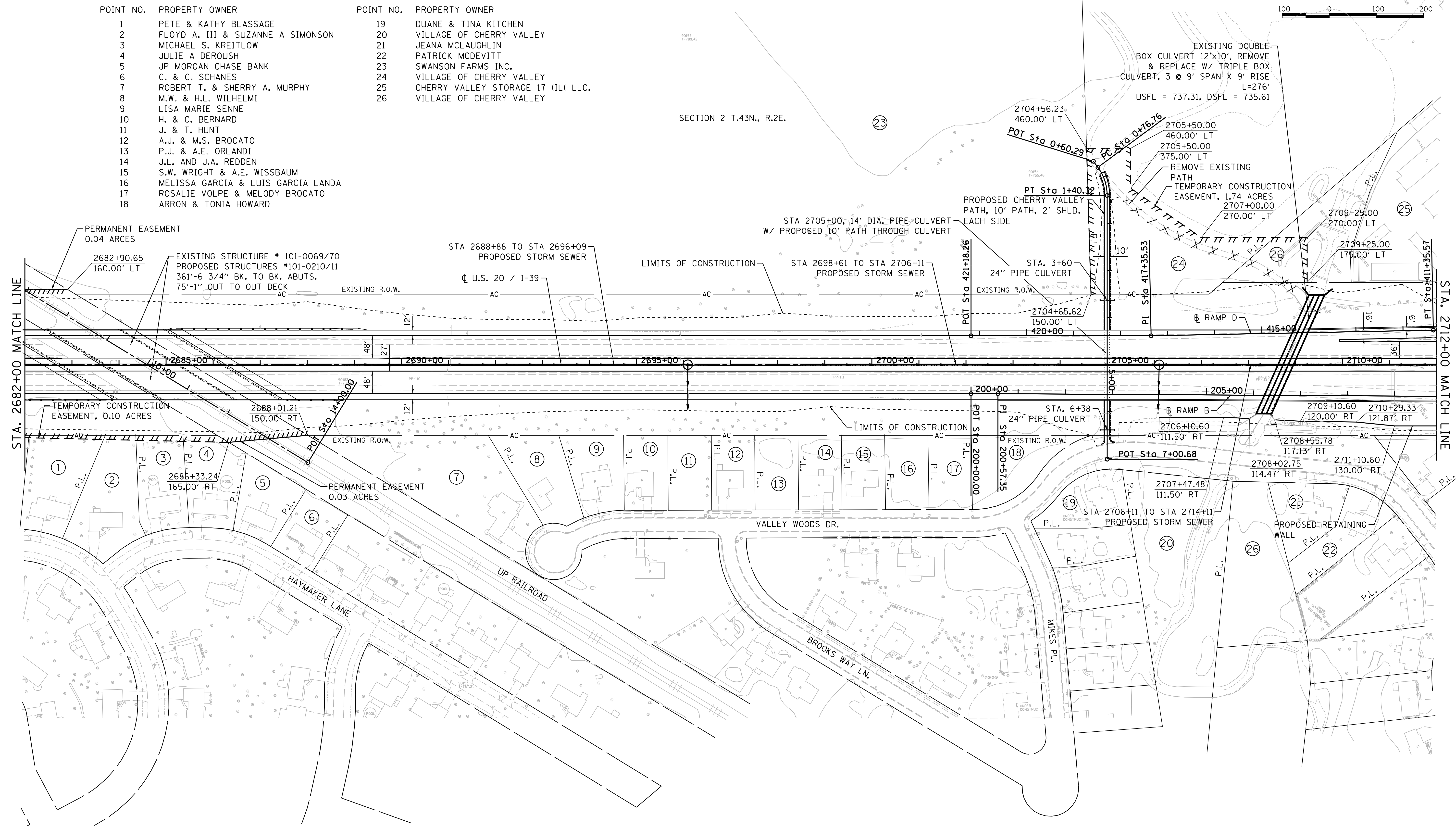
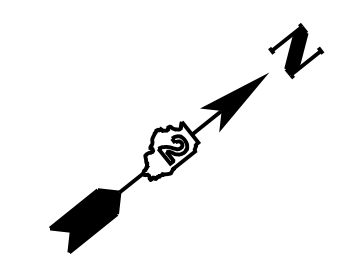
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 U.S. 20 PLAN SHEET
 JOB NO. P-92-111-06**

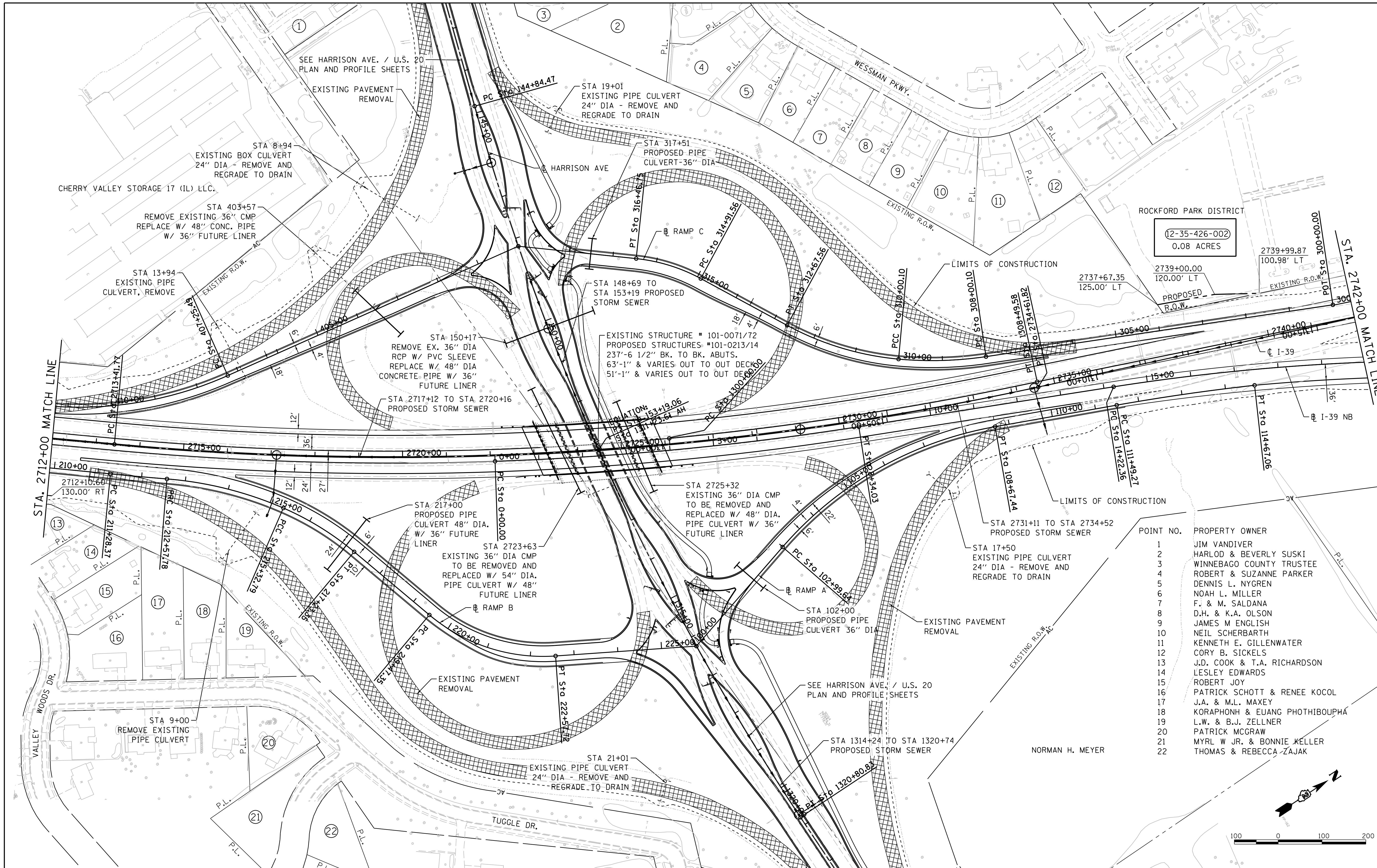
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1.5)R	WINNEBAGO	xx	19
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

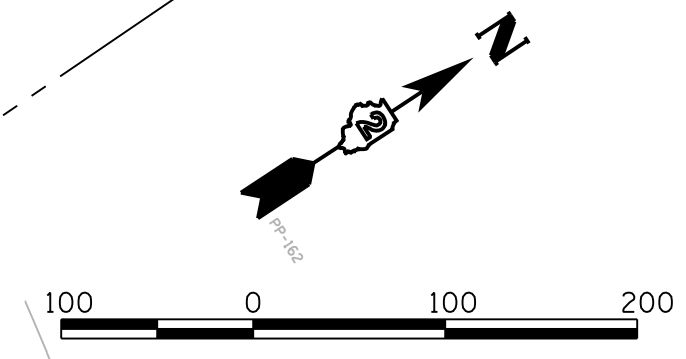
POINT NO.	PROPERTY OWNER	POINT NO.	PROPERTY OWNER
1	PETE & KATHY BLASSAGE	19	DUANE & TINA KITCHEN
2	FLOYD A. III & SUZANNE A SIMONSON	20	VILLAGE OF CHERRY VALLEY
3	MICHAEL S. KREITLOW	21	JEANA MCLAUGHLIN
4	JULIE A DEROUSH	22	PATRICK MCDEVITT
5	JP MORGAN CHASE BANK	23	SWANSON FARMS INC.
6	C. & C. SCHANES	24	VILLAGE OF CHERRY VALLEY
7	ROBERT T. & SHERRY A. MURPHY	25	CHERRY VALLEY STORAGE 17 (IL) LLC.
8	M.W. & H.L. WILHELMI	26	VILLAGE OF CHERRY VALLEY
9	LISA MARIE SENNE		
10	H. & C. BERNARD		
11	J. & T. HUNT		
12	A.J. & M.S. BROCATO		
13	P.J. & A.E. ORLANDI		
14	J.L. AND J.A. REDDEN		
15	S.W. WRIGHT & A.E. WISSBAUM		
16	MELISSA GARCIA & LUIS GARCIA LANDA		
17	ROSALIE VOLPE & MELODY BROCATO		
18	ARRON & TONIA HOWARD		



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PLOT SCALE = 100.0000' / in.	CHECKED - S.M.	REVISOR -	REVISOR -		SCALE: 1"=100'	SHEET NO. 5 OF 7 SHEETS	STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
PLOT DATE = 3/30/2017	DATE -	REVISOR -	REVISOR -									
Sheet 5												



POINT NO.	PROPERTY OWNER
1	JIM VANDIVER
2	HARLOD & BEVERLY SUSKI
3	WINNEBAGO COUNTY TRUSTEE
4	ROBERT & SUZANNE PARKER
5	DENNIS L. NYGREN
6	NOAH L. MILLER
7	F. & M. SALDANA
8	D.H. & K.A. OLSON
9	JAMES M ENGLISH
10	NEIL SCHERBARTH
11	KENNETH E. GILLENWATER
12	CORY B. SICKELS
13	J.D. COOK & T.A. RICHARDSON
14	LESLEY EDWARDS
15	ROBERT JOY
16	PATRICK SCHOTT & RENEE KOCOL
17	J.A. & M.L. MAXEY
18	KORAPHONH & EUANG PHOTHIBOUPHA
19	L.W. & B.J. ZELLNER
20	PATRICK MCGRAW
21	MYRL W JR. & BONNIE KELLER
22	THOMAS & REBECCA ZAJAK



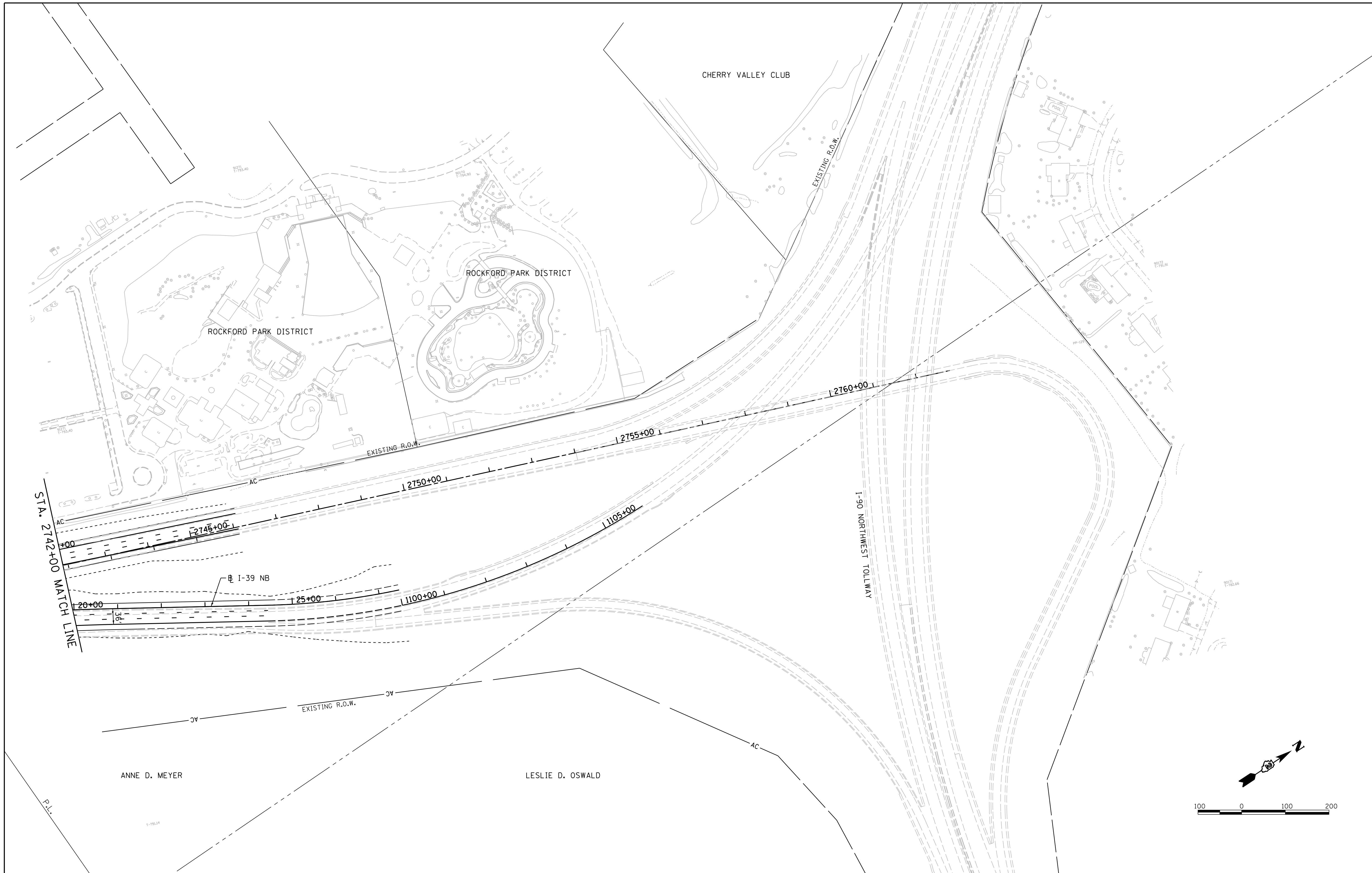
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		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 U.S. 20 PLAN SHEET
 JOB NO. P-92-111-06**

SCALE: 1"=100' SHEET NO. 6 OF 7 SHEETS STA. TO STA.

F.A.I. RTE. 39	SECTION (201-31K & (4-1,5)R 06S2055	COUNTY WINNEBAGO	TOTAL SHEETS xx	SHEET NO. 21
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



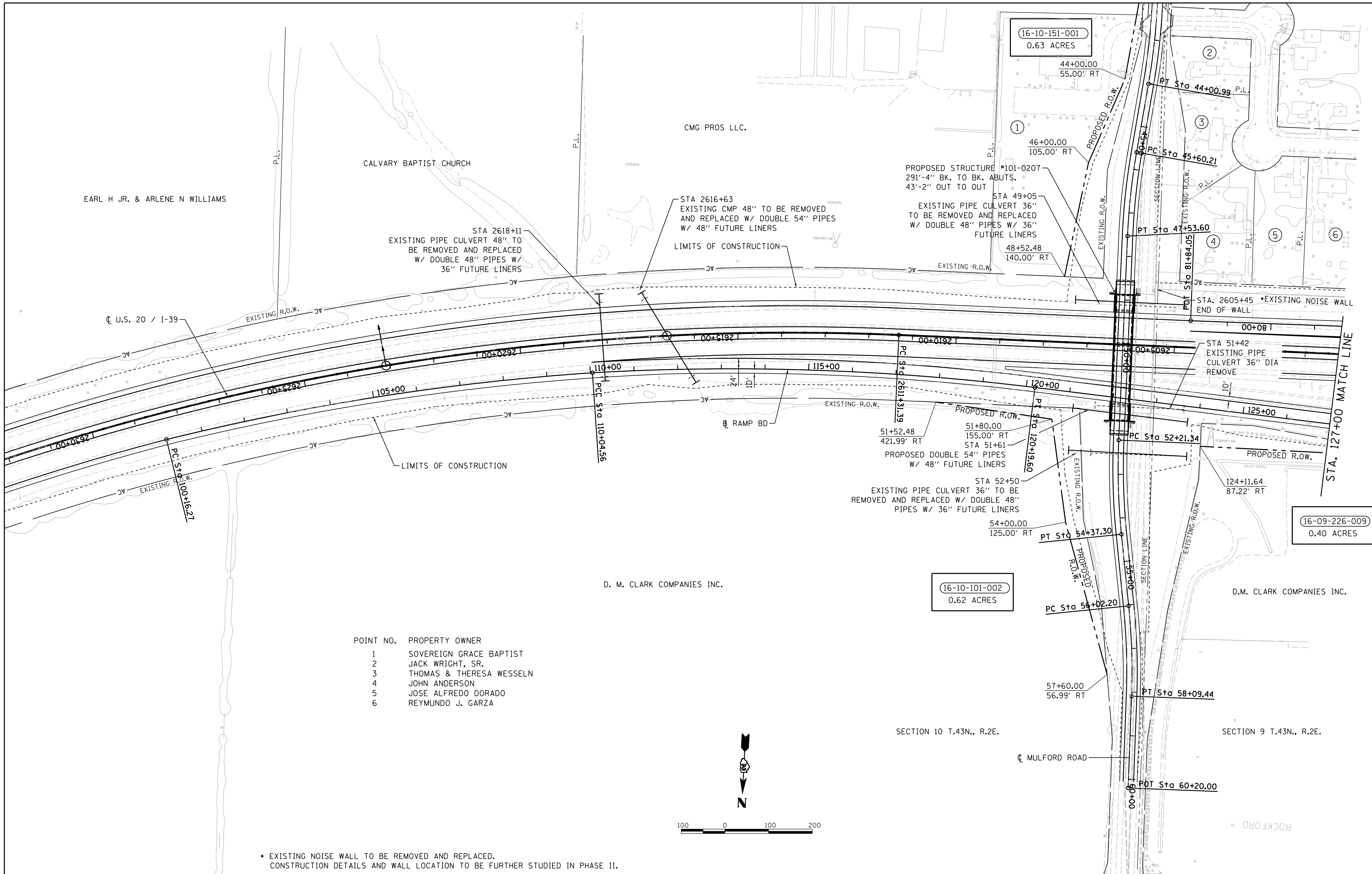
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		CHECKED - S.M.	REVISED -
		DATE -	REVISED -
Sheet 7	PLOT SCALE = 100.0000' / in.		
	PLOT DATE = 3/30/2017		

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

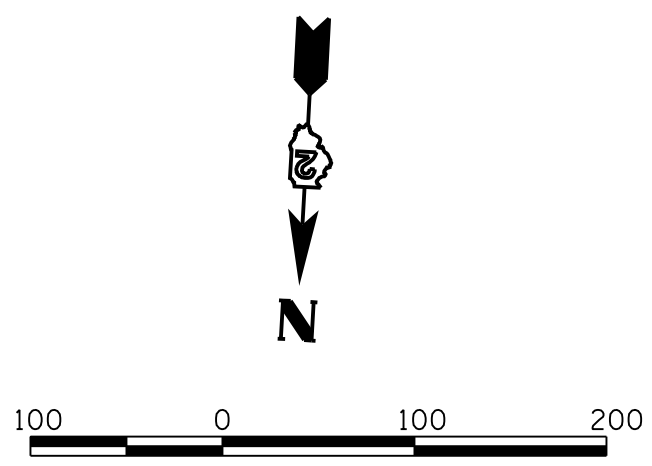
**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 U.S. 20 PLAN SHEET
 JOB NO. P-92-111-06**

SCALE: 1"=100' SHEET NO. 7 OF 7 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1,5)R	WINNEBAGO	xx	22
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



POINT NO.	PROPERTY OWNER
1	SOVEREIGN GRACE BAPTIST
2	JACK WRIGHT, SR.
3	THOMAS & THERESA WESSELN
4	JOHN ANDERSON
5	JOSE ALFREDO DORADO
6	REYMUNDO J. GARZA



* EXISTING NOISE WALL TO BE REMOVED AND REPLACED. CONSTRUCTION DETAILS AND WALL LOCATION TO BE FURTHER STUDIED IN PHASE II.

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		DATE -	REVISED -

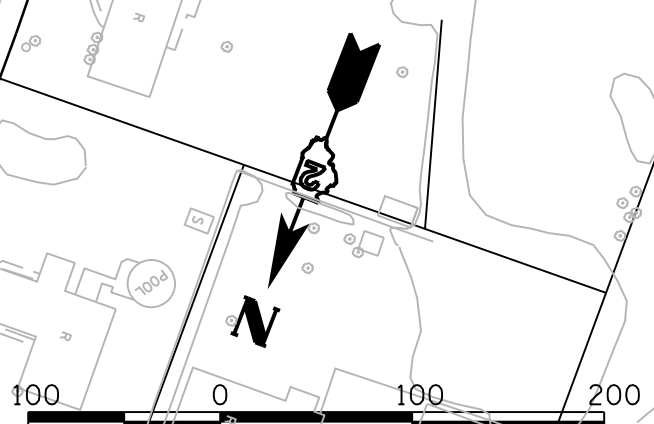
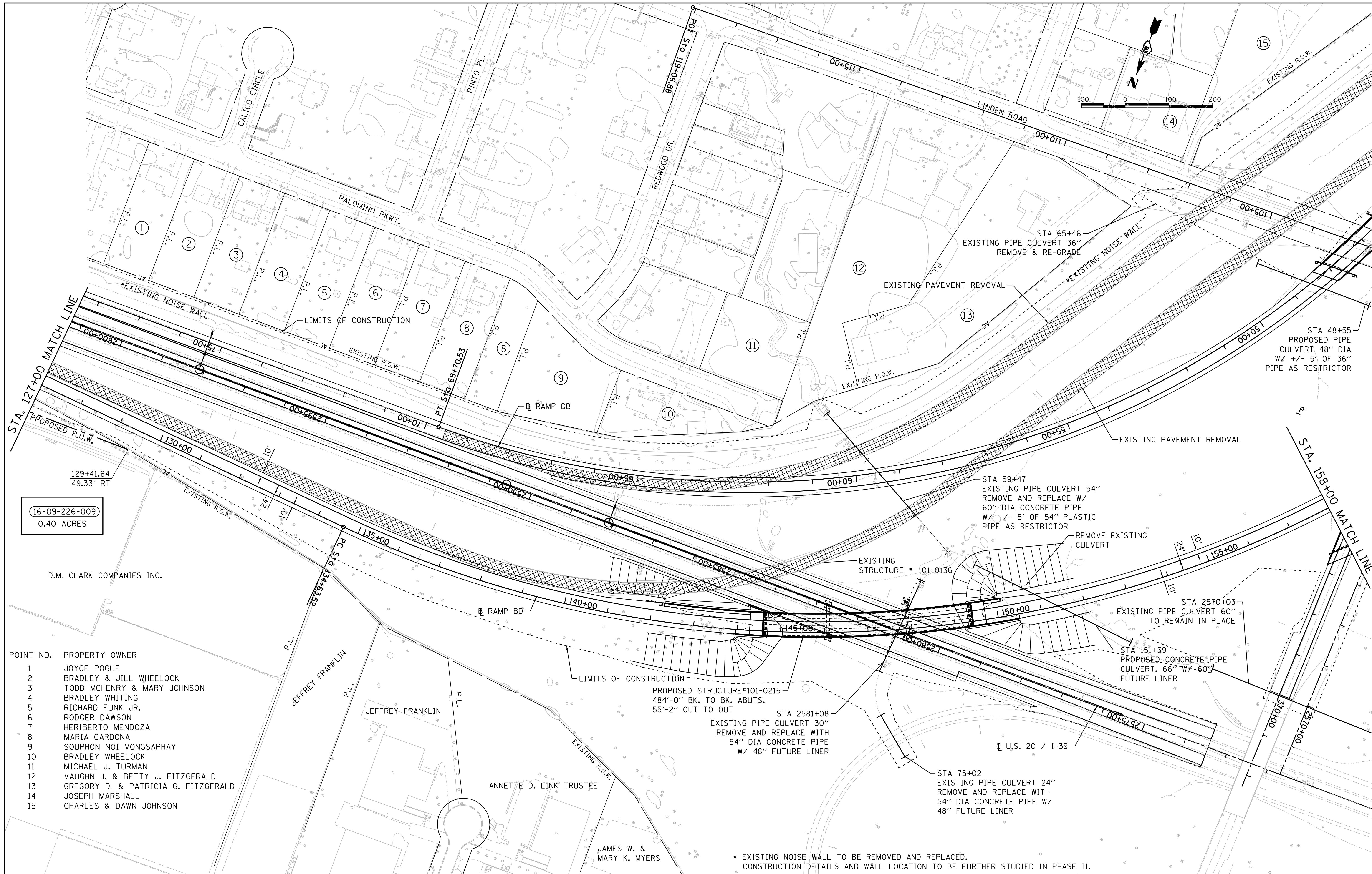
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
RAMP BD PLAN SHEET
JOB NO. P-92-111-06**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3K & (4-1,5)R	WINNEBAGO	xx	23
06S2055		CONTRACT NO.		

SCALE: 1"=100' SHEET NO. 1 OF 4 SHEETS STA. TO STA.

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



POINT NO.	PROPERTY OWNER
1	JOYCE POGUE
2	BRADLEY & JILL WHEELOCK
3	TODD MCHENRY & MARY JOHNSON
4	BRADLEY WHITING
5	RICHARD FUNK JR.
6	RODGER DAWSON
7	HERIBERTO MENDOZA
8	MARIA CARDONA
9	SOUPHON NOI VONGSAPHAY
10	BRADLEY WHEELOCK
11	MICHAEL J. TURMAN
12	VAUGHN J. & BETTY J. FITZGERALD
13	GREGORY D. & PATRICIA G. FITZGERALD
14	JOSEPH MARSHALL
15	CHARLES & DAWN JOHNSON

129+41.64
49.33' RT

16-09-226-009
0.40 ACRES

* EXISTING NOISE WALL TO BE REMOVED AND REPLACED.
CONSTRUCTION DETAILS AND WALL LOCATION TO BE FURTHER STUDIED IN PHASE II.

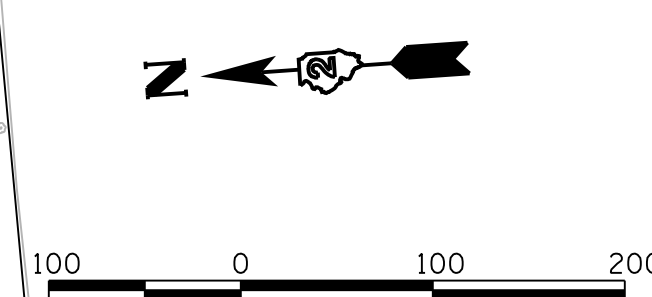
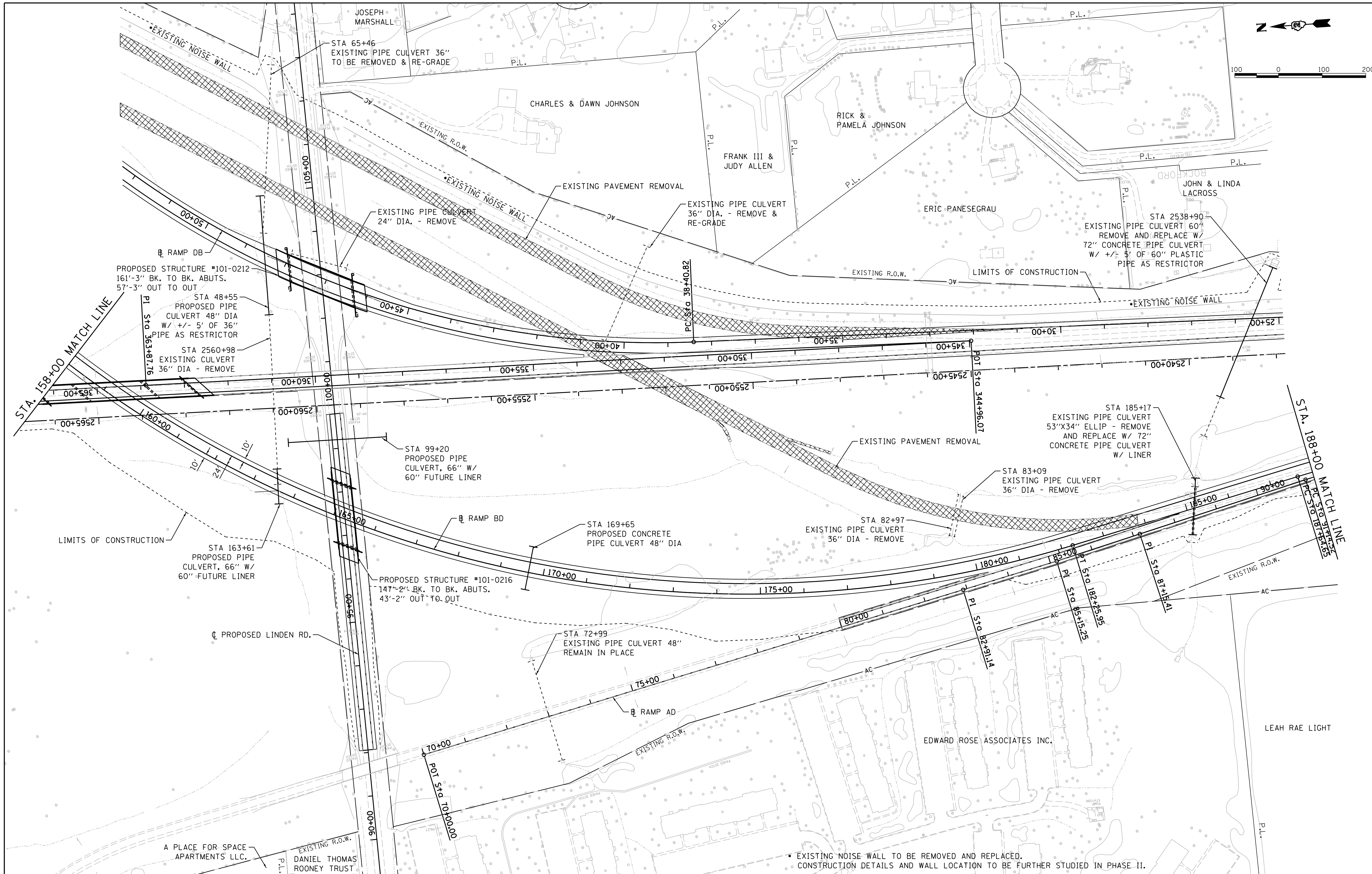
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		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
RAMP BD PLAN SHEET
JOB NO. P-92-111-06**

SCALE: 1"=100' SHEET NO. 2 OF 4 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1,5)R 06S2055	WINNEBAGO	xx	24
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



EXISTING NOISE WALL TO BE REMOVED AND REPLACED.
 CONSTRUCTION DETAILS AND WALL LOCATION TO BE FURTHER STUDIED IN PHASE II.

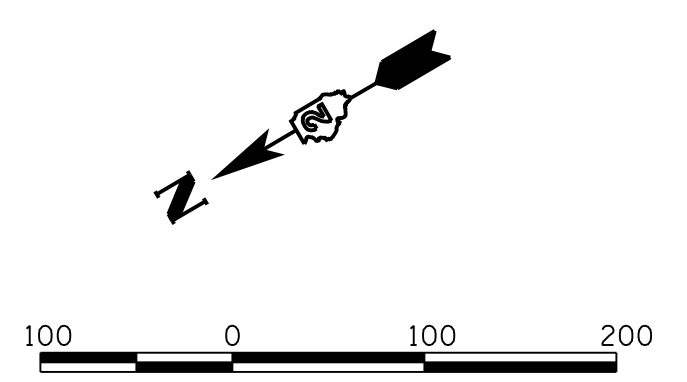
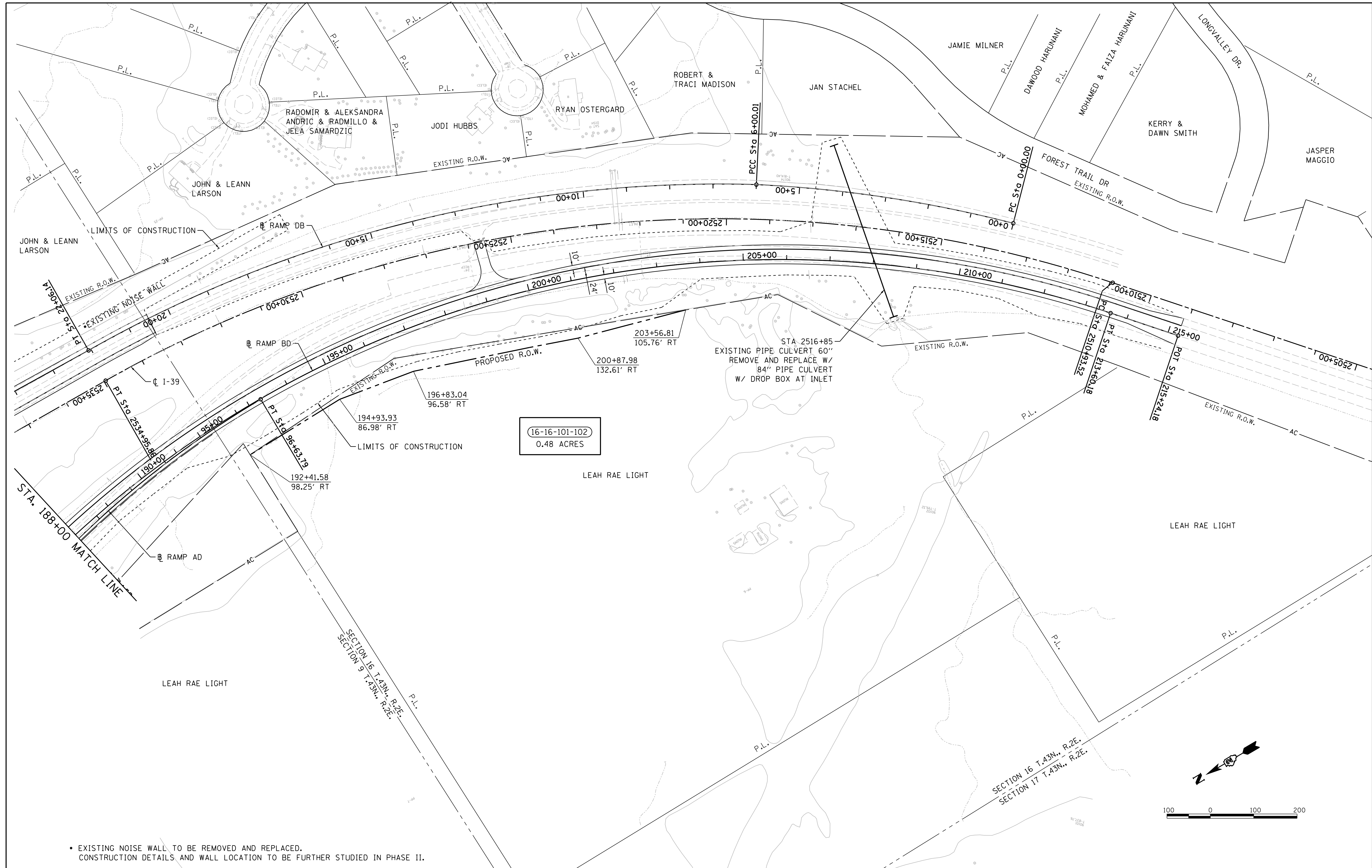
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		CHECKED - S.M.	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 RAMP BD PLAN SHEET
 JOB NO. P-92-111-06**

F.A.I. RTE. 39	SECTION (201-3K & (4-1,5)R)	COUNTY WINNEBAGO	TOTAL SHEETS xx	SHEET NO. 25
PLOT SCALE = 100.0000' / in.		CONTRACT NO.		
PLOT DATE = 3/30/2017		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		

SCALE: 1"=100' SHEET NO. 3 OF 4 SHEETS STA. TO STA.



• EXISTING NOISE WALL TO BE REMOVED AND REPLACED.
 CONSTRUCTION DETAILS AND WALL LOCATION TO BE FURTHER STUDIED IN PHASE II.

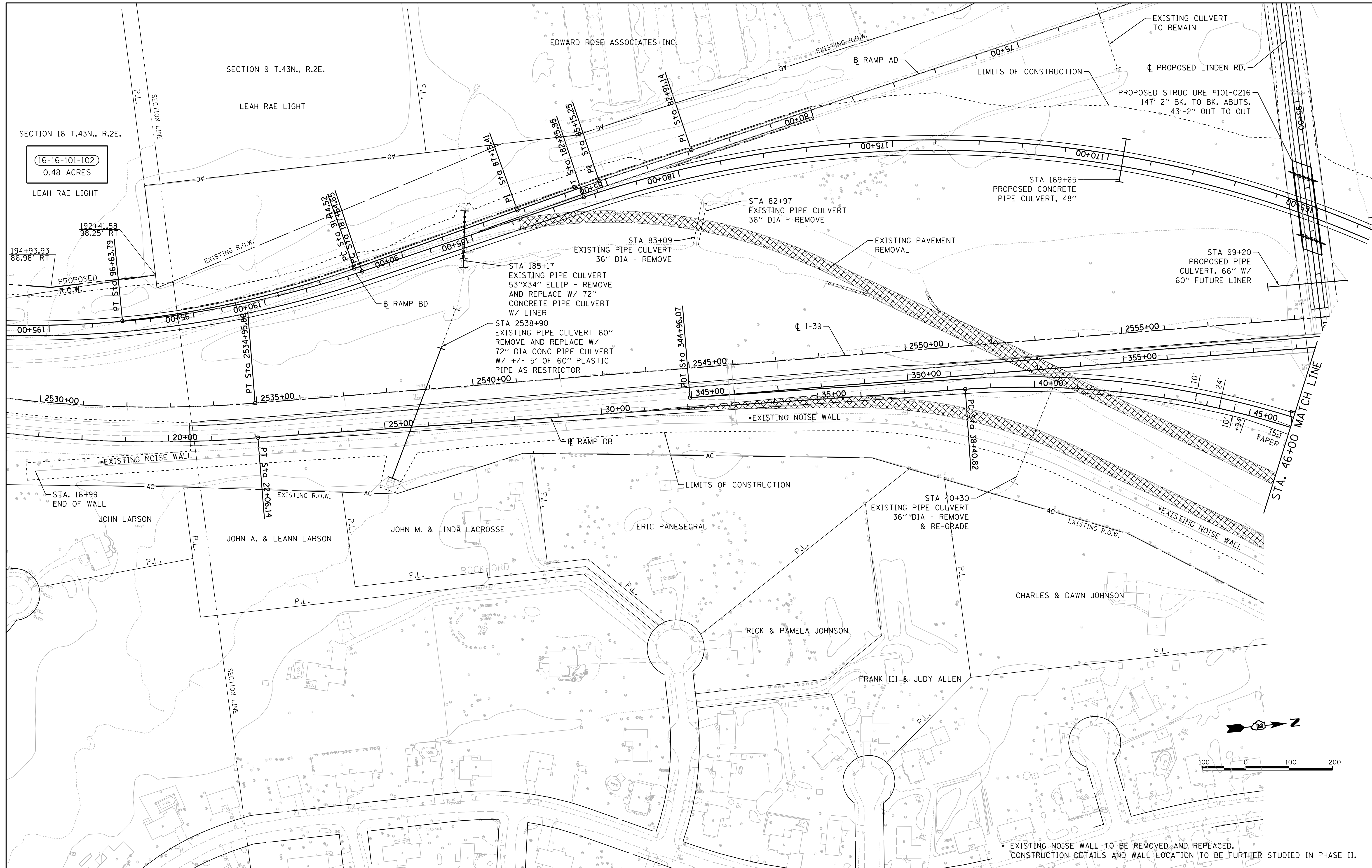
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		CHECKED - S.M.	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 RAMP BD PLAN SHEET
 JOB NO. P-92-111-06**

SCALE: 1"=100' SHEET NO. 4 OF 4 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3K & (4-1,5)R)	WINNEBAGO	xx	26
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



EXISTING NOISE WALL TO BE REMOVED AND REPLACED.
 CONSTRUCTION DETAILS AND WALL LOCATION TO BE FURTHER STUDIED IN PHASE II.

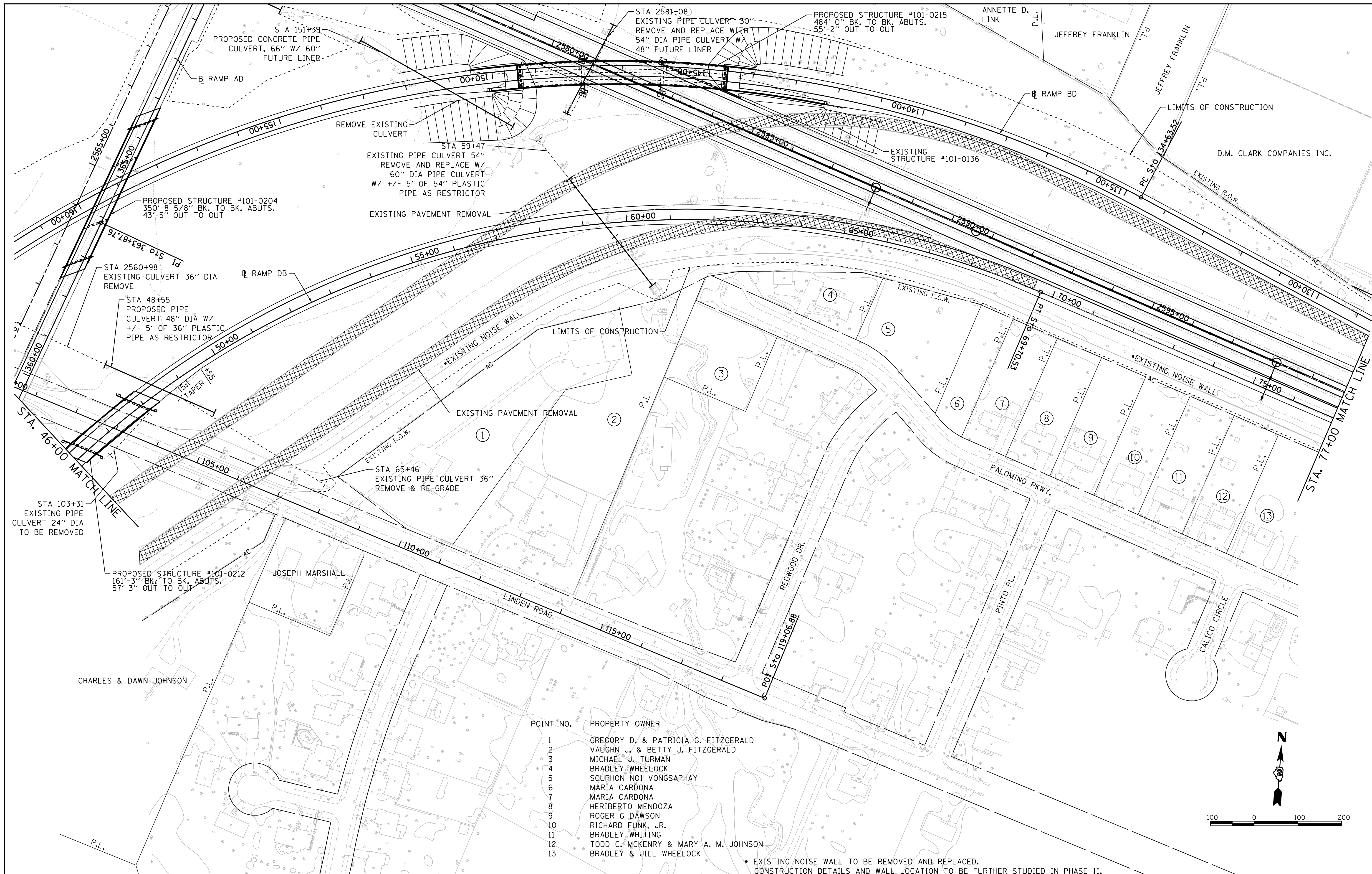
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UNTITLED SHEET		CHECKED - S.M.	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 RAMP DB PLAN SHEET
 JOB NO. P-92-111-06**

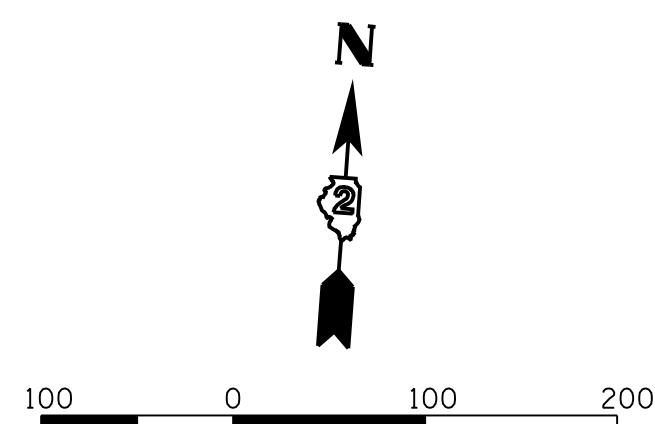
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1,5)R 06S2055	WINNEBAGO	xx	27
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	CONTRACT NO.	



POINT NO.	PROPERTY OWNER
1	GREGORY D. & PATRICIA G. FITZGERALD
2	VAUGHN J. & BETTY J. FITZGERALD
3	MICHAEL J. TURMAN
4	BRADLEY WHELOCK
5	SOUPHON NOI VONGSAPHAY
6	MARIA CARDONA
7	MARIA CARDONA
8	HERIBERTO MENDOZA
9	ROGER G DAWSON
10	RICHARD FUNK, JR.
11	BRADLEY WHITING
12	TODD C. MCKENRY & MARY A. M. JOHNSON
13	BRADLEY & JILL WHELOCK

* EXISTING NOISE WALL TO BE REMOVED AND REPLACED.
 CONSTRUCTION DETAILS AND WALL LOCATION TO BE FURTHER STUDIED IN PHASE II.



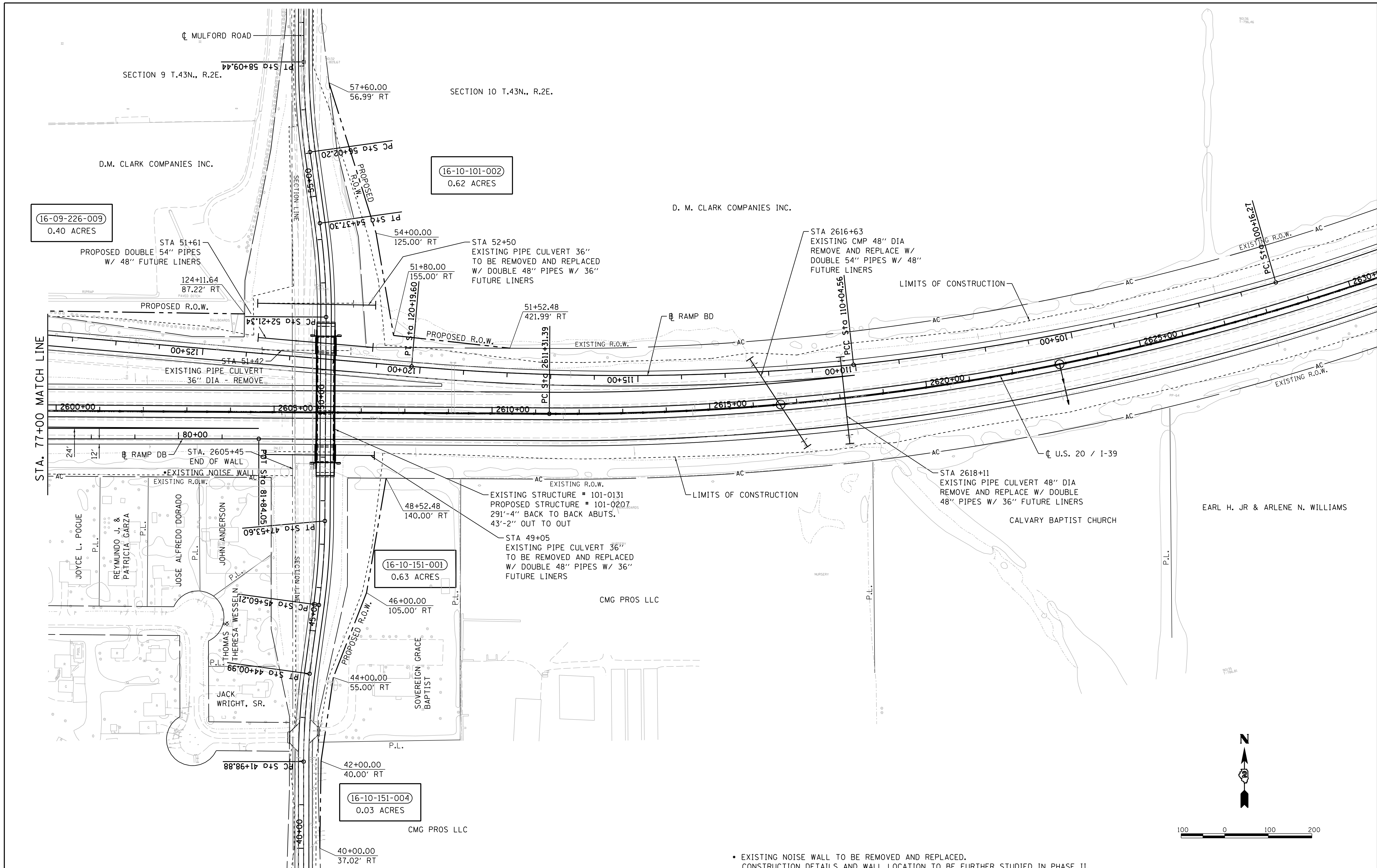
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		DATE =	REVISED =

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

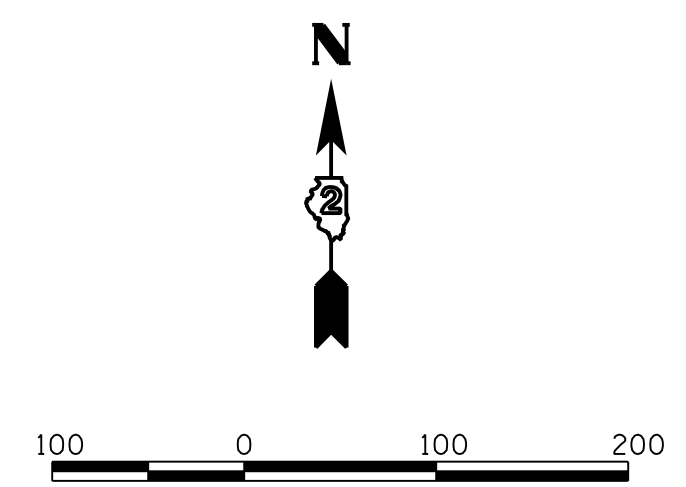
**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 RAMP DB PLAN SHEET
 JOB NO. P-92-111-06**

SCALE: 1"=100' SHEET NO. 2 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3K & (4-1,5)R	WINNEBAGO	xx	28
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



• EXISTING NOISE WALL TO BE REMOVED AND REPLACED.
 CONSTRUCTION DETAILS AND WALL LOCATION TO BE FURTHER STUDIED IN PHASE II.

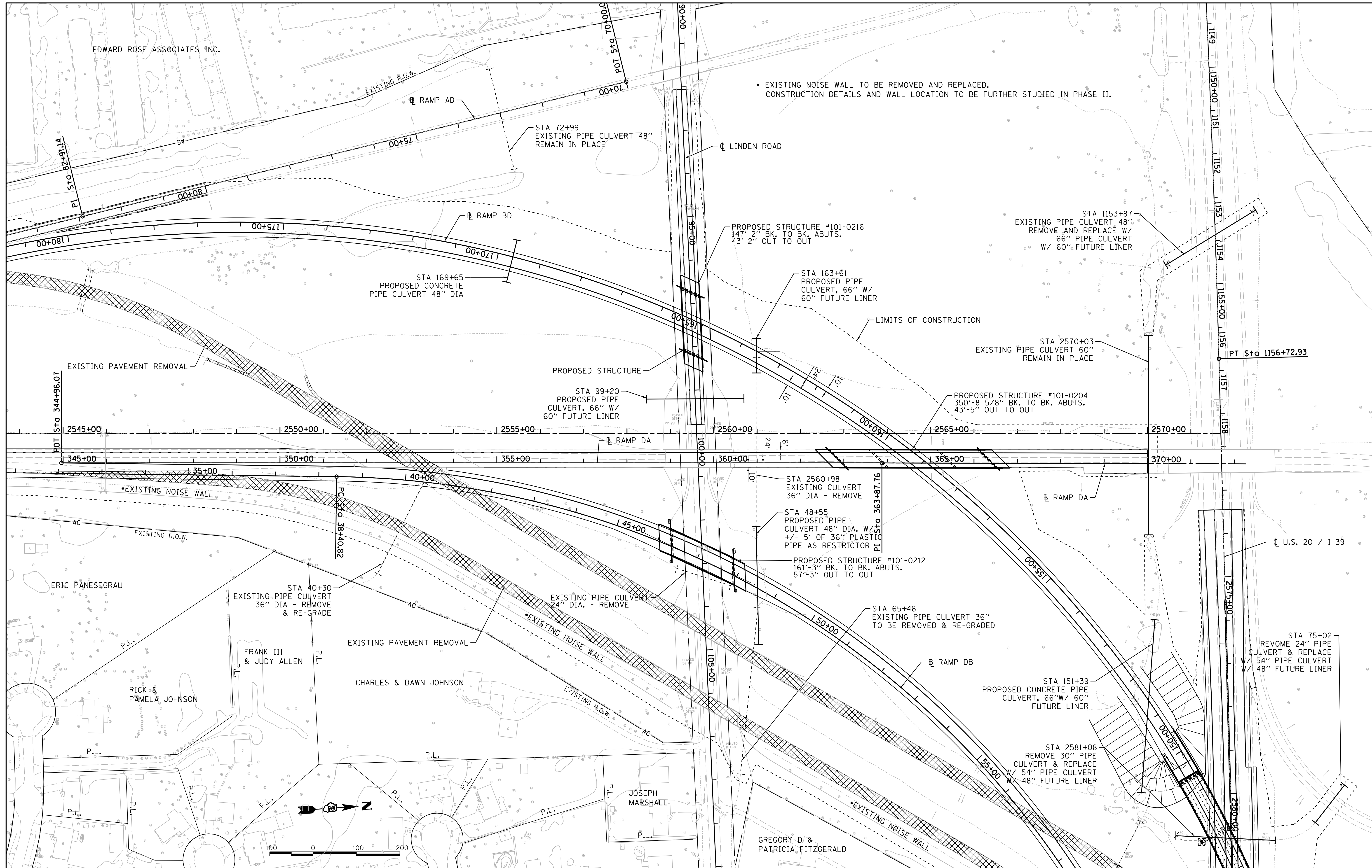


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	PLOT DATE = 3/30/2017	CHECKED - S.M.	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)	
RAMP DB PLAN SHEET	
JOB NO. P-92-111-06	
SCALE: 1"=100'	SHEET NO. 3 OF 3 SHEETS
STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1,5)R	WINNEBAGO	xx	29
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



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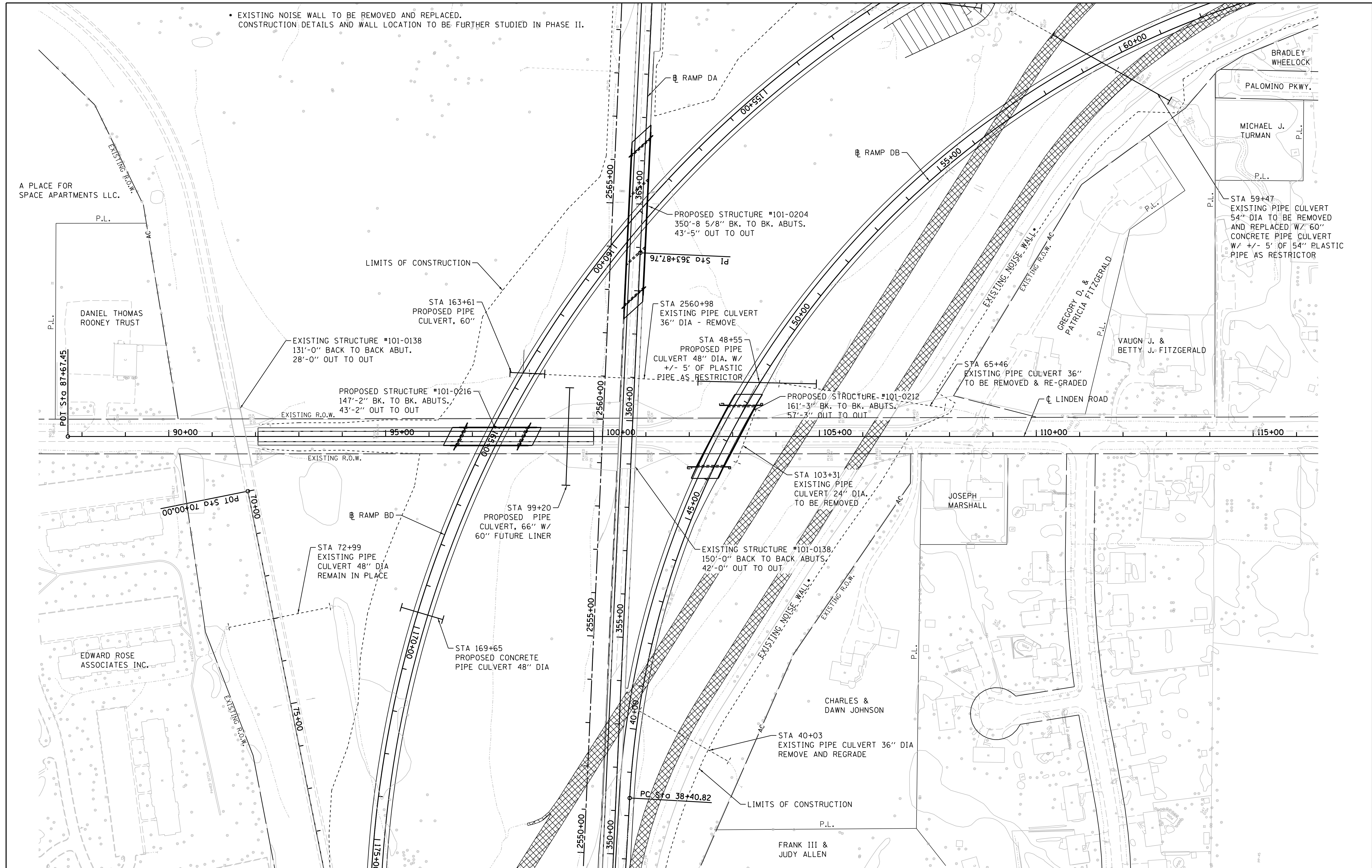
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 RAMP DA PLAN SHEET
 JOB NO. P-92-111-06**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1.5)R	WINNEBAGO	xx	30
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT	CONTRACT NO.	
06S2055				

SCALE: 1"=100' SHEET NO. OF SHEETS STA. TO STA.

• EXISTING NOISE WALL TO BE REMOVED AND REPLACED.
CONSTRUCTION DETAILS AND WALL LOCATION TO BE FURTHER STUDIED IN PHASE II.



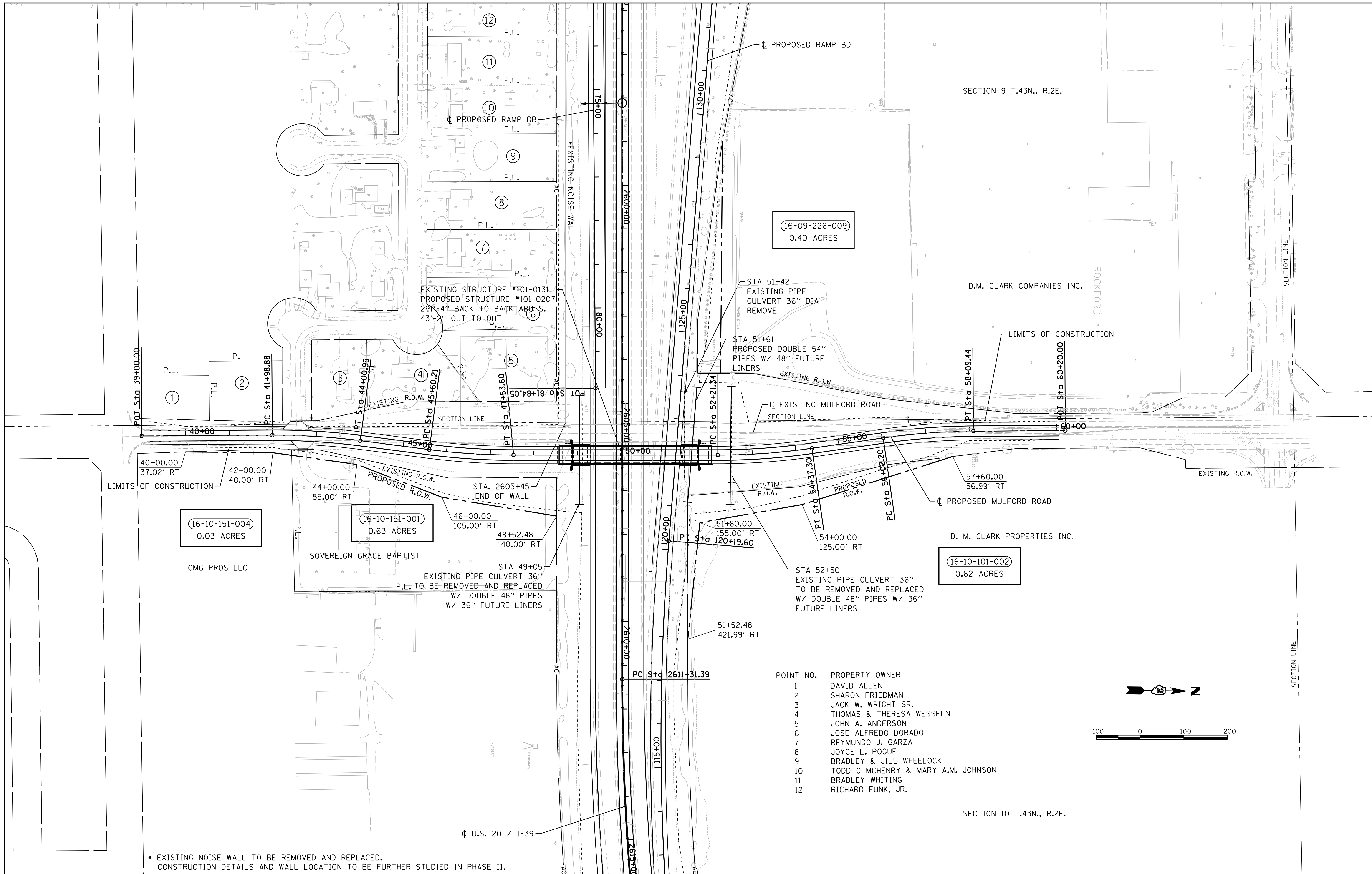
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UNTITLED SHEET		CHECKED - S.M.	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
LINDEN ROAD PLAN SHEET
JOB NO. P-92-111-06**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1,5)R 06S2055	WINNEBAGO	xx	31
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

SCALE: 1"=100' SHEET NO. 1 OF 1 SHEETS STA. TO STA.



• EXISTING NOISE WALL TO BE REMOVED AND REPLACED.
CONSTRUCTION DETAILS AND WALL LOCATION TO BE FURTHER STUDIED IN PHASE II.

POINT NO.	PROPERTY OWNER
1	DAVID ALLEN
2	SHARON FRIEDMAN
3	JACK W. WRIGHT SR.
4	THOMAS & THERESA WESSELN
5	JOHN A. ANDERSON
6	JOSE ALFREDO DORADO
7	REYMUNDO J. GARZA
8	JOYCE L. POGUE
9	BRADLEY & JILL WHEELOCK
10	TODD C MCHENRY & MARY A.M. JOHNSON
11	BRADLEY WHITING
12	RICHARD FUNK, JR.

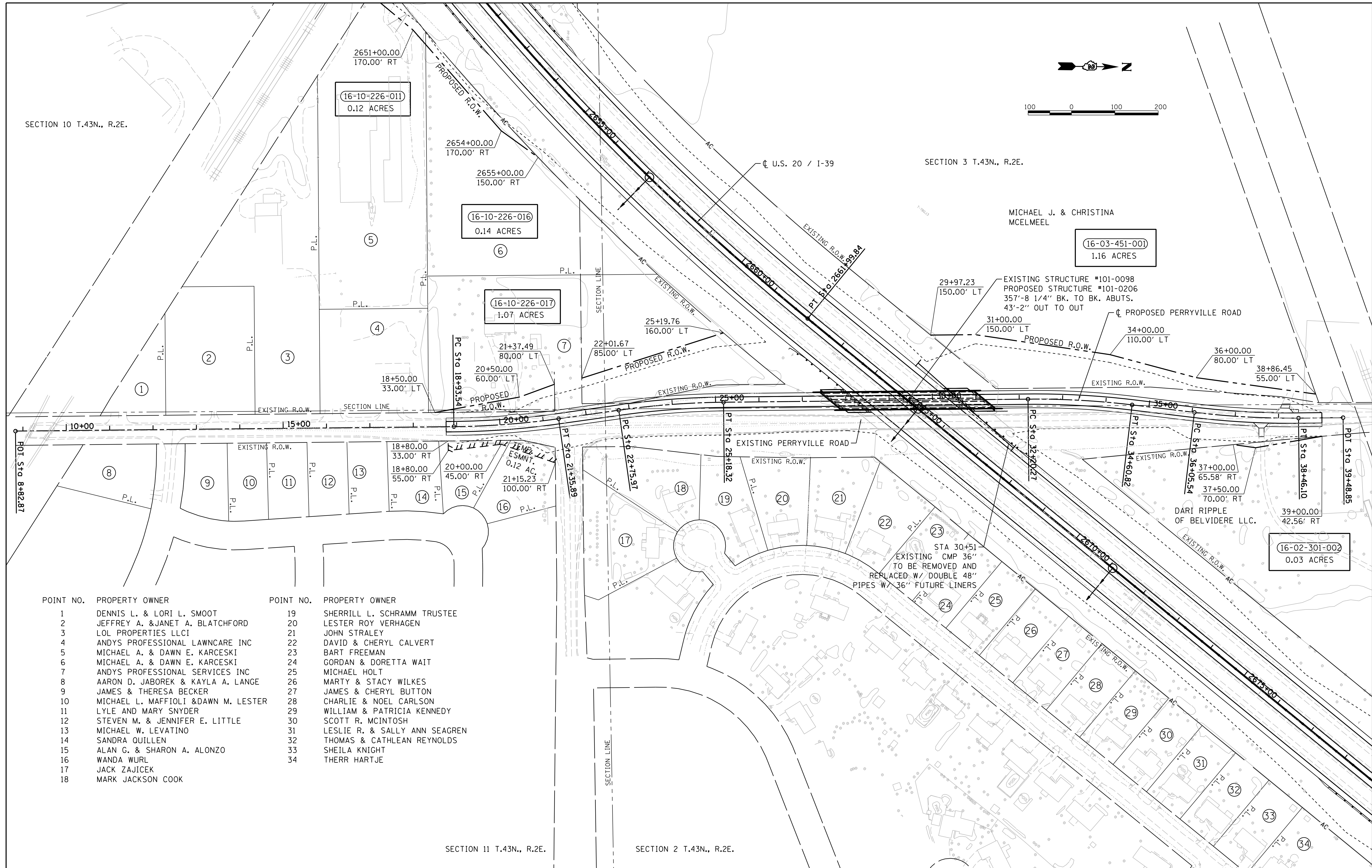
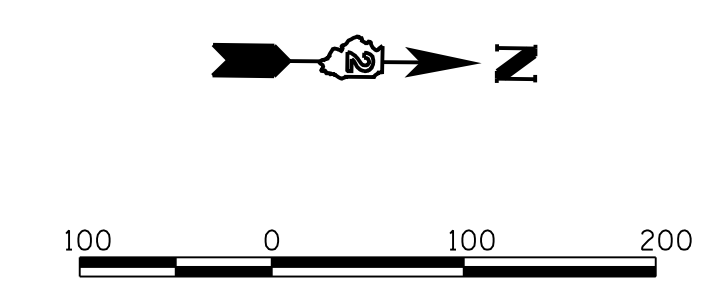
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UNTITLED SHEET		CHECKED - S.M.	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
MULFORD ROAD PLAN SHEET
JOB NO. P-92-111-06**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1,5)R	WINNEBAGO	xx	32
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

SCALE: 1"=100' SHEET NO. 1 OF 1 SHEETS STA. TO STA.



POINT NO.	PROPERTY OWNER	POINT NO.	PROPERTY OWNER
1	DENNIS L. & LORI L. SMOOT	19	SHERRILL L. SCHRAMM TRUSTEE
2	JEFFREY A. & JANET A. BLATCHFORD	20	LESTER ROY VERHAGEN
3	LOL PROPERTIES LLCI	21	JOHN STRALEY
4	ANDYS PROFESSIONAL LAWNCARE INC	22	DAVID & CHERYL CALVERT
5	MICHAEL A. & DAWN E. KARCESKI	23	BART FREEMAN
6	MICHAEL A. & DAWN E. KARCESKI	24	GORDAN & DORETTA WAIT
7	ANDYS PROFESSIONAL SERVICES INC	25	MICHAEL HOLT
8	AARON D. JABOREK & KAYLA A. LANGE	26	MARTY & STACY WILKES
9	JAMES & THERESA BECKER	27	JAMES & CHERYL BUTTON
10	MICHAEL L. MAFFIOLI & DAWN M. LESTER	28	CHARLIE & NOEL CARLSON
11	LYLE AND MARY SNYDER	29	WILLIAM & PATRICIA KENNEDY
12	STEVEN M. & JENNIFER E. LITTLE	30	SCOTT R. MCINTOSH
13	MICHAEL W. LEVATINO	31	LESLIE R. & SALLY ANN SEAGREN
14	SANDRA QUILLEN	32	THOMAS & CATHLEAN REYNOLDS
15	ALAN G. & SHARON A. ALONZO	33	SHEILA KNIGHT
16	WANDA WURL	34	THERR HARTJE
17	JACK ZAJICEK		
18	MARK JACKSON COOK		

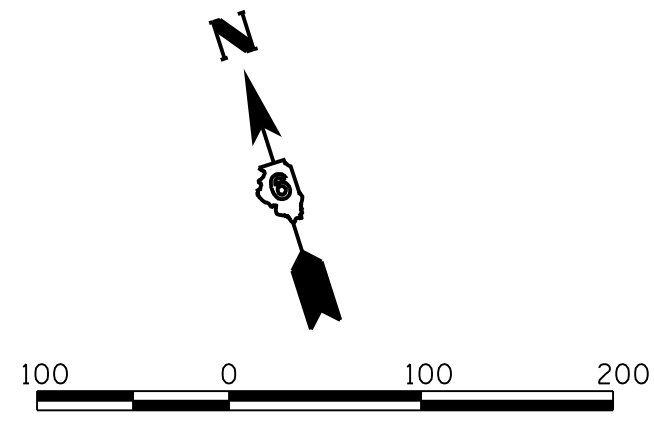
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

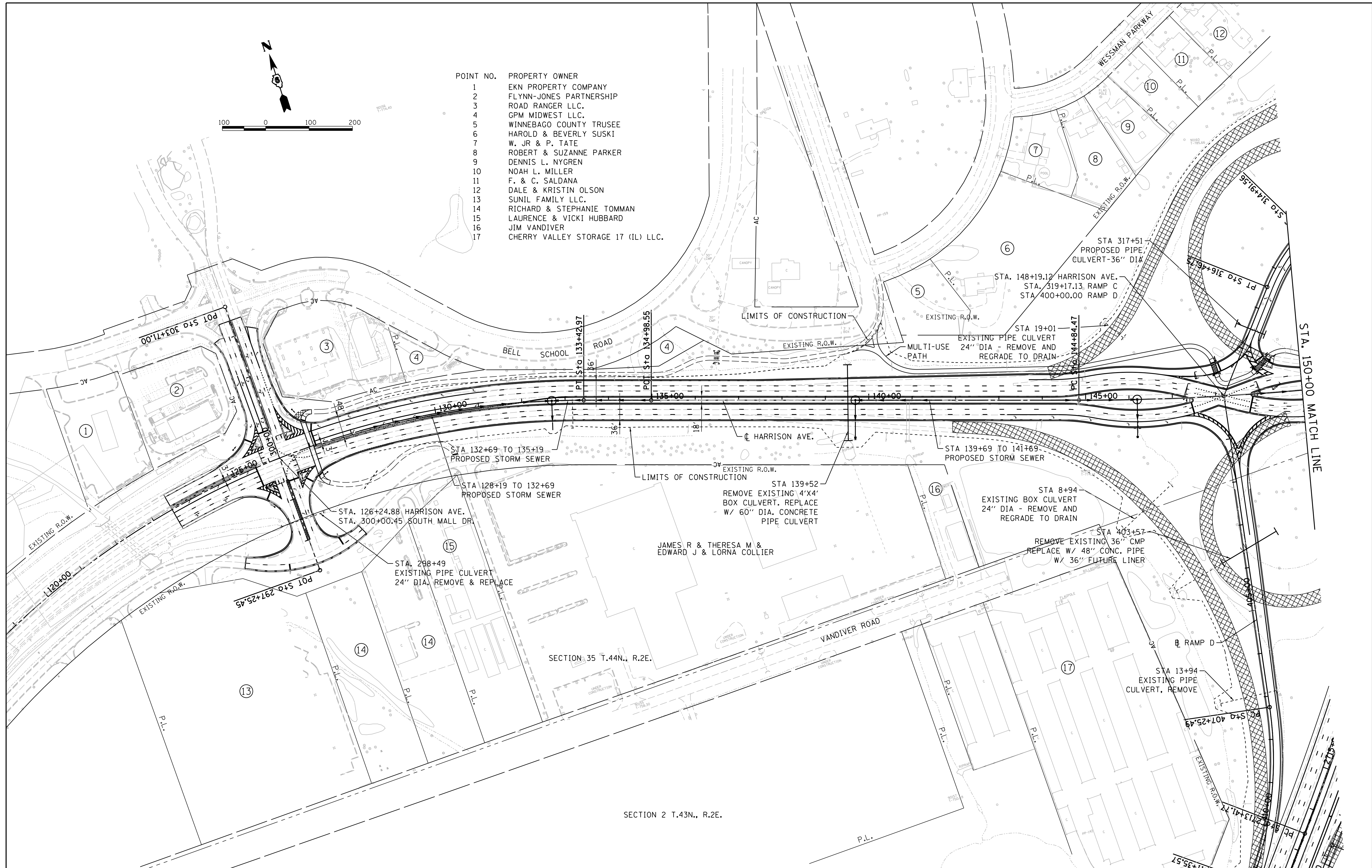
**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 PERRYVILLE ROAD PLAN SHEET
 JOB NO. P-92-111-06**

SCALE: 1"=100' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1,5)R 06S2055	WINNEBAGO	xx	33
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



POINT NO.	PROPERTY OWNER
1	EKN PROPERTY COMPANY
2	FLYNN-JONES PARTNERSHIP
3	ROAD RANGER LLC.
4	GPM MIDWEST LLC.
5	WINNEBAGO COUNTY TRUSEE
6	HAROLD & BEVERLY SUSKI
7	W. JR & P. TATE
8	ROBERT & SUZANNE PARKER
9	DENNIS L. NYGREN
10	NOAH L. MILLER
11	F. & C. SALDANA
12	DALE & KRISTIN OLSON
13	SUNIL FAMILY LLC.
14	RICHARD & STEPHANIE TOMMAN
15	LAURENCE & VICKI HUBBARD
16	JIM VANDIVER
17	CHERRY VALLEY STORAGE 17 (IL) LLC.



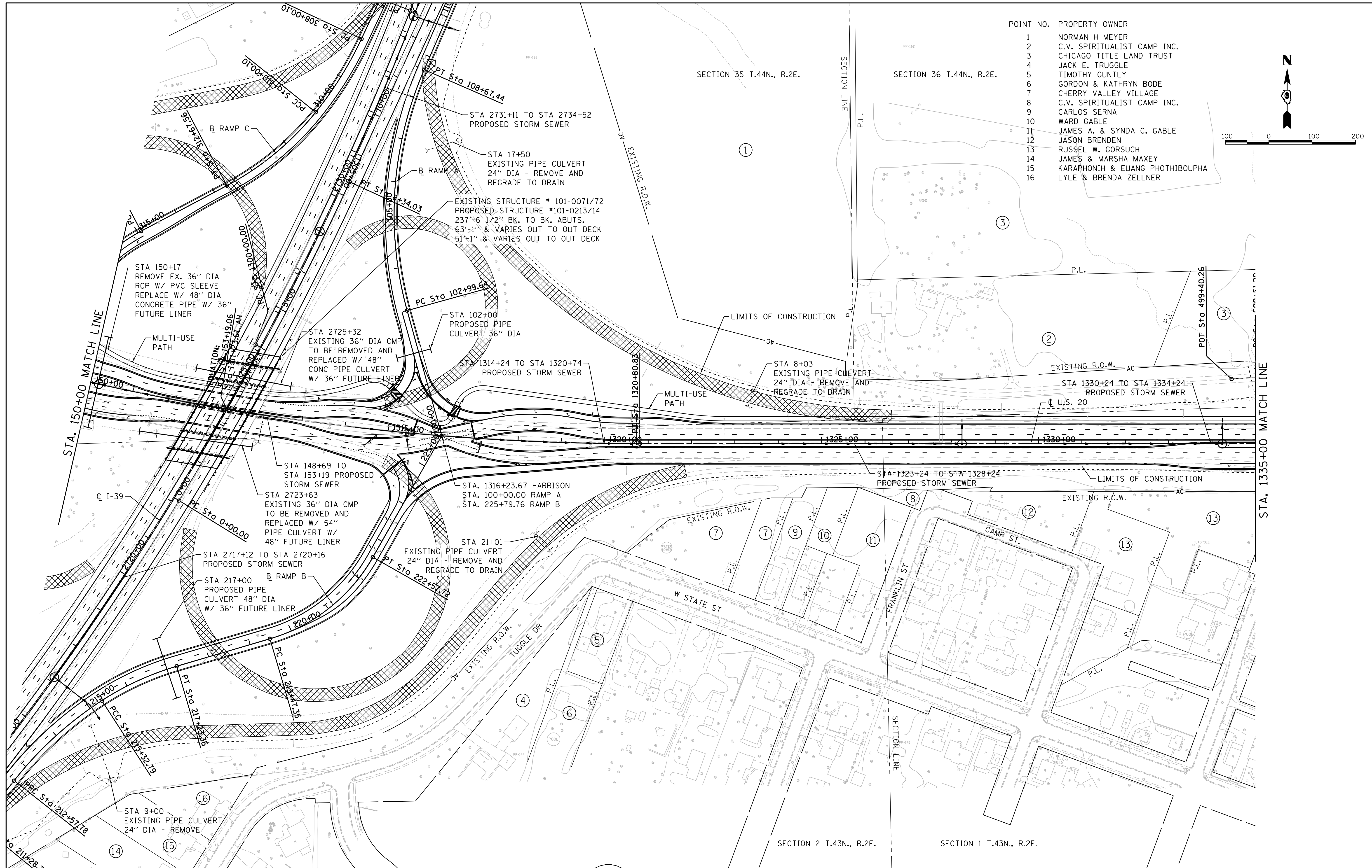
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

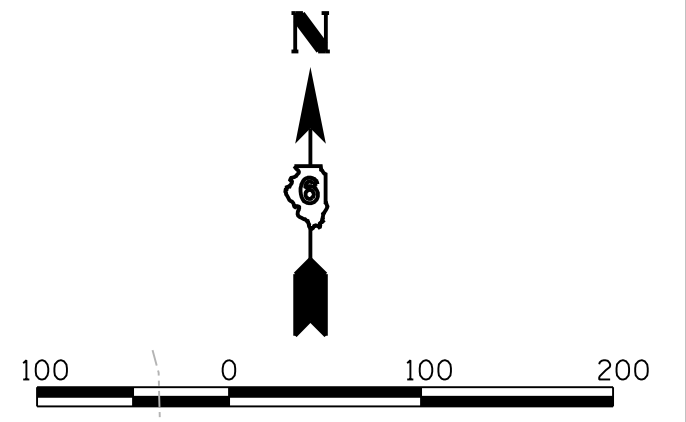
**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 HARRISON AVE PLAN SHEET
 JOB NO. P-92-111-06**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1.5)R 06S2055	WINNEBAGO	xx	34
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

SCALE: 1"=100' SHEET NO. 1 OF 3 SHEETS STA. TO STA.



POINT NO.	PROPERTY OWNER
1	NORMAN H MEYER
2	C.V. SPIRITUALIST CAMP INC.
3	CHICAGO TITLE LAND TRUST
4	JACK E. TRUGGLE
5	TIMOTHY GUNTLY
6	GORDON & KATHRYN BODE
7	CHERRY VALLEY VILLAGE
8	C.V. SPIRITUALIST CAMP INC.
9	CARLOS SERNA
10	WARD GABLE
11	JAMES A. & SYNDA C. GABLE
12	JASON BRENDEN
13	RUSSEL W. GORSUCH
14	JAMES & MARSHA MAXEY
15	KARAPHONIH & EUANG PHOTHIBOUPHA
16	LYLE & BRENDA ZELLNER



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		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

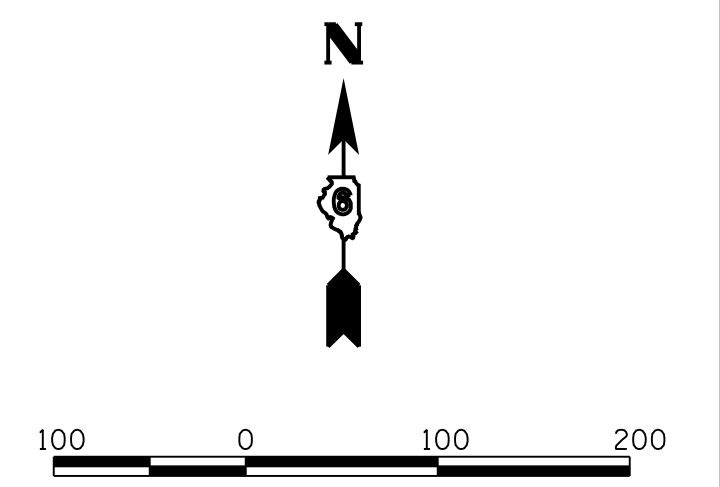
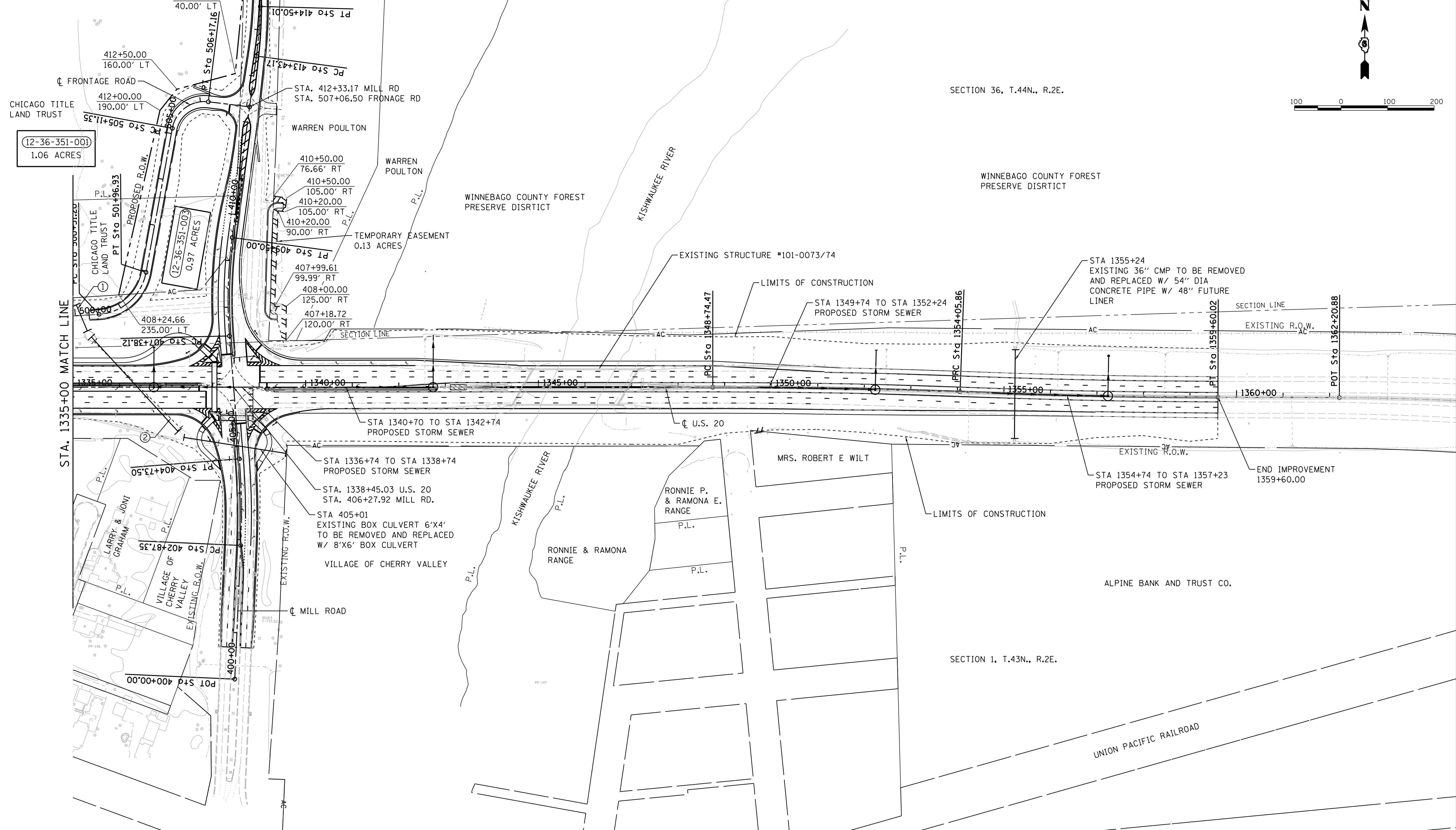
**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 HARRISON AVE PLAN SHEET
 JOB NO. P-92-111-06**

SCALE: 1"=100' SHEET NO. 2 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1,5)R)	WINNEBAGO	xx	35
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

① STA 500+13
 EXISTING BOX CULVERT, 6'x4', TO BE REMOVED
 AND REPLACED W/ 8'x6' BOX CULVERT

② STA 1336+43
 EXISTING BOX CULVERT 6'x4' TO BE
 REMOVED AND REPLACED W/ 8'x6' BOX
 CULVERT

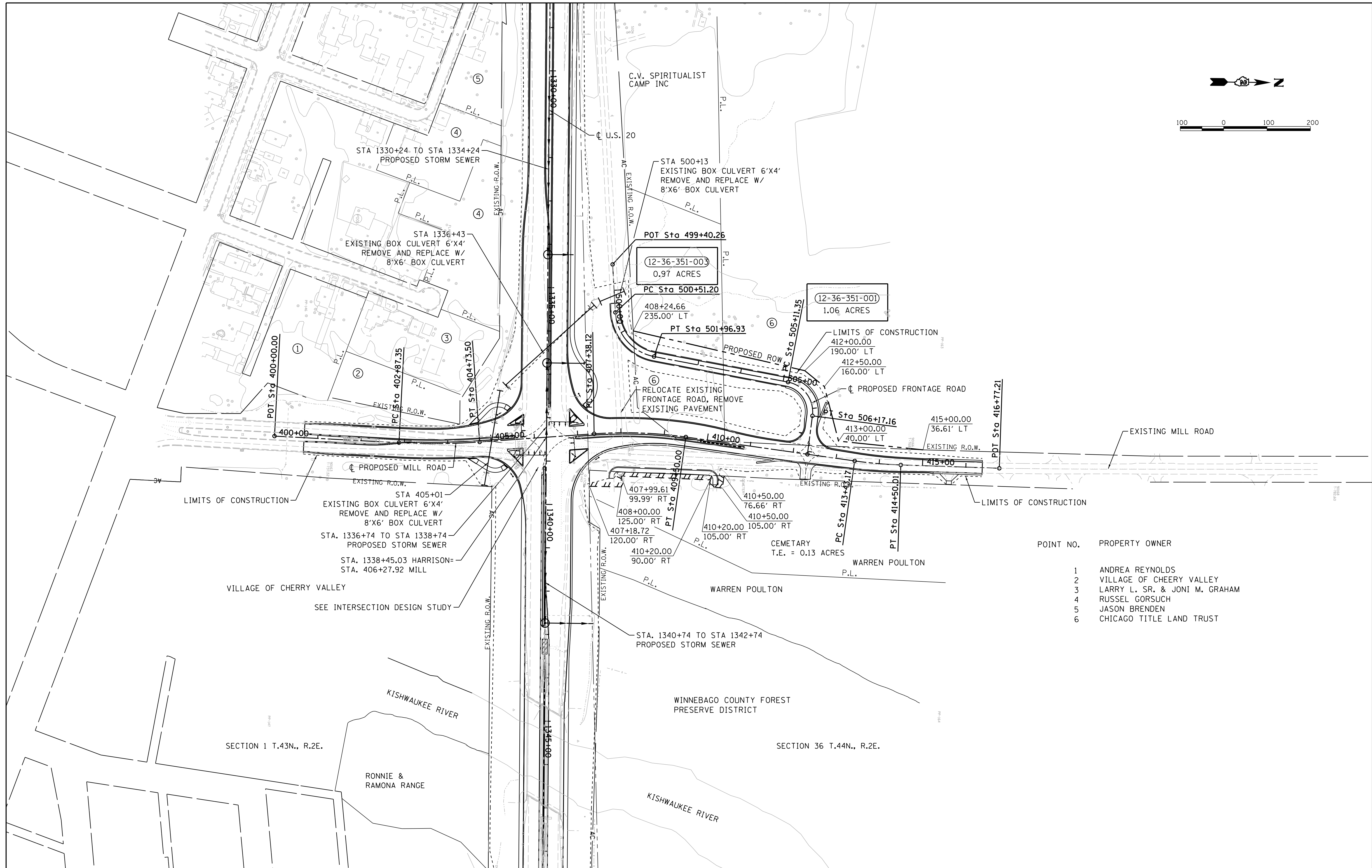
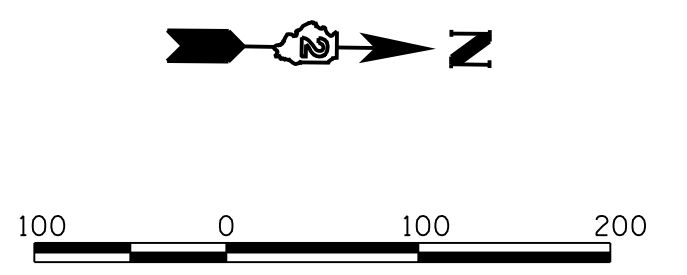


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		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 HARRISON AVE PLAN SHEET
 JOB NO. P-92-111-06**

F.A.I. RTE. 39	SECTION (201-3K & (4-1,5)R)	COUNTY WINNEBAGO	TOTAL SHEETS xx	SHEET NO. 36
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



POINT NO.	PROPERTY OWNER
1	ANDREA REYNOLDS
2	VILLAGE OF CHEERY VALLEY
3	LARRY L. SR. & JONI M. GRAHAM
4	RUSSEL GORSUCH
5	JASON BRENDEN
6	CHICAGO TITLE LAND TRUST

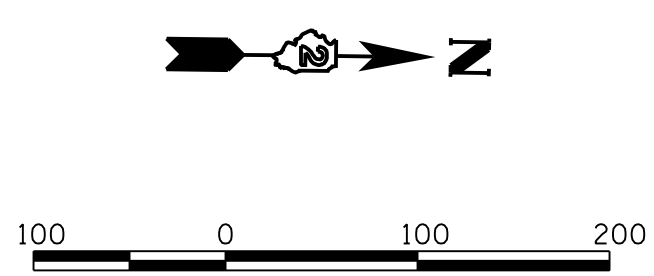
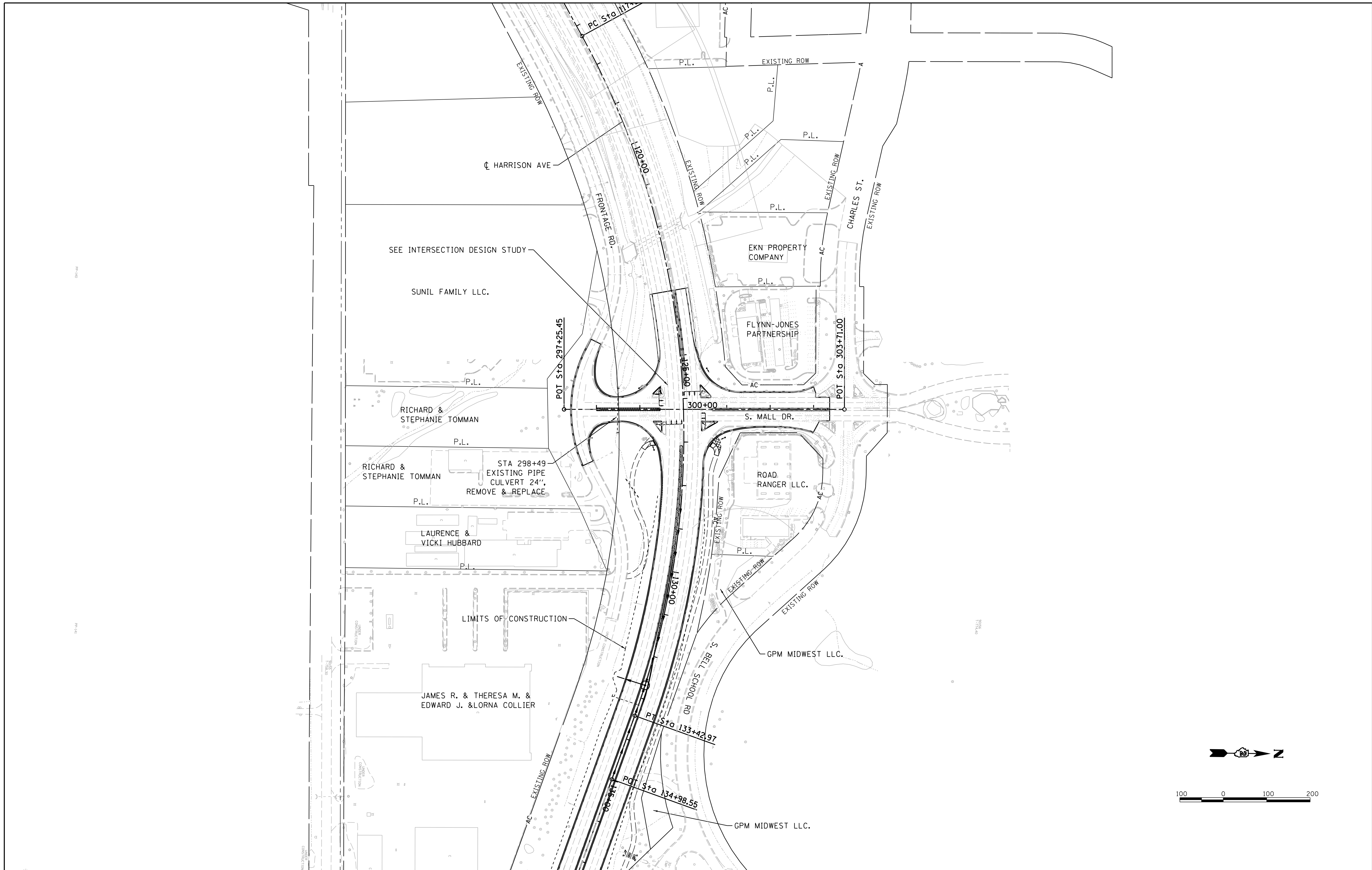
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 MILL ROAD PLAN SHEET
 JOB NO. P-92-111-06**

SCALE: 1"=100' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1,5)R	WINNEBAGO	xx	37
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



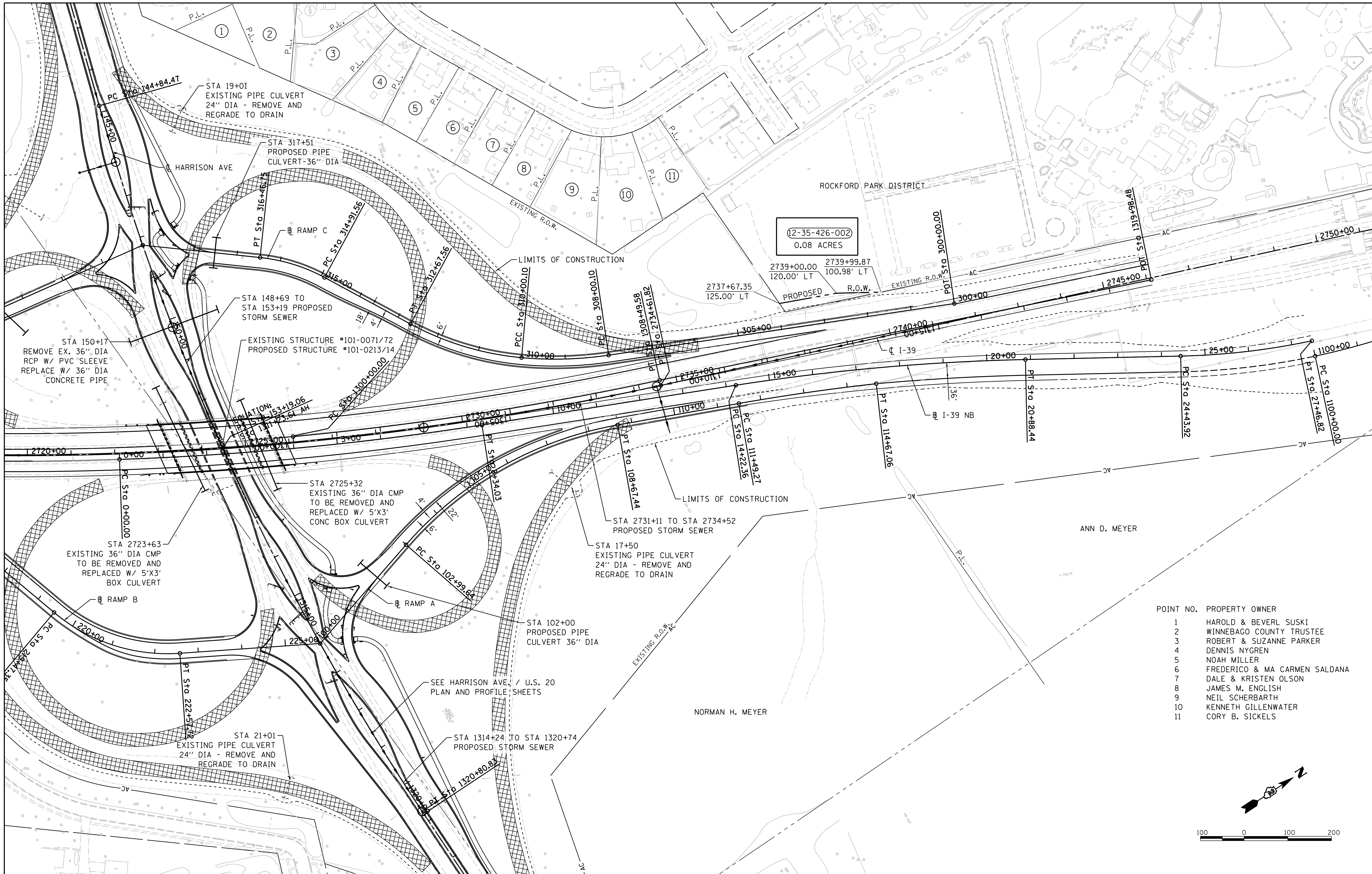
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

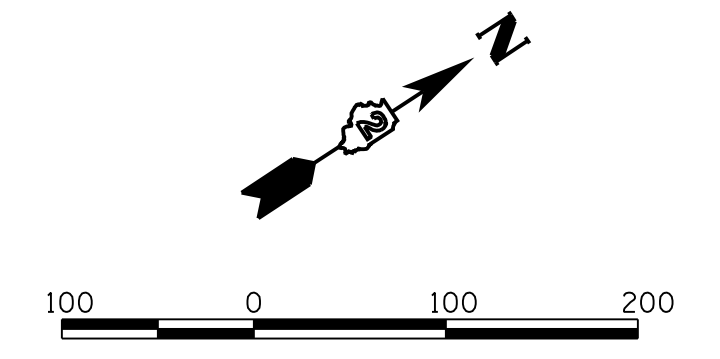
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 SOUTH MALL ROAD PLAN SHEET
 JOB NO. P-92-111-06**

SCALE: 1"=100' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3K & (4-1,5)R	WINNEBAGO	xx	38
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



POINT NO.	PROPERTY OWNER
1	HAROLD & BEVERL SUSKI
2	WINNEBAGO COUNTY TRUSTEE
3	ROBERT & SUZANNE PARKER
4	DENNIS NYGREN
5	NOAH MILLER
6	FREDERICO & MA CARMEN SALDANA
7	DALE & KRISTEN OLSON
8	JAMES M. ENGLISH
9	NEIL SCHERBARTH
10	KENNETH GILLENWATER
11	CORY B. SICKELS



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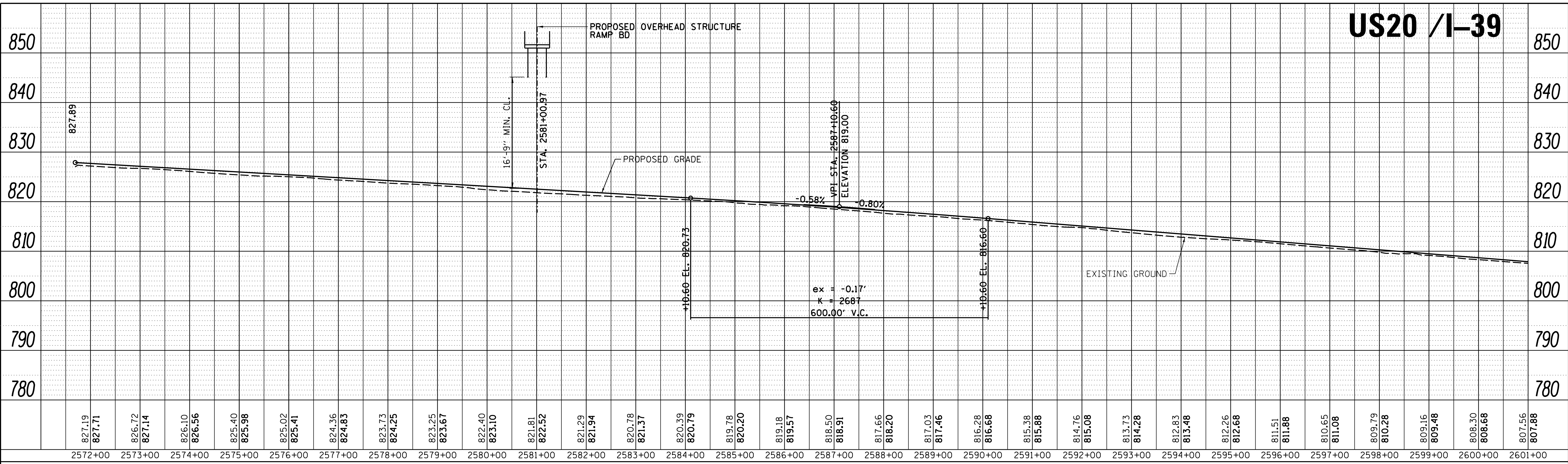
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
I-39 NB PLAN SHEET
JOB NO. P-92-111-06

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3IK & (4-1,5)R	WINNEBAGO	xx	39
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

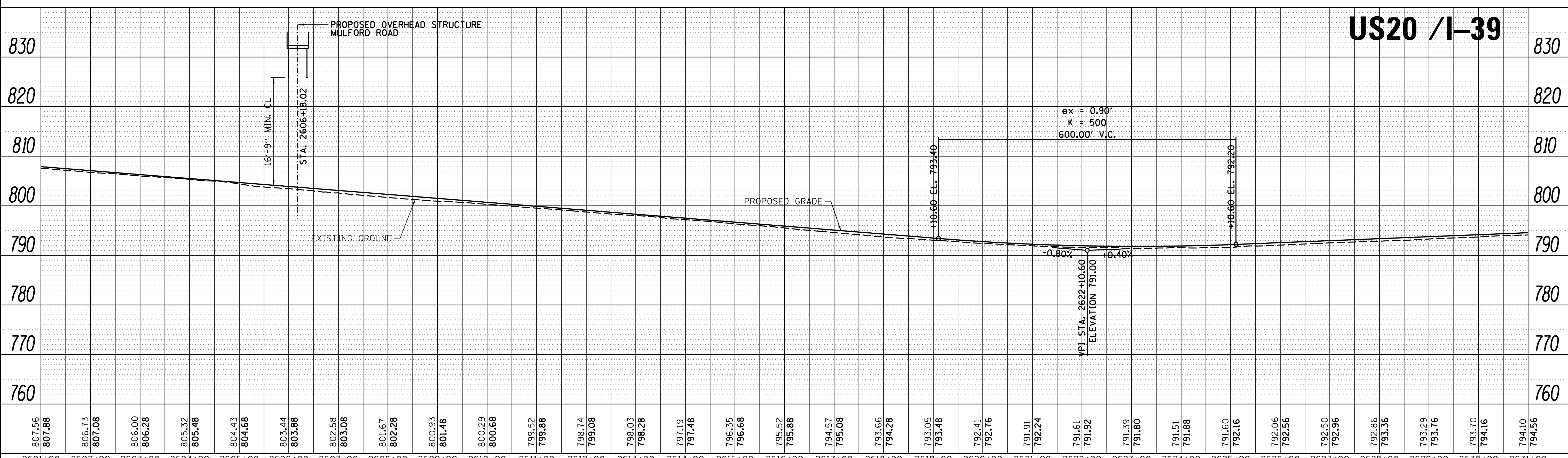
PROPOSED PROFILE SHEETS

US20 / I-39



PLAN	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHKD	
	NOTE BOOK NO.	
	CADD FILE NAME	

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PROFILE	SURVEYED	DATE
	PLOTTED	BY
	GRADES CHECKED	
	STRUCTURE NOTATIONS CHKD	
	NOTE BOOK NO.	
	CADD FILE NAME	

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

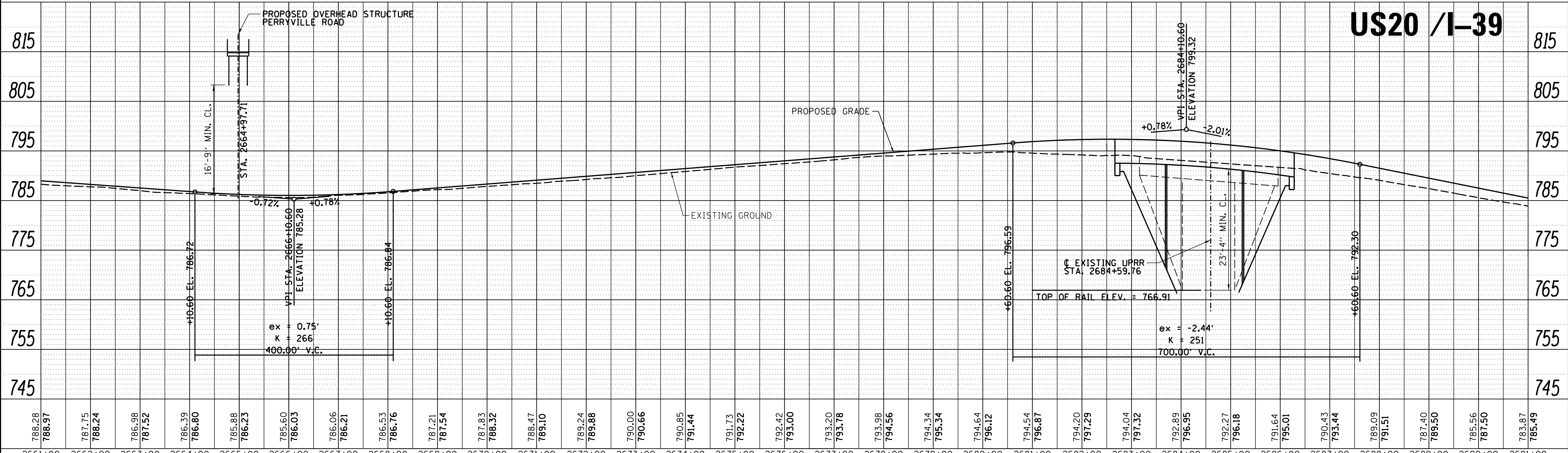
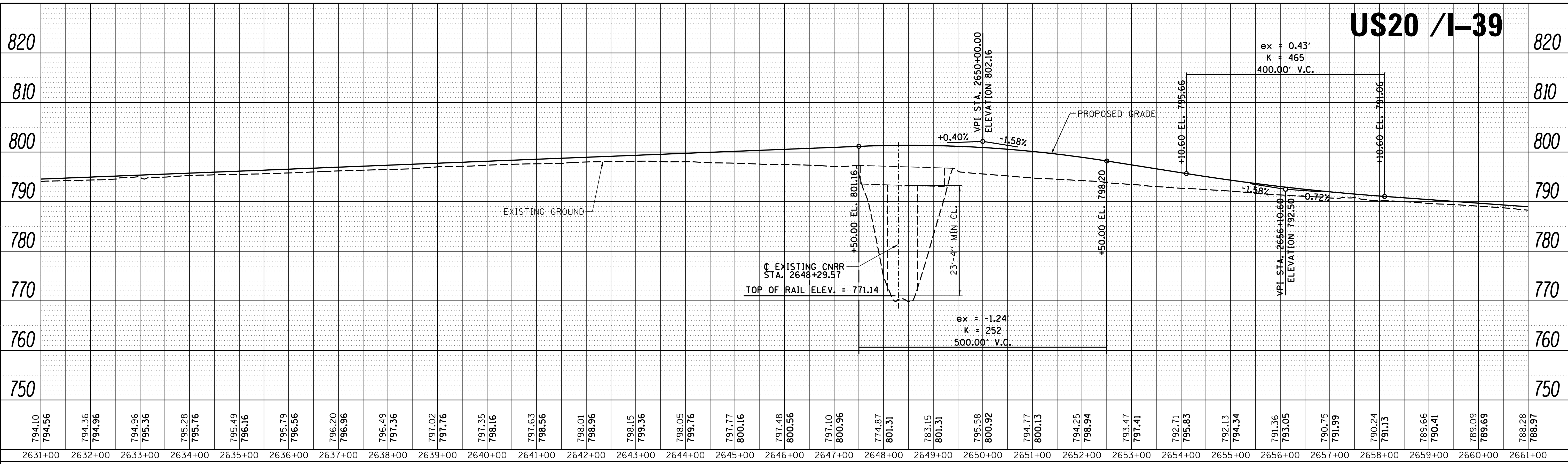
FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
U.S. 20 PROFILE SHEET
JOB NO. P-92-111-06
 SCALE: SHEET NO. 1 OF 4 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3K & (4-1,5)R)	WINNEBAGO	xx	40
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

PLAN	SURVEYED	BY	DATE
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	NO.		
	NO.		

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PROFILE	SURVEYED	BY	DATE
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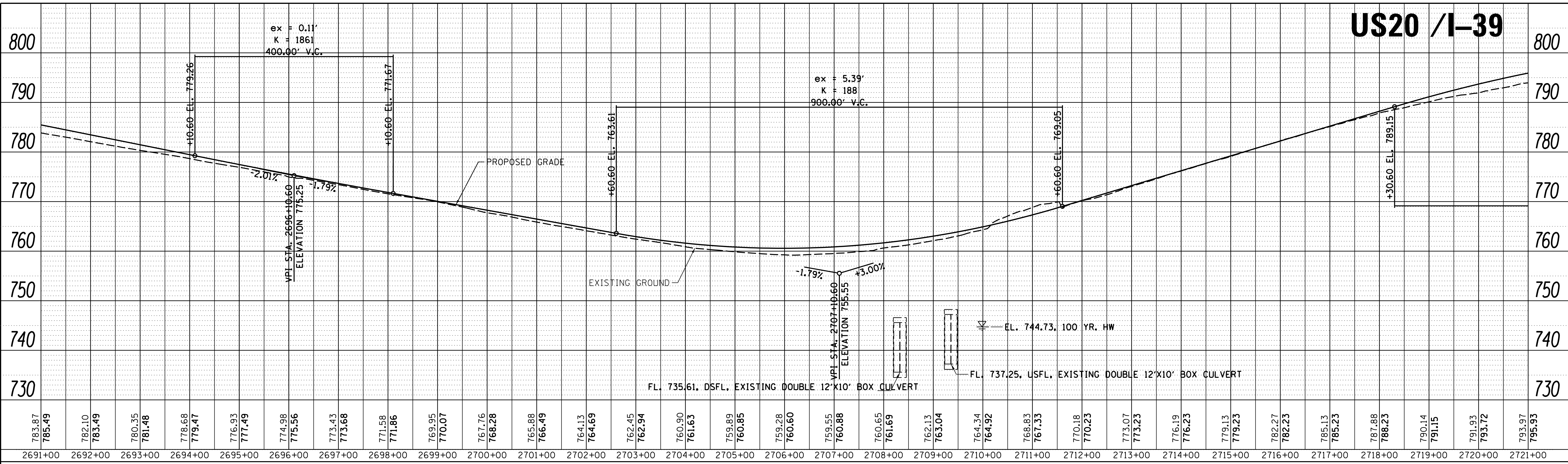
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PLOT SCALE = 100.0000' / 1"	CHECKED - S.M.	REVISED -				
PLOT DATE = 3/30/2017	DATE -	REVISED -				

PLAN	SURVEYED	BY	DATE
	PLOTTED		
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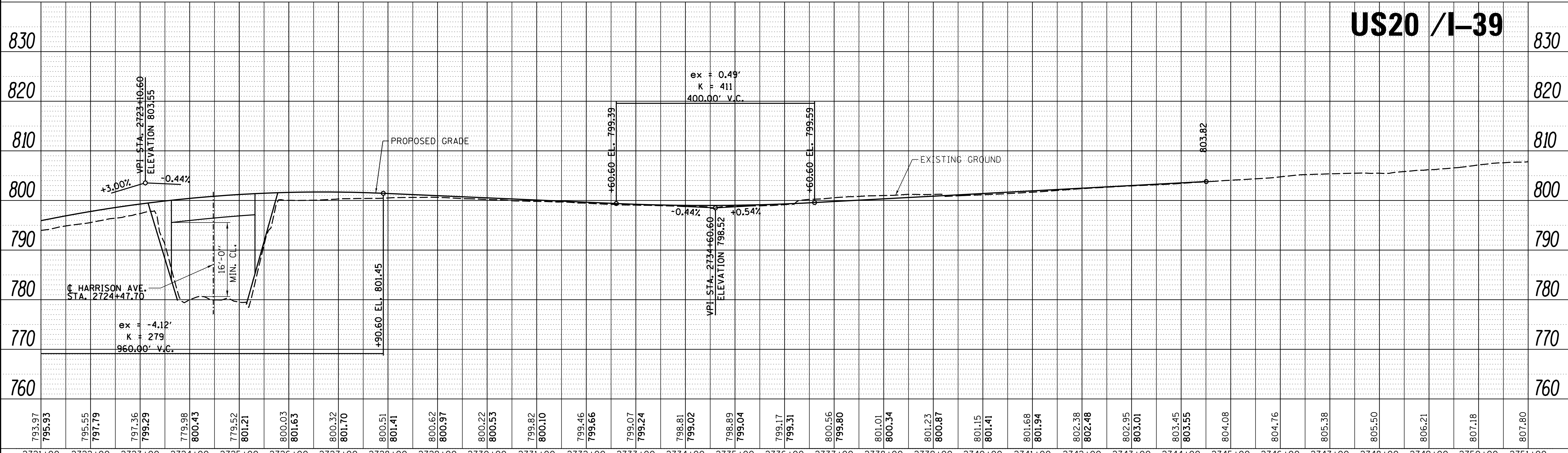
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PROFILE	SURVEYED	BY	DATE
	GRADES CHECKED		
	STRUCTURE		
	NOTATMS CHKD		
	NO.		



FILE NAME =	USER NAME = Johns00944	DESIGNED - J.M.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20) U.S. 20 PROFILE SHEET JOB NO. P-92-111-06	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default		DATE -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

US20 / I-39



PLAN	SURVEYED	BY	DATE
NOTE BOOK NO.	PLOTTED		
	CHECKED		
	DATE		
	CADD FILE NAME		



PROFILE	SURVEYED	BY	DATE
NOTE BOOK NO.	GRADES CHECKED		
	STRUCTURE NOTATIONS CHKD		

FILE NAME =	USER NAME = Johns00944	DESIGNED - J.M.	REVISED -
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Default	PLOT SCALE = 100.0000' / in.	CHECKED - S.M.	REVISED -
	PLOT DATE = 3/30/2017	DATE -	REVISED -

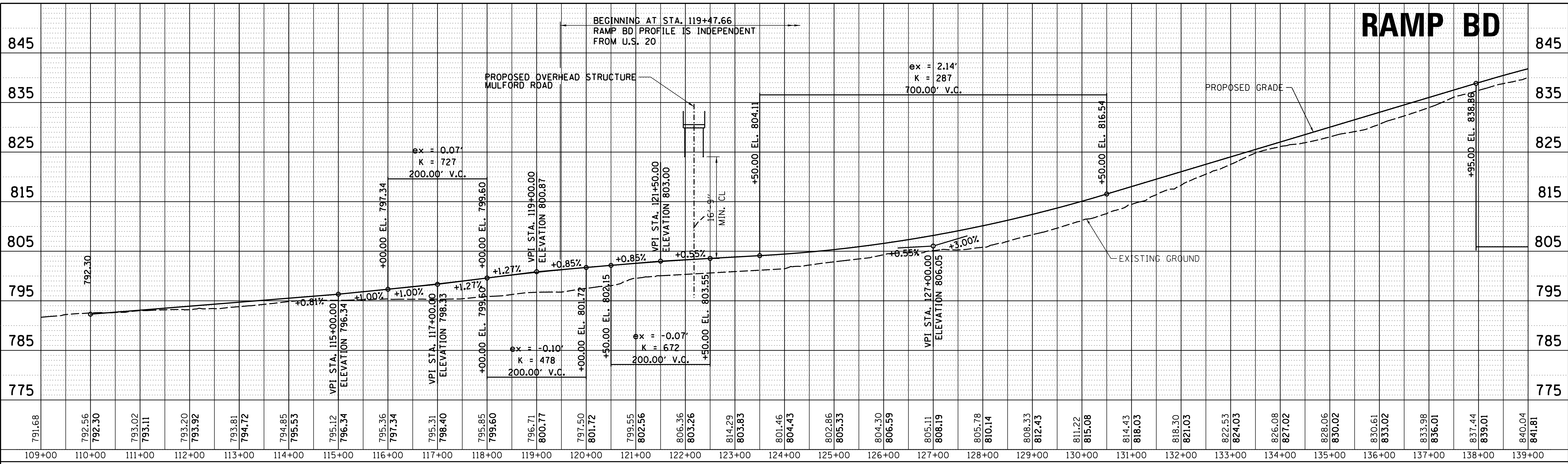
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)			
U.S. 20 PROFILE SHEET			
JOB NO. P-92-111-06			
SCALE:	SHEET NO. 4	OF 4 SHEETS	STA. TO STA.

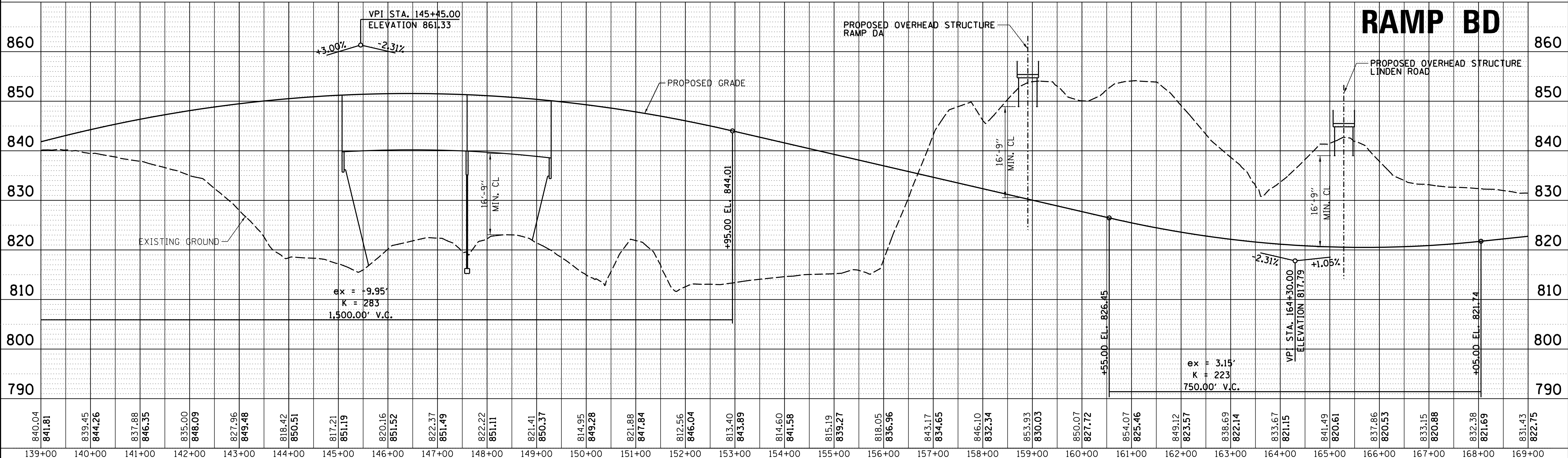
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06S2055		CONTRACT NO.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
	PLOTTED		
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	ALIGNED		
	CADD FILE NAME		
	NO.		

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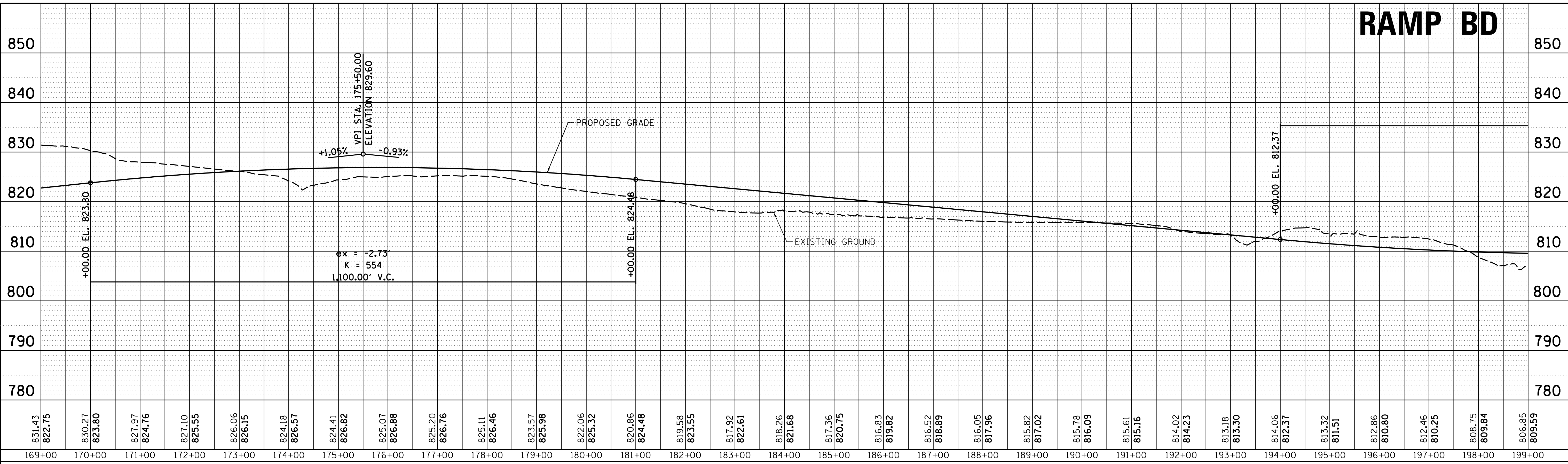
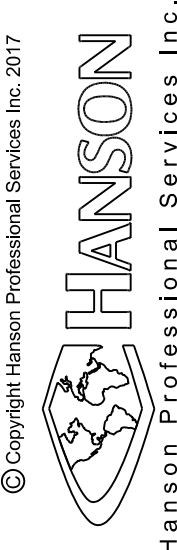


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	STRUCTURE		
	NOTATMS CHKD		
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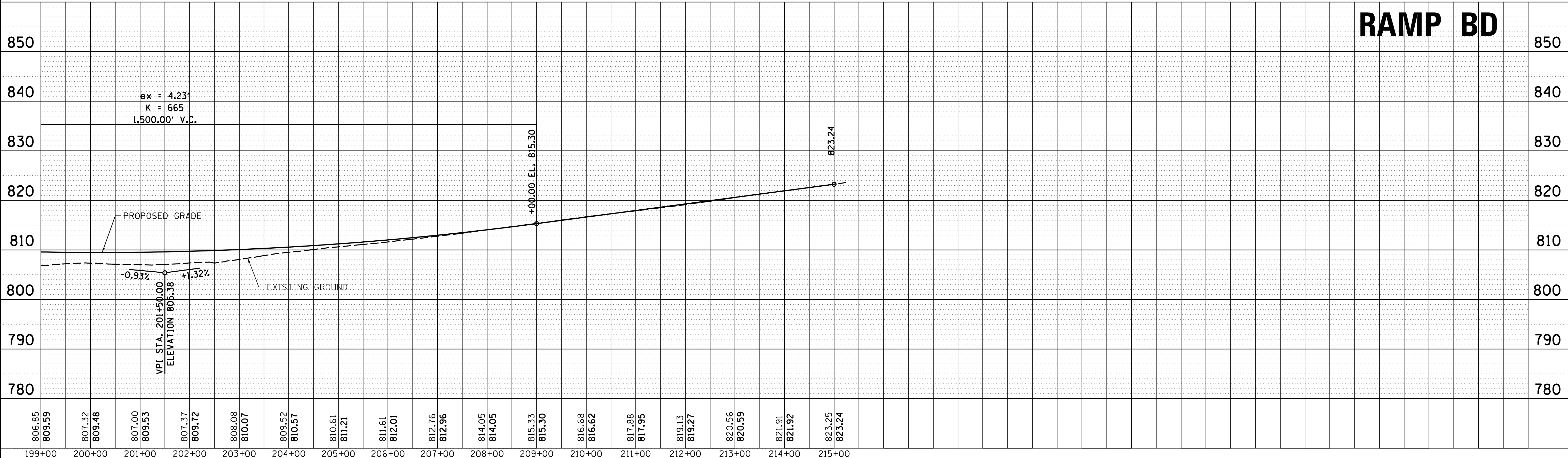


FILE NAME =	USER NAME = John00944	DESIGNED - J.M.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20) RAMP BD PROFILE SHEET JOB NO. P-92-111-06	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default		CHECKED - S.M.	REVISED -		SCALE:		CONTRACT NO.		
		DATE -	REVISED -		SHEET NO. OF SHEETS STA. TO STA.		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	NOTED		
	CHECKED		
	FILE NAME		



PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	NOTED		
	CHECKED		
	FILE NAME		



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		DATE -	REVISED -

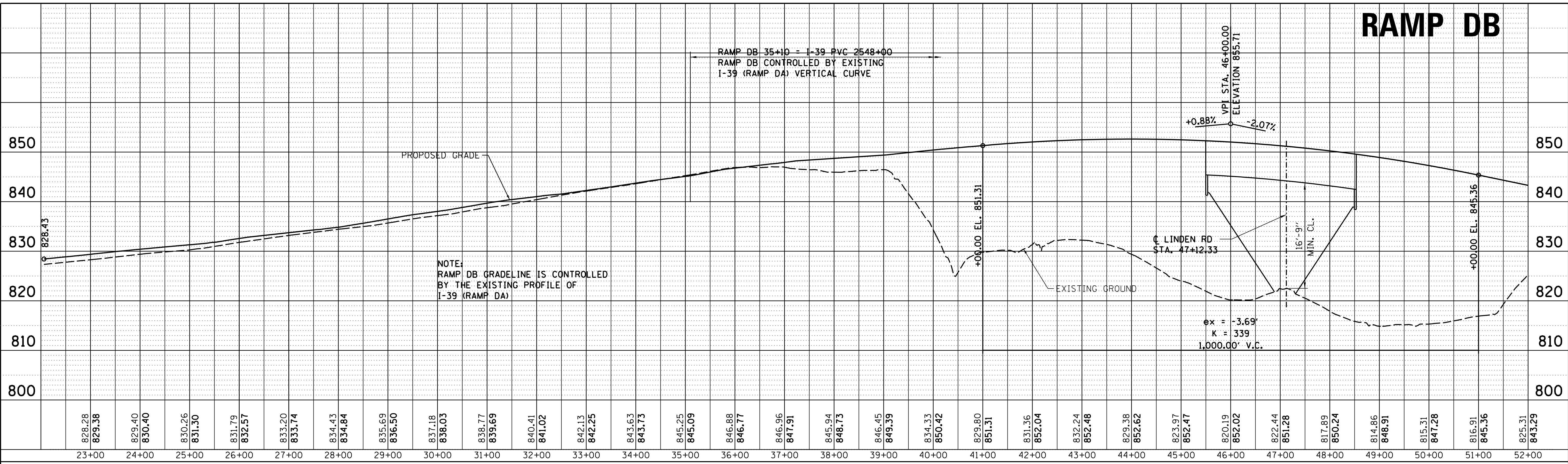
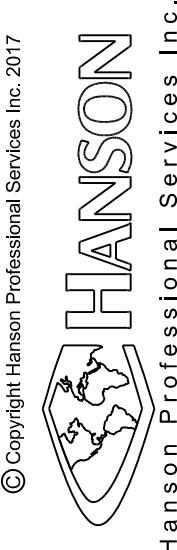
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
RAMP BD PROFILE SHEET
JOB NO. P-92-111-06**

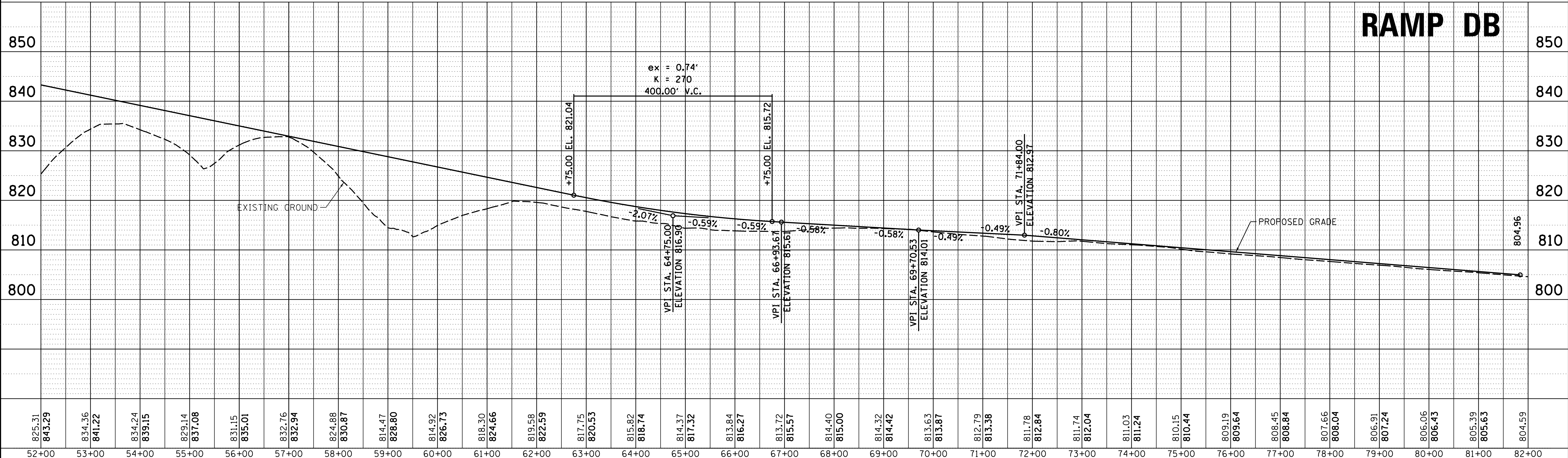
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06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

SCALE: SHEET NO. OF SHEETS STA. TO STA.

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	CHECKED		
	AT		
	CADD FILE NAME		
	NO.		

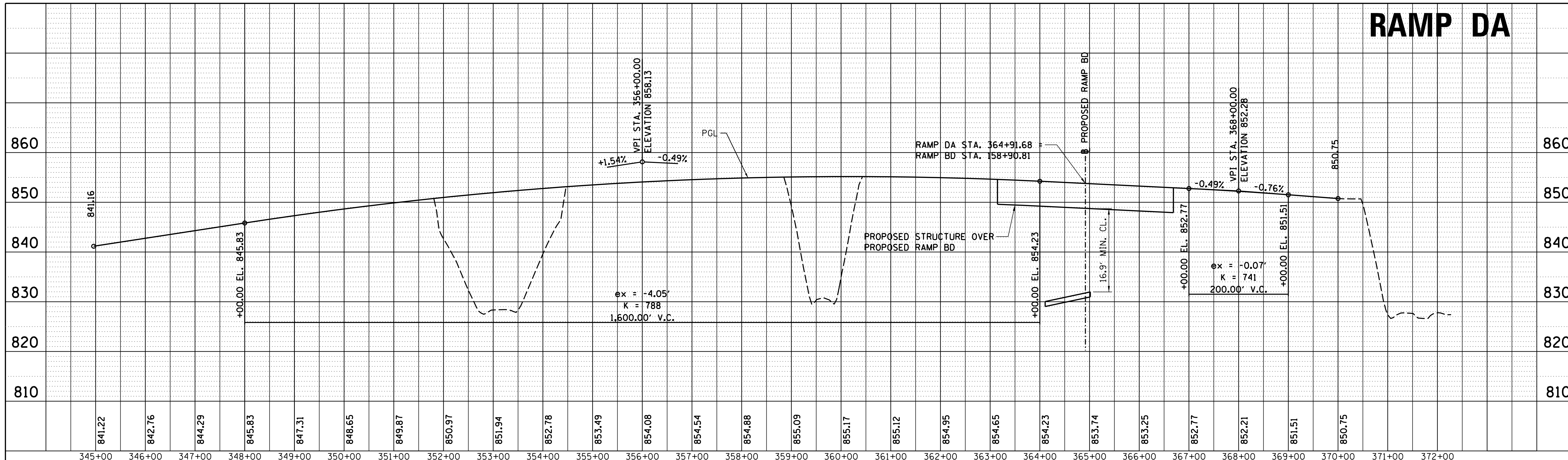


PROFILE	SURVEYED	BY	DATE
	GRADES CHECKED		
	NOTE BOOK		
	STRUCTURE		
	NOTATMS CHKD		
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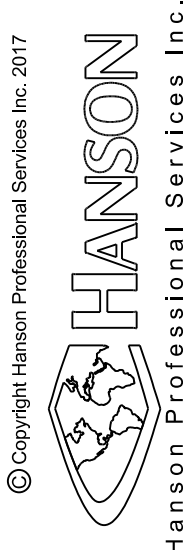


FILE NAME =	USER NAME = John00944	DESIGNED - J.M.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20) RAMP DB PROFILE SHEET JOB NO. P-92-111-06			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 100.0000' / 1"		CHECKED - S.M.	REVISED -					06S2055		CONTRACT NO.		
Default		DATE -	REVISED -					FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

RAMP DA



PLAN	SURVEYED	DATE
	PLOTTED	BY
	CHECKED	
	NOTED	
	CADD FILE NAME	
	NO.	



PROFILE	SURVEYED	DATE
	PLOTTED	BY
	CHECKED	
	NOTED	
	STRUCTURE	
	NOTATIS CHKD	
	NO.	

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346+00	842.76	
347+00	844.29	
348+00	845.83	845.83
349+00	847.31	
350+00	848.65	
351+00	849.87	
352+00	850.97	
353+00	851.94	
354+00	852.78	
355+00	853.49	
356+00	854.08	856.13
357+00	854.54	
358+00	854.88	
359+00	855.09	
360+00	855.17	
361+00	855.12	
362+00	854.95	
363+00	854.65	
364+00	854.23	854.23
365+00	853.74	
366+00	853.25	
367+00	852.77	852.77
368+00	852.21	852.28
369+00	851.51	851.51
370+00	850.75	850.75
371+00		
372+00		

FILE NAME =	I:\06\jobs\06S2055\CADD\Road\Sheet\C-7RampDA-pro-1.dgn	USER NAME =	Johns00944
DESIGNED -	J.M.	REVISED -	
DRAWN -	R.S.J.	REVISED -	
CHECKED -	S.M.	REVISED -	
DATE -		REVISED -	
PLOT SCALE =	100.0000' / in.		
PLOT DATE =	3/30/2017		

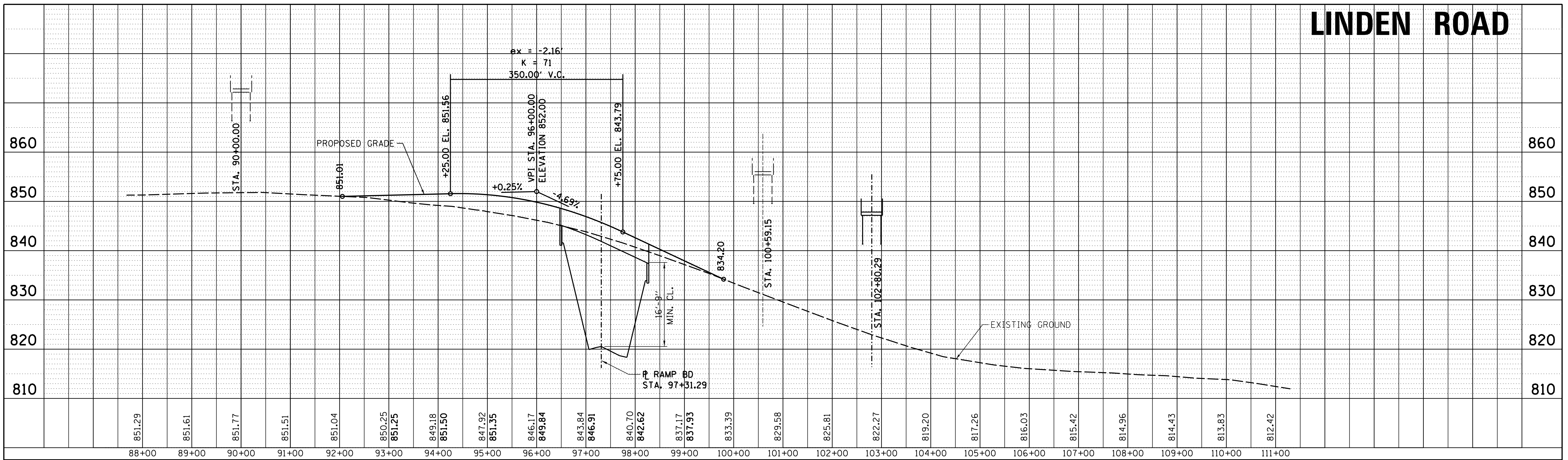
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
RAMP DA PROFILE SHEET
JOB NO. P-92-111-06**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3K & (4-1,5)R	WINNEBAGO	xx	47
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

SCALE: SHEET NO. OF SHEETS STA. TO STA.

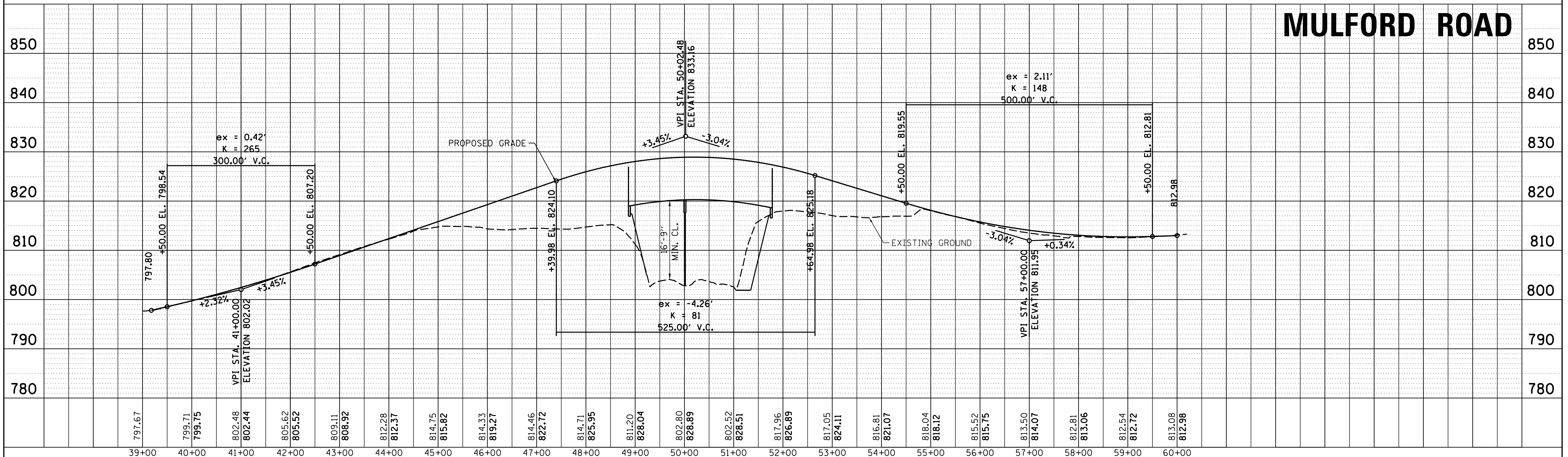
LINDEN ROAD



PLAN	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHKD		
	NOTE BOOK NO.		
	CADD FILE NAME		

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MULFORD ROAD

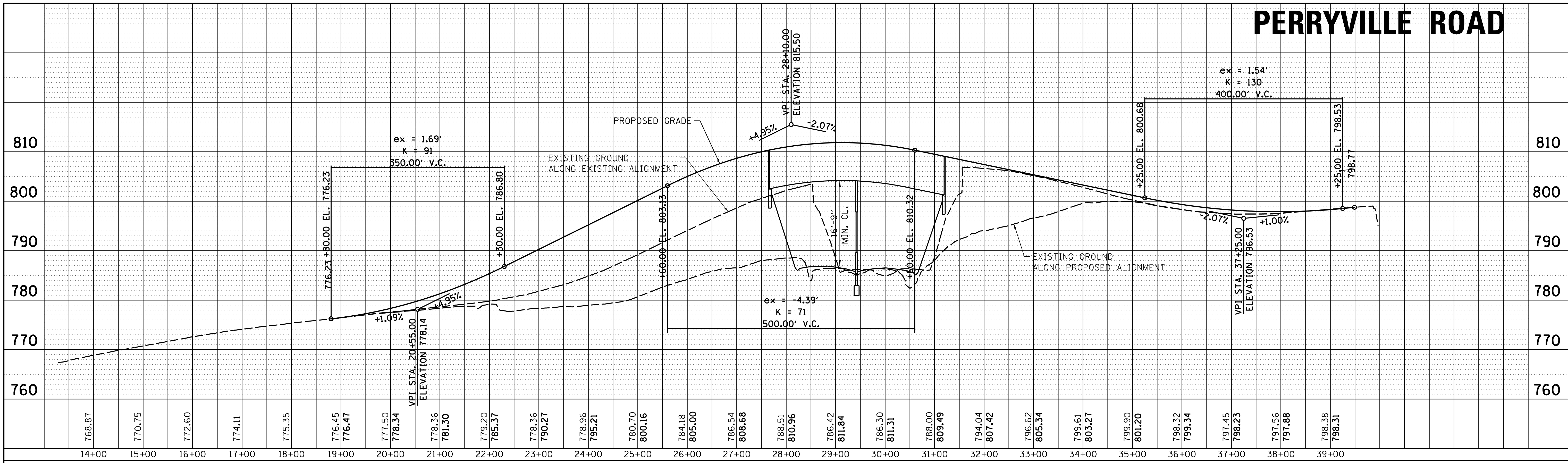


PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHKD		
	NOTE BOOK NO.		
	CADD FILE NAME		

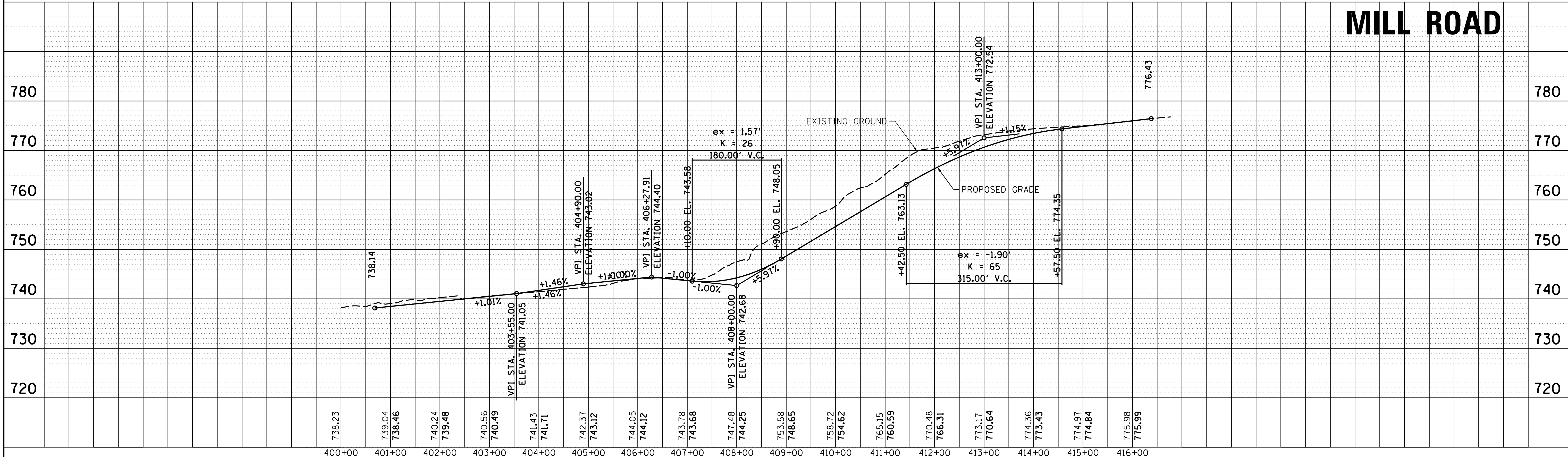
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FILE NAME =	USER NAME =	DESIGNED -	REVISOR -	STATE OF ILLINOIS		FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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		R.S.J.	-			JOB NO. P-92-111-06		06S2055		CONTRACT NO.		
		S.M.	-					FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
			-									
Default	PLOT DATE = 3/30/2017	DATE	REVISOR	SCALE:	SHEET NO.	OF	SHEETS	STA.	TO	STA.		

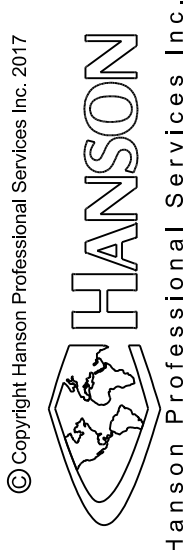
PERRYVILLE ROAD



MILL ROAD



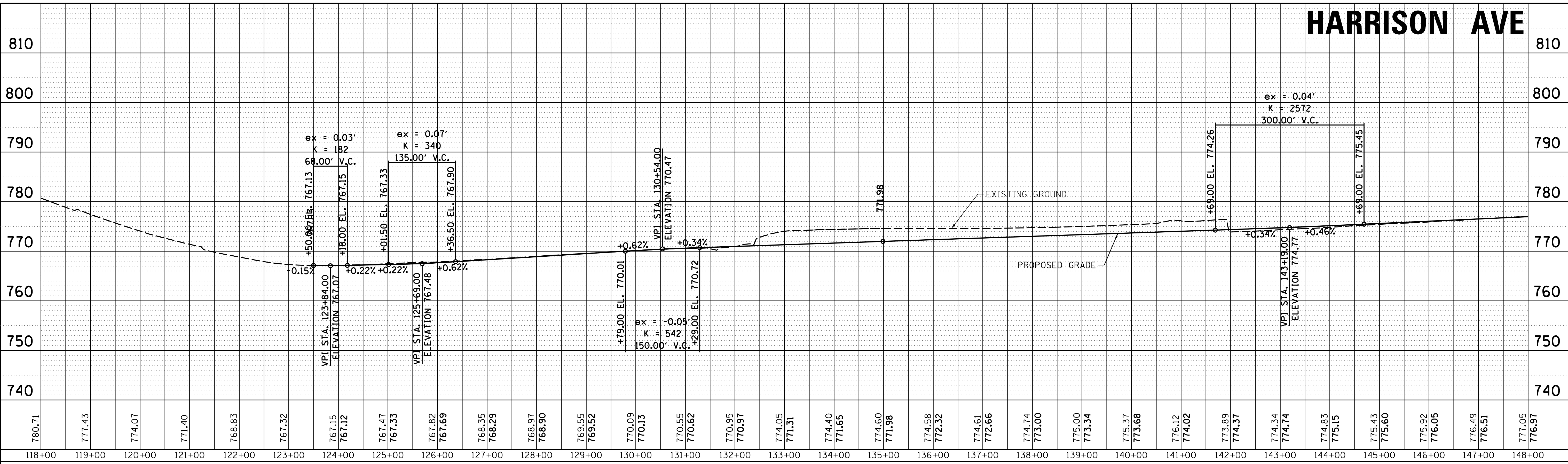
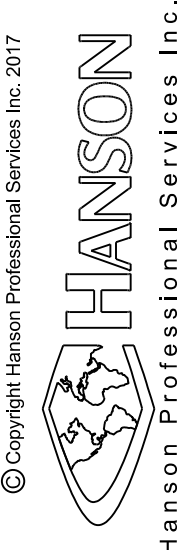
PLAN	SURVEYED	BY	DATE
	PLOTTED		
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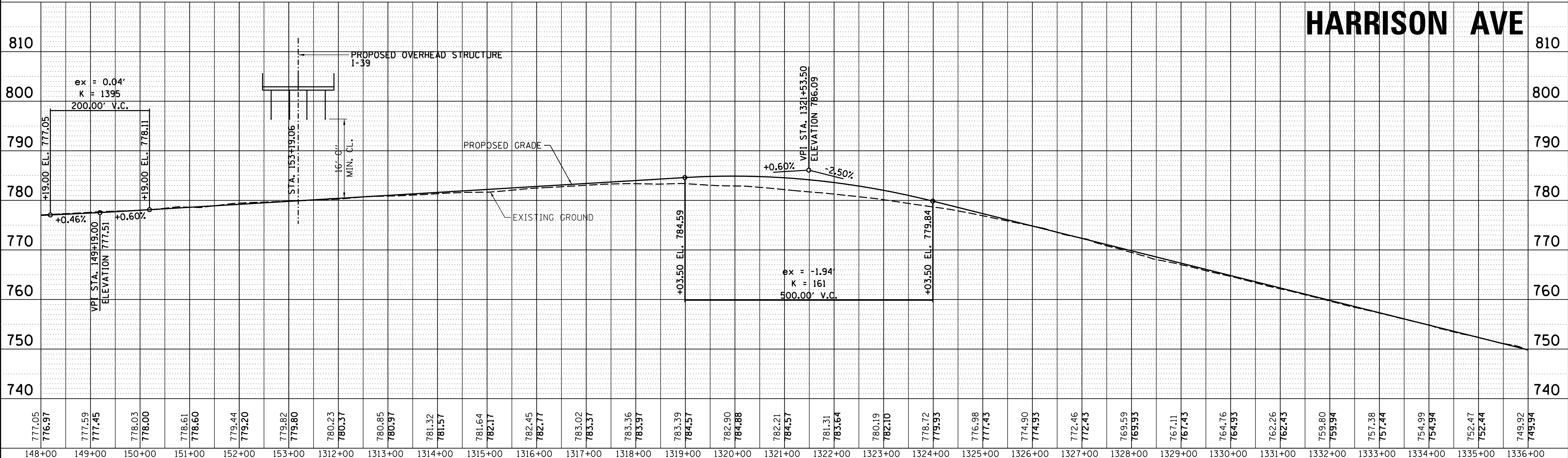
PROFILE	SURVEYED	BY	DATE
	GRADES CHECKED		
	STRUCTURE		
	NOTATMS CHKD		

FILE NAME =	USER NAME = Johns00944	DESIGNED - J.M.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20) PERRYVILLE RD /MILL RD PROFILE SHEET JOB NO. P-92-111-06			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 100.0000' / in.	CHECKED - S.M.	REVISED -			06S2055		CONTRACT NO.					
Default	DATE -	REVISED -			FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT					

PLAN	SURVEYED	BY	DATE
	PLOTTED		
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	FILE NAME		
	NO.		

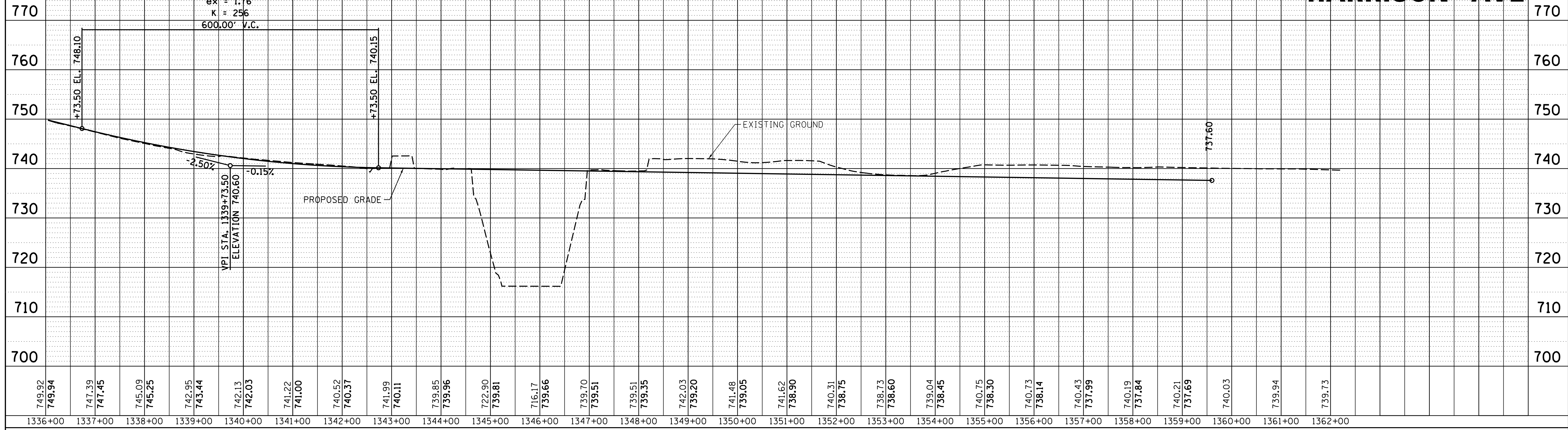


PROFILE	SURVEYED	BY	DATE
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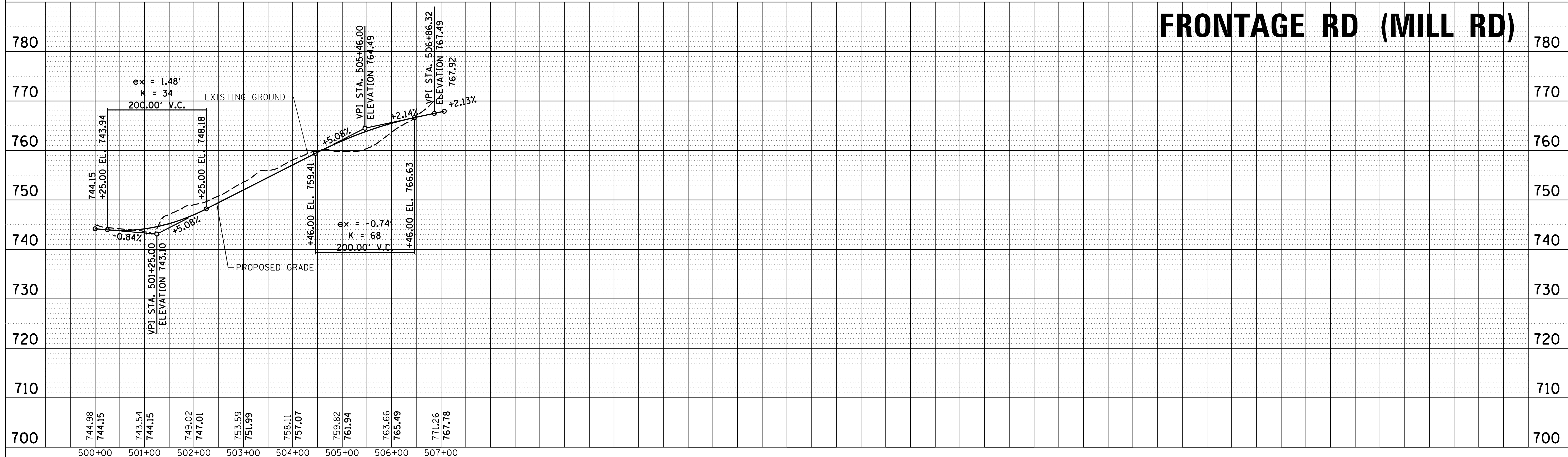


FILE NAME =	USER NAME = John00944	DESIGNED - J.M.	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20) HARRISON AVE PROFILE SHEET JOB NO. P-92-111-06			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 100.0000' / in.		CHECKED - S.M.	REVISED -		06S2055		CONTRACT NO.					
Default		DATE -	REVISED -		SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	

HARRISON AVE



FRONTAGE RD (MILL RD)



PLAN	SURVEYED	BY	DATE
	NOTED		
	CHECKED		
	DATE		
	FILE NAME		



PROFILE	SURVEYED	BY	DATE
	GRADES		
	CHECKED		
	STRUCTURE		
	NOTATIONS		
	CHKD		

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DATE = 3/30/2017	DATE -		REVISED -

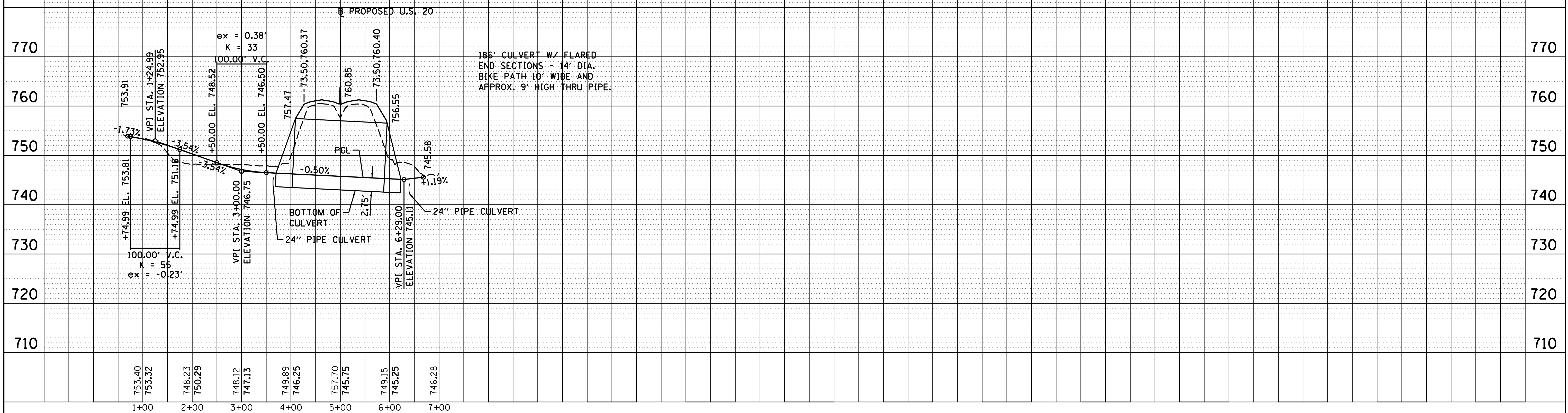
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
HARRISON AVE /FRONTAGE RD PROFILE SHEET
JOB NO. P-92-111-06**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3K & (4-1,5)R	WINNEBAGO	xx	51
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

SCALE: SHEET NO. OF SHEETS STA. TO STA.

CHERRY VALLEY PATH



PLAN	SURVEYED	BY	DATE
	PLOTTED		
	GRADES CHECKED		
	STRUCTURE NOTATIONS CHECKED		
	NOTE BOOK NO.		
	CADD FILE NAME		

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PROFILE	SURVEYED	BY	DATE
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	NOTE BOOK NO.		
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	PLOT DATE = 3/30/2017		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

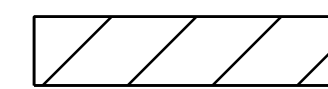
FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
CHERRY VALLEY PATH
JOB NO. P-92-111-06

SCALE:	SHEET NO.	OF	SHEETS	STA.	TO	STA.
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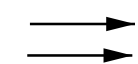
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39	(201-3K & (4-1,5)R	WINNEBAGO	xx	53
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

STAGE CONSTRUCTION PLANS & FIGURES

LEGEND

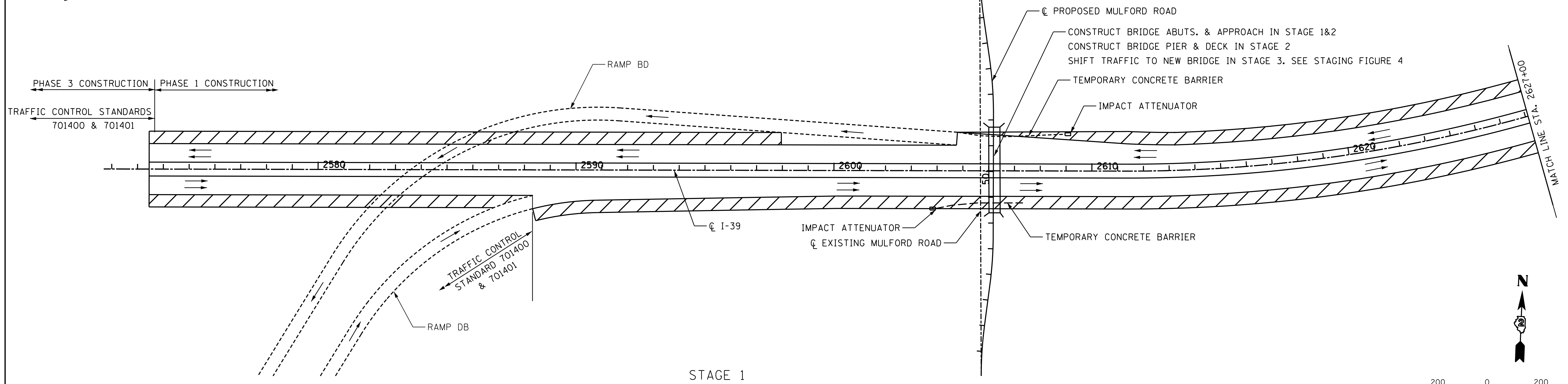


WORK ZONE



TRAFFIC FLOW

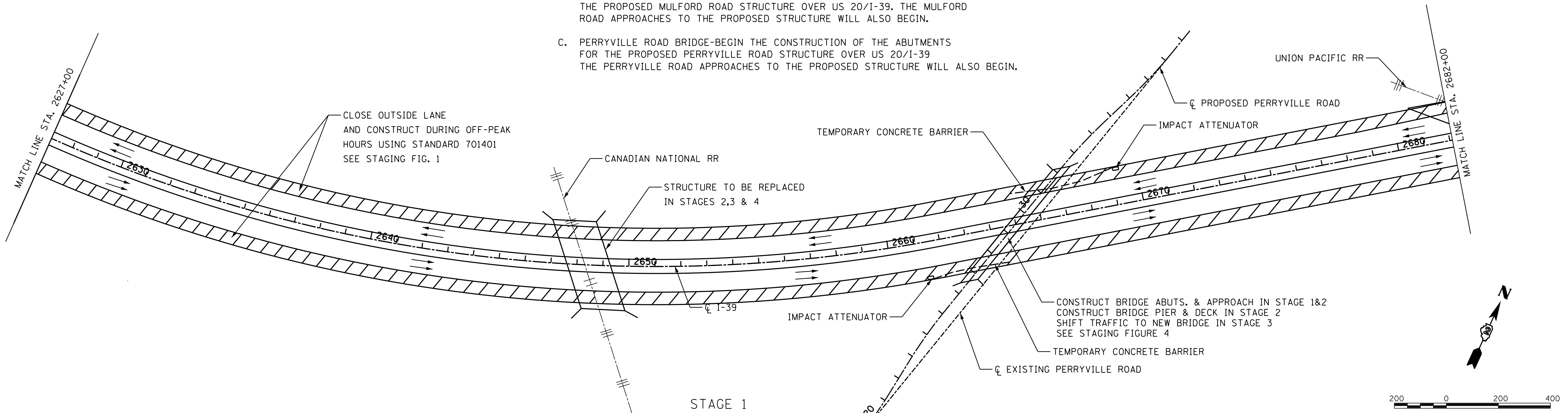
NOTE: CLOSE OUTSIDE LANE AND CONSTRUCT SHOULDER STRENGTHENING DURING OFF PEAK HOURS. SEE STAGING FIG. 1



STAGE 1

CONSTRUCTION SUMMARY

- A. US 20/I-39 - THE OUTSIDE LANES OF US 20/I-39 WILL BE CLOSED DURING OFF PEAK HOURS TO STRENGTHEN THE OUTSIDE SHOULDERS.
- B. MULFORD ROAD BRIDGE-BEGIN THE CONSTRUCTION OF THE ABUTMENTS FOR THE PROPOSED MULFORD ROAD STRUCTURE OVER US 20/I-39. THE MULFORD ROAD APPROACHES TO THE PROPOSED STRUCTURE WILL ALSO BEGIN.
- C. PERRYVILLE ROAD BRIDGE-BEGIN THE CONSTRUCTION OF THE ABUTMENTS FOR THE PROPOSED PERRYVILLE ROAD STRUCTURE OVER US 20/I-39. THE PERRYVILLE ROAD APPROACHES TO THE PROPOSED STRUCTURE WILL ALSO BEGIN.



STAGE 1

FILE NAME =	USER NAME = Johns00944	DESIGNED - J.M.	REVISED -
I:\06jobs\0652055\CADD\Road\Sheet\c-7m\c-ph1st1-01.dgn		DRAWN - R.S.J.	REVISED -
		CHECKED - S.M.	REVISED -
stage	PLOT DATE = 3/30/2017	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
STAGE CONSTRUCTION PHASE 1 STAGE 1
JOB NO. P-92-111-06**

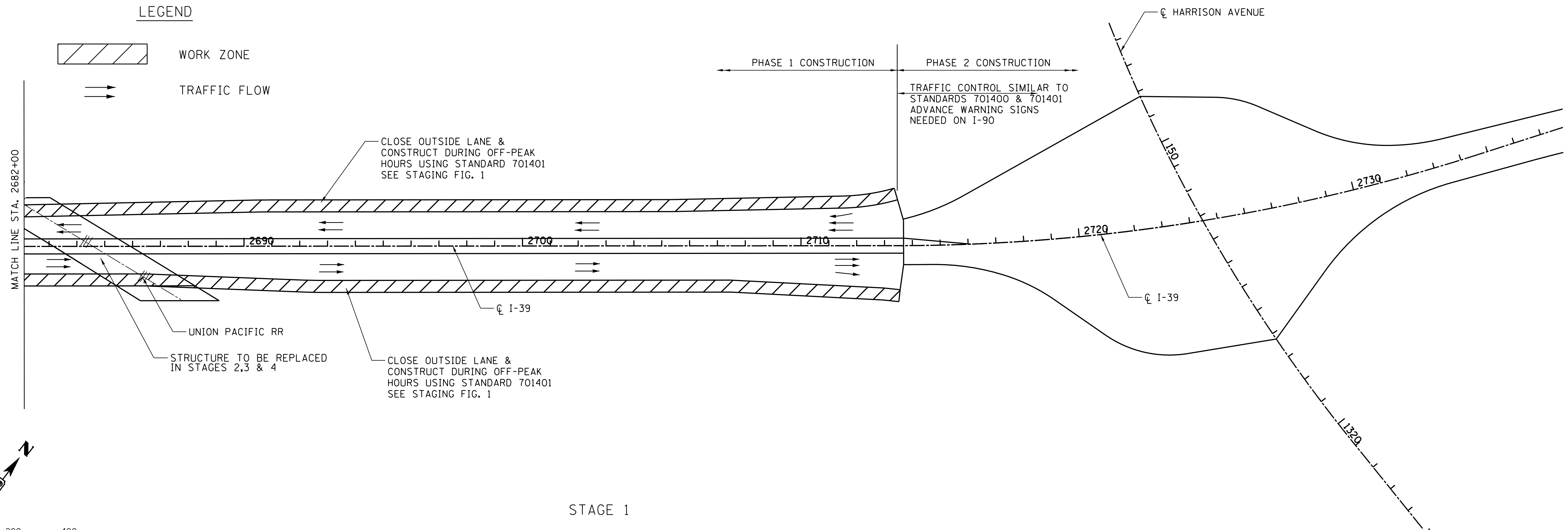
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1,5)R	WINNEBAGO	xx	54
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

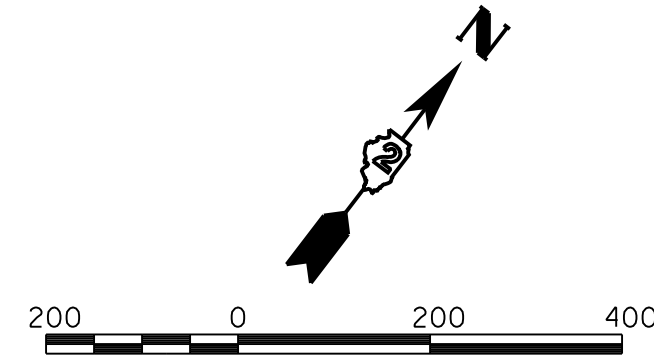
LEGEND

 WORK ZONE

 TRAFFIC FLOW



STAGE 1



CONSTRUCTION SUMMARY

- A. US 20/I-39 - THE OUTSIDE LANES OF US 20/I-39 WILL BE CLOSED DURING OFF PEAK HOURS TO STRENGTHEN THE OUTSIDE SHOULDERS.
- B. MULFORD ROAD BRIDGE-BEGIN THE CONSTRUCTION OF THE ABUTMENTS FOR THE PROPOSED MULFORD ROAD STRUCTURE OVER US 20/I-39. THE MULFORD ROAD APPROACHES TO THE PROPOSED STRUCTURE WILL ALSO BEGIN.
- C. PERRYVILLE ROAD BRIDGE-BEGIN THE CONSTRUCTION OF THE ABUTMENTS FOR THE PROPOSED PERRYVILLE ROAD STRUCTURE OVER US 20/I-39. THE PERRYVILLE ROAD APPROACHES TO THE PROPOSED STRUCTURE WILL ALSO BEGIN.

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FILE NAME =	USER NAME = Johns00944	DESIGNED - J.M.	REVISED -
I:\06jobs\0652055\CADD\Road\Sheet\c-7m...t_ph1st1-028.dgn		DRAWN - R.S.J.	REVISED -
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	PLOT DATE = 3/30/2017	DATE -	REVISED -

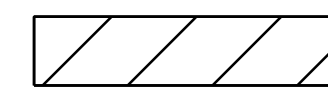
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
STAGE CONSTRUCTION PHASE 1 STAGE 1
JOB NO. P-92-111-06

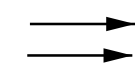
SCALE: 1"=200' SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1,5)R	WINNEBAGO	xx	55
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

LEGEND



WORK ZONE



TRAFFIC FLOW

PHASE 3 CONSTRUCTION PHASE 1 CONSTRUCTION

TRAFFIC CONTROL
SIMILAR TO STANDARDS 701400 & 701402
EXCEPT NO LANE CLOSURES

SEE STAGING FIG. 5
FOR LANE
TRANSITIONS

TEMPORARY CONCRETE BARRIER

TRAFFIC CONTROL
STANDARD 701400

STAGE 2

NOTE: SHIFT TRAFFIC TO STRENGTHENED OUTSIDE SHOULDER AND OUTSIDE LANE. CONSTRUCT PROPOSED INSIDE SHOULDER AND INSIDE LANE. SEE STAGING FIG. 1

☐ PROPOSED MULFORD ROAD

CONSTRUCT BRIDGE ABUTS. APPROACH IN STAGE 1&2
CONSTRUCT BRIDGE PIER & DECK IN STAGE 2
BRIDGE BEAMS WILL BE SET IN STAGE 2.
OFF-PEAK TEMPORARY CLOSURES WILL BE ALLOWED FOR EACH BEAM. SHIFT TRAFFIC TO NEW BRIDGE IN STAGE 3. SEE STAGING FIGURE 4.

TEMPORARY CONCRETE BARRIER
IMPACT ATTENUATOR

MATCH LINE STA. 2621+00

SHIFT TRAFFIC TO STRENGTHEN SHOULDERS AND THE OUTSIDE LANE. CONSTRUCT NEW FULL DEPTH PAVEMENT AND MEDIAN.

☐ I-39

IMPACT ATTENUATOR
TEMPORARY CONCRETE BARRIER

☐ EXISTING MULFORD ROAD



CONSTRUCTION SUMMARY

- A. US 20/I-39 - THE EASTBOUND AND WESTBOUND TRAFFIC WILL BE SHIFTED TO THE OUTSIDE TO UTILIZE THE SHOULDER STRENGTHENED IN STAGE 1 AND THE EXISTING OUTSIDE LANE OF US 20/I-39 TO MAINTAIN TWO LANES OF TRAFFIC. THE PROPOSED INSIDE SHOULDER AND INSIDE LANE WILL BE RECONSTRUCTED. THE PROPOSED INSIDE LANES AND SHOULDERS FOR THE STRUCTURES OVER THE UNION PACIFIC AND THE CANADIAN NATIONAL RAILROADS WILL BE CONSTRUCTED.
- B. MULFORD ROAD BRIDGE-THE CONSTRUCTION OF THE BRIDGE ABUTMENTS AND APPROACHES WILL CONTINUE FROM STAGE 1. THE PIER WILL BE CONSTRUCTED IN THE MEDIAN. BEAMS WILL BE SET DURING OFF PEAK HOURS AND ROLLING ROAD CLOSURES WILL BE UTILIZED FOR A SPECIFIC AMOUNT OF TIME TO SET THE BEAMS. THE DECK AND ROADWAY APPROACHES WILL BE COMPLETED IN STAGE 2.

- C. PERRYVILLE ROAD BRIDGE-THE CONSTRUCTION OF THE BRIDGE ABUTMENTS AND APPROACHES WILL CONTINUE FROM STAGE 1. THE PIER WILL BE CONSTRUCTED IN THE MEDIAN. BEAMS WILL BE SET DURING OFF PEAK HOURS AND ROLLING ROAD CLOSURES WILL BE UTILIZED FOR A SPECIFIC AMOUNT OF TIME TO SET THE BEAMS. THE DECK AND ROADWAY APPROACHES WILL BE COMPLETED IN STAGE 2 AND THE EXISTING STRUCTURE WILL BE REMOVED. THE REMOVAL OF THE EXISTING BEAMS WILL BE ACCOMMODATED UTILIZING THE SAME TYPE OF TRAFFIC CONTROL AS SETTING THE BEAMS FOR THE PROPOSED STRUCTURE.

☐ PROPOSED PERRYVILLE ROAD

TEMPORARY CONCRETE BARRIER
IMPACT ATTENUATOR

UNION PACIFIC RR

MATCH LINE STA. 2682+00

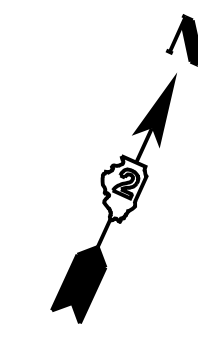
CONSTRUCT BRIDGE ABUTS. & APPROACH IN STAGE 1&2
CONSTRUCT BRIDGE PIER & DECK IN STAGE 2
BRIDGE BEAMS WILL BE SET IN STAGE 2. OFF-PEAK TEMPORARY CLOSURES WILL BE ALLOWED FOR EACH BEAM. SHIFT TRAFFIC TO NEW BRIDGE IN STAGE 3. SEE STAGING FIGURE 4.

CANADIAN NATIONAL RR

STRUCTURE TO BE REPLACED
IN STAGES 2,3 & 4

STAGE 2

☐ EXISTING PERRYVILLE ROAD



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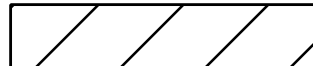
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
STAGE CONSTRUCTION PHASE 1 STAGE 2
JOB NO. P-92-111-06**

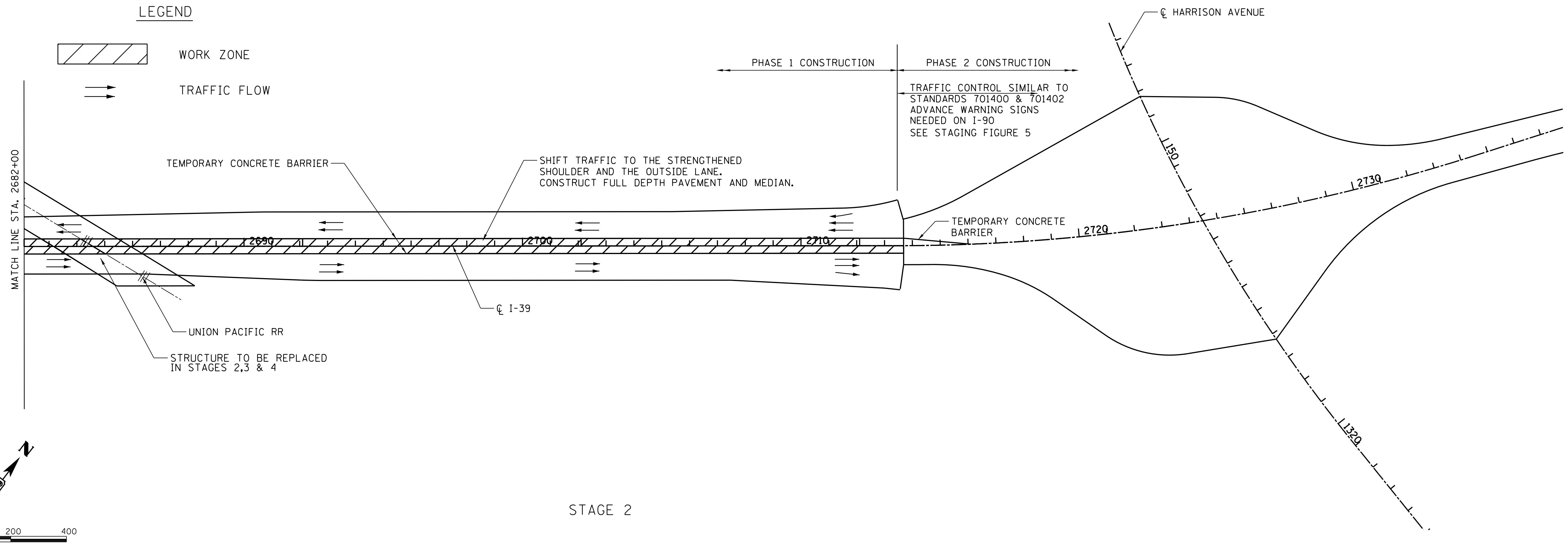
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1,5)R)	WINNEBAGO	xx	56
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT	CONTRACT NO.	

LEGEND

 WORK ZONE

 TRAFFIC FLOW



STAGE 2

CONSTRUCTION SUMMARY

- A. US 20/I-39 - THE EASTBOUND AND WESTBOUND TRAFFIC WILL BE SHIFTED TO THE OUTSIDE TO UTILIZE THE SHOULDER STRENGTHENED IN STAGE 1 AND THE EXISTING OUTSIDE LANE OF US 20/I-39 TO MAINTAIN TWO LANES OF TRAFFIC. THE PROPOSED INSIDE SHOULDER AND INSIDE LANE WILL BE RECONSTRUCTED. THE PROPOSED INSIDE LANES AND SHOULDERS FOR THE STRUCTURES OVER THE UNION PACIFIC AND THE CANADIAN NATIONAL RAILROADS WILL BE CONSTRUCTED.
- B. MULFORD ROAD BRIDGE-THE CONSTRUCTION OF THE BRIDGE ABUTMENTS AND APPROACHES WILL CONTINUE FROM STAGE 1. THE PIER WILL BE CONSTRUCTED IN THE MEDIAN. BEAMS WILL BE SET DURING OFF PEAK HOURS AND ROLLING ROAD CLOSURES WILL BE UTILIZED FOR A SPECIFIC AMOUNT OF TIME TO SET THE BEAMS. THE DECK AND ROADWAY APPROACHES WILL BE COMPLETED IN STAGE 2.
- C. PERRYVILLE ROAD BRIDGE-THE CONSTRUCTION OF THE BRIDGE ABUTMENTS AND APPROACHES WILL CONTINUE FROM STAGE 1. THE PIER WILL BE CONSTRUCTED IN THE MEDIAN. BEAMS WILL BE SET DURING OFF PEAK HOURS AND ROLLING ROAD CLOSURES WILL BE UTILIZED FOR A SPECIFIC AMOUNT OF TIME TO SET THE BEAMS. THE DECK AND ROADWAY APPROACHES WILL BE COMPLETED IN STAGE 2 AND THE EXISTING STRUCTURE WILL BE REMOVED. THE REMOVAL OF THE EXISTING BEAMS WILL BE ACCOMMODATED UTILIZING THE SAME TYPE OF TRAFFIC CONTROL AS SETTING THE BEAMS FOR THE PROPOSED STRUCTURE.

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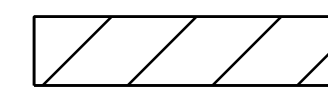
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
STAGE CONSTRUCTION PHASE 1 STAGE 2
JOB NO. P-92-111-06

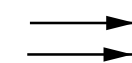
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

LEGEND



WORK ZONE



TRAFFIC FLOW

PHASE 3 CONSTRUCTION PHASE 1 CONSTRUCTION

SEE STAGING FIGURE 6 LANES TRANSITIONS

SEE STAGING FIGURE 8 FOR WESTBOUND LANE CONFIGURATION. IMPACT ATTENUATOR PROVIDE AT BRIDGE PIER LOCATIONS.

TEMPORARY CONCRETE BARRIER

MAINTAIN ACCESS ACROSS WORK ZONE USING STANDARD 701411

SHIFT WESTBOUND TRAFFIC TO THE MEDIAN LANES CONSTRUCTED IN STAGE 2. EASTBOUND TRAFFIC REMAINS IN LANE CONFIGURATION FROM STAGE 2. SEE STAGING FIGURE 2.

TRAFFIC CONTROL STANDARDS 701400 & 701402

TRAFFIC CONTROL STANDARD 701400

STAGE 3

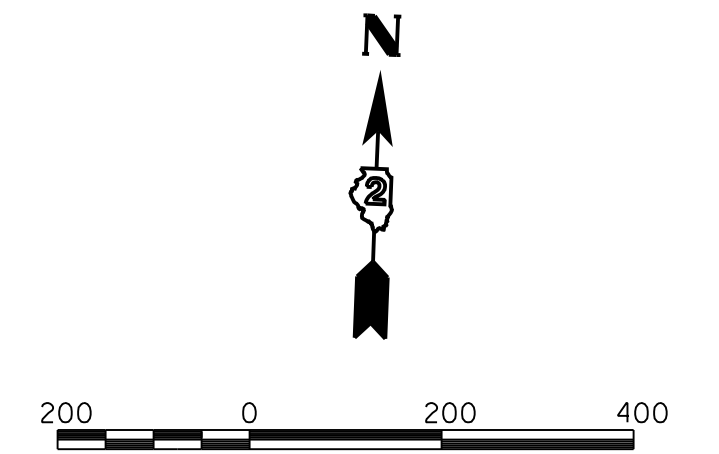
CL I-39

BRIDGE

CL EXISTING MULFORD ROAD

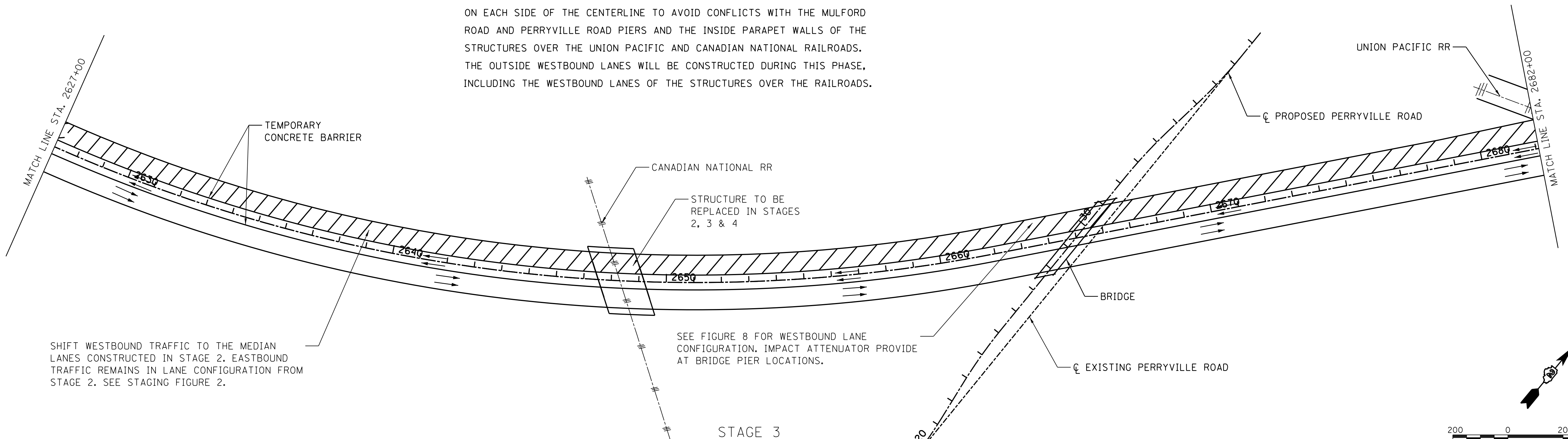
CL PROPOSED MULFORD ROAD

MATCH LINE STA. 2627+00



CONSTRUCTION SUMMARY

A US 20/I-39 - THE EASTBOUND TRAFFIC WILL REMAIN IN THE LANE CONFIGURATION FROM STAGE 2. THE WESTBOUND TRAFFIC WILL BE SHIFTED TO THE MEDIAN LANES CONSTRUCTED IN STAGE 2. A WESTBOUND LANE WILL BE ON EACH SIDE OF THE CENTERLINE TO AVOID CONFLICTS WITH THE MULFORD ROAD AND PERRYVILLE ROAD PIERS AND THE INSIDE PARAPET WALLS OF THE STRUCTURES OVER THE UNION PACIFIC AND CANADIAN NATIONAL RAILROADS. THE OUTSIDE WESTBOUND LANES WILL BE CONSTRUCTED DURING THIS PHASE, INCLUDING THE WESTBOUND LANES OF THE STRUCTURES OVER THE RAILROADS.



SHIFT WESTBOUND TRAFFIC TO THE MEDIAN LANES CONSTRUCTED IN STAGE 2. EASTBOUND TRAFFIC REMAINS IN LANE CONFIGURATION FROM STAGE 2. SEE STAGING FIGURE 2.

SEE FIGURE 8 FOR WESTBOUND LANE CONFIGURATION. IMPACT ATTENUATOR PROVIDE AT BRIDGE PIER LOCATIONS.

STAGE 3

UNION PACIFIC RR

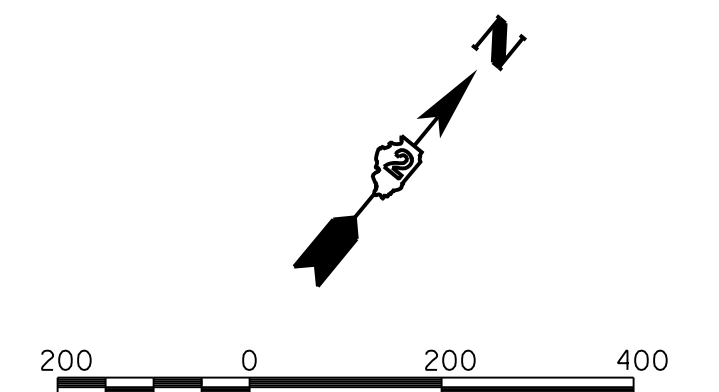
CL PROPOSED PERRYVILLE ROAD

CANADIAN NATIONAL RR

STRUCTURE TO BE REPLACED IN STAGES 2, 3 & 4

BRIDGE

CL EXISTING PERRYVILLE ROAD



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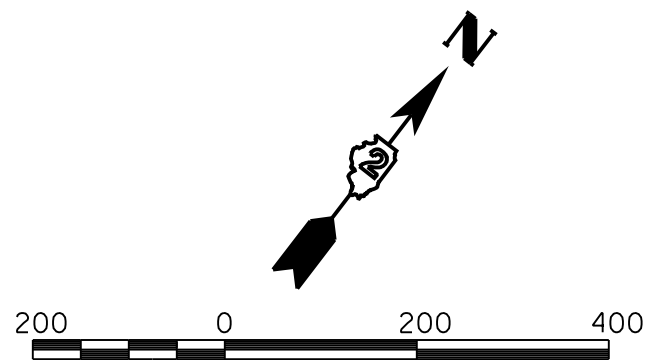
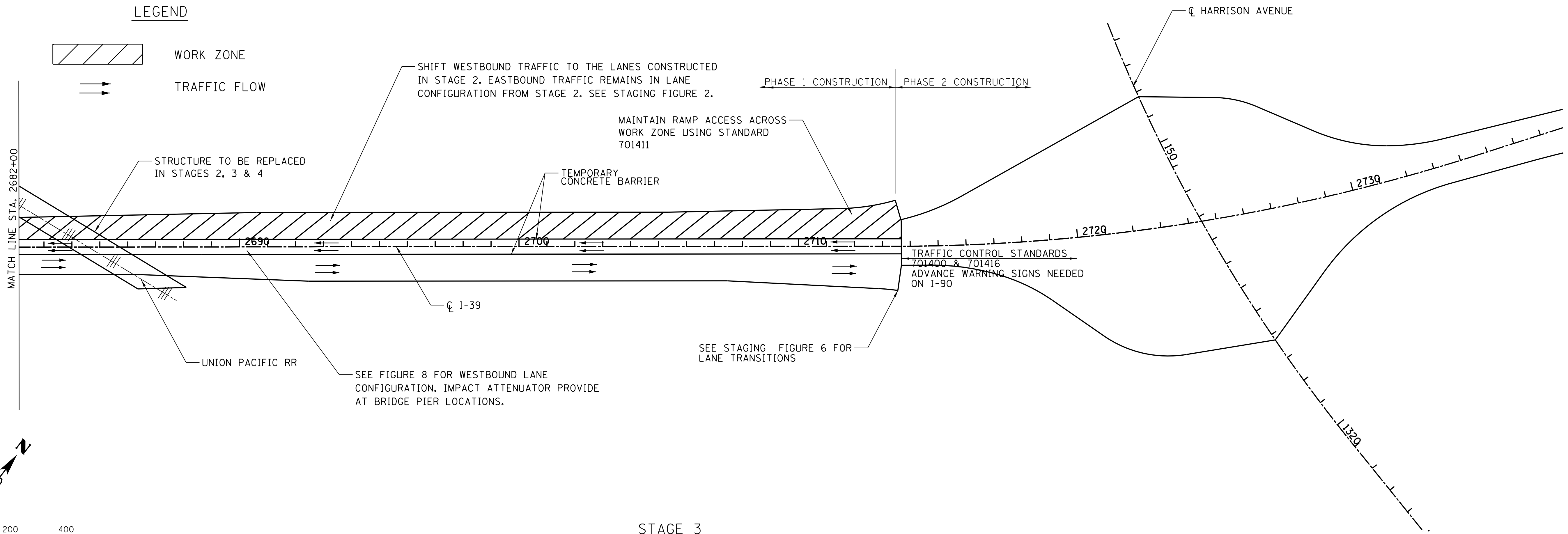
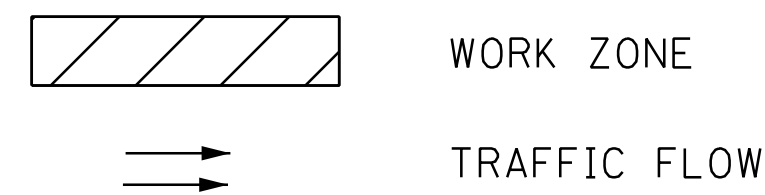
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
STAGE CONSTRUCTION PHASE 1 STAGE 3
JOB NO. P-92-111-06**

SCALE: 1"=200' SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1,5)R	WINNEBAGO	xx	58
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

LEGEND



STAGE 3

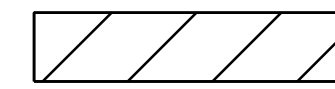
CONSTRUCTION SUMMARY

A US 20/I-39 - THE EASTBOUND TRAFFIC WILL REMAIN IN THE LANE CONFIGURATION FROM STAGE 2. THE WESTBOUND TRAFFIC WILL BE SHIFTED TO THE MEDIAN LANES CONSTRUCTED IN STAGE 2. A WESTBOUND LANE WILL BE ON EACH SIDE OF THE CENTERLINE TO AVOID CONFLICTS WITH THE MULFORD ROAD AND PERRYVILLE ROAD PIERS AND THE INSIDE PARAPET WALLS OF THE STRUCTURES OVER THE UNION PACIFIC AND CANADIAN NATIONAL RAILROADS. THE OUTSIDE WESTBOUND LANES WILL BE CONSTRUCTED DURING THIS PHASE, INCLUDING THE WESTBOUND LANES OF THE STRUCTURES OVER THE RAILROADS.

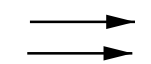
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PLOT SCALE = 200.0000' / in.	CHECKED - S.M.	REVISIED -	SCALE: 1"=200'			SHEET NO. OF SHEETS	STA. TO STA.	06S2055		CONTRACT NO.	
PLOT DATE = 3/30/2017	DATE -	REVISIED -	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT								

LEGEND



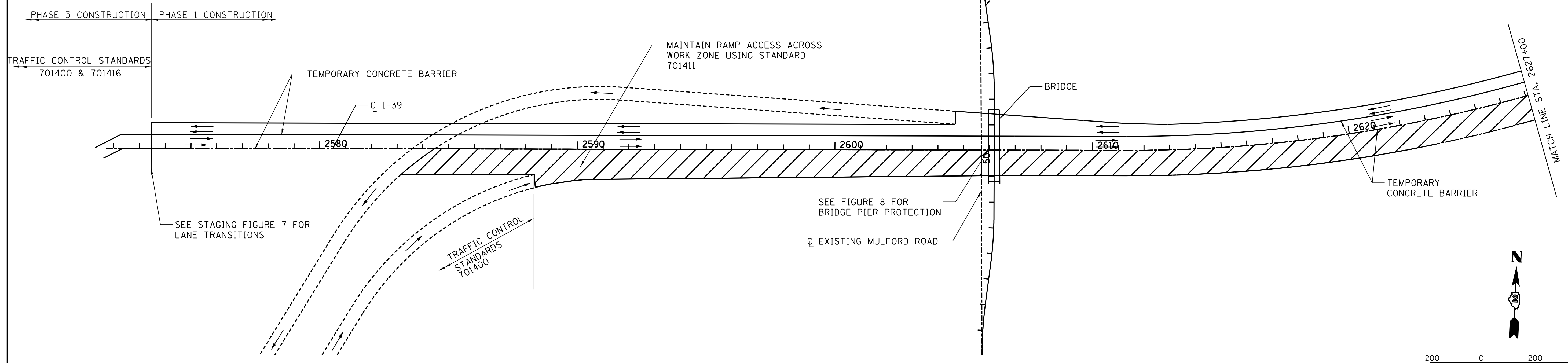
WORK ZONE



TRAFFIC FLOW

PHASE 3 CONSTRUCTION | PHASE 1 CONSTRUCTION

TRAFFIC CONTROL STANDARDS
701400 & 701416

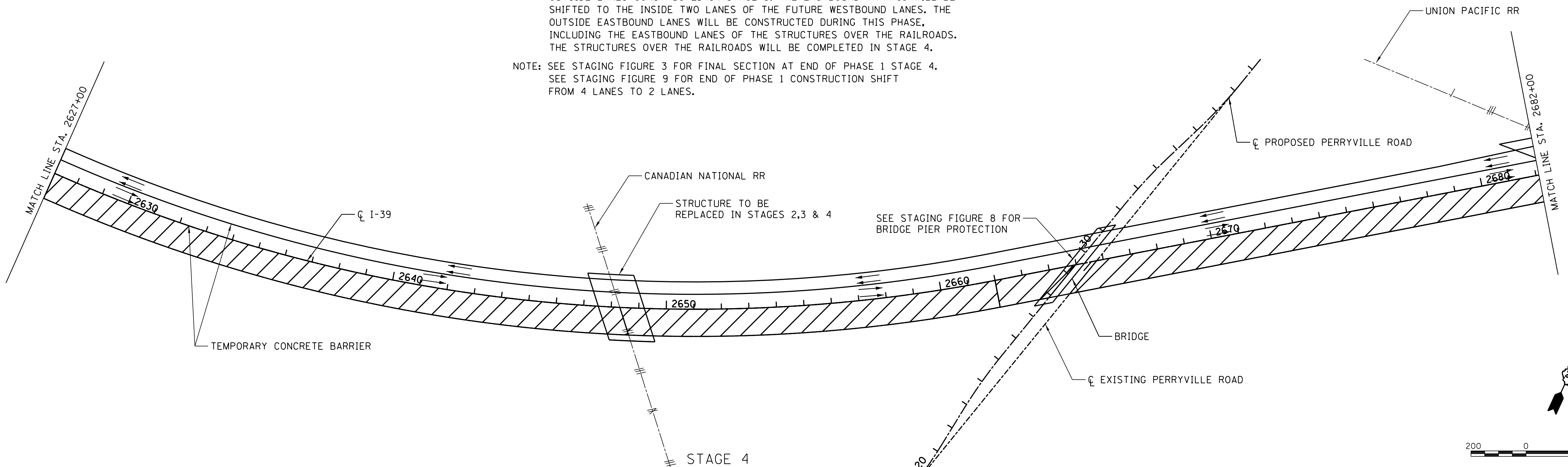


STAGE 4

CONSTRUCTION SUMMARY

A. US 20/1-39 - THE WESTBOUND TRAFFIC IS SHIFTED TO THE WESTBOUND OUTSIDE LANES CONSTRUCTED IN STAGE 3. THE EASTBOUND TRAFFIC WILL BE SHIFTED TO THE INSIDE TWO LANES OF THE FUTURE WESTBOUND LANES. THE OUTSIDE EASTBOUND LANES WILL BE CONSTRUCTED DURING THIS PHASE, INCLUDING THE EASTBOUND LANES OF THE STRUCTURES OVER THE RAILROADS. THE STRUCTURES OVER THE RAILROADS WILL BE COMPLETED IN STAGE 4.

NOTE: SEE STAGING FIGURE 3 FOR FINAL SECTION AT END OF PHASE 1 STAGE 4. SEE STAGING FIGURE 9 FOR END OF PHASE 1 CONSTRUCTION SHIFT FROM 4 LANES TO 2 LANES.



STAGE 4

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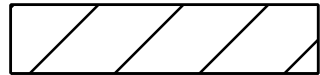
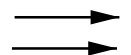
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

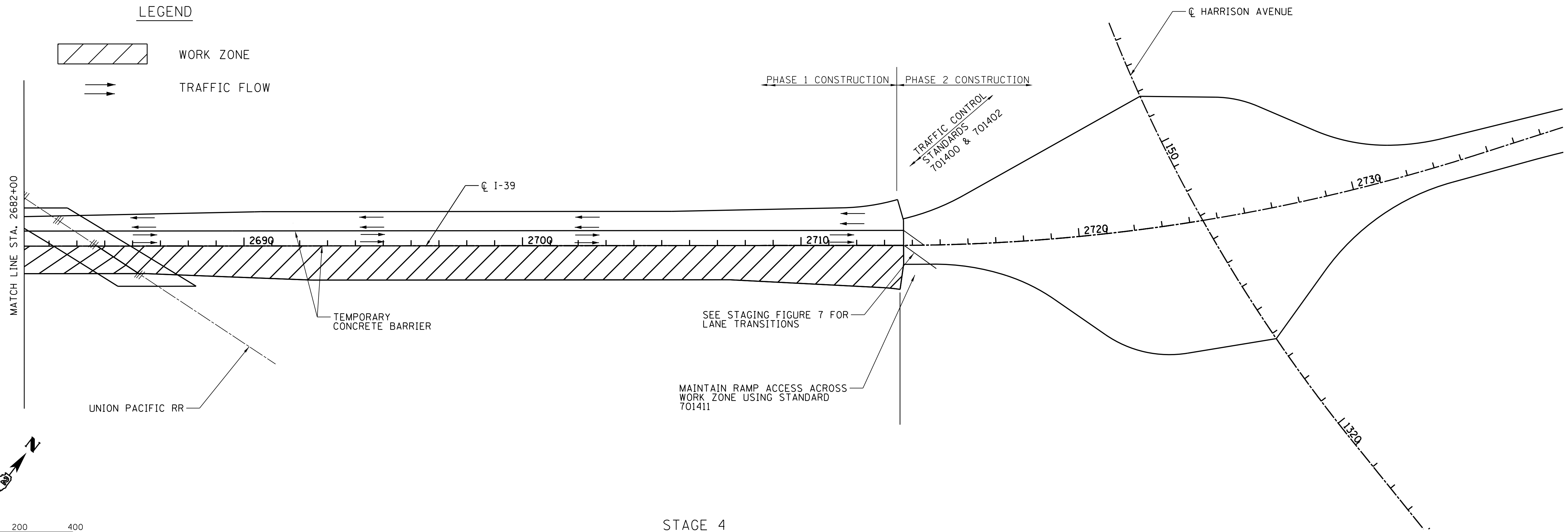
**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
STAGE CONSTRUCTION PHASE 1 STAGE 4
JOB NO. P-92-111-06**

SCALE: 1"=200' SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1,5)R	WINNEBAGO	xx	60
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

LEGEND

-  WORK ZONE
-  TRAFFIC FLOW



STAGE 4

CONSTRUCTION SUMMARY

- A. US 20/1-39 - THE WESTBOUND TRAFFIC IS SHIFTED TO THE WESTBOUND OUTSIDE LANES CONSTRUCTED IN STAGE 3. THE EASTBOUND TRAFFIC WILL BE SHIFTED TO THE INSIDE TWO LANES OF THE FUTURE WESTBOUND LANES. THE OUTSIDE EASTBOUND LANES WILL BE CONSTRUCTED DURING THIS PHASE, INCLUDING THE EASTBOUND LANES OF THE STRUCTURES OVER THE RAILROADS. THE STRUCTURES OVER THE RAILROADS WILL BE COMPLETED IN STAGE 4.
- NOTE: SEE STAGING FIGURE 3 FOR FINAL SECTION AT END OF PHASE 1 STAGE 4.
SEE STAGING FIGURE 9 FOR END OF PHASE 1 CONSTRUCTION SHIFT FROM 4 LANES TO 2 LANES.

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stage	PLOT DATE = 3/30/2017	DATE -	REVISED -

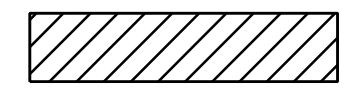
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
STAGE CONSTRUCTION PHASE 1 STAGE 4
JOB NO. P-92-111-06

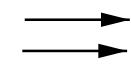
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

LEGEND



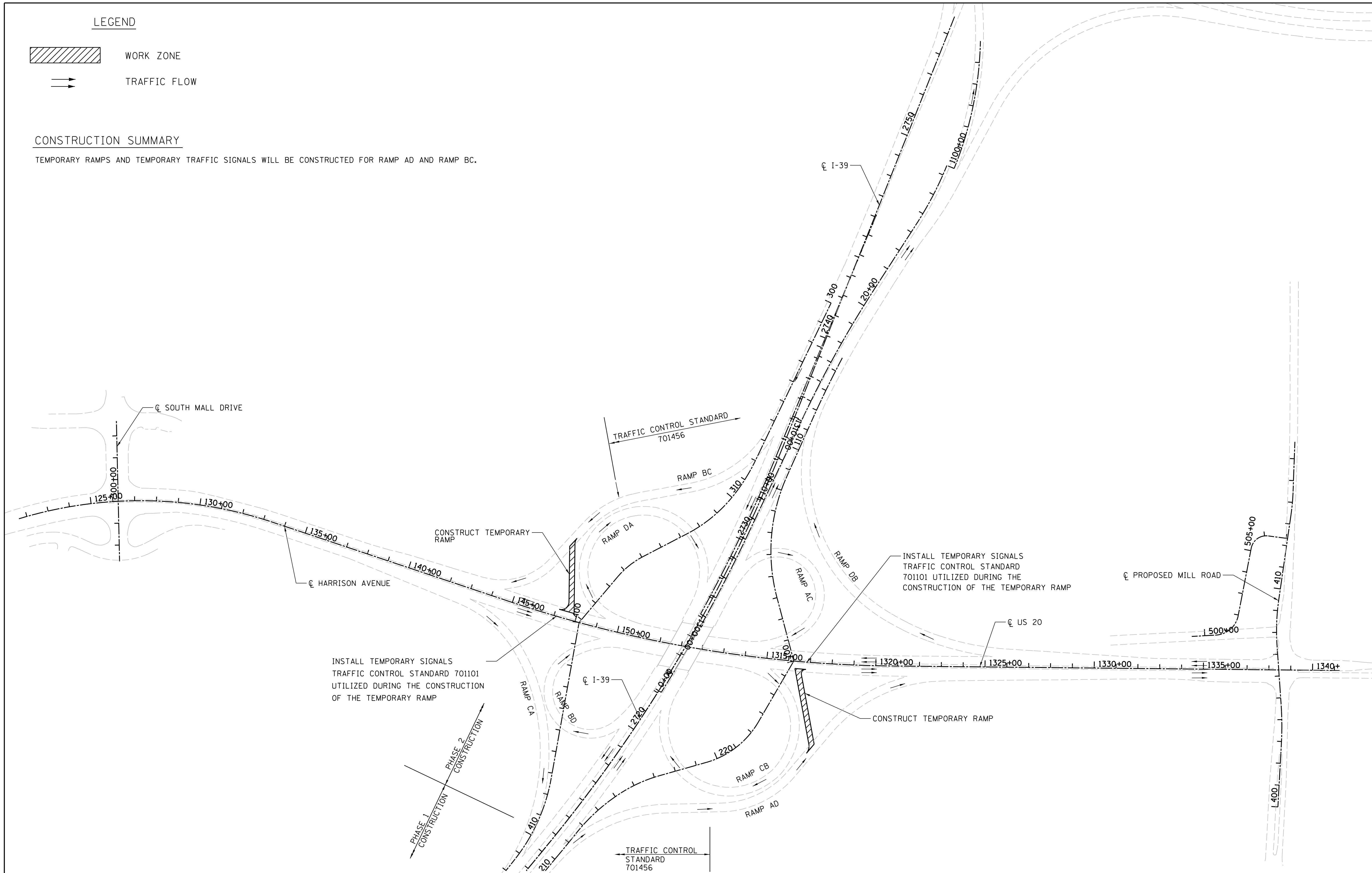
WORK ZONE



TRAFFIC FLOW

CONSTRUCTION SUMMARY

TEMPORARY RAMPS AND TEMPORARY TRAFFIC SIGNALS WILL BE CONSTRUCTED FOR RAMP AD AND RAMP BC.



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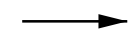

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
STAGE CONSTRUCTION PHASE 2 STAGE 1
JOB NO. P-92-111-06

SCALE: 1"=200' SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

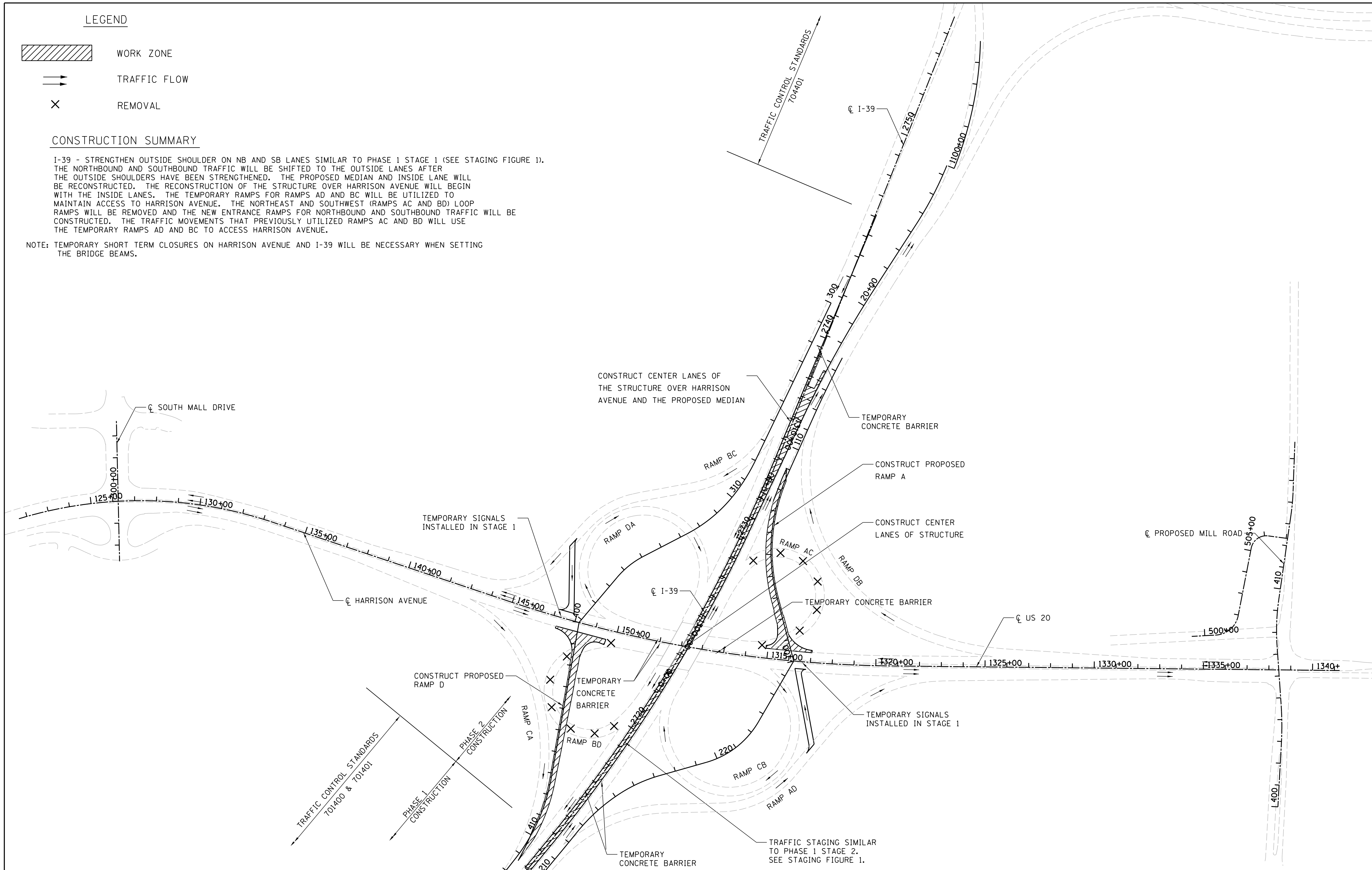
LEGEND

-  WORK ZONE
-  TRAFFIC FLOW
-  REMOVAL

CONSTRUCTION SUMMARY

I-39 - STRENGTHEN OUTSIDE SHOULDER ON NB AND SB LANES SIMILAR TO PHASE 1 STAGE 1 (SEE STAGING FIGURE 1). THE NORTHBOUND AND SOUTHBOUND TRAFFIC WILL BE SHIFTED TO THE OUTSIDE LANES AFTER THE OUTSIDE SHOULDERS HAVE BEEN STRENGTHENED. THE PROPOSED MEDIAN AND INSIDE LANE WILL BE RECONSTRUCTED. THE RECONSTRUCTION OF THE STRUCTURE OVER HARRISON AVENUE WILL BEGIN WITH THE INSIDE LANES. THE TEMPORARY RAMPS FOR RAMPS AD AND BC WILL BE UTILIZED TO MAINTAIN ACCESS TO HARRISON AVENUE. THE NORTHEAST AND SOUTHWEST (RAMPS AC AND BD) LOOP RAMPS WILL BE REMOVED AND THE NEW ENTRANCE RAMPS FOR NORTHBOUND AND SOUTHBOUND TRAFFIC WILL BE CONSTRUCTED. THE TRAFFIC MOVEMENTS THAT PREVIOUSLY UTILIZED RAMPS AC AND BD WILL USE THE TEMPORARY RAMPS AD AND BC TO ACCESS HARRISON AVENUE.

NOTE: TEMPORARY SHORT TERM CLOSURES ON HARRISON AVENUE AND I-39 WILL BE NECESSARY WHEN SETTING THE BRIDGE BEAMS.



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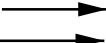

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
STAGE CONSTRUCTION PHASE 2 STAGE 2
JOB NO. P-92-111-06**

SCALE: 1"=200' SHEET NO. OF SHEETS STA. TO STA.

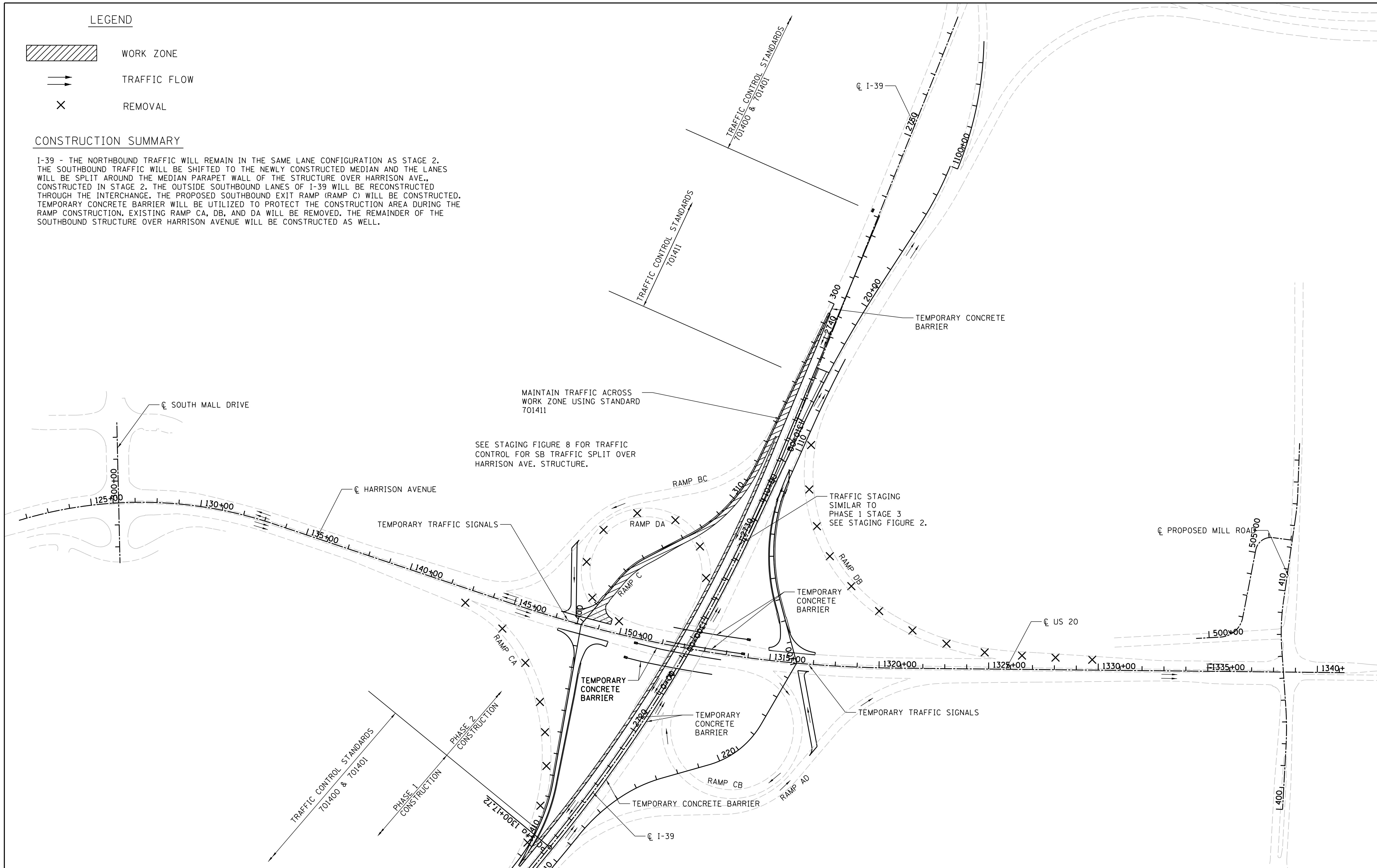
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06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

LEGEND

-  WORK ZONE
-  TRAFFIC FLOW
-  REMOVAL

CONSTRUCTION SUMMARY

I-39 - THE NORTHBOUND TRAFFIC WILL REMAIN IN THE SAME LANE CONFIGURATION AS STAGE 2. THE SOUTHBOUND TRAFFIC WILL BE SHIFTED TO THE NEWLY CONSTRUCTED MEDIAN AND THE LANES WILL BE SPLIT AROUND THE MEDIAN PARAPET WALL OF THE STRUCTURE OVER HARRISON AVE.. CONSTRUCTED IN STAGE 2. THE OUTSIDE SOUTHBOUND LANES OF I-39 WILL BE RECONSTRUCTED THROUGH THE INTERCHANGE. THE PROPOSED SOUTHBOUND EXIT RAMP (RAMP C) WILL BE CONSTRUCTED. TEMPORARY CONCRETE BARRIER WILL BE UTILIZED TO PROTECT THE CONSTRUCTION AREA DURING THE RAMP CONSTRUCTION. EXISTING RAMP CA, DB, AND DA WILL BE REMOVED. THE REMAINDER OF THE SOUTHBOUND STRUCTURE OVER HARRISON AVENUE WILL BE CONSTRUCTED AS WELL.



FILE NAME =	USER NAME = Johns00944	DESIGNED - J.M.	REVISED -
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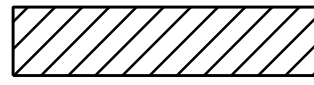
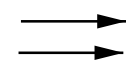

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
STAGE CONSTRUCTION PHASE 2 STAGE 3
JOB NO. P-92-111-06

SCALE: 1"=200' SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1,5)R	WINNEBAGO	xx	64
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

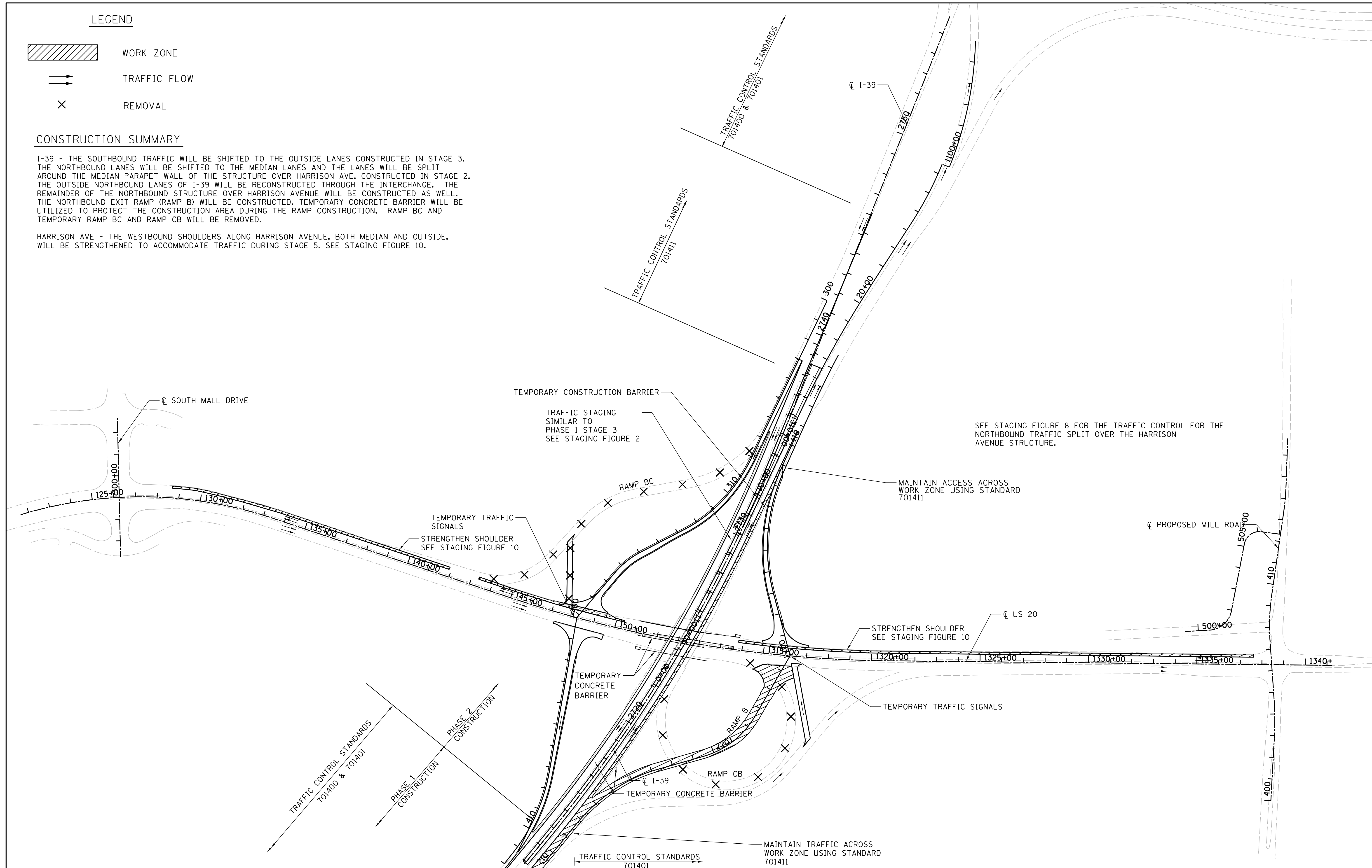
LEGEND

-  WORK ZONE
-  TRAFFIC FLOW
-  REMOVAL

CONSTRUCTION SUMMARY

I-39 - THE SOUTHBOUND TRAFFIC WILL BE SHIFTED TO THE OUTSIDE LANES CONSTRUCTED IN STAGE 3. THE NORTHBOUND LANES WILL BE SHIFTED TO THE MEDIAN LANES AND THE LANES WILL BE SPLIT AROUND THE MEDIAN PARAPET WALL OF THE STRUCTURE OVER HARRISON AVE. CONSTRUCTED IN STAGE 2. THE OUTSIDE NORTHBOUND LANES OF I-39 WILL BE RECONSTRUCTED THROUGH THE INTERCHANGE. THE REMAINDER OF THE NORTHBOUND STRUCTURE OVER HARRISON AVENUE WILL BE CONSTRUCTED AS WELL. THE NORTHBOUND EXIT RAMP (RAMP B) WILL BE CONSTRUCTED. TEMPORARY CONCRETE BARRIER WILL BE UTILIZED TO PROTECT THE CONSTRUCTION AREA DURING THE RAMP CONSTRUCTION. RAMP BC AND TEMPORARY RAMP BC AND RAMP CB WILL BE REMOVED.

HARRISON AVE - THE WESTBOUND SHOULDERS ALONG HARRISON AVENUE, BOTH MEDIAN AND OUTSIDE, WILL BE STRENGTHENED TO ACCOMMODATE TRAFFIC DURING STAGE 5. SEE STAGING FIGURE 10.



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FILE NAME =	USER NAME = Johns00944	DESIGNED -	REVISED -
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stage	PLOT SCALE = 200.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 3/30/2017	DATE -	REVISED -

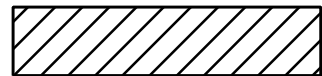
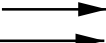

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
STAGE CONSTRUCTION PHASE 2 STAGE 4
JOB NO. P-92-111-06

SCALE: 1"=200' SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1,5)R	WINNEBAGO	xx	65
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

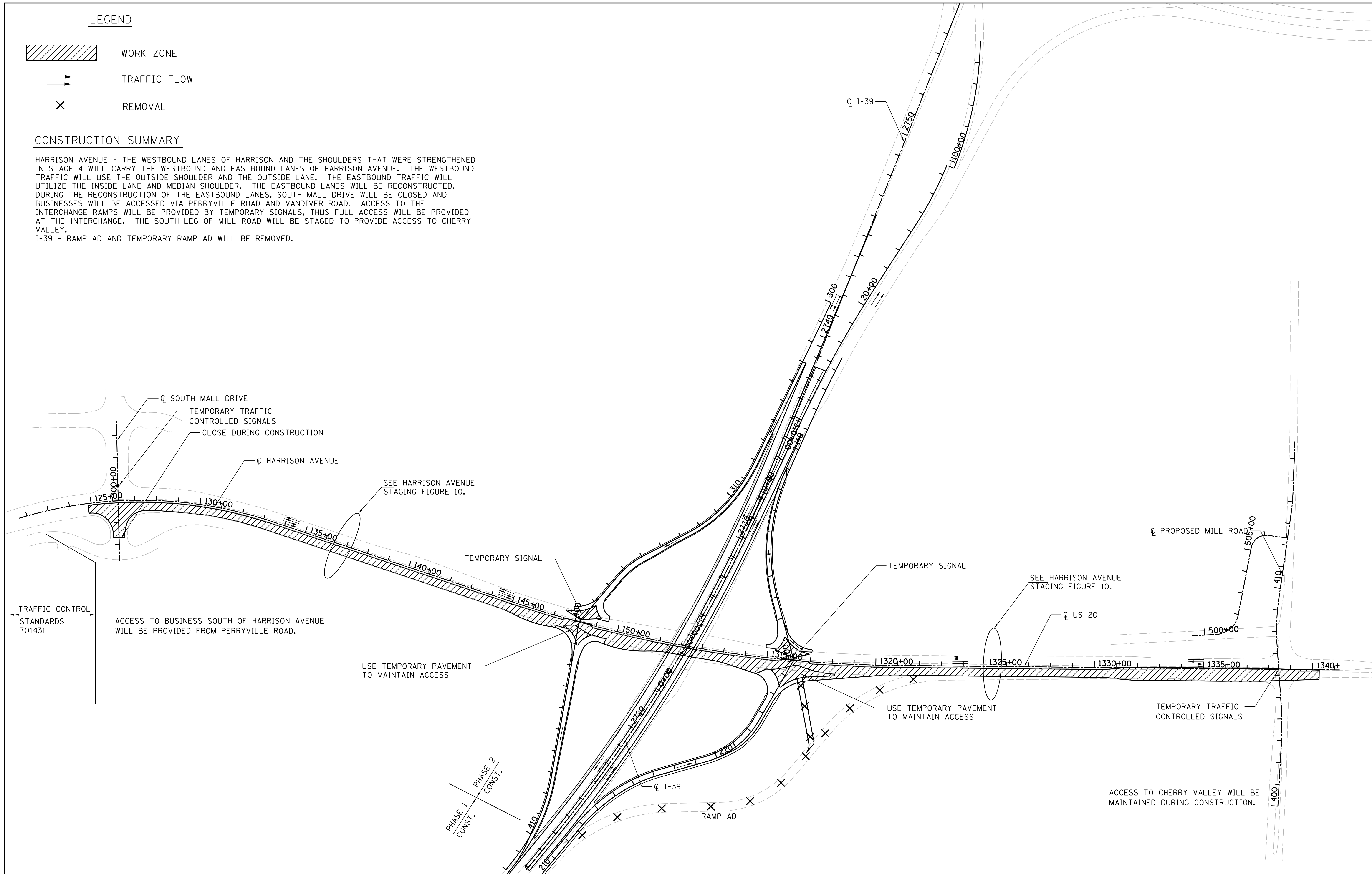
LEGEND

-  WORK ZONE
-  TRAFFIC FLOW
-  REMOVAL

CONSTRUCTION SUMMARY

HARRISON AVENUE - THE WESTBOUND LANES OF HARRISON AND THE SHOULDERS THAT WERE STRENGTHENED IN STAGE 4 WILL CARRY THE WESTBOUND AND EASTBOUND LANES OF HARRISON AVENUE. THE WESTBOUND TRAFFIC WILL USE THE OUTSIDE SHOULDER AND THE OUTSIDE LANE. THE EASTBOUND TRAFFIC WILL UTILIZE THE INSIDE LANE AND MEDIAN SHOULDER. THE EASTBOUND LANES WILL BE RECONSTRUCTED. DURING THE RECONSTRUCTION OF THE EASTBOUND LANES, SOUTH MALL DRIVE WILL BE CLOSED AND BUSINESSES WILL BE ACCESSED VIA PERRYVILLE ROAD AND VANDIVER ROAD. ACCESS TO THE INTERCHANGE RAMP WILL BE PROVIDED BY TEMPORARY SIGNALS, THUS FULL ACCESS WILL BE PROVIDED AT THE INTERCHANGE. THE SOUTH LEG OF MILL ROAD WILL BE STAGED TO PROVIDE ACCESS TO CHERRY VALLEY.

I-39 - RAMP AD AND TEMPORARY RAMP AD WILL BE REMOVED.



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		DATE -	REVISED -
stage	PLOT DATE = 3/30/2017		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
STAGE CONSTRUCTION PHASE 2 STAGE 5
JOB NO. P-92-111-06**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1,5)R	WINNEBAGO	xx	66
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

SCALE: 1"=200' SHEET NO. OF SHEETS STA. TO STA.

LEGEND



NOTE: AFTER THE COMPLETION OF STAGE 6 THE HARRISON AVENUE LANES ADJACENT TO THE MEDIAN WILL BE TEMPORARILY CLOSED TO COMPLETE THE INSTALLATION OF THE MEDIAN SURFACE.

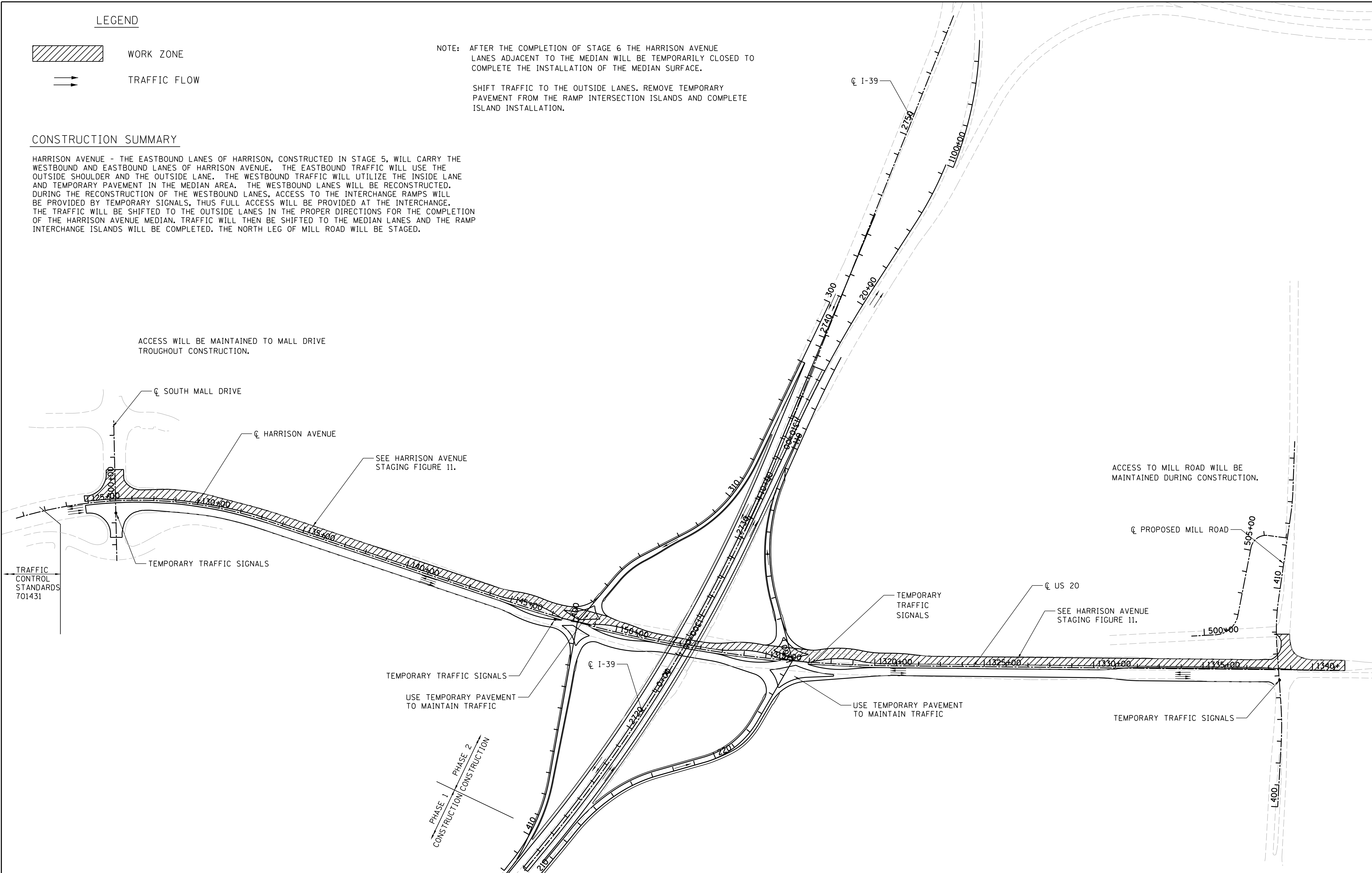
SHIFT TRAFFIC TO THE OUTSIDE LANES. REMOVE TEMPORARY PAVEMENT FROM THE RAMP INTERSECTION ISLANDS AND COMPLETE ISLAND INSTALLATION.

CONSTRUCTION SUMMARY

HARRISON AVENUE - THE EASTBOUND LANES OF HARRISON, CONSTRUCTED IN STAGE 5, WILL CARRY THE WESTBOUND AND EASTBOUND LANES OF HARRISON AVENUE. THE EASTBOUND TRAFFIC WILL USE THE OUTSIDE SHOULDER AND THE OUTSIDE LANE. THE WESTBOUND TRAFFIC WILL UTILIZE THE INSIDE LANE AND TEMPORARY PAVEMENT IN THE MEDIAN AREA. THE WESTBOUND LANES WILL BE RECONSTRUCTED. DURING THE RECONSTRUCTION OF THE WESTBOUND LANES, ACCESS TO THE INTERCHANGE RAMP WILL BE PROVIDED BY TEMPORARY SIGNALS, THUS FULL ACCESS WILL BE PROVIDED AT THE INTERCHANGE. THE TRAFFIC WILL BE SHIFTED TO THE OUTSIDE LANES IN THE PROPER DIRECTIONS FOR THE COMPLETION OF THE HARRISON AVENUE MEDIAN. TRAFFIC WILL THEN BE SHIFTED TO THE MEDIAN LANES AND THE RAMP INTERCHANGE ISLANDS WILL BE COMPLETED. THE NORTH LEG OF MILL ROAD WILL BE STAGED.

ACCESS WILL BE MAINTAINED TO MALL DRIVE THROUGHOUT CONSTRUCTION.

ACCESS TO MILL ROAD WILL BE MAINTAINED DURING CONSTRUCTION.



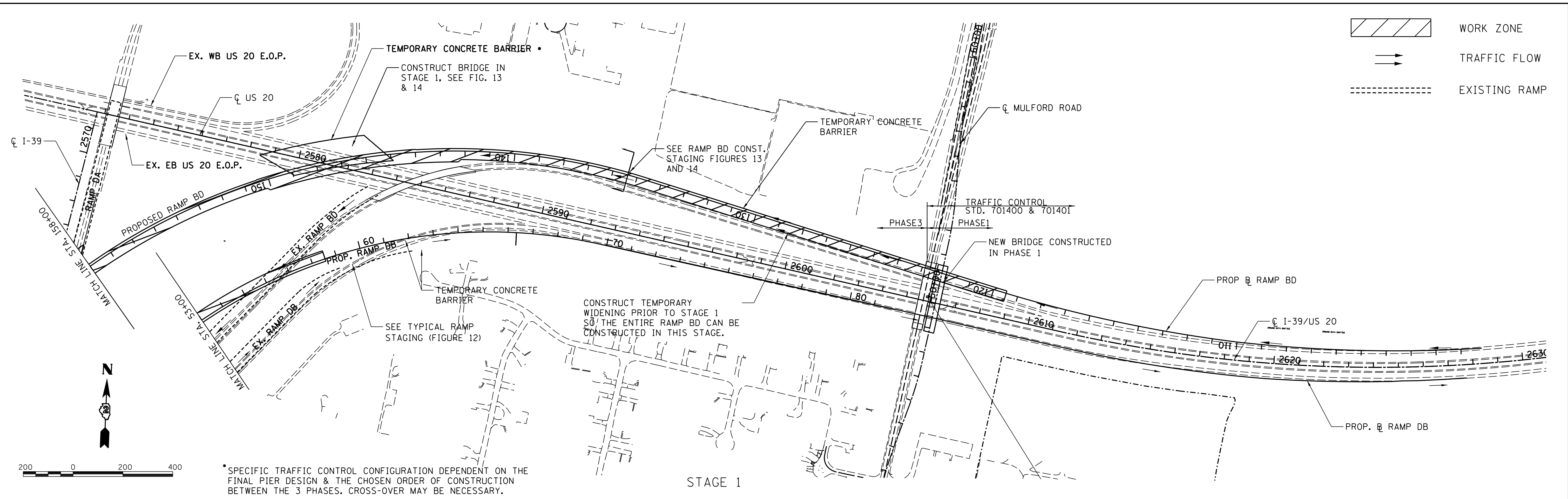
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
STAGE CONSTRUCTION PHASE 2 STAGE 6
JOB NO. P-92-111-06**

SCALE: 1"=200' SHEET NO. OF SHEETS STA. TO STA.

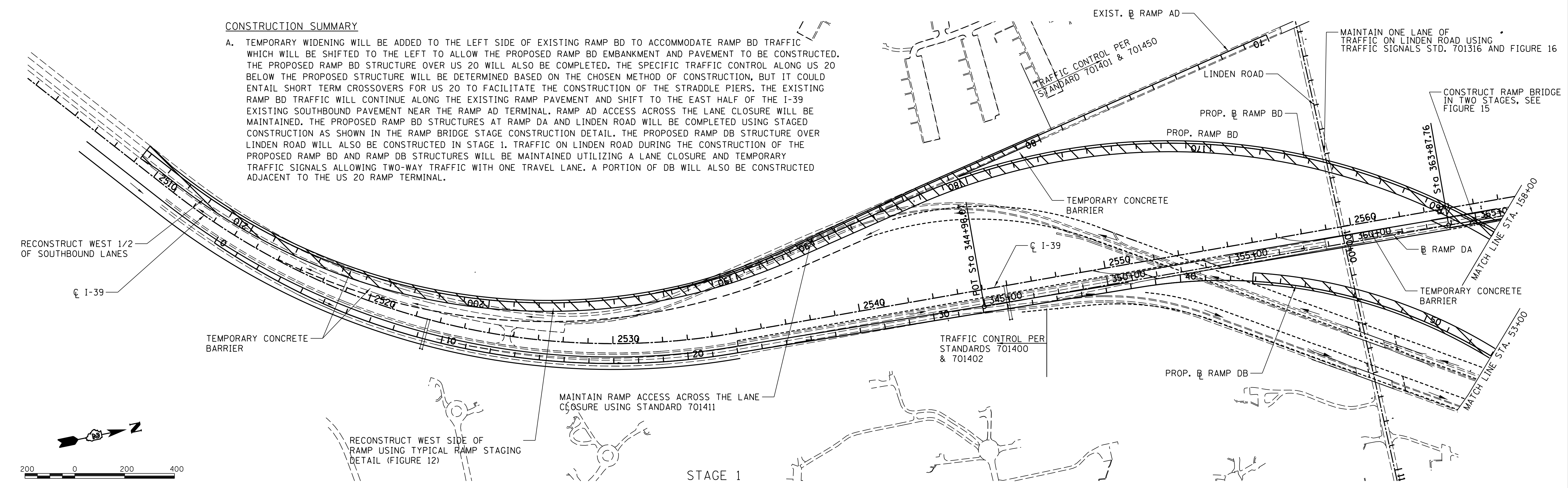
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1,5)R	WINNEBAGO	xx	67
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



* SPECIFIC TRAFFIC CONTROL CONFIGURATION DEPENDENT ON THE FINAL PIER DESIGN & THE CHOSEN ORDER OF CONSTRUCTION BETWEEN THE 3 PHASES. CROSS-OVER MAY BE NECESSARY.

CONSTRUCTION SUMMARY

A. TEMPORARY WIDENING WILL BE ADDED TO THE LEFT SIDE OF EXISTING RAMP BD TO ACCOMMODATE RAMP BD TRAFFIC WHICH WILL BE SHIFTED TO THE LEFT TO ALLOW THE PROPOSED RAMP BD EMBANKMENT AND PAVEMENT TO BE CONSTRUCTED. THE PROPOSED RAMP BD STRUCTURE OVER US 20 WILL ALSO BE COMPLETED. THE SPECIFIC TRAFFIC CONTROL ALONG US 20 BELOW THE PROPOSED STRUCTURE WILL BE DETERMINED BASED ON THE CHOSEN METHOD OF CONSTRUCTION, BUT IT COULD ENTAIL SHORT TERM CROSSEOVERS FOR US 20 TO FACILITATE THE CONSTRUCTION OF THE STRADDLE PIERS. THE EXISTING RAMP BD TRAFFIC WILL CONTINUE ALONG THE EXISTING RAMP PAVEMENT AND SHIFT TO THE EAST HALF OF THE I-39 EXISTING SOUTHBOUND PAVEMENT NEAR THE RAMP AD TERMINAL. RAMP AD ACCESS ACROSS THE LANE CLOSURE WILL BE MAINTAINED. THE PROPOSED RAMP BD STRUCTURES AT RAMP DA AND LINDEN ROAD WILL BE COMPLETED USING STAGED CONSTRUCTION AS SHOWN IN THE RAMP BRIDGE STAGE CONSTRUCTION DETAIL. THE PROPOSED RAMP DB STRUCTURE OVER LINDEN ROAD WILL ALSO BE CONSTRUCTED IN STAGE 1. TRAFFIC ON LINDEN ROAD DURING THE CONSTRUCTION OF THE PROPOSED RAMP BD AND RAMP DB STRUCTURES WILL BE MAINTAINED UTILIZING A LANE CLOSURE AND TEMPORARY TRAFFIC SIGNALS ALLOWING TWO-WAY TRAFFIC WITH ONE TRAVEL LANE. A PORTION OF DB WILL ALSO BE CONSTRUCTED ADJACENT TO THE US 20 RAMP TERMINAL.



MAINTAIN RAMP ACCESS ACROSS THE LANE CLOSURE USING STANDARD 701411

RECONSTRUCT WEST SIDE OF RAMP USING TYPICAL RAMP STAGING DETAIL (FIGURE 12)

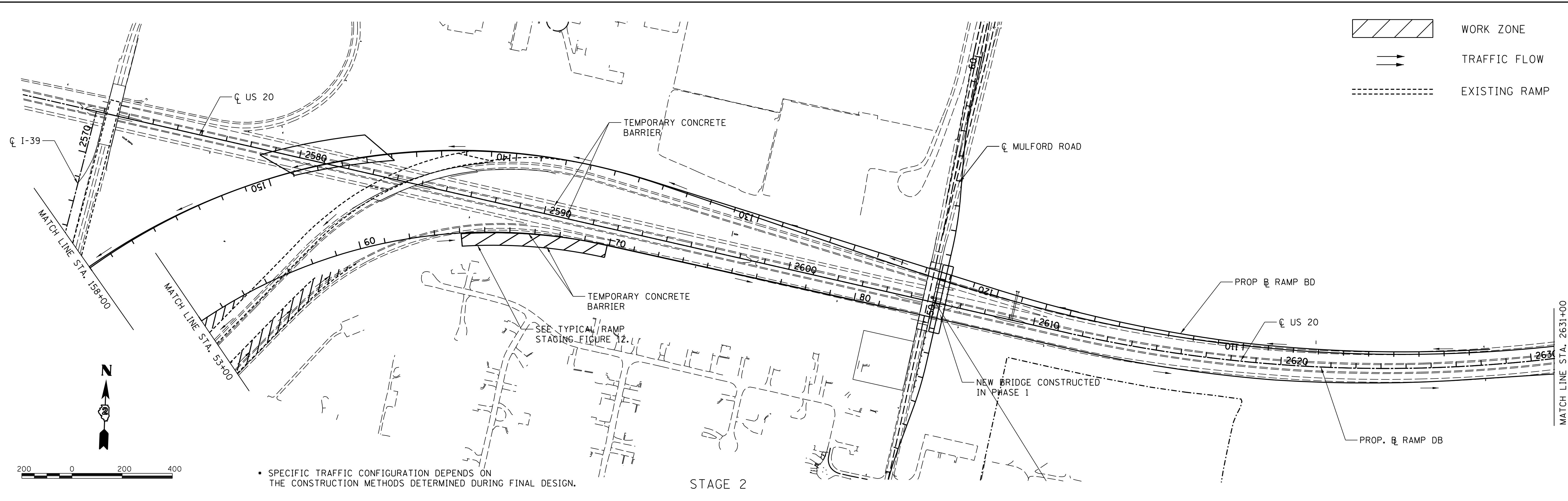
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
STAGE CONSTRUCTION PHASE 3 STAGE 1
JOB NO. P-92-111-06**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3K & (4-1,5)R)	WINNEBAGO	xx	68
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

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200.0000' / in.	-	-	-
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3/30/2017	-	-	-

SCALE: 1"=200' SHEET NO. OF SHEETS STA. TO STA.



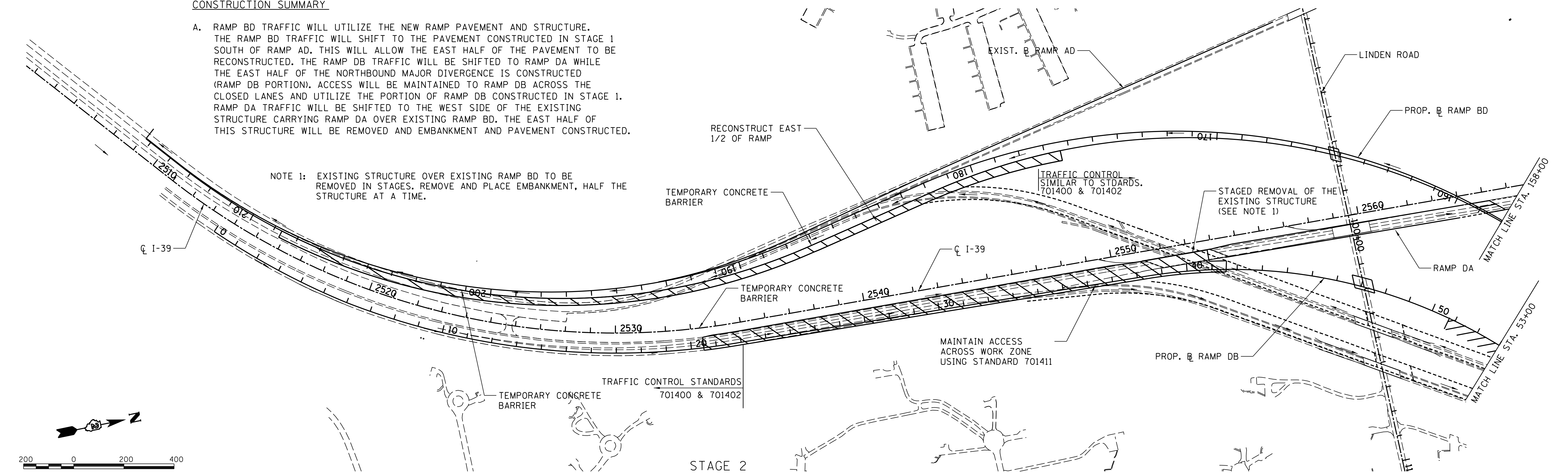
• SPECIFIC TRAFFIC CONFIGURATION DEPENDS ON THE CONSTRUCTION METHODS DETERMINED DURING FINAL DESIGN.

STAGE 2

CONSTRUCTION SUMMARY

A. RAMP BD TRAFFIC WILL UTILIZE THE NEW RAMP PAVEMENT AND STRUCTURE. THE RAMP BD TRAFFIC WILL SHIFT TO THE PAVEMENT CONSTRUCTED IN STAGE 1 SOUTH OF RAMP AD. THIS WILL ALLOW THE EAST HALF OF THE PAVEMENT TO BE RECONSTRUCTED. THE RAMP DB TRAFFIC WILL BE SHIFTED TO RAMP DA WHILE THE EAST HALF OF THE NORTHBOUND MAJOR DIVERGENCE IS CONSTRUCTED (RAMP DB PORTION). ACCESS WILL BE MAINTAINED TO RAMP DB ACROSS THE CLOSED LANES AND UTILIZE THE PORTION OF RAMP DB CONSTRUCTED IN STAGE 1. RAMP DA TRAFFIC WILL BE SHIFTED TO THE WEST SIDE OF THE EXISTING STRUCTURE CARRYING RAMP DA OVER EXISTING RAMP BD. THE EAST HALF OF THIS STRUCTURE WILL BE REMOVED AND EMBANKMENT AND PAVEMENT CONSTRUCTED.

NOTE 1: EXISTING STRUCTURE OVER EXISTING RAMP BD TO BE REMOVED IN STAGES. REMOVE AND PLACE EMBANKMENT, HALF THE STRUCTURE AT A TIME.



STAGE 2

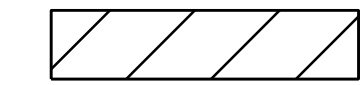
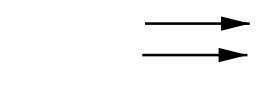
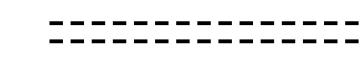
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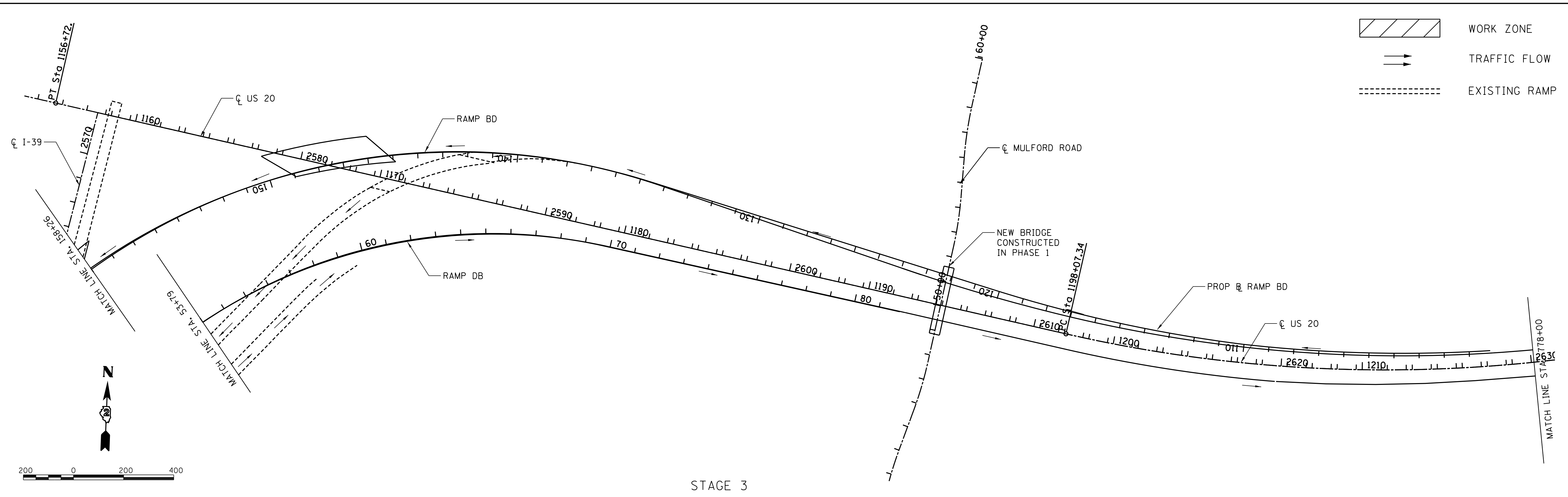
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
STAGE CONSTRUCTION PHASE 3 STAGE 2
JOB NO. P-92-111-06**

SCALE: 1"=200' SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1,5)R	WINNEBAGO	xx	69
06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

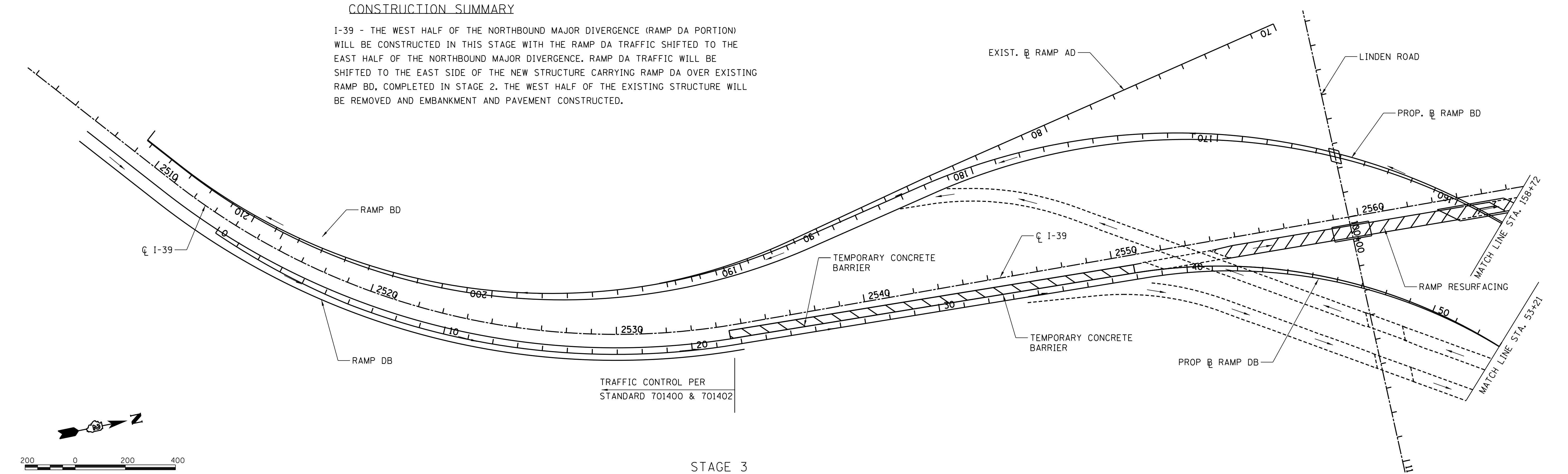
 WORK ZONE
 TRAFFIC FLOW
 EXISTING RAMP



STAGE 3

CONSTRUCTION SUMMARY

I-39 - THE WEST HALF OF THE NORTHBOUND MAJOR DIVERGENCE (RAMP DA PORTION) WILL BE CONSTRUCTED IN THIS STAGE WITH THE RAMP DA TRAFFIC SHIFTED TO THE EAST HALF OF THE NORTHBOUND MAJOR DIVERGENCE. RAMP DA TRAFFIC WILL BE SHIFTED TO THE EAST SIDE OF THE NEW STRUCTURE CARRYING RAMP DA OVER EXISTING RAMP BD, COMPLETED IN STAGE 2. THE WEST HALF OF THE EXISTING STRUCTURE WILL BE REMOVED AND EMBANKMENT AND PAVEMENT CONSTRUCTED.



STAGE 3

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stage	PLOT DATE = 3/30/2017	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 STAGE CONSTRUCTION PHASE 3 STAGE 3
 JOB NO. P-92-111-06**

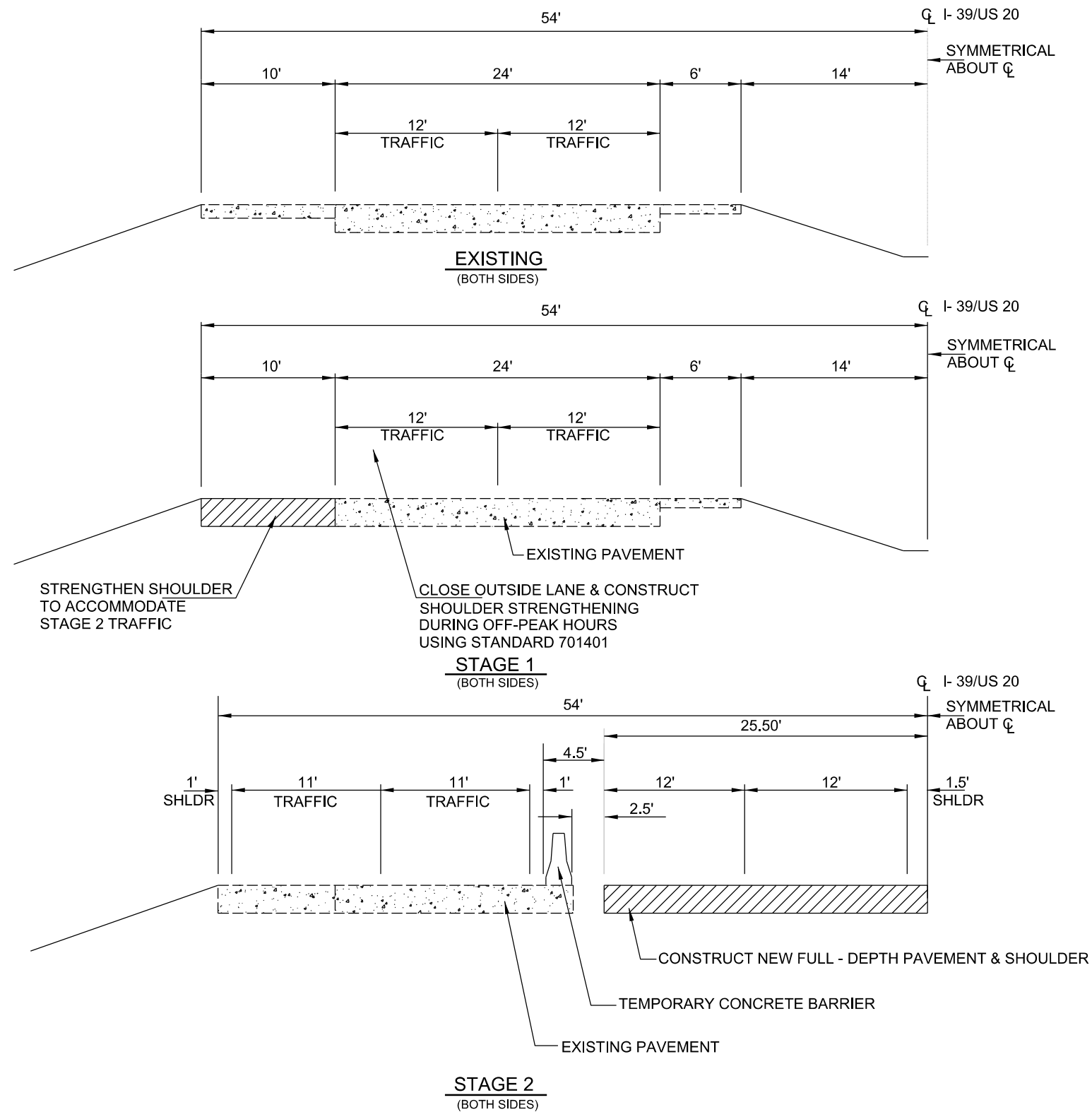
SCALE: 1"=200' SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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06S2055		CONTRACT NO.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

Johns00944

6/1/2016

I:\06\jobs\06S2055\Admin\4-Reports\1-STAGING\FIGURE 1 - TYPICAL PH Idgn

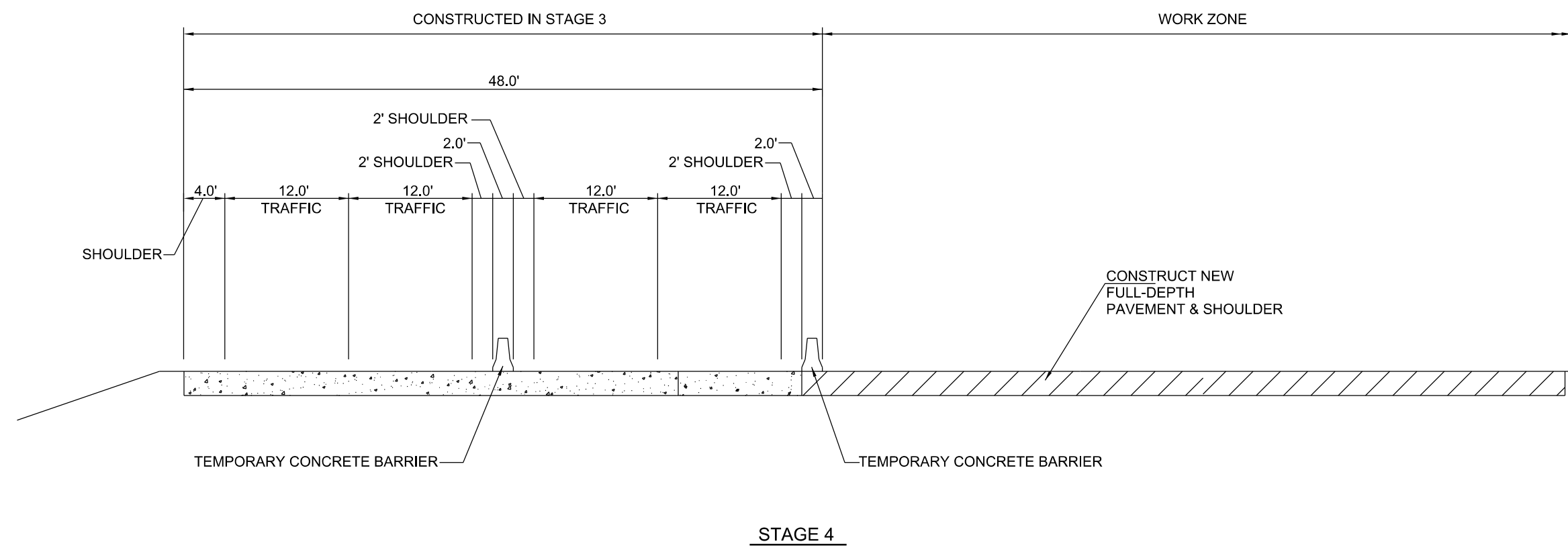
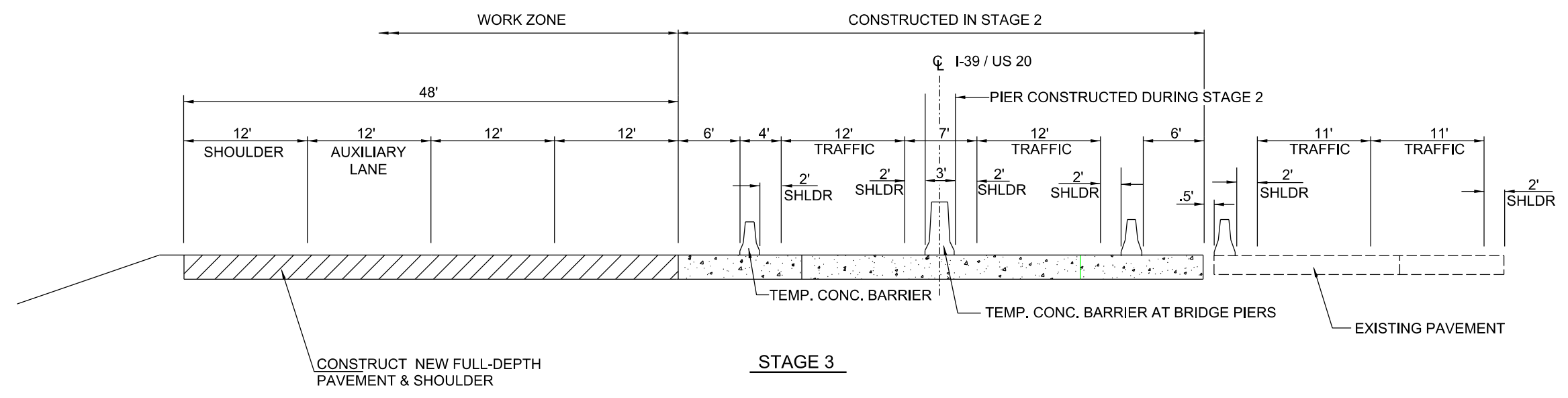


TYPICAL SECTIONS

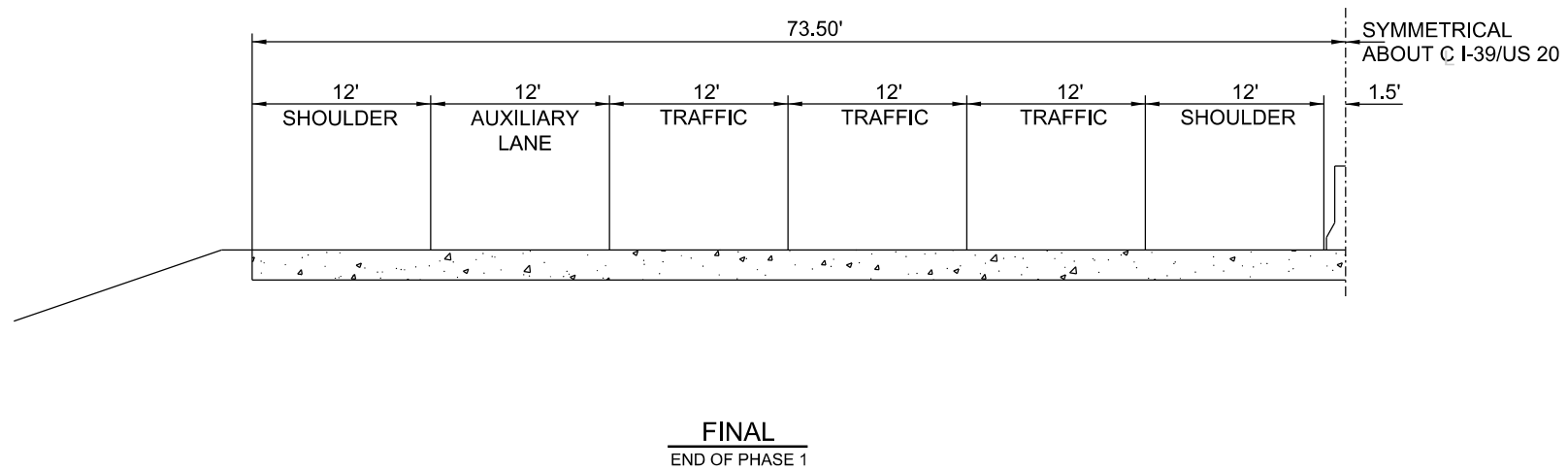


STAGE CONSTRUCTION PHASE 1	
FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)	
SECTIONS (201-3) K & (4-1,5) K	
WINNEBAGO COUNTY	
JOB NO. P-92-111-06	
HANSON NO. 06S2055	STAGING FIGURE NO. 1

I:\06_jobs\06S2055\Admin\4-Reports\1-STAGING\FIGURE 2 - TYPICAL PH 1.dgn 6/1/2016 Johns00944

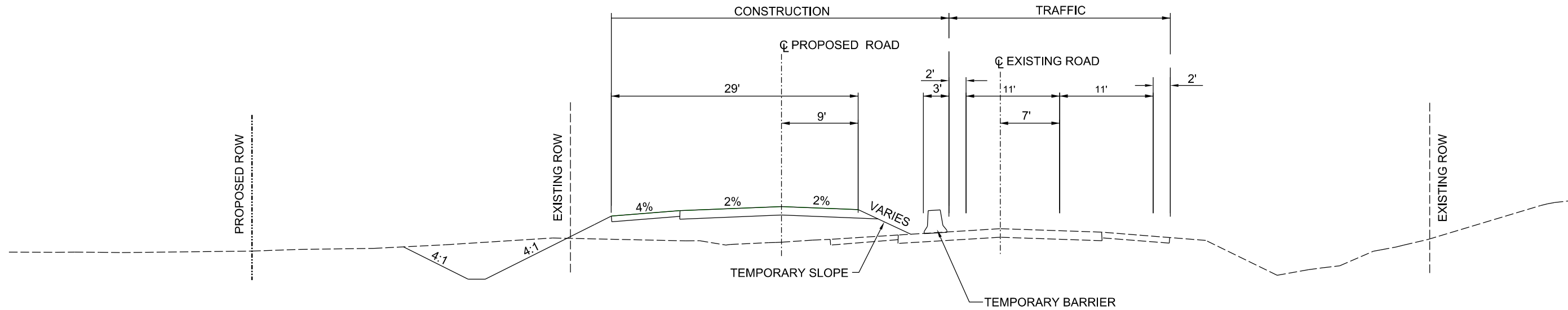


STAGE CONSTRUCTION PHASE 1
 FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 SECTIONS (201-3) K & (4-1,5) K
 WINNEBAGO COUNTY
 JOB NO. P-92-111-06
 HANSON NO. 06S2055 STAGING FIGURE NO. 2

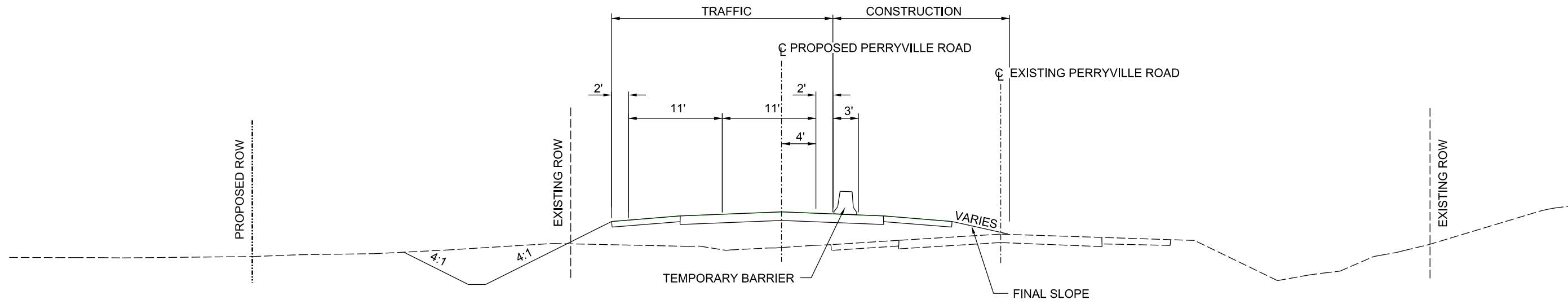


STAGE CONSTRUCTION PHASE 1	
FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20) SECTIONS (201-3) K & (4-1,5) K WINNEBAGO COUNTY JOB NO. P-92-111-06	
HANSON NO. 06S2055	STAGING FIGURE NO. 3

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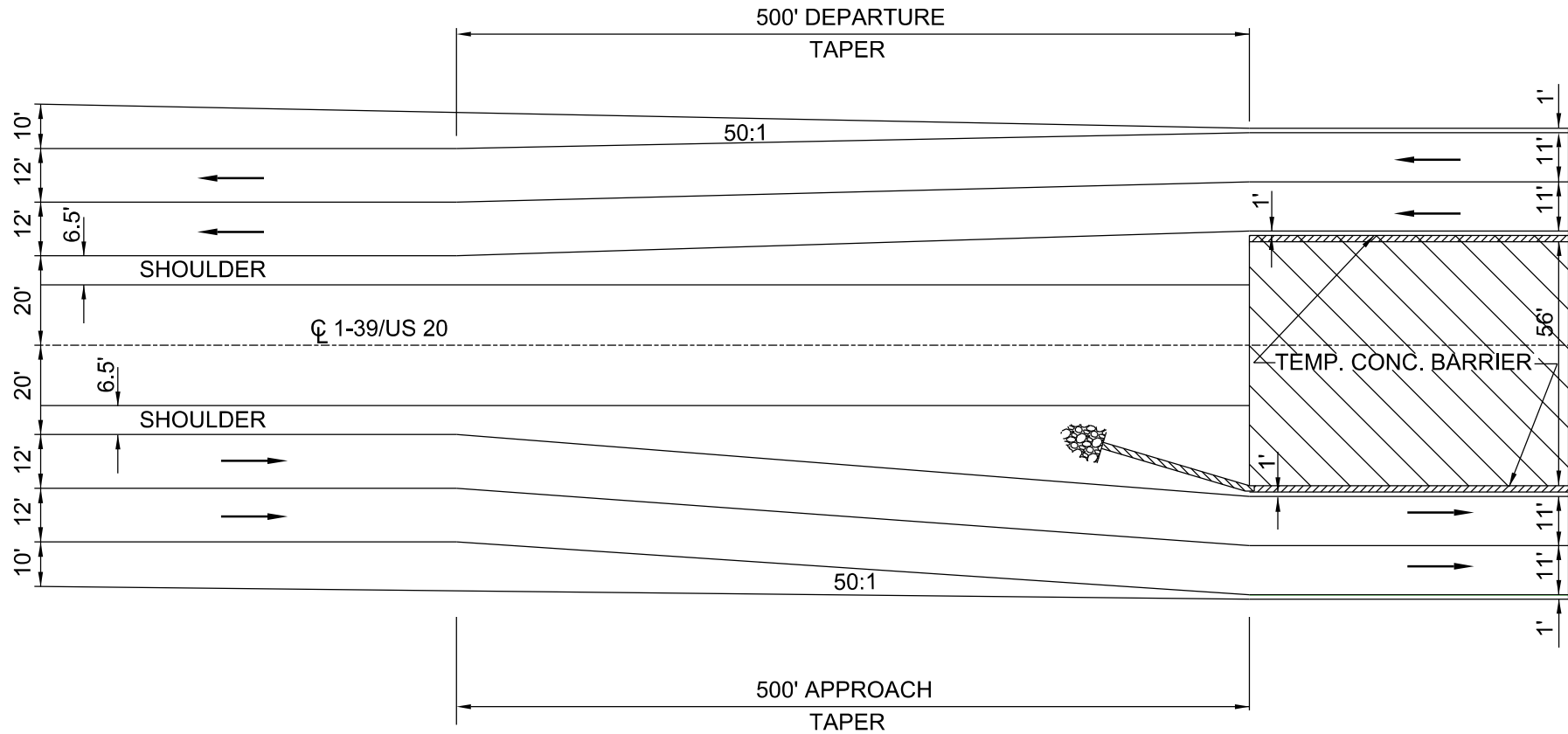
PERRYVILLE & MULFORD ROAD - STAGE 1



PERRYVILLE ROAD - STAGE 2
MULFORD ROAD SIMILAR



STAGE CONSTRUCTION PHASE 1
FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20) SECTIONS (201-3) K & (4-1,5) K WINNEBAGO COUNTY JOB NO. P-92-111-06
HANSON NO. 06S2055 STAGING FIGURE NO. 4



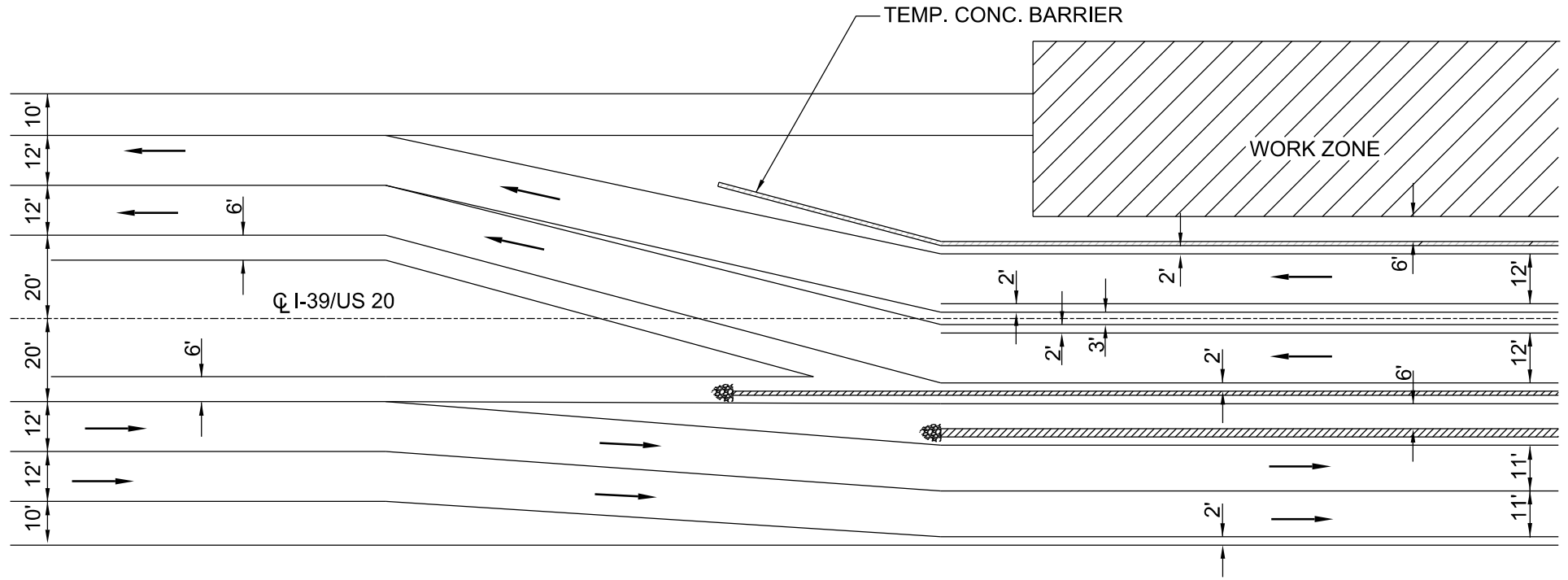
TRAFFIC CONTROL BEGIN & END PHASE 1
STAGE 2

TRAFFIC CONTROL SIMILAR TO STANDARDS
 701400 & 701402 WITH NO LANE CLOSURE

- LEGEND**
- TRAFFIC
 - WORK ZONE
 - ATTENUATOR



STAGE CONSTRUCTION PHASE 1
FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20) SECTIONS (201-3) K & (4-1,5) K WINNEBAGO COUNTY JOB NO. P-92-111-06
STAGING FIGURE NO. 5



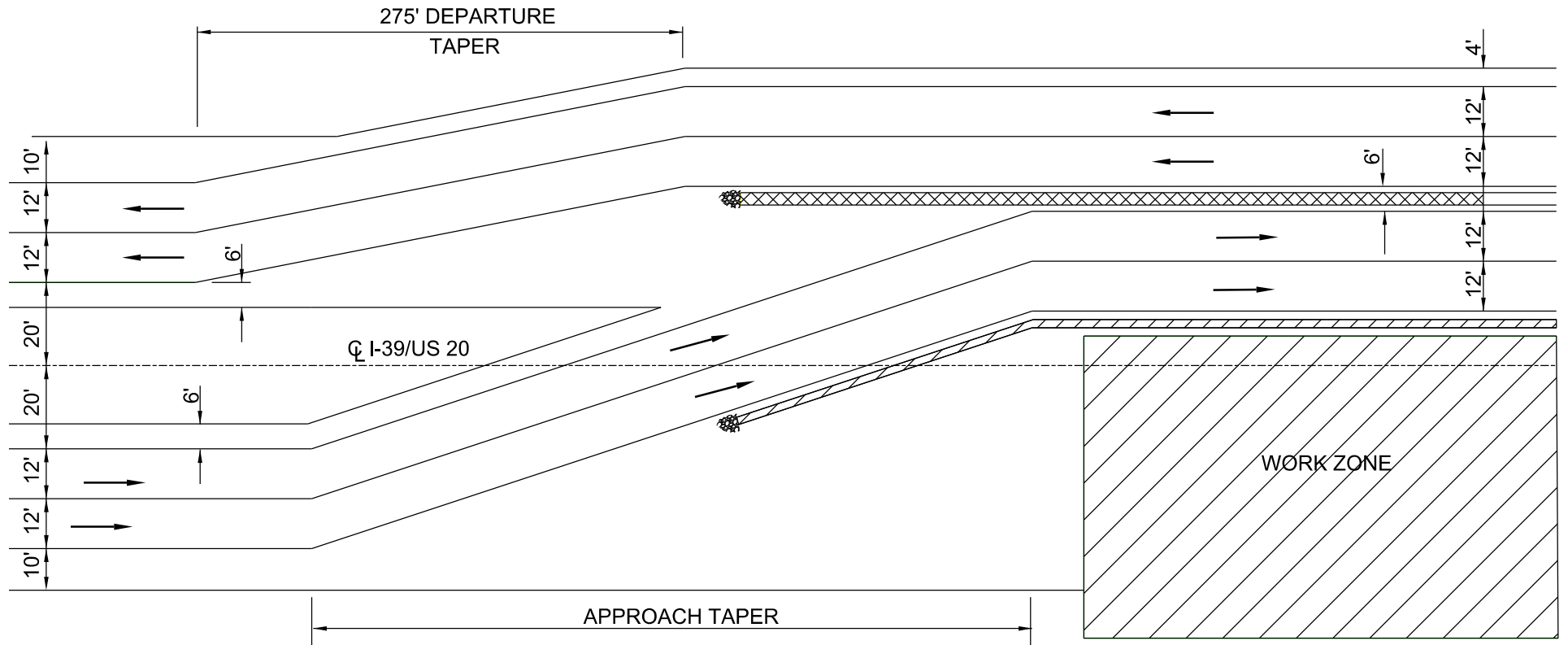
TRAFFIC CONTROL BEGIN AND END PHASE 1
STAGE 3

TRAFFIC CONTROL SIMILAR TO STANDARDS
 701400 & 701416 WITH NO LANE CLOSURE

- LEGEND**
- TRAFFIC
 - WORK ZONE
 - IMPACT ATTENUATOR



STAGE CONSTRUCTION PHASE 1
FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20) SECTIONS (201-3) K & (4-1,5) K WINNEBAGO COUNTY JOB NO. P-92-111-06
STAGING FIGURE NO. 6



TRAFFIC CONTROL BEGIN & END PHASE 1

STAGE 4





TRAFFIC CONTROL SIMILAR TO STANDARDS 701400 & 701416 WITH NO LANE CLOSURE

STAGE CONSTRUCTION PHASE 1

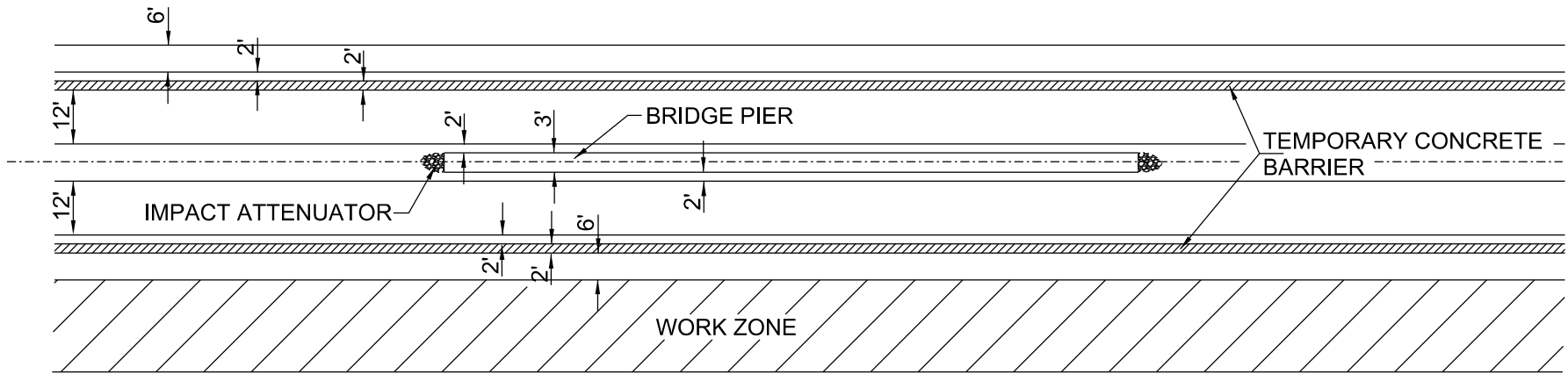
FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
SECTIONS (201-3) K & (4-1,5) K
WINNEBAGO COUNTY
JOB NO. P-92-111-06

STAGING FIGURE NO. 7

LEGEND

-  TRAFFIC
-  WORK ZONE
-  TEMPORARY BARRIER
-  IMPACT ATTENUATOR









USE TEMPORARY CONCRETE BARRIER & IMPACT ATTENUATOR AT BRIDGE PIER.

STAGE 3

LEGEND

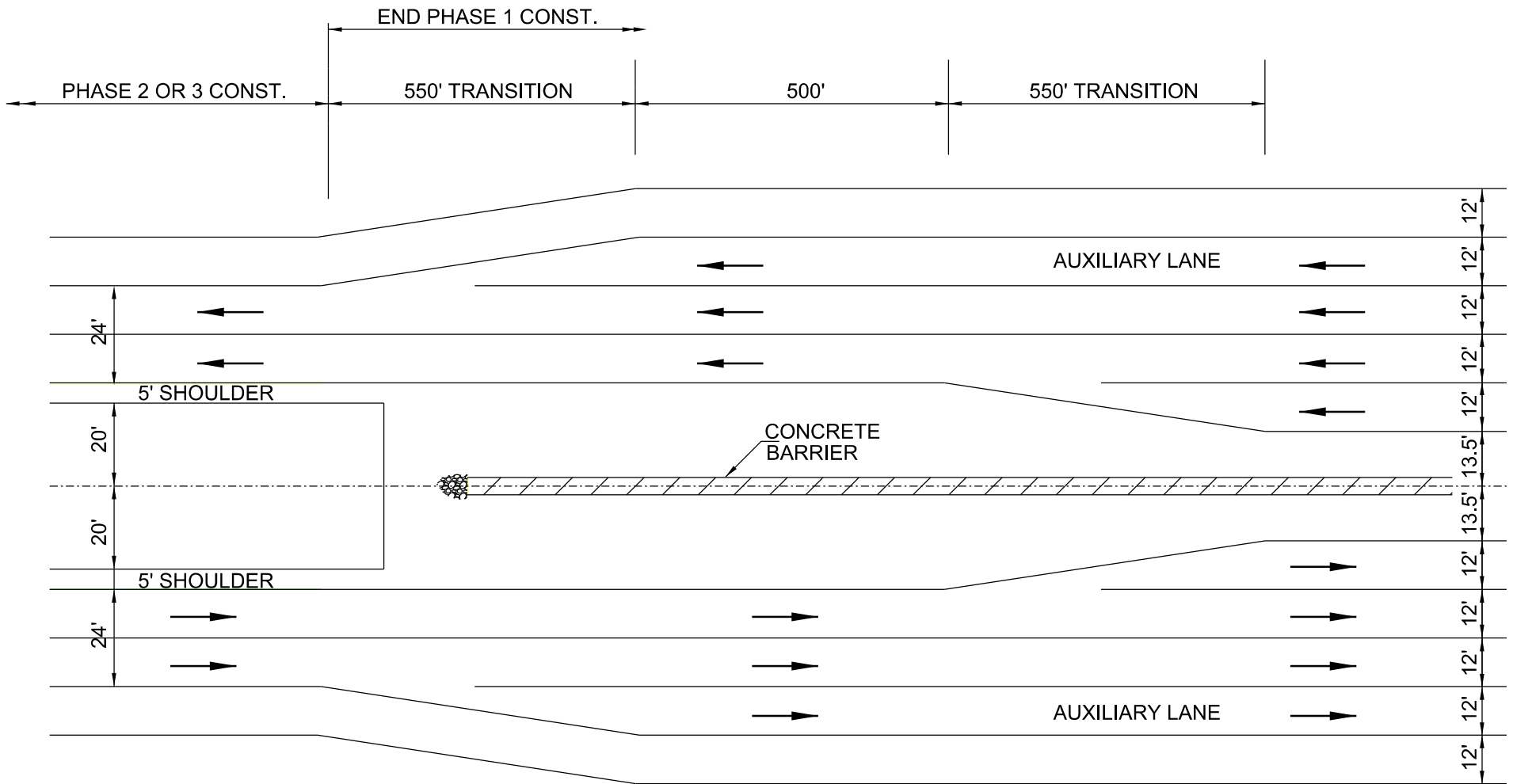
-  TRAFFIC
-  WORK ZONE
-  TEMPORARY BARRIER
-  IMPACT ATTENUATOR



STAGE CONSTRUCTION PHASE 1




FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 SECTIONS (201-3) K & (4-1,5) K
 WINNEBAGO COUNTY
 JOB NO. P-92-111-06

STAGING FIGURE NO. 8



LANE TRANSITIONS AT EACH END OF THE PHASE 1 CONSTRUCTION PRIOR TO CONSTRUCTION OF PHASE 2 OR 3. SHOWS TRANSITION FROM PROPOSED 4 LANE SECTION TO EXISTING 2 LANE SECTION.

LEGEND

-  TRAFFIC
-  WORK ZONE
-  IMPACT ATTENUATOR



STAGE CONSTRUCTION PHASE 1

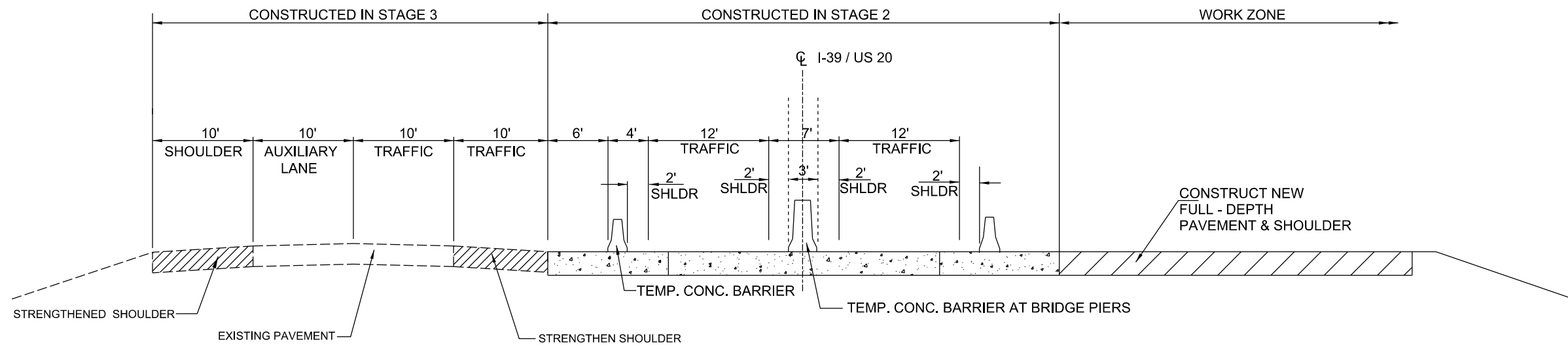
FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
 SECTIONS (201-3) K & (4-1,5) K
 WINNEBAGO COUNTY
 JOB NO. P-92-111-06

STAGING FIGURE NO. 9

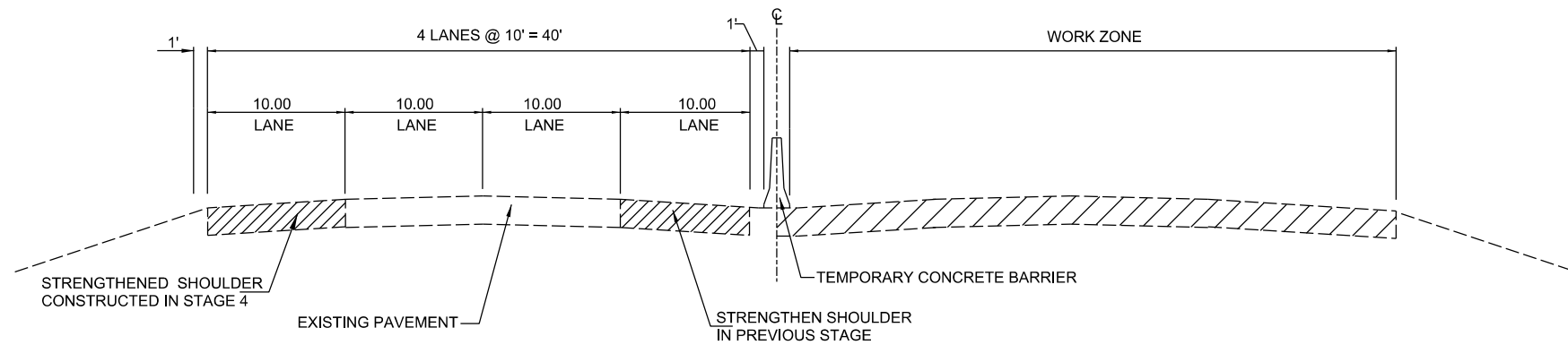
Johns00944

6/2/2016

I:\06 Jobs\06S2055\Admin\4-Reports\1-STAGING\FIGURE 10 - TYPICAL PH 2.dgn



HARRISON AVENUE
STAGE CONSTRUCTION PHASE 2 - STAGE 4



HARRISON AVENUE
STAGE CONSTRUCTION PHASE 2 - STAGE 5

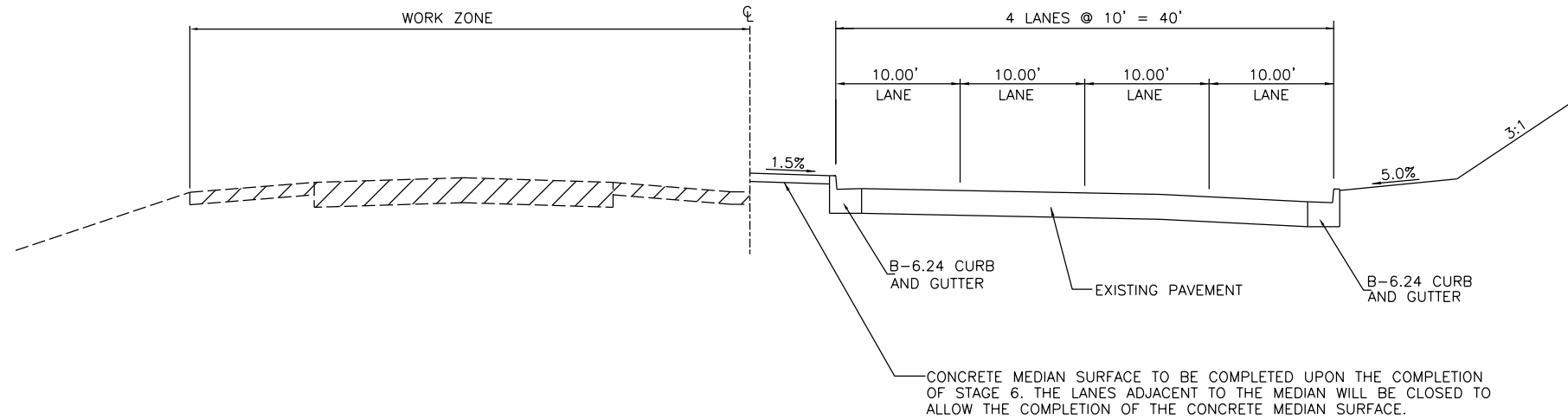


STAGE CONSTRUCTION PHASE 2	
FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)	
SECTIONS (201-3) K & (4-1,5) K	
WINNEBAGO COUNTY	
JOB NO. P-92-111-06	
HANSON NO. 06S2055	STAGING FIGURE NO. 10

Johns00944

6/1/2016

I:\06 Jobs\06S2055\Admin\4-Reports\STAGING\FIGURE 11 - TYPICAL PH 2.dgn



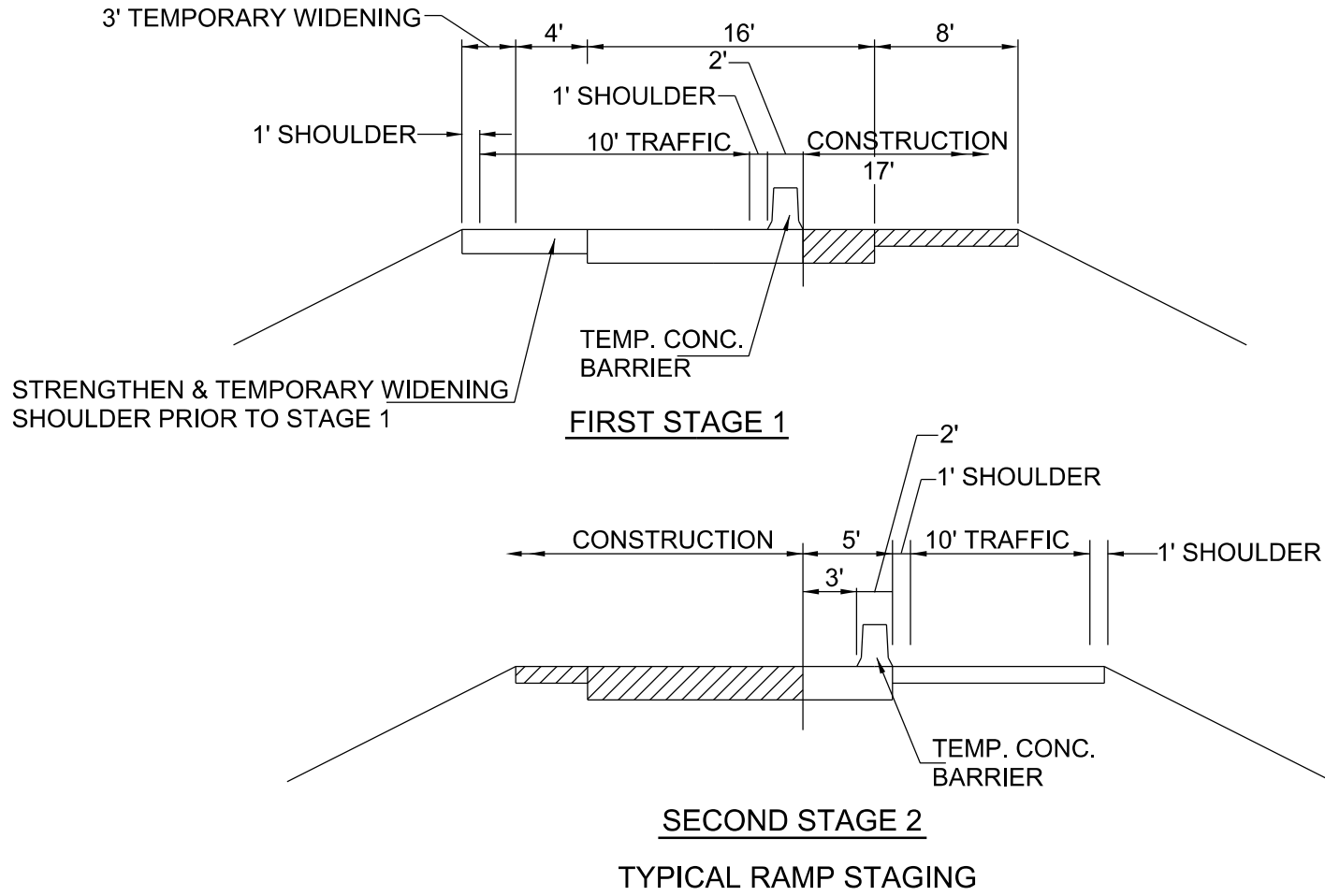
HARRISON AVENUE
STAGE CONSTRUCTION PHASE 2 - STAGE 6



STAGE CONSTRUCTION PHASE 2

FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20)
SECTIONS (201-3) K & (4-1,5) K
WINNEBAGO COUNTY
JOB NO. P-92-111-06

HANSON NO. 06S2055 STAGING FIGURE NO. 11

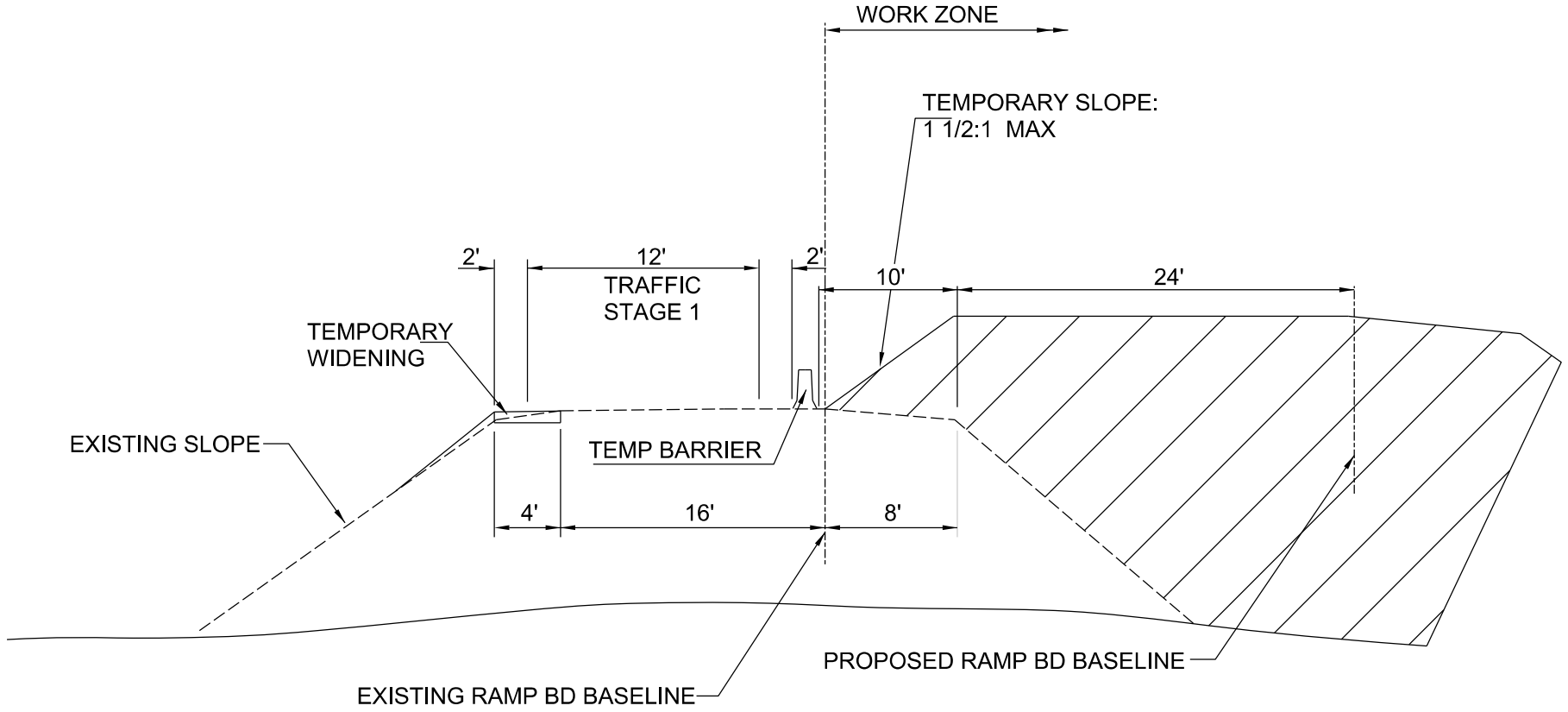


LEGEND

- TRAFFIC
- ▨ CONSTRUCTION ZONE



STAGE CONSTRUCTION PHASE 3
FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20) SECTIONS (201-3) K & (4-1,5) K WINNEBAGO COUNTY JOB NO. P-92-111-06
STAGING FIGURE NO. 12



RAMP BD STAGE 1A

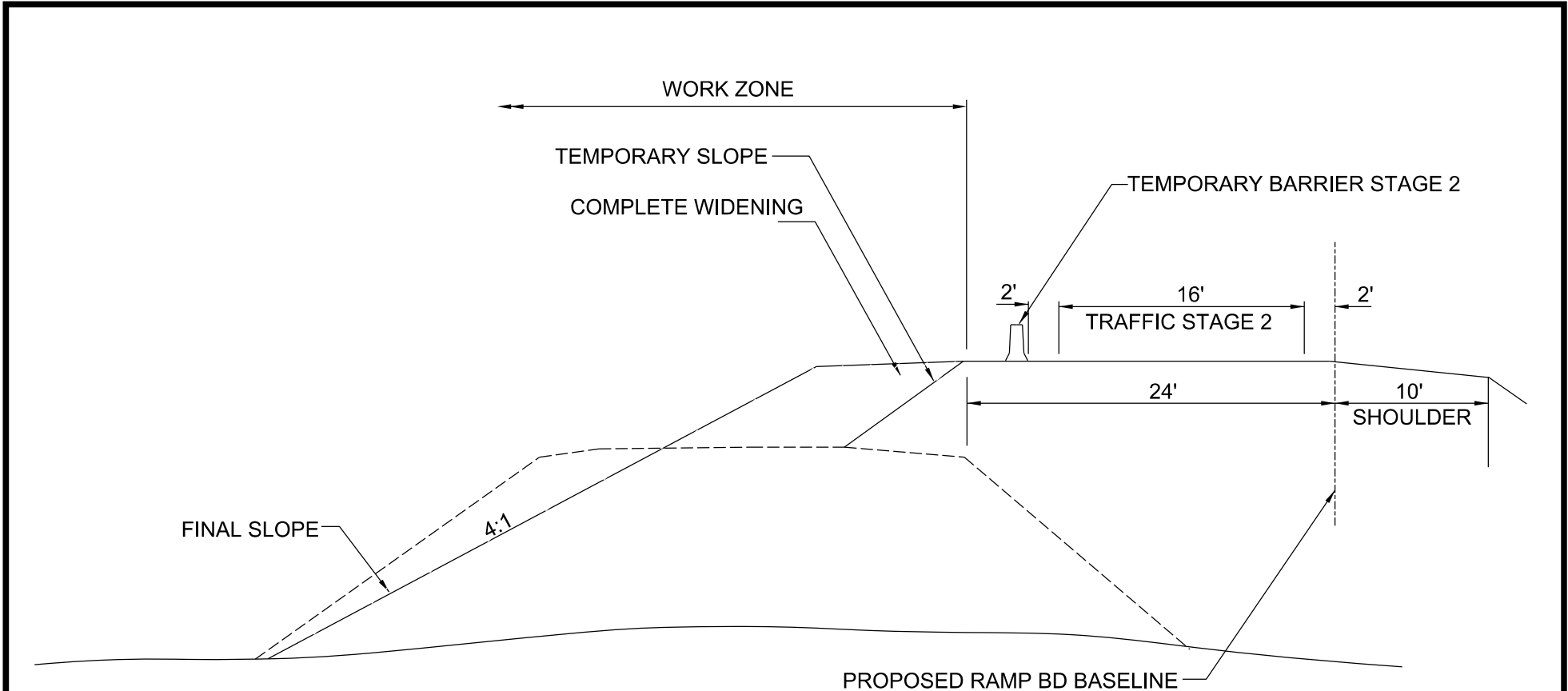
LEGEND

- TRAFFIC
- ▨ CONSTRUCTION ZONE



STAGE CONSTRUCTION PHASE 3
FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20) SECTIONS (201-3) K & (4-1,5) K WINNEBAGO COUNTY JOB NO. P-92-111-06
STAGING FIGURE NO. 13

I:\06\jobs\06S2055\Admin\N4-Report\STAGING\FIGURE 14 - CONST. PH 3.dgn 6/2/2016 Johns00944



RAMP BD STAGE 1B

LEGEND

- TRAFFIC
- ▨ CONSTRUCTION ZONE

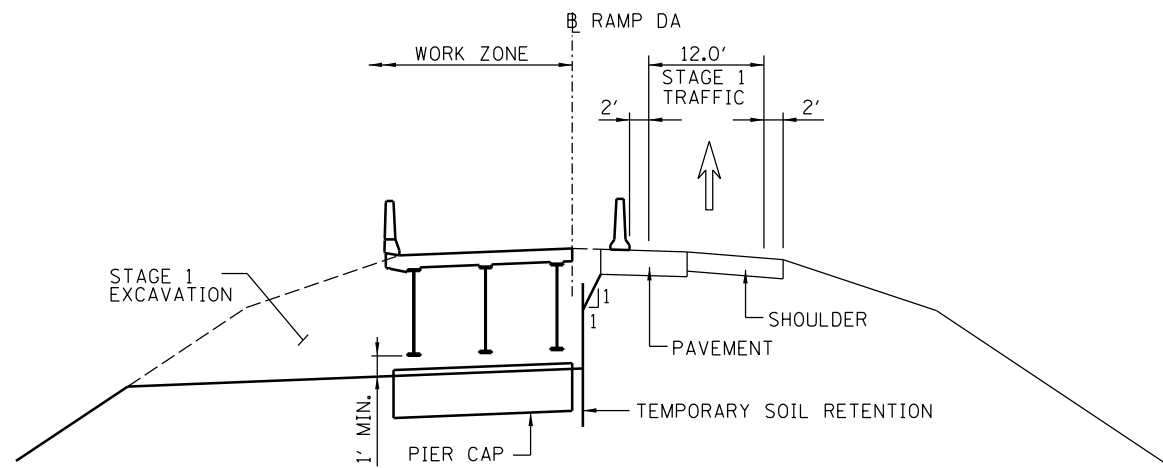


STAGE CONSTRUCTION PHASE 3
FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20) SECTIONS (201-3) K & (4-1,5) K WINNEBAGO COUNTY JOB NO. P-92-111-06
STAGING FIGURE NO. 14

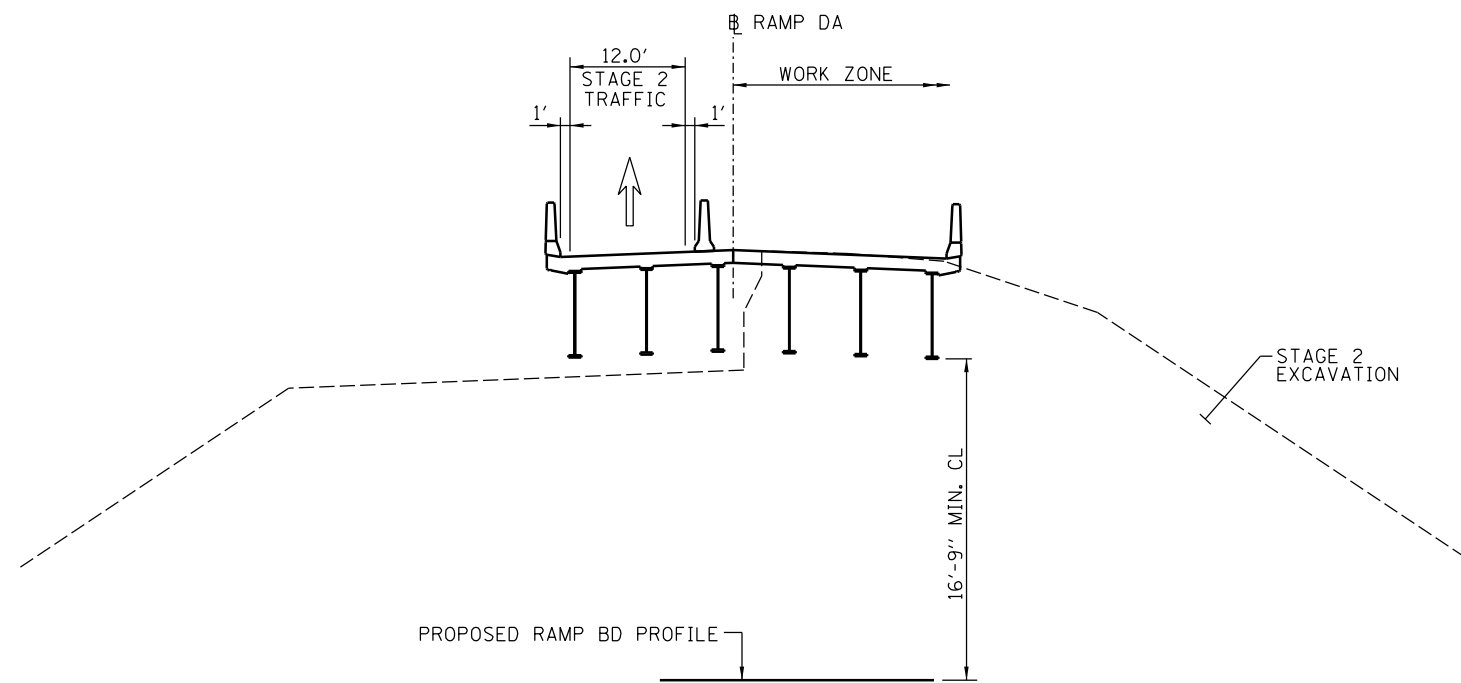
Johns00944

6/2/2016

I:\06\jobs\06S2055\Admin\4-Reports\15-TYPICAL PH 3.dgn



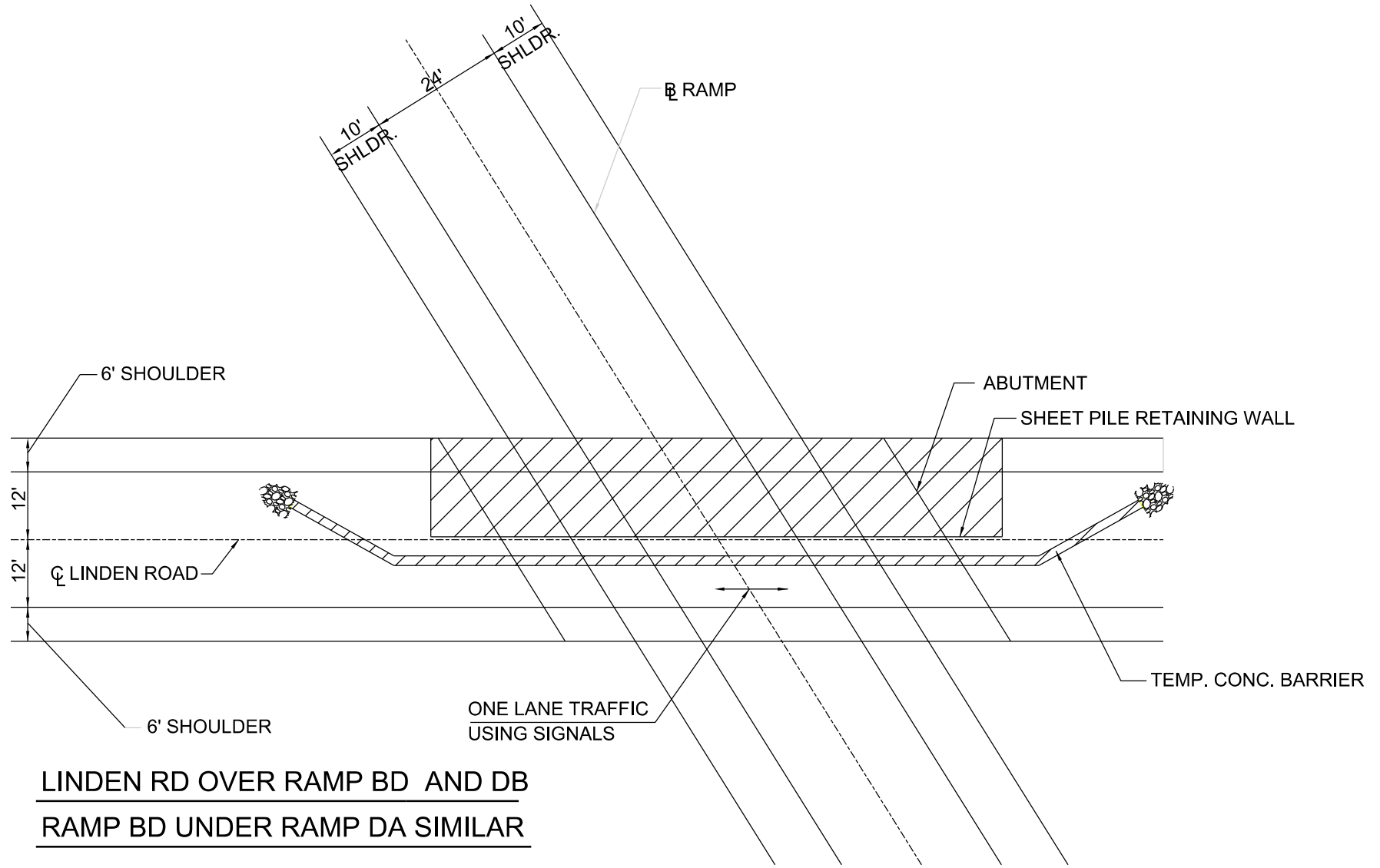
STAGE 1 TYPICAL
RAMP DA OVER RAMP BD



STAGE 2 TYPICAL
RAMP DA OVER RAMP BD






STAGE CONSTRUCTION PHASE 3
FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20) SECTIONS (201-3) K & (4-1,5) K WINNEBAGO COUNTY JOB NO. P-92-111-06
HANSON NO. 06S2055 STAGING FIGURE NO. 15



LINDEN RD OVER RAMP BD AND DB
RAMP BD UNDER RAMP DA SIMILAR

LEGEND

-  TRAFFIC
-  CONSTRUCTION ZONE
-  IMPACT ATTENUATOR



STAGE CONSTRUCTION PHASE 3
FAI ROUTE 39 (I-39) & FAP ROUTE 301 (US 20) SECTIONS (201-3) K & (4-1,5) K WINNEBAGO COUNTY JOB NO. P-92-111-06
STAGING FIGURE NO. 16

INTERCHANGE & INTERSECTION DESIGN STUDIES

ELEMENTS CONTROLLING DESIGN

- DESIGN DESIGNATION FAL ROUTE 39 (INTERSTATE 39) - INTERSTATE FREEWAY
FAP ROUTE 301 (US ROUTE 20 BYPASS) - MAJOR ARTERIAL/FREEWAY
- FAL 39 IS THE PREFERENCE ROUTE
- ANTICIPATED YEAR OF CONSTRUCTION 2015
- TRAFFIC CONTROL TO BE N/A
- DESIGN CRITERIA NEW CONSTRUCTION - CHAPTER 37 AND CHAPTER 44 BDE MANUAL
- DESIGN VEHICLE : WB-65
- TRUCK ROUTE DESIGNATION : INTERSTATE 39 & US ROUTE 20 ARE CLASS I TRUCK ROUTES
FAL ROUTE 39 FAP ROUTE 301 RAMP AD (EXIST) RAMP DA (EXIST)
- DESIGN SPEED 70 MPH 70 MPH 70 MPH 50 MPH 50 MPH
- POSTED SPEED 65 MPH 65 MPH 65 MPH 45 MPH 45 MPH

GENERAL NOTES

- PROFILES ARE/NOT PROVIDED, SINCE APPROACH GRADES ARE ON NEW ALIGNMENTS
- TYPE N/A CURB AND GUTTER TO BE USED ON OUTER EDGES OF PAVEMENT
- TYPE N/A CURB AND GUTTER TO BE USED ON CHANNELIZING ISLAND
- ALL DIMENSIONS ARE SHOWN F-F OF CURB
- ALL ROUTES ARE FULLY ACCESS CONTROLLED
- PROPOSED RIGHT-OF-WAY IS PRELIMINARY AND SUBJECT TO REVISION DURING PREPARATION OF THE FINAL PLANS.
- NO SECTION OF THE INTERCHANGE HAS BEEN IDENTIFIED AS A TOP 5% CRASH LOCATION BETWEEN 2008 AND 2010.
- SCOPE OF WORK : RECONSTRUCTION OF THE INTERCHANGE AT I-39 AND US 20 TO ACCOMMODATE HIGH SPEED MOVEMENT ON NORTHBOUND AND SOUTHBOUND I-39

EXIST. CURVE I39C4
PI STA. = 2482+69.55
Δ = 50° 10' 11" (RT)
D = 1° 30' 00"
R = 3,819.80'
T = 1,788.09'
L = 3,344.71'
E = 397.80'
P.C. STA. = 2464+81.46
P.T. STA. = 2498+26.17

EXIST. CURVE I39C1
PI STA. = 2523+70.40
Δ = 48° 02' 45" (LT)
D = 2° 00' 00"
R = 2,864.85'
T = 1,276.89'
L = 2,402.34'
E = 271.68'
P.C. STA. = 2510+93.52
P.T. STA. = 2534+95.86

EXIST. CURVE PRAMPDB-1
PI STA. = 3+01.05
Δ = 11° 41' 36" (LT)
D = 1° 56' 56"
R = 2,939.98'
T = 301.05'
L = 600.01'
E = 15.37'
P.C. STA. = 0+00.00
P.C.C. STA. = 6+00.01

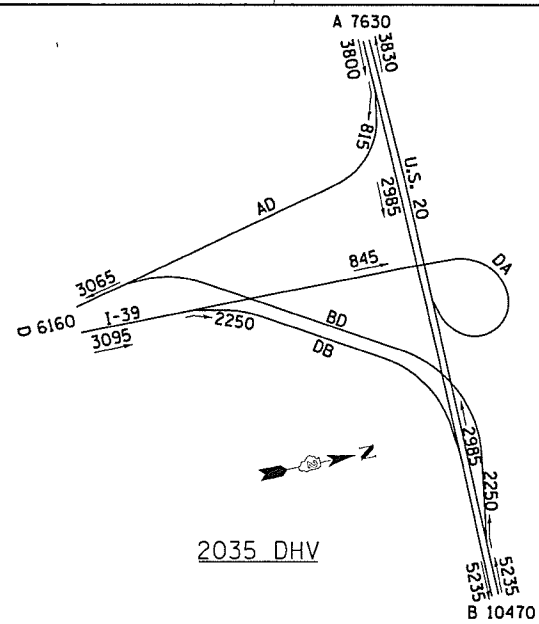
PROP. CURVE PRAMPDB-2
PI STA. = 14+23.59
Δ = 31° 14' 58" (LT)
D = 1° 56' 44"
R = 2,944.85'
T = 823.58'
L = 1,606.13'
E = 113.00'
P.C.C. STA. = 6+00.01
P.T. STA. = 22+06.14

PROP. CURVE PRAMPDB-3
PI STA. = 58+02.34
Δ = 87° 28' 22" (RT)
D = 2° 47' 42"
R = 2,050.00'
T = 1,961.51'
L = 3,129.71'
E = 787.26'
P.C. STA. = 38+40.82
P.T. STA. = 69+70.53

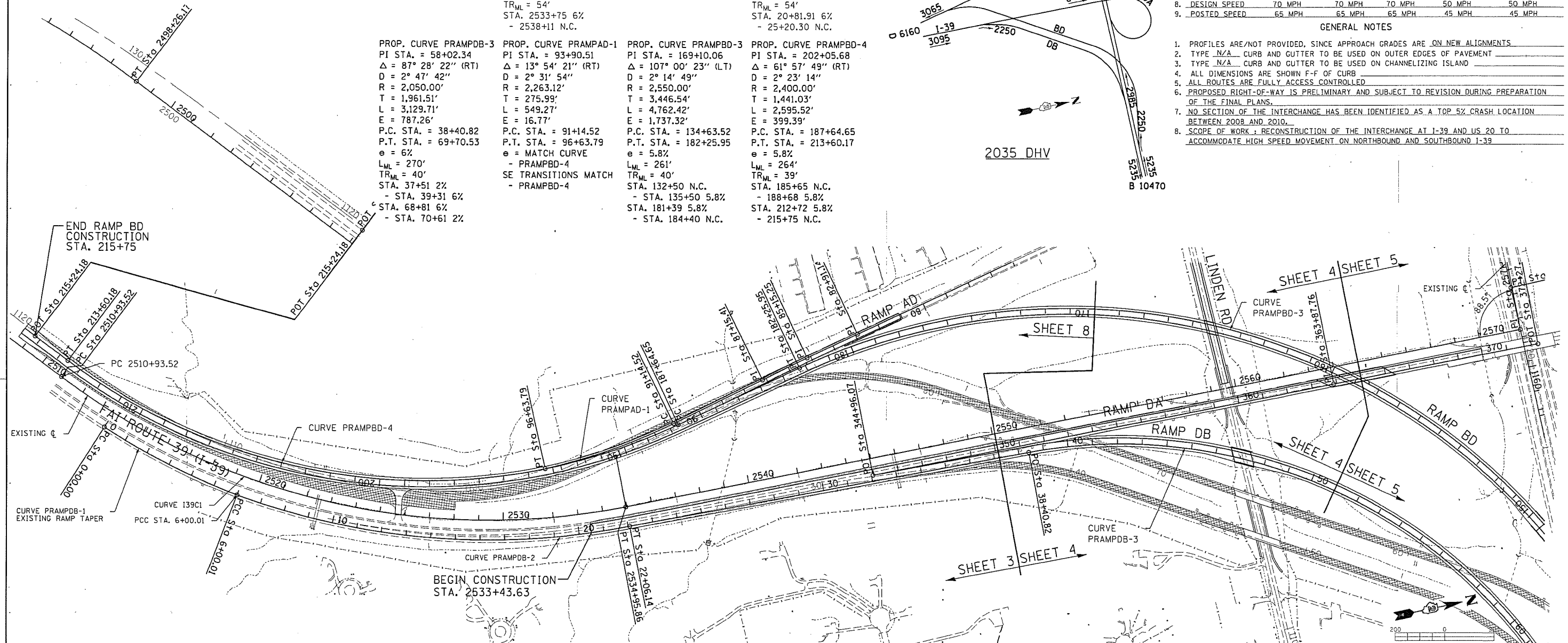
PROP. CURVE PRAMPAD-1
PI STA. = 93+90.51
Δ = 13° 54' 21" (RT)
D = 2° 31' 54"
R = 2,263.12'
T = 275.99'
L = 549.27'
E = 16.77'
P.C. STA. = 91+14.52
P.T. STA. = 96+63.79

PROP. CURVE PRAMPDB-3
PI STA. = 169+10.06
Δ = 107° 00' 23" (LT)
D = 2° 14' 49"
R = 2,550.00'
T = 3,446.54'
L = 4,762.42'
E = 1,737.32'
P.C. STA. = 134+63.52
P.T. STA. = 182+25.95

PROP. CURVE PRAMPDB-4
PI STA. = 202+05.68
Δ = 61° 57' 49" (RT)
D = 2° 23' 14"
R = 2,400.00'
T = 1,441.03'
L = 2,595.52'
E = 399.39'
P.C. STA. = 187+64.65
P.T. STA. = 213+60.17



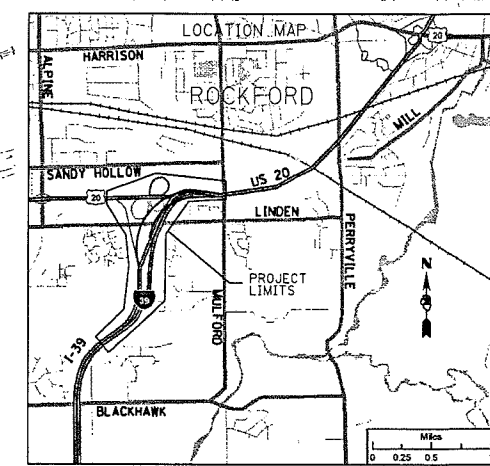
2035 DHV



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PLOT SCALE = 200.0000 / 1 in.
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TRAFFIC VOLUMES

MOVEMENT	YEAR 2015 30TH MAXIMUM HOUR TRAFFIC (EXISTING)		PERCENT TRUCK TRAFFIC IN 30TH MAX HOUR	ESTIMATED PERCENT INCREASE BY 2025		YEAR 2025 30TH MAXIMUM HOUR TRAFFIC (FORECASTED)		ESTIMATED PERCENT INCREASE BY 2035		YEAR 2035 30TH MAXIMUM HOUR TRAFFIC (FORECASTED)	
	AM	PM		AM	PM	AM	PM	AM	PM	AM	PM
AB	1820	1820	9%	28%	28%	2330	2330	64%	64%	2985	2985
AD	495	495	15%	28%	28%	635	635	65%	65%	815	815
AC											
BA	1820	1820	11%	28%	28%	2330	2330	64%	64%	2985	2985
BC											
BD	1370	1370	40%	28%	28%	1755	1755	64%	64%	2250	2250
CD											
CA											
CB											
DC											
DB	1370	1370	40%	28%	28%	1755	1755	64%	64%	2250	2250
DA	515	515	17%	28%	28%	660	660	64%	64%	845	845
TOTAL A	4650	4650				5955	5955			7630	7630
TOTAL B	6380	6380				8170	8170			10470	10470
TOTAL C											
TOTAL D	3750	3750				4805	4805			6160	6160



INTERCHANGE DESIGN STUDY

FAL ROUTE 39 (INTERSTATE 39)
WITH FAP ROUTE 301 (U.S. 20 BYPASS)

SEC. NO. (201-31K & 4-1,51K) PROJ. NO. P-92-111-06
SCALE 1"=200' COUNTY WINNEBAGO

SUN : _____ REV. NO. _____

DESIGNED BY HANSON PROFESSIONAL SERVICES DATE 06-13-12

SATISFACTORY *Jan M. McE...* 9/27/12
DISTRICT GEOMETRICS ENGINEER DATE

SATISFACTORY *Paul...* 10/1/12
DISTRICT PRODUCTION ENGINEER DATE

SATISFACTORY *Paul...* 10-2-12
DISTRICT ESTIMATIONS ENGINEER DATE

APPROVED *Paul...* 10/12/12
DEPUTY DIRECTOR OF HIGHWAYS, REGION TWO ENGINEER DATE

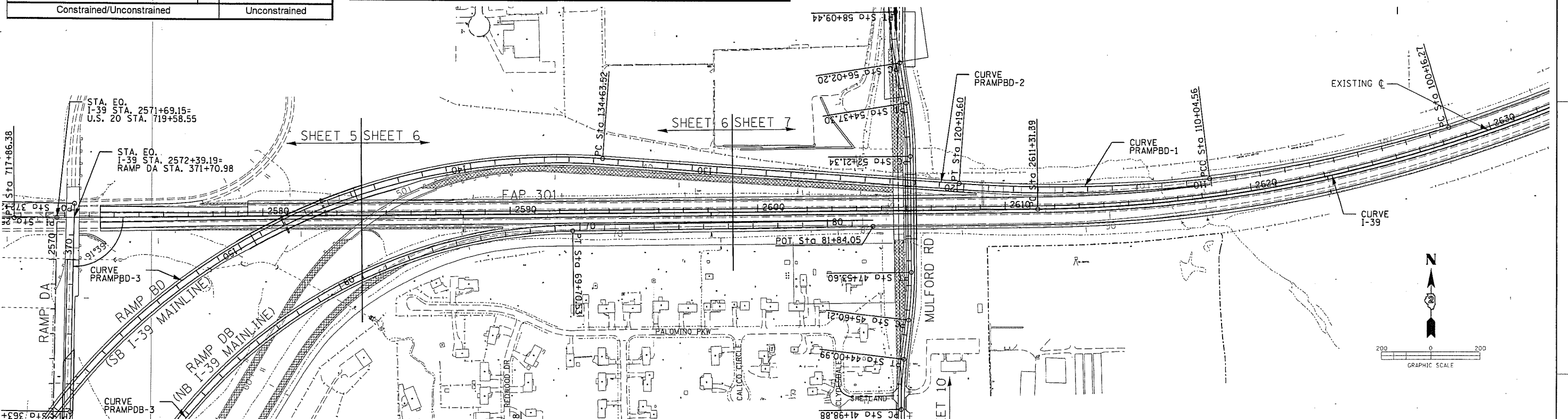
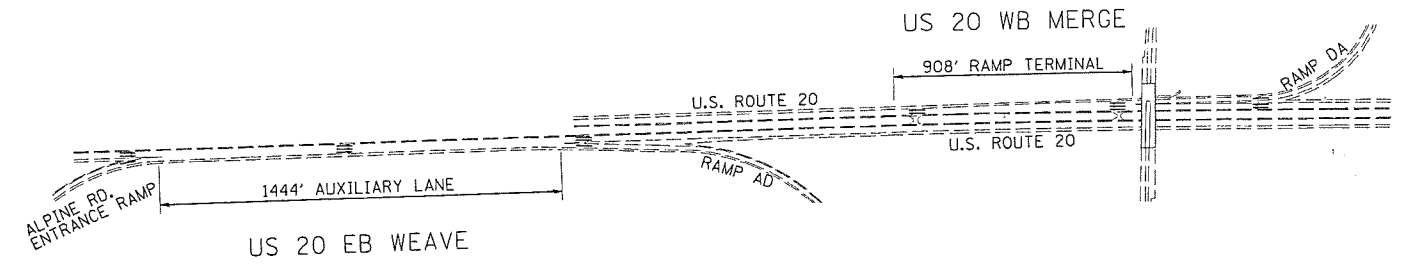
CADD FILE NAME : () I.D.S. SHEET 1 OF 23

ELEMENTS CONTROLLING DESIGN

9. DESIGN EXCEPTIONS : U.S. 20 WESTBOUND MERGE IS LOS F THE MERGE IS OUTSIDE THE PROJECT LIMITS OF THIS IMPROVEMENT. AN ADJACENT ONGOING IMPROVEMENT STUDY WILL ADDRESS THIS LOCATION.
 THE DISTANCE BETWEEN CRITICAL SECTION C-C AND THE PT FOR CURVE DB-3 IS LESS THAN 200'. THIS IS TO FACILITATE THE CHANGE IN CROSS SLOPE PRECEDING THE CURVE. US 20 EASTBOUND WEAVE, ALPINE ROAD TO I-39 RAMP AD IS LOS E. THE WEAVE AREA IS OUTSIDE THE PROJECT LIMITS OF THIS IMPROVEMENT AND WILL BE ADDRESSED IN A SEPARATE PROJECT.
 STOPPING SIGHT DISTANCE FOR RAMP DB IS LESS THAN REQUIRED FOR 70 MPH (730 FEET) DUE TO THE COMBINATION OF HORIZONTAL AND VERTICAL CURVATURE. AN 18' WIDE LEFT SHOULDER ON THE STRUCTURE OVER US 20 IS PROPOSED TO PROVIDE SSD REQUIRED FOR A 65 MPH DESIGN (645 FEET).
 STOPPING SIGHT DISTANCE FOR RAMP DB IS LESS THAN REQUIRED FOR 70 MPH (730 FEET) DUE TO THE COMBINATION OF HORIZONTAL AND VERTICAL CURVATURE. AN 18' WIDE LEFT SHOULDER ON THE STRUCTURE OVER LINCOLN RD IS PROPOSED TO PROVIDE SSD OF 637 FEET, 8 FEET LESS THAN 65 MPH SSD.

INTERCHANGE TYPE STUDY		PROGRAM USED HCS+ 5.21	
Weaving Section		U.S. 20 EASTBOUND	
Terrain/Grade		Level	
Type of Weave		Type A	
Number of Total Lanes		3	
Length of Weaving (ft(m))		1444' (440m)	
Volume 1 (Through Volume)	AM	2201	
	PM	2201	
Volume 2 (Exit Weave)	AM	619	
	PM	619	
Volume 3 (Entrance Weave)	AM	784	
	PM	784	
Volume 4 (Ramp-Ramp)	AM	196	
	PM	196	
TRUCK TYPES		Trucks & Busses, RV	
% Trucks		9%	
% Other Vehicles		2%	
Population Factor		1%	
Level of Service (Weaving Vehicles)	AM	D	
	PM	D	
Level of Service (Non-Weaving Vehicles)	AM	C	
	PM	C	
Weaving Speed	AM	42.1	
	PM	42.1	
Non-Weaving Speed	AM	48.1	
	PM	48.1	
Constrained/Unconstrained		Unconstrained	

INTERCHANGE TYPE STUDY				PROGRAM USED HCS+ 5.21			
APPROACH		U.S. 20 WESTBOUND		I-39 SOUTHBOUND			
RAMP TYPE		MERGE		MERGE			
DISTANCE TO ADJACENT RAMPS (ft (m))		3926' (1,197m)		6880' (2,097m)			
TERRAIN/GRADE		LEVEL		LEVEL			
FREEWAY VOLUME	AM	2985		2250			
	PM	2985		2250			
RAMP VOLUME	AM	845		815			
	PM	845		815			
TRUCK TYPES		Trucks & Busses, RV		Trucks & Busses, RV			
% OF TRUCKS ON FREEWAY		11%		40%			
PEAK HOUR FACTOR		0.90		0.90			
% TRUCKS ON RAMP		17%		15%			
NUMBER OF LANES ON FREEWAY		2		2			
DESIGN SPEED		35		45			
CHECKPOINT VOLUME (FREEWAY + RAMP)	AM	3830		3065			
	PM	3830		3065			
DENSITY/SPEED * (PC/mi/LN)/(mph) (FREEWAY)	AM	34.4 / 53.0 mph		30.8 / 57.0 mph			
	PM	34.4 / 53.0 mph		30.8 / 57.0 mph			
LEVEL OF SERVICE (RAMP)		AM		D			
		PM		D			

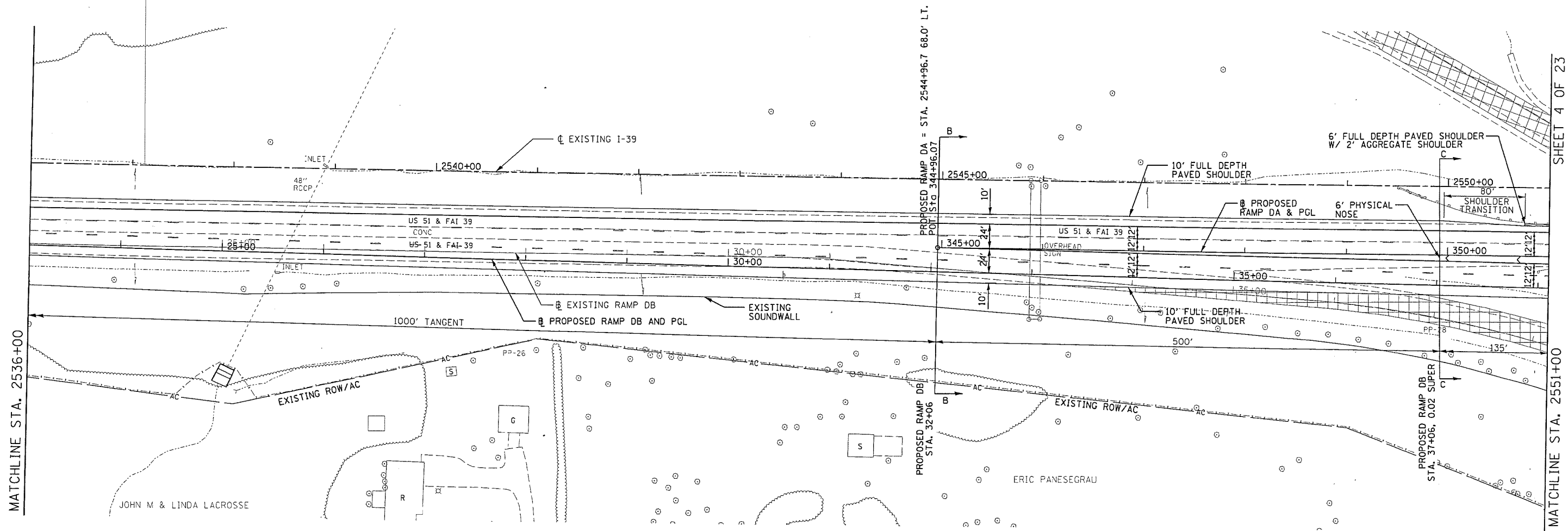


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 PLOT SCALE = 30.0000 / 1"
 USER NAME = singh0238

MATCHLINE STA. 2536+00

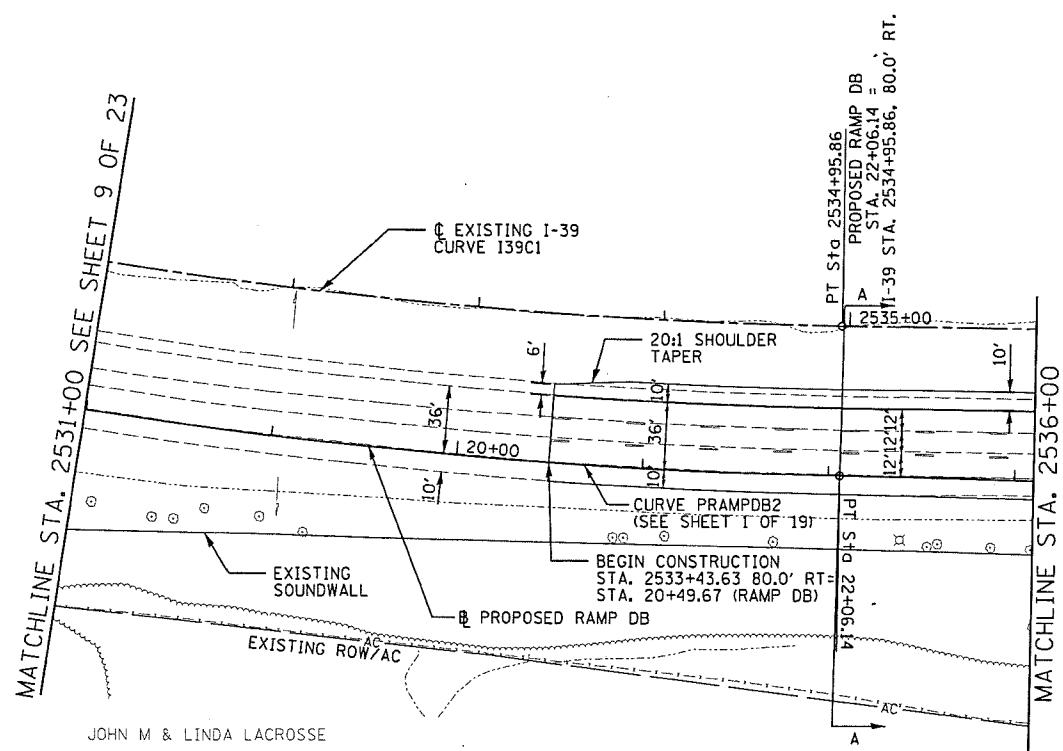
SHEET 4 OF 23

MATCHLINE STA. 2551+00

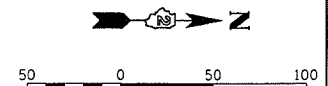


NOTE:
 EXISTING AUXILIARY LANE TAPER AND AUXILIARY LANE FOR RAMP DB TO REMAIN.
 EXISTING RAMP TAPER FROM STA 2513+45.96 TO STA 2519+30.65 TO REMAIN
 AUXILIARY LANE FROM STA 2519+30.65 TO STA 2534+96.28

MATCHLINE STA. 2531+00 SEE SHEET 9 OF 23



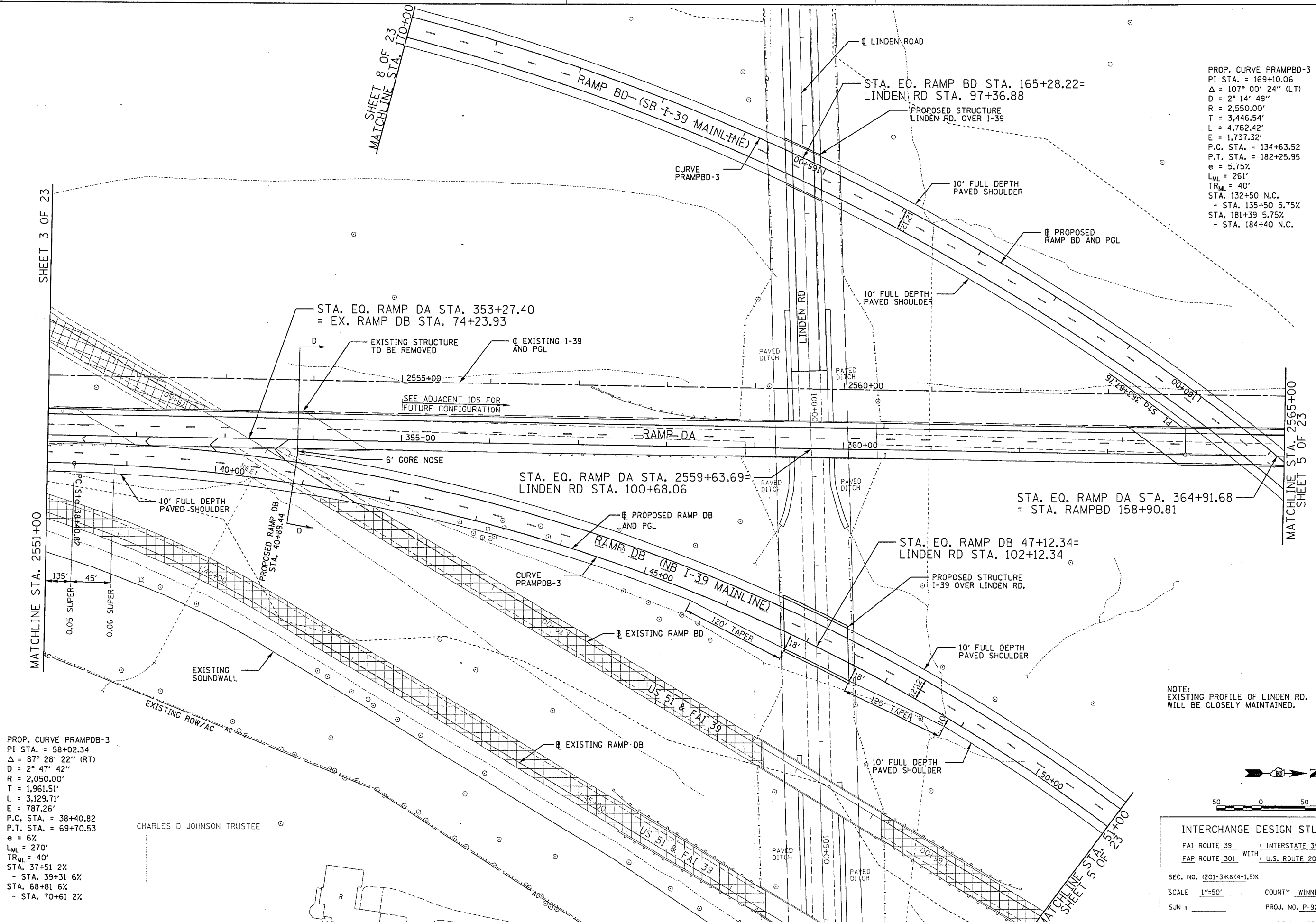
EXIST. CURVE 139C1
 PI STA. = 2523+70.40
 $\Delta = 48^\circ 02' 45''$ (LT)
 $D = 2^\circ 00' 00''$
 $R = 2,864.85'$
 $T = 1,276.89'$
 $L = 2,402.34'$
 $E = 271.68'$
 P.C. STA. = 2510+93.52
 P.T. STA. = 2534+95.86
 $e = 6\%$
 $L_{ML} = 360'$
 $TR = 75'$
 STA. 2533+75 6%
 - 2538+11 N.C.



INTERCHANGE DESIGN STUDY
 FAI ROUTE 39 (INTERSTATE 39)
 FAP ROUTE 301 WITH (U.S. ROUTE 20-BYPASS)
 SEC. NO. (201-3)K&(4-1),5K
 SCALE 1"=50' COUNTY WINNEBAGO
 SUN : _____ PROJ. NO. P-92-111-06
 I.D.S. SHEET 3 OF 23

PLOT DATE = 08/14/2012
 PLOT SCALE = 1"=50'
 USER NAME = slmg/rlz38

PROP. CURVE PRAMPBD-3
 PI STA. = 169+10.06
 $\Delta = 107^\circ 00' 24''$ (LT)
 $D = 2^\circ 14' 49''$
 $R = 2,550.00'$
 $T = 3,446.54'$
 $L = 4,762.42'$
 $E = 1,737.32'$
 P.C. STA. = 134+63.52
 P.T. STA. = 182+25.95
 $e = 5.75\%$
 $L_{ML} = 261'$
 $TR_{ML} = 40'$
 STA. 132+50 N.C.
 - STA. 135+50 5.75%
 STA. 181+39 5.75%
 - STA. 184+40 N.C.



SHEET 3 OF 23

MATCHLINE STA. 2551+00

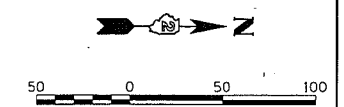
SHEET 8 OF 23
 MATCHLINE STA. 170+00

MATCHLINE STA. 2565+00
 SHEET 5 OF 23

PROP. CURVE PRAMPDB-3
 PI STA. = 58+02.34
 $\Delta = 87^\circ 28' 22''$ (RT)
 $D = 2^\circ 47' 42''$
 $R = 2,050.00'$
 $T = 1,961.51'$
 $L = 3,129.71'$
 $E = 787.26'$
 P.C. STA. = 38+40.82
 P.T. STA. = 69+70.53
 $e = 6\%$
 $L_{ML} = 270'$
 $TR_{ML} = 40'$
 STA. 37+51 2%
 - STA. 39+31 6%
 STA. 68+81 6%
 - STA. 70+61 2%

CHARLES D JOHNSON TRUSTEE

NOTE:
 EXISTING PROFILE OF LINDEN RD.
 WILL BE CLOSELY MAINTAINED.



INTERCHANGE DESIGN STUDY
 FAI ROUTE 39 (INTERSTATE 39)
 RAMP ROUTE 301 WITH (U.S. ROUTE 20 BYPASS)
 SEC. NO. (201-3)K&(4-1.5)K
 SCALE 1"=50' COUNTY WINNEBAGO
 SJN : _____ PROJ. NO. P-92-111-06
 I.D.S. SHEET 4 OF 23

STA. EQ. I-39 STA. 2572+39.19=
RAMP DA STA. 371+70.98

NOTE:
THE MEDIAN TRANSITION FROM TURF TO CONCRETE BARRIER SHALL BE ANALYZED IN PHASE 2 SO IT MEETS THE APPROPRIATE ROADSIDE SAFETY STANDARDS.

BEGIN CONSTRUCTION STA. 2573+45.00

STA. EQ. I-39
STA 2581+12.79 =
RAMP BD STA. 147+59.66
LANE DROP TAPER 840'

PROP. CURVE PRAMPBD-3
PI STA. = 169+10.06
 $\Delta = 107^\circ 00' 24''$ (LT)
D = 2° 14' 49"
R = 2,550.00'
T = 3,446.54'
L = 4,762.42'
E = 1,737.32'
P.C. STA. = 134+63.52
P.T. STA. = 182+25.95
e = 5.8%
L_M = 261'
TR_M = 40'
STA. 132+90 N.C.
- STA. 135+51 5.8%
STA. 181+39 5.8%
- STA. 184+40 N.C.

PROP. CURVE PRAMPDB-3
PI STA. = 58+02.34
 $\Delta = 87^\circ 28' 22''$ (RT)
D = 2° 47' 42"
R = 2,050.00'
T = 1,961.51'
L = 3,129.71'
E = 787.26'
P.C. STA. = 38+40.82
P.T. STA. = 69+70.53
e = 6%
L_M = 270'
TR_M = 40'
STA. 68+81 6%
- STA. 70+61 2%

NOTE:
SHOULDER WIDENED TO 18 FEET ACROSS STRUCTURE TO PROVIDE FOR 70 MPH S.S.D.

NOTE:
US 20 WESTBOUND CROWLINE SHIFT STA. 2578+49.60 TO STA. 2579+39.60

MATCHLINE STA. 2565+00
SHEET 4 OF 23

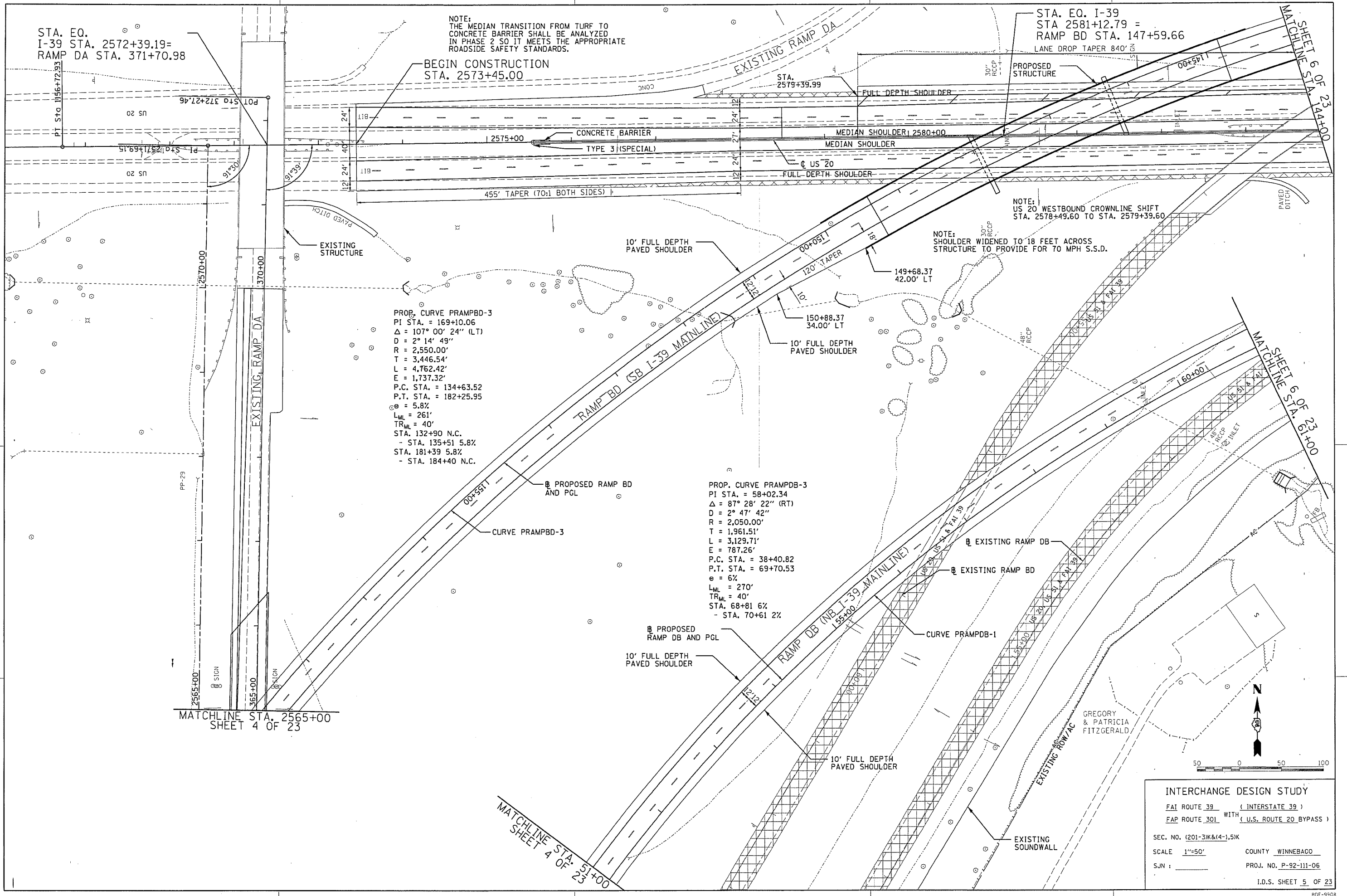
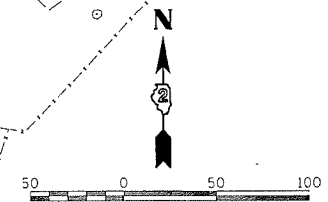
MATCHLINE STA. 51+00
SHEET 4 OF 23

MATCHLINE STA. 61+00
SHEET 6 OF 23

MATCHLINE STA. 23+14+00
SHEET 6 OF 23

PLT DATE = 08/14/2012
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USER NAME = ojmj01238

INTERCHANGE DESIGN STUDY
 I-39 (INTERSTATE 39)
 WITH U.S. ROUTE 20 BYPASS (U.S. ROUTE 301)
 SEC. NO. (201-3K&(4-1.5K)
 SCALE 1"=50' COUNTY WINNEBAGO
 SJN : _____ PROJ. NO. P-92-111-06
 I.D.S. SHEET 5 OF 23



NOTE:
SHOULDER WIDENED TO 18 FEET ACROSS
STRUCTURE TO PROVIDE FOR 70 MPH S.S.D.

PROP. CURVE PRAMPDB-3
PI STA. = 169+10.06
 $\Delta = 107^\circ 00' 24''$ (LT)
 $D = 2^\circ 14' 49''$
 $R = 2,550.00'$
 $T = 3,446.54'$
 $L = 4,762.42'$
 $E = 1,737.32'$
P.C. STA. = 134+63.52
P.T. STA. = 182+25.95
 $e = 5.8\%$
 $L_{uL} = 261'$
 $TR_{uL} = 40'$
STA. 132+90 N.C. - STA. 135+51 5.8%
STA. 181+39 5.8% - STA. 184+40 N.C.

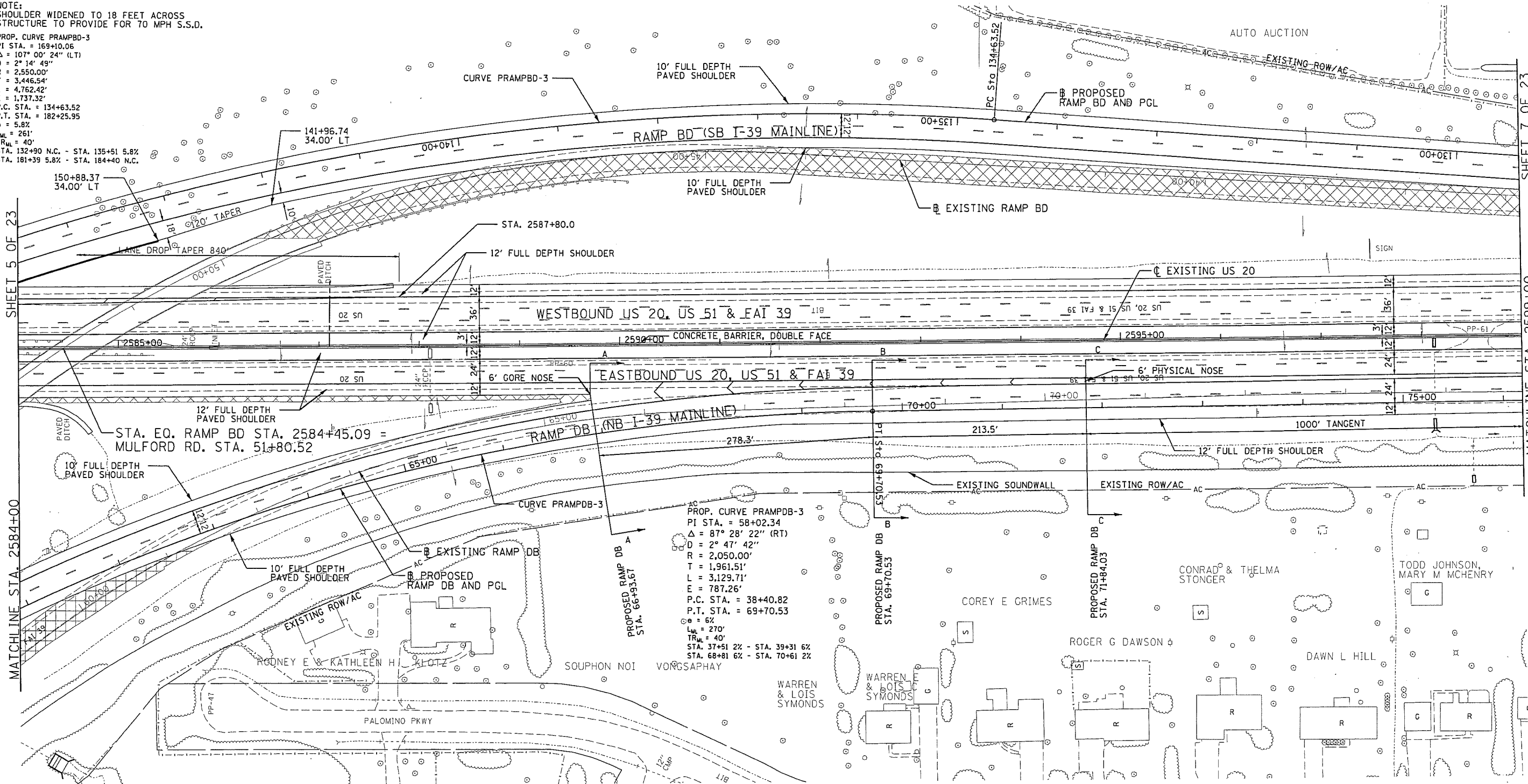
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PLOT SCALE = 1/4"=1'-0"
USER NAME = bmg@1238

SHEET 5 OF 23

SHEET 7 OF 23

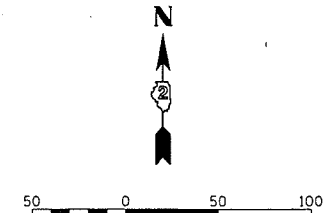
MATCHLINE STA. 2599+00

MATCHLINE STA. 2584+00



PROP. CURVE PRAMPDB-3
PI STA. = 58+02.34
 $\Delta = 87^\circ 28' 22''$ (RT)
 $D = 2^\circ 47' 42''$
 $R = 2,050.00'$
 $T = 1,961.51'$
 $L = 3,129.71'$
 $E = 787.26'$
P.C. STA. = 38+40.82
P.T. STA. = 69+70.53
 $e = 6\%$
 $L_{uL} = 270'$
 $TR_{uL} = 40'$
STA. 37+51 2% - STA. 39+31 6%
STA. 68+81 6% - STA. 70+61 2%

NOTE: US 20 EASTBOUND CROWLINE SHIFT STA. 736+70 - 737+60



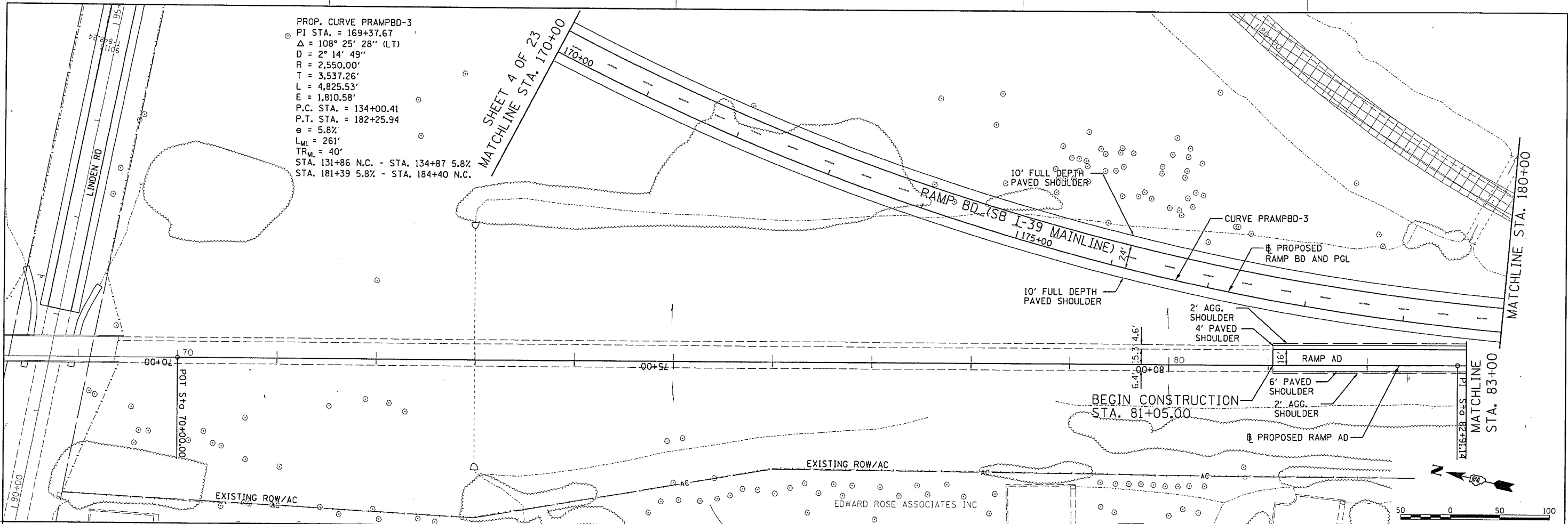
INTERCHANGE DESIGN STUDY
 FAI ROUTE 39 (INTERSTATE 39)
 FAP ROUTE 301 WITH (U.S. ROUTE 20 BYPASS)
 SEC. NO. (201-3)K&(4-1)K
 SCALE 1"=50' COUNTY WINNEBAGO
 SJN: _____ PROJ. NO. P-92-111-06
 I.D.S. SHEET 6 OF 23

PLOT DATE = 08/14/2012
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 PLOT SCALE = 50.0000 / in.
 USER NAME = 6img0238

PROP. CURVE PRAMPBD-3
 PI STA. = 169+37.67
 $\Delta = 108^\circ 25' 28''$ (LT)
 $D = 2^\circ 14' 49''$
 $R = 2,550.00'$
 $T = 3,537.26'$
 $L = 4,825.53'$
 $E = 1,810.58'$
 $P.C. STA. = 134+00.41$
 $P.T. STA. = 182+25.94$
 $e = 5.8\%$
 $L_M = 261'$
 $TR_M = 40'$
 STA. 131+86 N.C. - STA. 134+87 5.8%
 STA. 181+39 5.8% - STA. 184+40 N.C.

SHEET 4 OF 23
 MATCHLINE STA. 170+00

MATCHLINE STA. 180+00

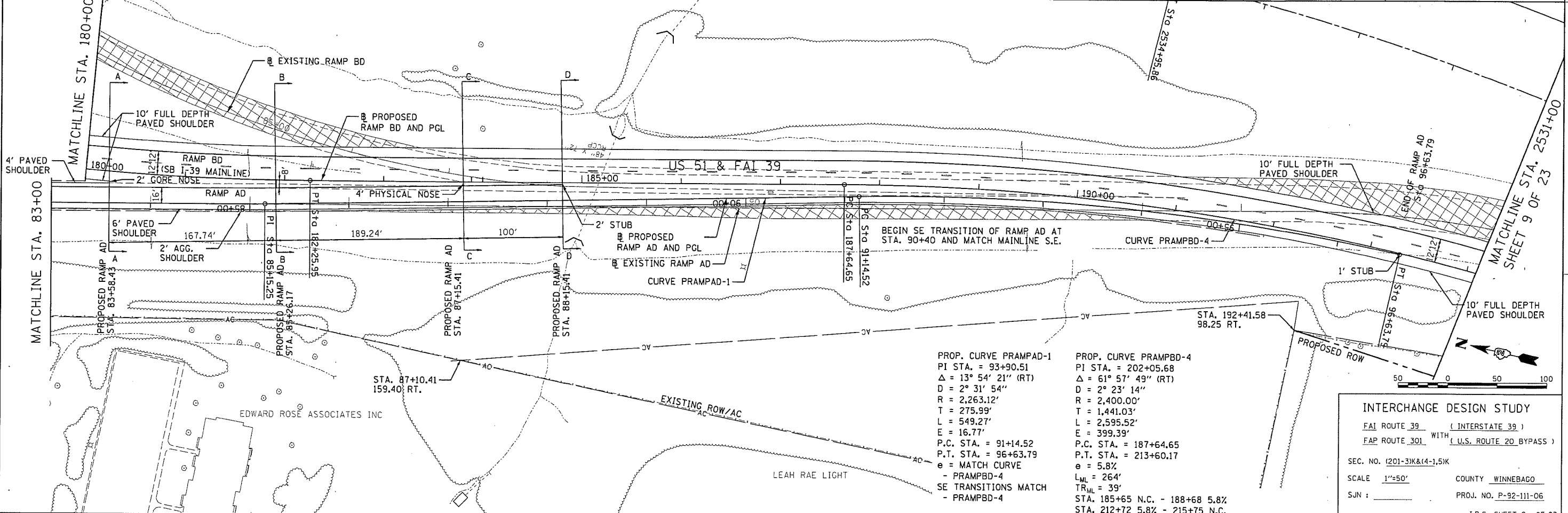


BEGIN CONSTRUCTION
 STA. 81+05.00

EXISTING ROW/AC
 EDWARD ROSE ASSOCIATES INC

MATCHLINE STA. 180+00
 MATCHLINE STA. 83+00

MATCHLINE STA. 2531+00
 SHEET 9 OF 23



US 51 & FAI 39

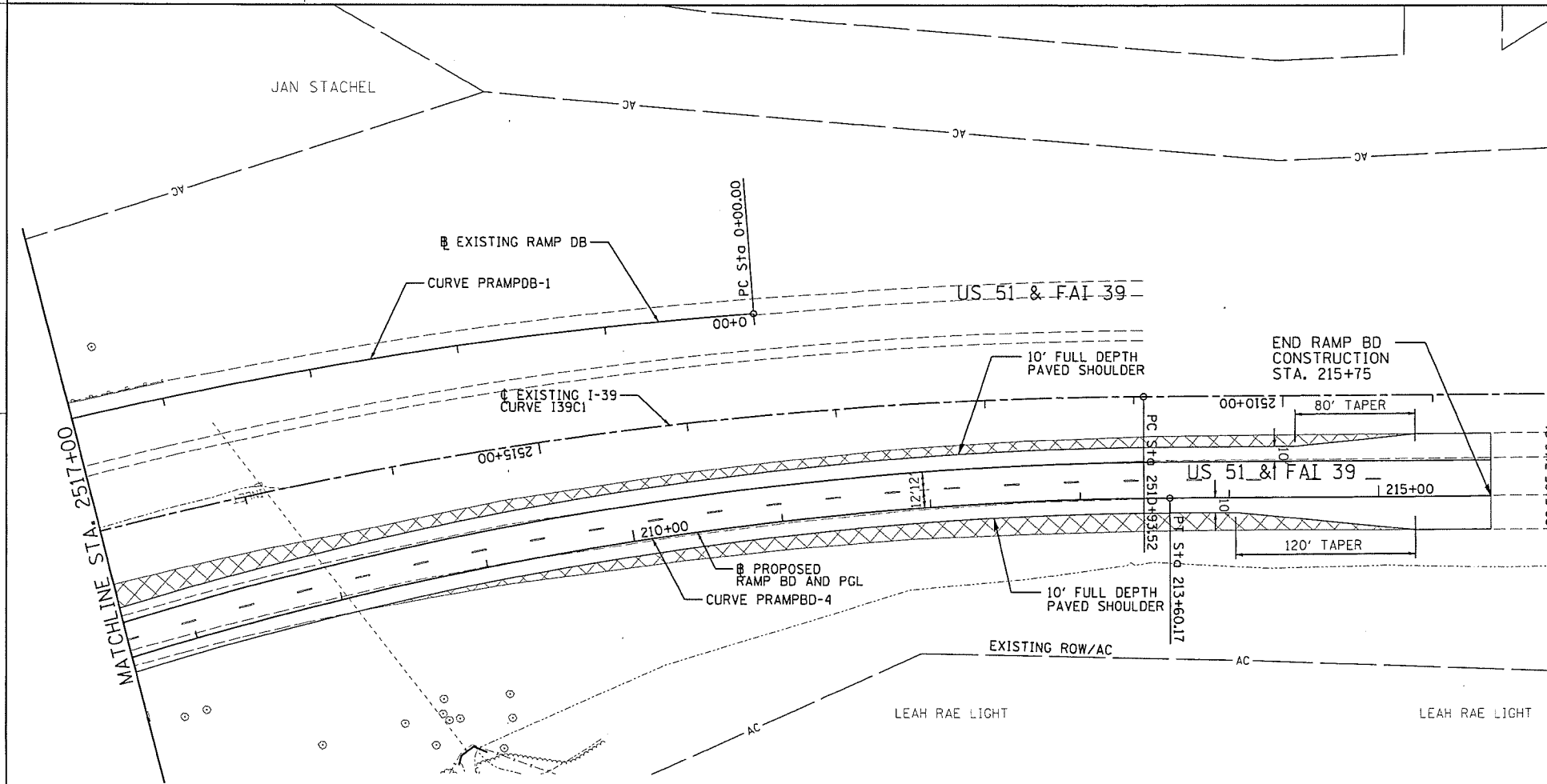
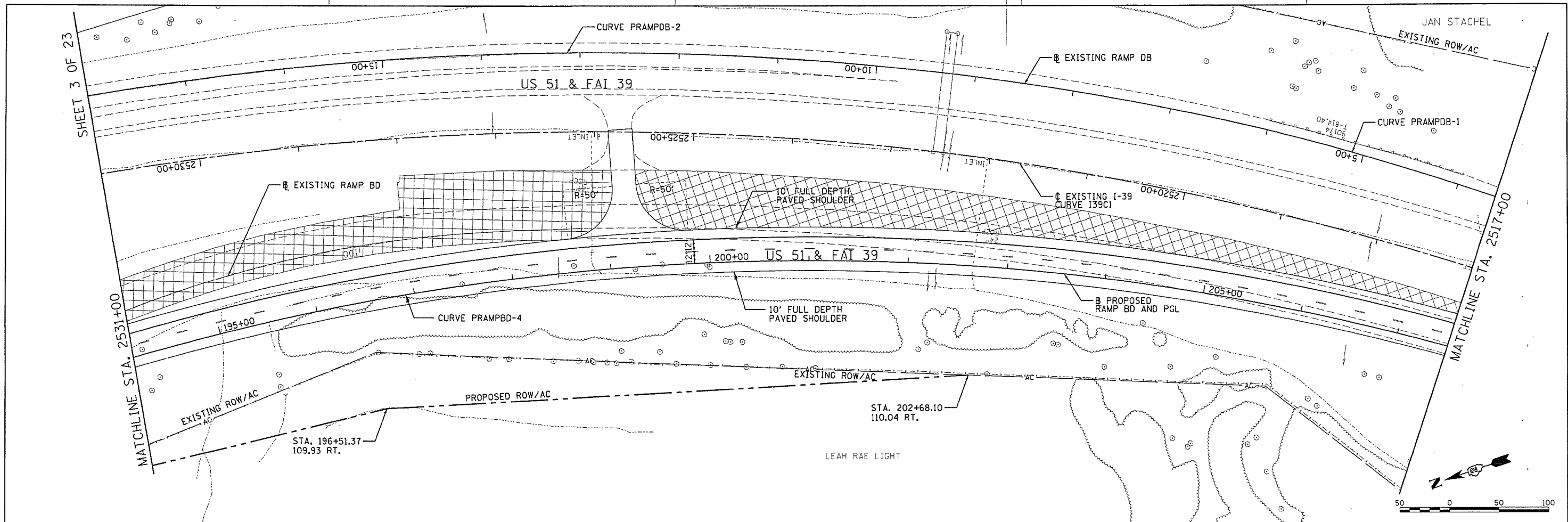
BEGIN SE TRANSITION OF RAMP AD AT
 STA. 90+40 AND MATCH MAINLINE S.E.

PROP. CURVE PRAMPAD-1
 PI STA. = 93+90.51
 $\Delta = 13^\circ 54' 21''$ (RT)
 $D = 2^\circ 31' 54''$
 $R = 2,263.12'$
 $T = 275.99'$
 $L = 549.27'$
 $E = 16.77'$
 $P.C. STA. = 91+14.52$
 $P.T. STA. = 96+63.79$
 $e = \text{MATCH CURVE}$
 $L_M = 39'$
 SE TRANSITIONS MATCH
 - PRAMPBD-4
 - PRAMPBD-4

PROP. CURVE PRAMPBD-4
 PI STA. = 202+05.68
 $\Delta = 61^\circ 57' 49''$ (RT)
 $D = 2^\circ 23' 14''$
 $R = 2,400.00'$
 $T = 1,441.03'$
 $L = 2,595.52'$
 $E = 399.39'$
 $P.C. STA. = 187+64.65$
 $P.T. STA. = 213+60.17$
 $e = 5.8\%$
 $L_M = 264'$
 $TR_M = 39'$
 STA. 185+65 N.C. - 188+68 5.8%
 STA. 212+72 5.8% - 215+75 N.C.

INTERCHANGE DESIGN STUDY
 FAI ROUTE 39 (INTERSTATE 39)
 FAP ROUTE 301 WITH (U.S. ROUTE 20 BYPASS)
 SEC. NO. (201-3JK&14-1,5K)
 SCALE 1"=50' COUNTY WINNEBAGO
 SJN : _____ PROJ. NO. P-92-111-06
 I.D.S. SHEET 8 OF 23

PLOT DATE = 08/14/2012
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 USER NAME = 9652855
 USER NAME = 9652855



NOTE:
 EXISTING AUXILIARY LANE TAPER AND AUXILIARY LANE FOR RAMP DB TO REMAIN.
 EXISTING RAMP TAPER FROM STA 2513+45.96 TO STA 2519+30.65 TO REMAIN
 AUXILIARY LANE FROM STA 2519+30.65 TO STA 2534+96.28

EXIST. CURVE PRAMPDB-1
 PI STA. = 3+01.05
 $\Delta = 11^\circ 41' 36''$ (LT)
 $D = 1^\circ 56' 56''$
 $R = 2,939.98'$
 $T = 301.05'$
 $L = 600.01'$
 $E = 15.37'$
 P.C. STA. = 0+00.00
 P.C.C. STA. = 6+00.01

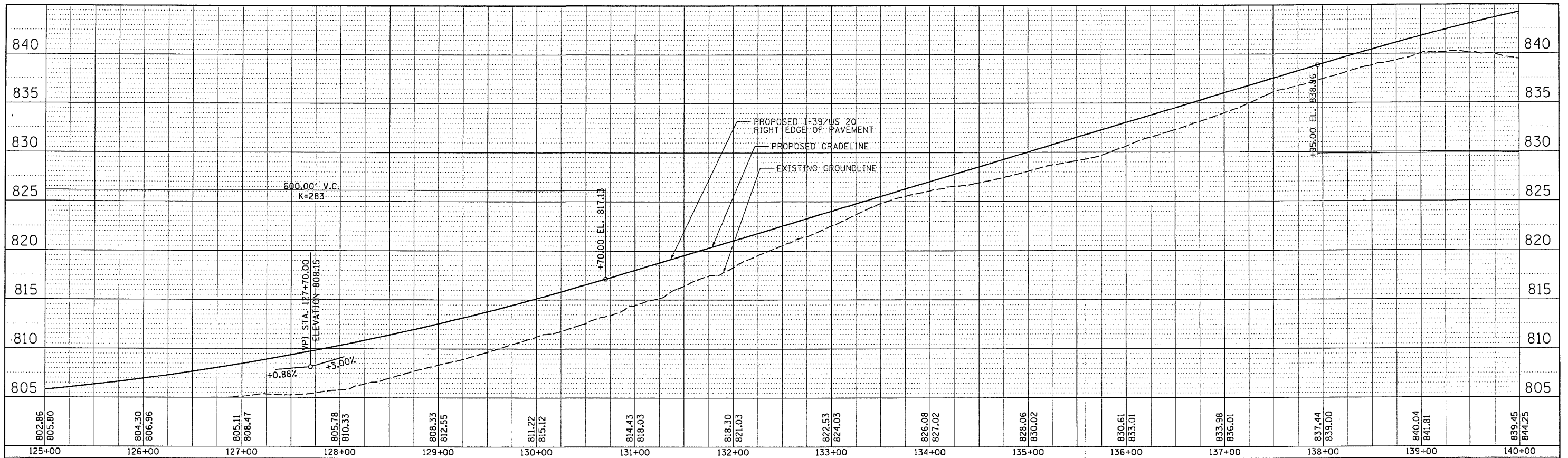
PROP. CURVE PRAMPDB-2
 PI STA. = 14+23.59
 $\Delta = 31^\circ 14' 58''$ (LT)
 $D = 1^\circ 56' 44''$
 $R = 2,944.85'$
 $T = 823.58'$
 $L = 1,606.13'$
 $E = 113.00'$
 P.C.C. STA. = 6+00.01
 P.T. STA. = 22+06.14
 $e = 6\%$
 $L_{ML} = 360'$
 $TR_{ML} = 54'$
 STA. 20+81.91 6%
 - STA. 25+20.30 N.C.

PROP. CURVE PRAMPDB-4
 PI STA. = 202+05.68
 $\Delta = 61^\circ 57' 49''$ (RT)
 $D = 2^\circ 23' 14''$
 $R = 2,400.00'$
 $T = 1,441.03'$
 $L = 2,595.52'$
 $E = 399.39'$
 P.C. STA. = 187+64.65
 P.T. STA. = 213+60.17
 $e = 5.8\%$
 $L_{ML} = 264'$
 $TR_{ML} = 39'$
 STA. 185+65 N.C. - 188+68 5.8%
 STA. 212+725 5.8% - 215+75 N.C.

EXIST. CURVE I39C1
 PI STA. = 2523+70.40
 $\Delta = 48^\circ 02' 45''$ (LT)
 $D = 2^\circ 00' 00''$
 $R = 2,864.85'$
 $T = 1,276.89'$
 $L = 2,402.34'$
 $E = 271.68'$
 P.C. STA. = 2510+93.52
 P.T. STA. = 2534+95.86
 $e = 6\%$
 $L_{ML} = 360'$
 $TR_{ML} = 54'$
 STA. 2533+75 6%
 - STA. 2538+11 N.C.

INTERCHANGE DESIGN STUDY
 FAI ROUTE 39 (INTERSTATE 39)
 FAP ROUTE 301 WITH (U.S. ROUTE 20 BYPASS)
 SEC. NO. 201-3K&(4-1,5K)
 SCALE 1"=50' COUNTY WINNEBAGO
 SJN : PROJ. NO. P-92-111-06
 I.D.S. SHEET 9 OF 23

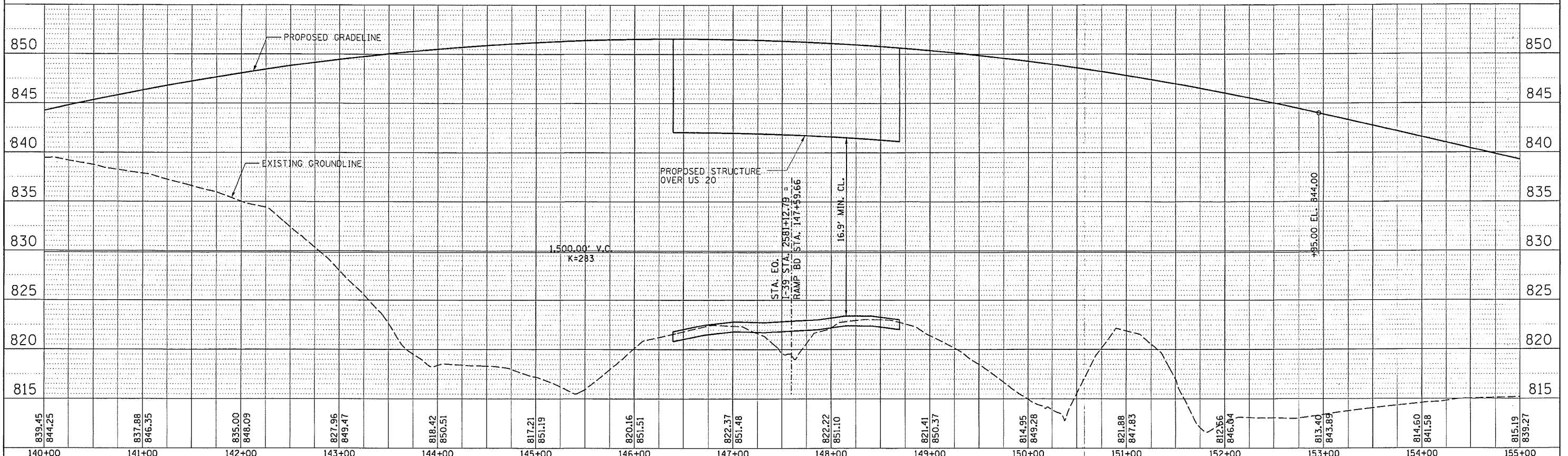
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	NOTED		
	ALIGNED		
	CHECKED		
	FILED		
	NO.		
	FILE NAME		



RAMP BD STA. 125+00 TO STA. 140+00

RAMP BD STA. 140+00 TO STA. 155+00

PROFILE	DESIGNED	BY	DATE
	NOTED		
	GRADES		
	CHECKED		
	STRUCTURE		
	NOTATIONS		
	CHKD		
	NO.		
	FILE NAME		

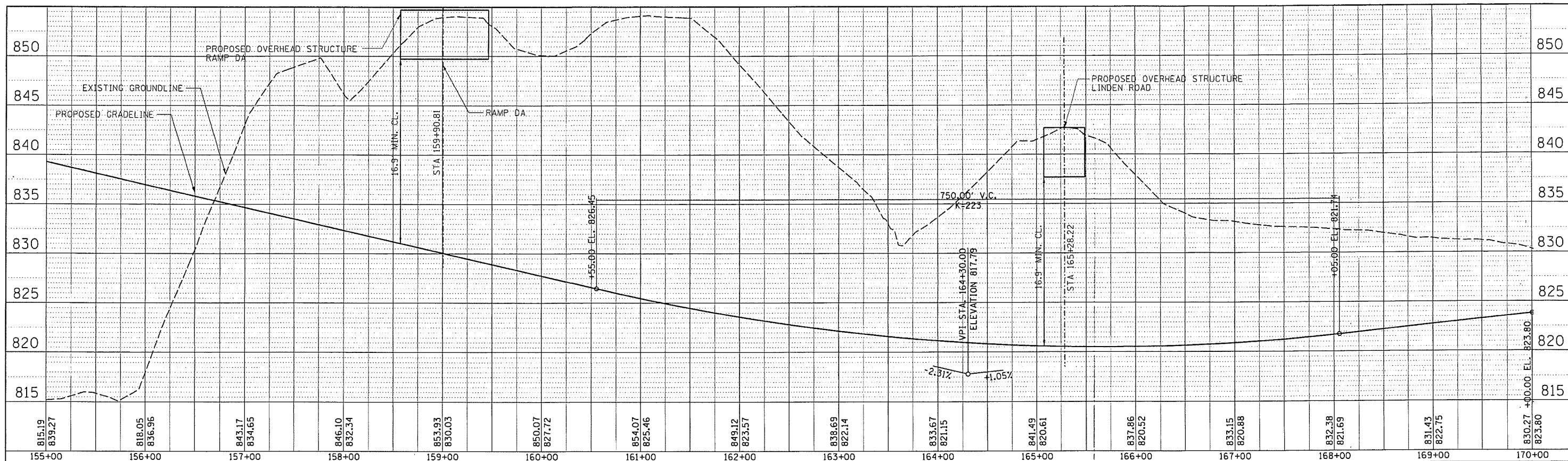


FILE NAME =	USER NAME = slmg21238	DESIGNED - DPA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RAMP BD (SB MAINLINE I-39) STA. 125+00 TO STA. 155+00 PROFILE	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT SCALE = 50.0000' / 1" =		CHECKED - DPA	REVISED -			CONTRACT NO.					
PLOT DATE = 08/14/2012		DATE - 01/28/11	REVISED -			FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT					

SCALE: SHEET NO. OF SHEETS STA. TO STA.

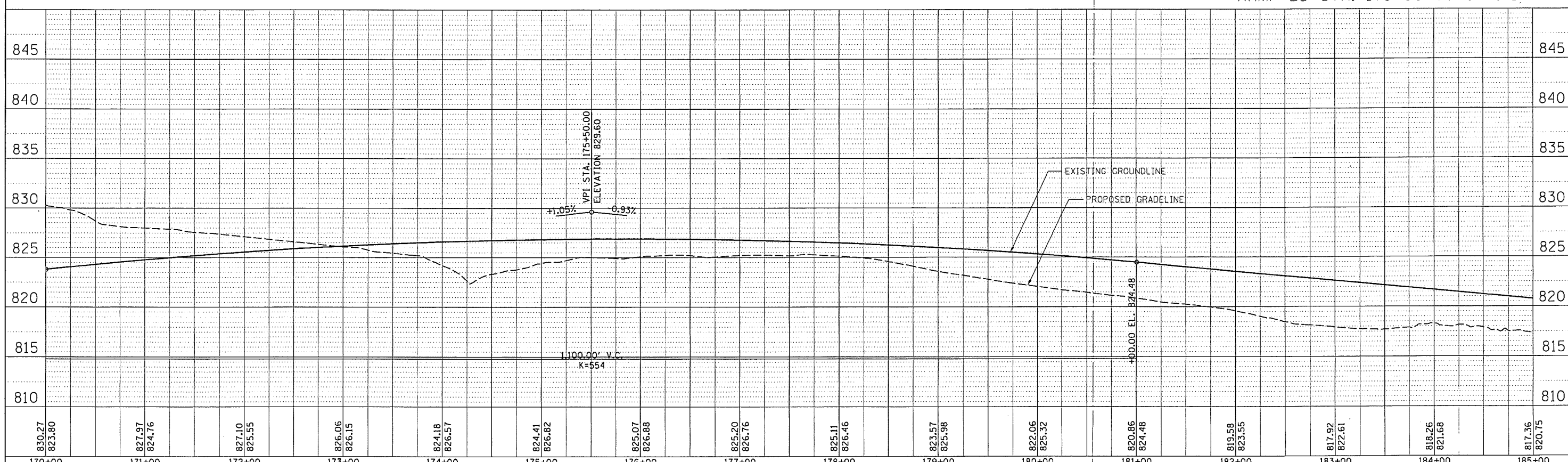
PLAN	NO.	BY	DATE
DESIGNED			
DRAWN			
CHECKED			
DATE			

PROFILE	NO.	BY	DATE
DESIGNED			
DRAWN			
CHECKED			
DATE			



RAMP BD STA. 155+00 TO STA. 170+00

RAMP BD STA. 170+00 TO STA. 185+00



830.27 823.80	827.97 824.76	827.10 825.55	826.06 826.15	824.18 826.57	824.41 826.82	825.07 826.88	825.20 826.76	825.11 826.46	823.57 825.98	822.06 825.32	820.86 824.48	819.58 823.55	817.92 822.61	818.26 821.68	817.36 820.75
170+00	171+00	172+00	173+00	174+00	175+00	176+00	177+00	178+00	179+00	180+00	181+00	182+00	183+00	184+00	185+00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

RAMP BD (SB I-39 MAINLINE) STA. 155+00 TO STA. 185+00 PROFILE

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PLOT SCALE = 50.0000' / 1" =
PLOT DATE = 08/14/2012

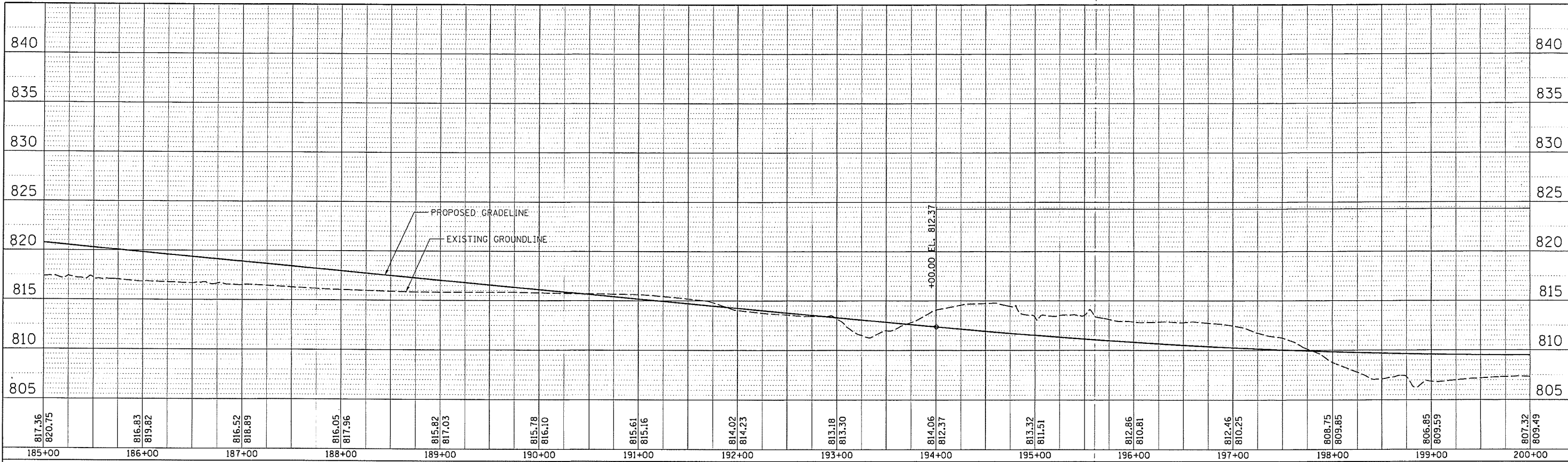
DESIGNED - DPA
DRAWN - JDM
CHECKED - DPA
DATE - 01/28/11

REVISED -
REVISED -
REVISED -
REVISED -

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE. 39	SECTION (201-3)K & (4-1.5)K	COUNTY WINNEBAGO	TOTAL SHEETS 23	SHEET NO. 13
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO.	

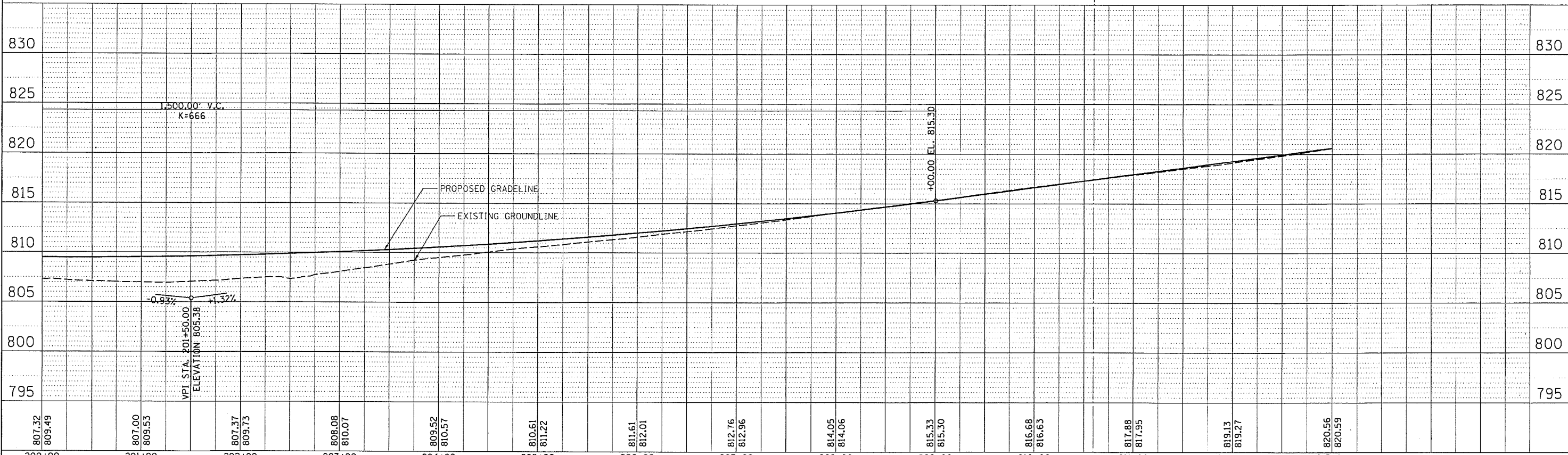
PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	REVISIONS		
	NO. OF WAYS CHECKED		
	CADD FILE NAME		
	NO.		



RAMP BD STA. 175+00 TO STA. 200+00

RAMP BD STA. 200+00 TO STA. 215+00

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	REVISIONS		
	NO. OF WAYS CHECKED		
	STRUCTURE NOTATION/NO. OF PKG		
	NO.		



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 DESIGNED - DPA
 DRAWN - JDM
 CHECKED - DPA
 DATE - 01/28/11
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 PLOT DATE = 08/14/2012

REVISED -
 REVISED -
 REVISED -
 REVISED -

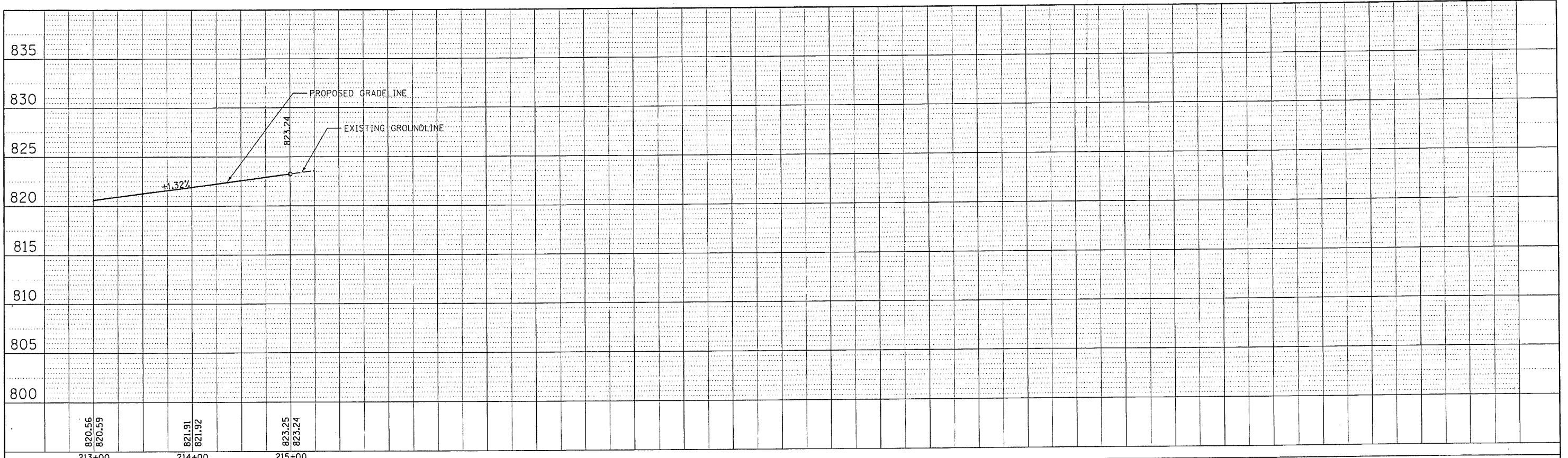
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

RAMP BD (SB I-39 MAINLINE) STA. 175+00 TO 215+00 PROFILE

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & (4-1,5)K	WINNEBAGO	23	14
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

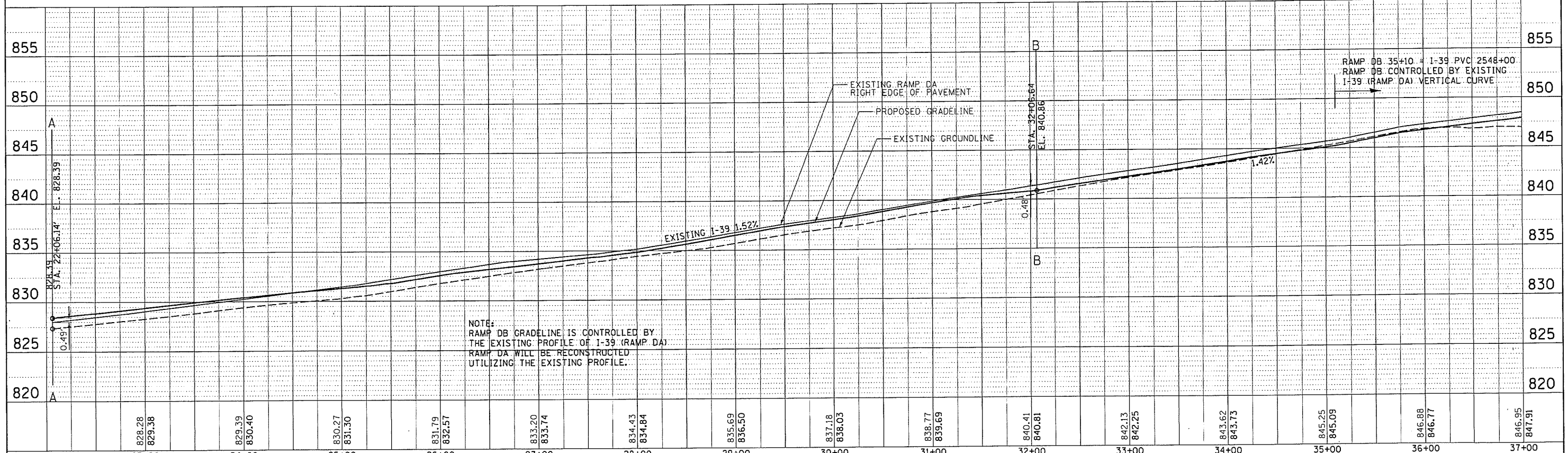
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NO. _____	BY _____	DATE _____
NO. _____	CHECKED _____	DATE _____
NO. _____	DATE _____	DATE _____



RAMP BD STA. 213+00 TO STA. 215+00

RAMP DB STA. 22+00 TO STA. 37+00

PROFILE	REVISIONS	DATE
NO. _____	BY _____	DATE _____
NO. _____	CHECKED _____	DATE _____
NO. _____	DATE _____	DATE _____

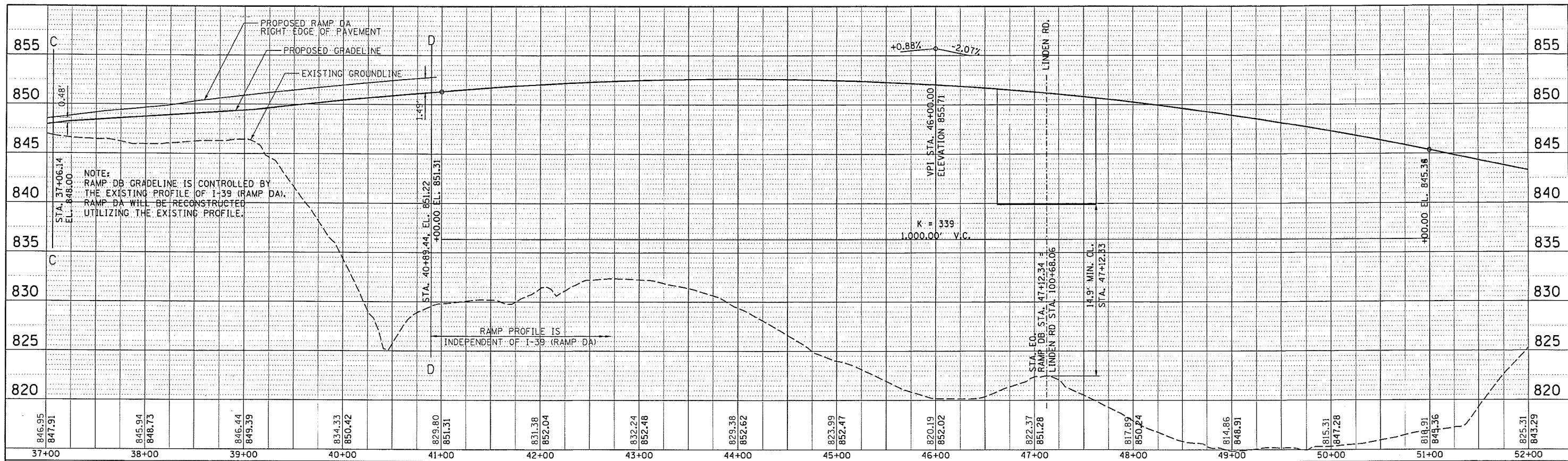


NOTE:
RAMP DB GRADELINE IS CONTROLLED BY THE EXISTING PROFILE OF I-39 (RAMP DA). RAMP DA WILL BE RECONSTRUCTED UTILIZING THE EXISTING PROFILE.

RAMP DB 35+10 = I-39 PVC 2548+00. RAMP DB CONTROLLED BY EXISTING I-39 (RAMP DA) VERTICAL CURVE.

FILE NAME = I:\063jobs\2652055\CADD\Road\Sheet\VC-PF-11.dgn	USER NAME = oimj01238	DESIGNED - DPA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RAMP BD (SB I-39 MAINLINE) STA. 213+00 TO STA. 215+00 PROFILE RAMP DB (NB I-39 MAINLINE) STA. 22+00 TO STA. 37+00 PROFILE	F.A.I. RTE. 39	SECTION (201-3IK & 14-1,5IK)	COUNTY WINNEBAGO	TOTAL SHEETS 23	SHEET NO. 15		
PLOT SCALE = 50.0000' / 1" =	CHECKED - DPA	REVISED -	SCALE:			SHEET NO. OF SHEETS STA. TO STA.	CONTRACT NO.					
PLOT DATE = 08/14/2012	DATE - 01/28/11	REVISED -	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT									

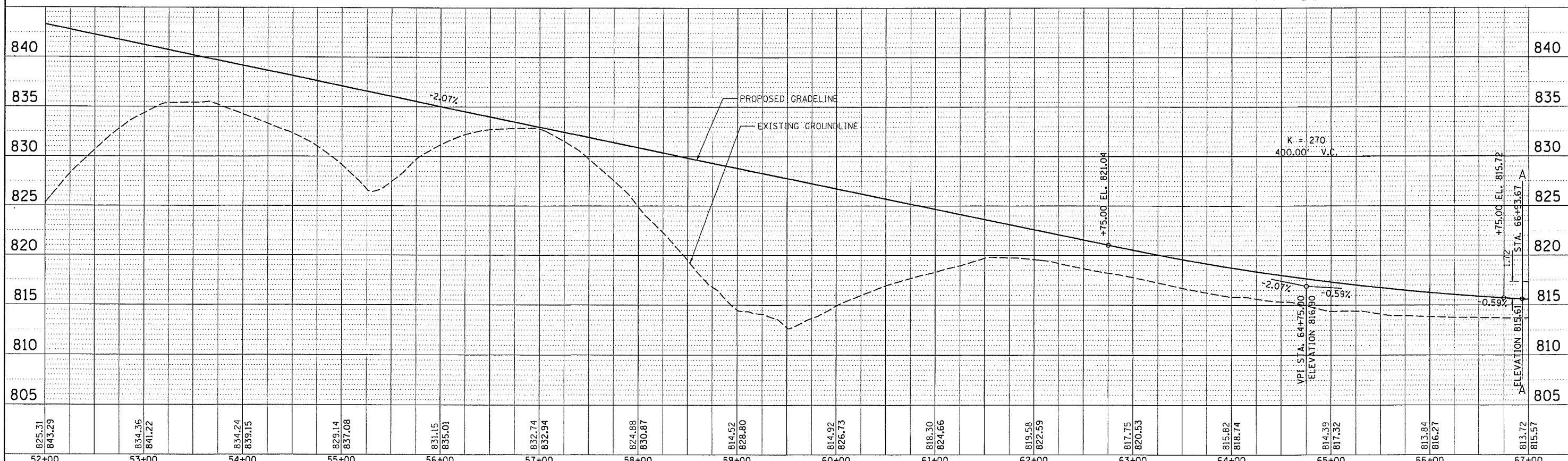
PLAN	SURVEYED	BY	DATE
	PLOTTED		
	NOTE BOOK		
	NO.		
	CADD FILE NAME		



RAMP DB STA. 37+00 TO STA. 52+00

RAMP DB STA. 52+00 TO STA. 67+00

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
	NOTE BOOK		
	NO.		
	STRUCTURE NOTATION		



FILE NAME =	USER NAME = eimj01238	DESIGNED - DPA	REVISED -
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PLOT DATE = 08/14/2012		DATE - 01/28/11	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

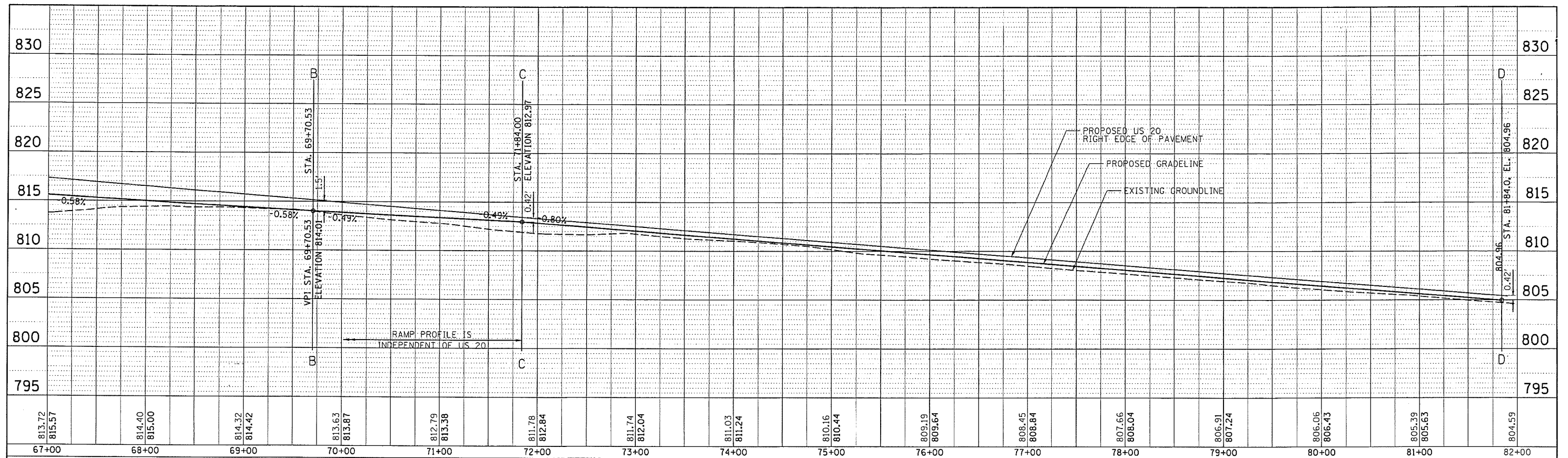
RAMP DB (NB I-39 MAINLINE) STA. 37+00 TO STA. 67+00 PROFILE

SCALE:	SHEET NO.	OF	SHEETS	STA.	TO	STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1)5K	WINNEBAGO	23	16
CONTRACT NO.				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	BY	DATE
	PLOTTED		
	ALIGNED		
	CHECKED		
	NO. _____		
	NOTE BOOK		
	NO. _____		
	CADD FILE NAME		

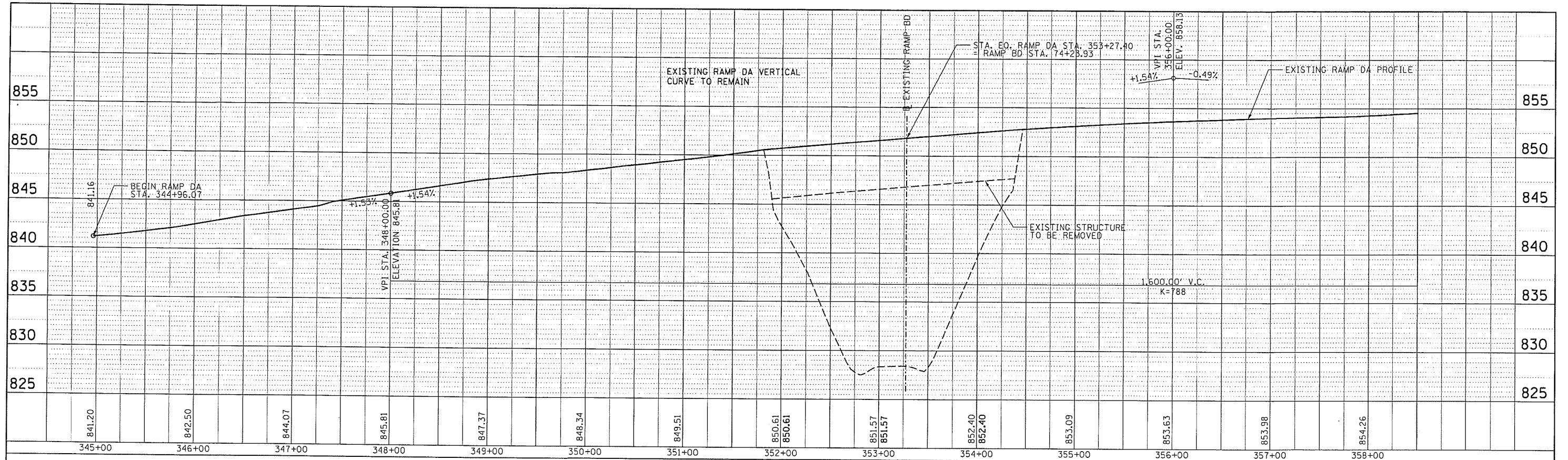
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	PLOTTED		
	ALIGNED		
	CHECKED		
	NO. _____		
	NOTE BOOK		
	NO. _____		
	STRUCTURE NOTATIONS CHK'D		



RAMP DB STA. 67+00 TO STA. 82+00

FILE NAME =	USER NAME = almy01238	DESIGNED - DPA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	RAMP DB (NB I-39 MAINLINE) STA. 67+00 TO STA. 82+00	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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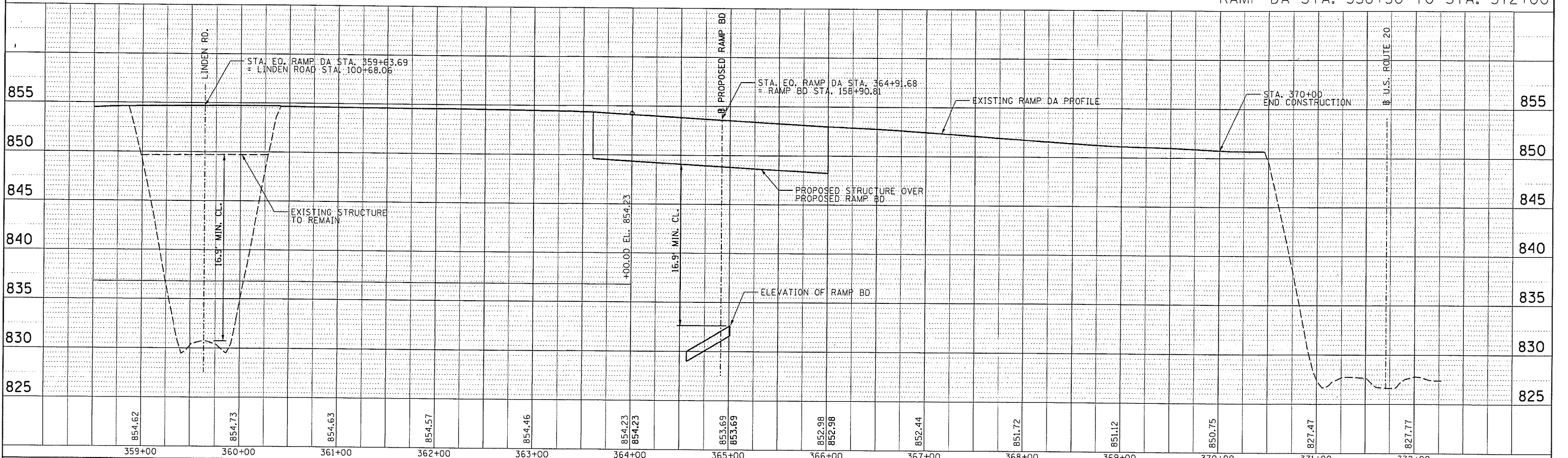
PLAN	SURVEYED	BY	DATE
	PLOTTED		
	CHECKED		
	BY		
	DATE		
	NO. OF PAGES CHECKED		
	CADD FILE NAME		



RAMP DA STA. 344+96.07 TO STA. 358+50

RAMP DA STA. 358+50 TO STA. 372+00

PROFILE	SURVEYED	BY	DATE
	DESIGNED		
	CHECKED		
	BY		
	DATE		
	NO. OF PAGES CHECKED		
	STRUCTURE NOTATING DPMO		



FILE NAME = I:\06Jobs\065295\CADD\Road\Sheet\C-PF-18.dgn
 USER NAME = ajm01238
 PLOT SCALE = 50.0000' / 1" =
 PLOT DATE = 08/14/2012

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DRAWN -	JDM	REVISED -	
CHECKED -	DPA	REVISED -	
DATE -	01/28/11	REVISED -	

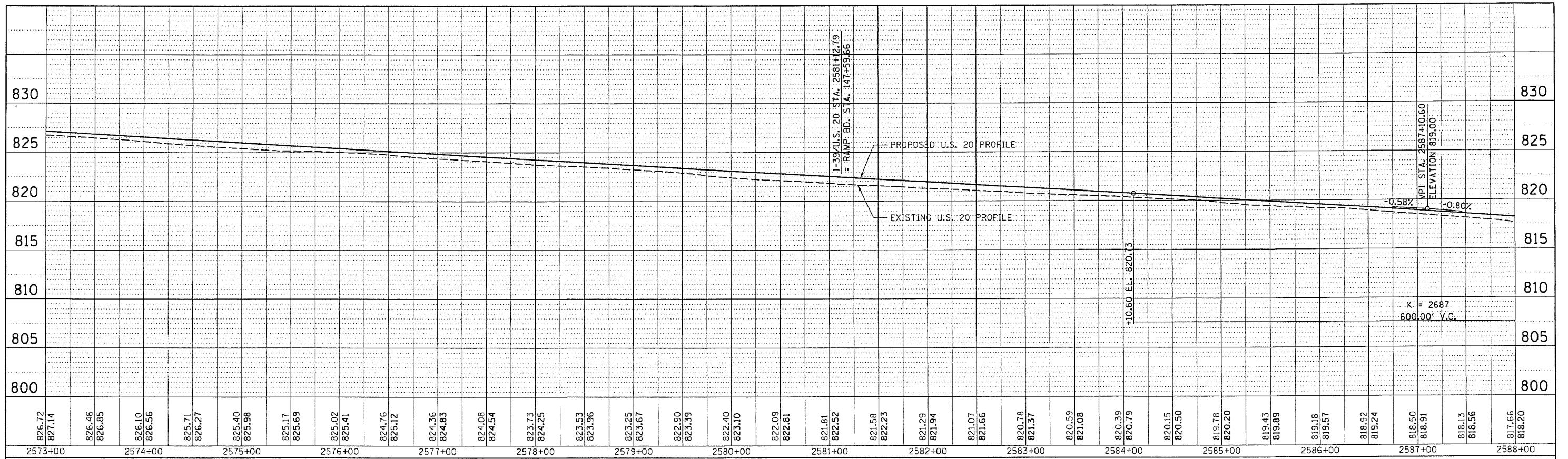
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

RAMP DA STA. 344 + 96.07 TO STA. 372 + 00 PROFILE

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-31K & 4-1,5K)	WINNEBAGO	23	18
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

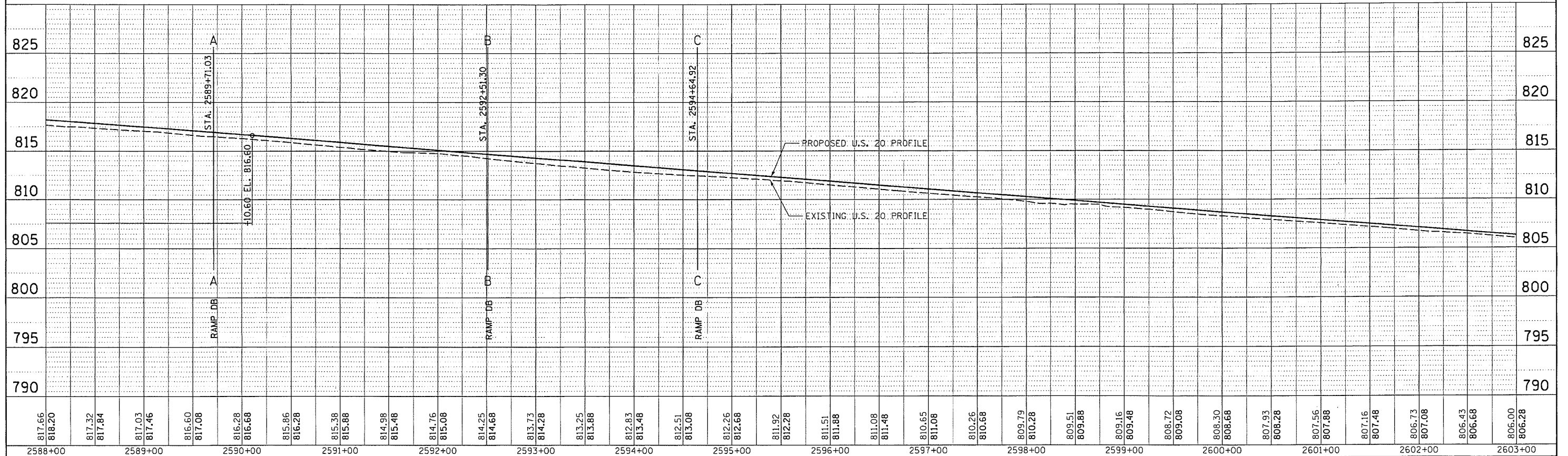
PLAN	SURVEYED	BY	DATE
NO. _____	PLOTTED		
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	RTI OF WAY CHECKED		
	CADD FILE NAME		



I-39/U.S. 20 STA. 2573+00 TO STA. 2588+00

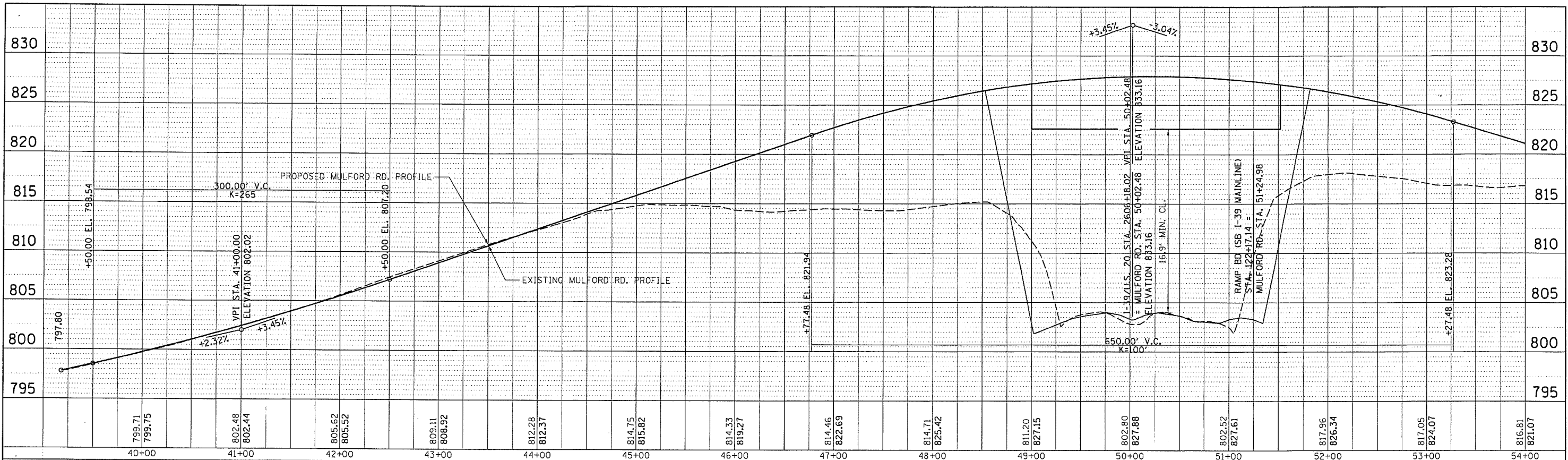
I-39/U.S. 20 STA. 2588+00 TO STA. 2603+00

PROFILE	SURVEYED	BY	DATE
NO. _____	PLOTTED		
	NOTED		
	RTI OF WAY CHECKED		
	CADD FILE NAME		



FILE NAME =	USER NAME =	DESIGNED -	REVISED -	<p align="center">STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</p> <p align="center">I-39/U.S. 20 STA. 2573+00 TO STA. 2603+00 PROFILE</p>	F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 50.0000' / in.		CHECKED -	REVISED -		CONTRACT NO.				
PLOT DATE = 08/14/2012		DATE -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
		01/28/11			SCALE:	SHEET NO.	OF	SHEETS	STA.

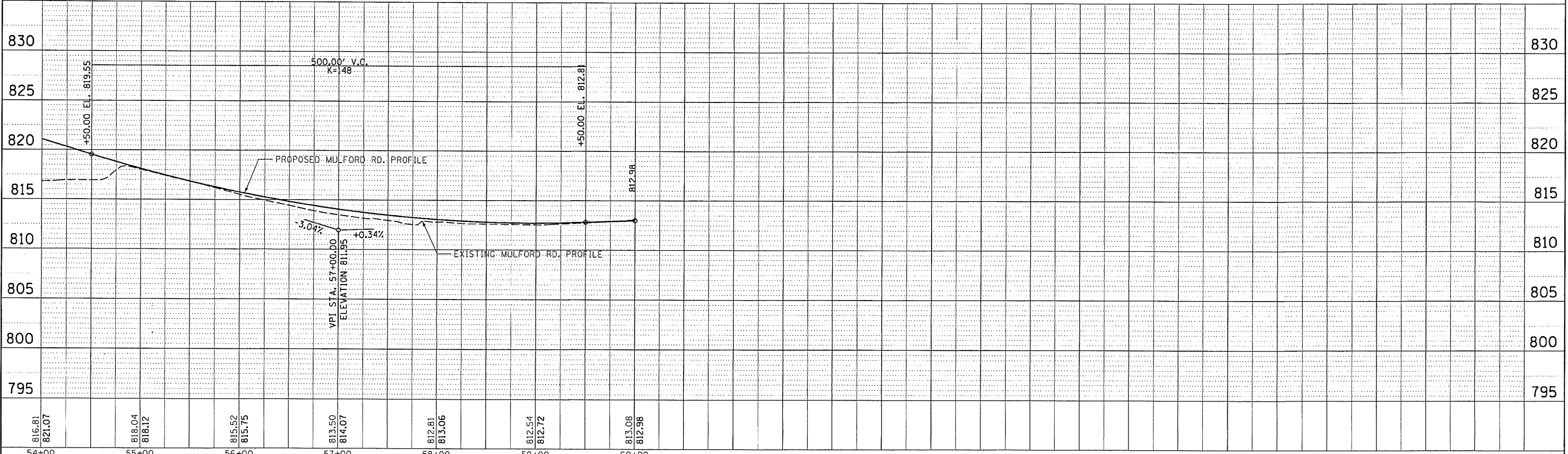
DATE	
BY	
SURVEYED	
ALIGNED	
CHECKED	
NOTE BOOK	
NO.	
PLAN	
NO.	



MULFORD RD. STA. 39+17.94 TO STA. 54+00

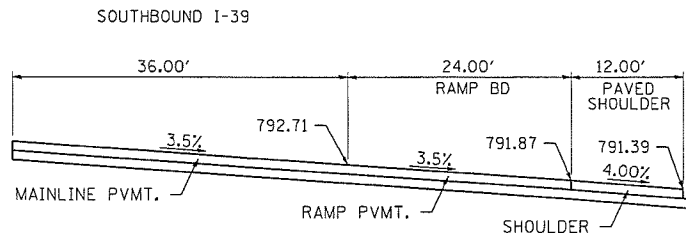
MULFORD RD. STA. 54+00 TO STA. 60+00

DATE	
BY	
DESIGNED	
DRAWN	
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PROFILE	
NO.	

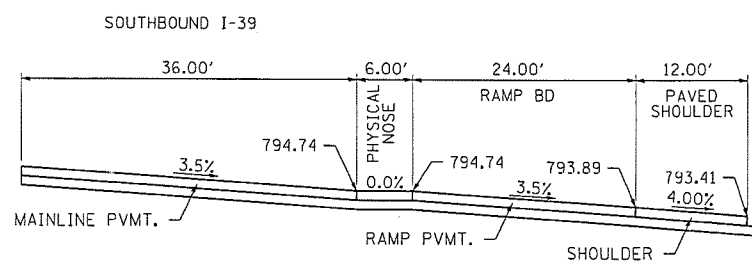


FILE NAME =	USER NAME = e1my01238	DESIGNED - DPA	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MULFORD RD. STA. 39+17.94 TO STA. 54+00 PROFILE MULFORD RD. STA. 54+00 TO STA. 60+00 PROFILE	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT DATE = 28/14/2012	DATE - 01/28/11	CHECKED - DPA	REVISED -			CONTRACT NO.				
		DATE - 01/28/11	REVISED -			FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				

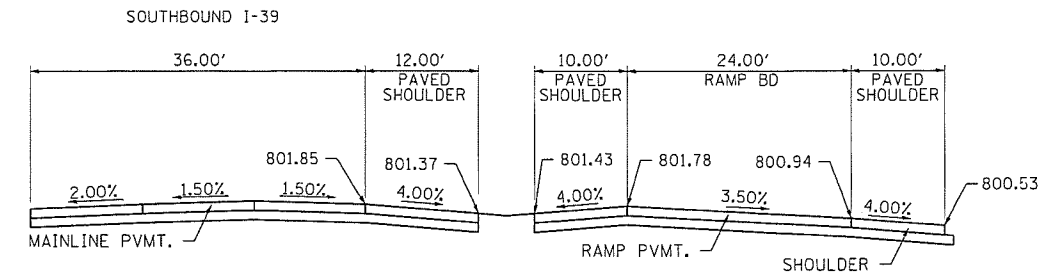
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 USER NAME = elmg0238



SECTION B-B
 RAMP BD STA. 110+04.56
 I-39 STA. 2518+39

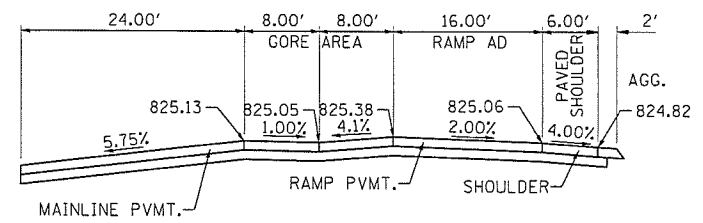


SECTION C-C
 RAMP BD STA. 112+53.79
 I-39 STA. 2615+86.90

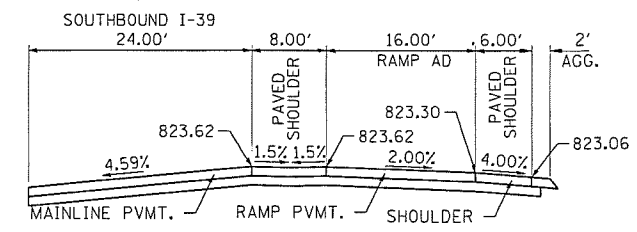


SECTION D-D
 RAMP BD STA. 119+47.66
 I-39 STA. 2608+86.68

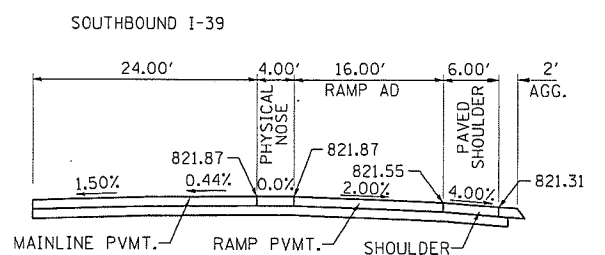
"RAMP BD"
 SOUTHBOUND I-39
 ONE SIDED MAJOR DIVERGENCE



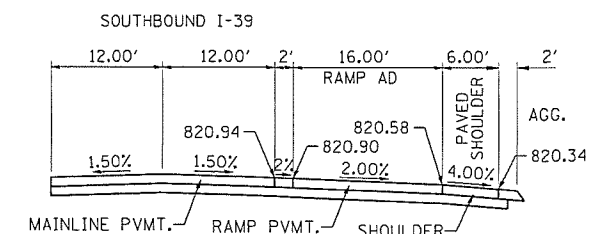
SECTION A-A
 RAMP AD STA. 83+58.43
 RAMP BD STA. 180+24.37



SECTION B-B
 RAMP AD STA. 85+26.17
 RAMP BD STA. 181+91.78



SECTION C-C
 RAMP AD STA. 87+15.41
 RAMP BD STA. 183+80.26

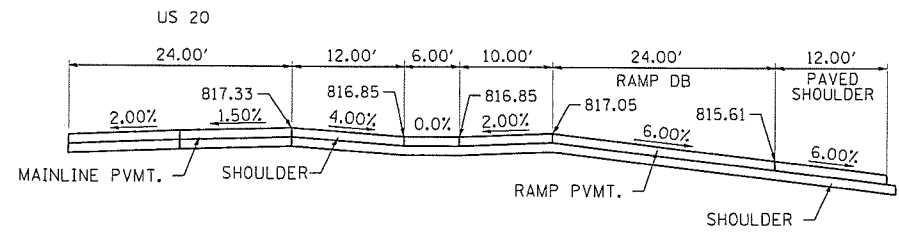


SECTION D-D
 RAMP AD STA. 88+15.41
 RAMP BD STA. 184+80.28

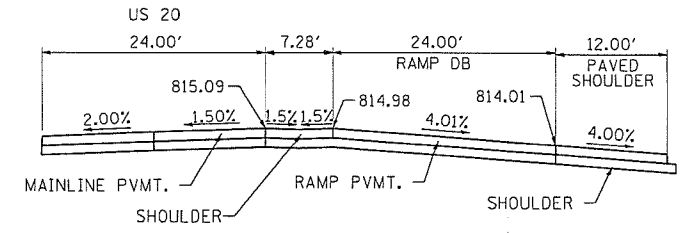
"RAMP AD"
 U.S. ROUTE 20 SOUTHBOUND ENTRANCE RAMP
 WITH SOUTHBOUND I-39 (RAMP BD)

INTERCHANGE DESIGN STUDY
 I-39 (INTERSTATE 39)
 WITH I-39 (U.S. ROUTE 20 BYPASS)
 SEC. NO. (201-3)K&(4-1)5K
 SCALE 1"=50' COUNTY WINNEBAGO
 SJN : _____ PROJ. NO. P-92-111-06
 I.D.S. SHEET 22 OF 23

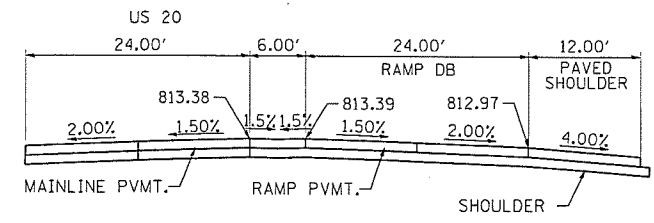
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 FILE NAME = I:\05\14\0652895\CADD\Road\Sheet\I-39-23.dgn
 PLOT SCALE = 1/8"=50' / 1"=100'
 USER NAME = elmj@123



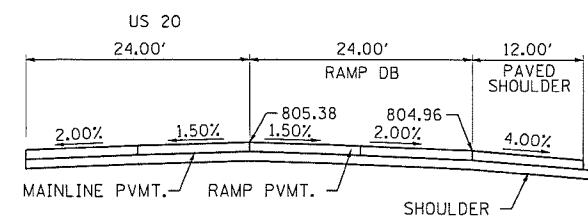
SECTION A-A
 RAMP DB STA. 66+93.67
 US 20 737+60.43



SECTION B-B
 RAMP DB STA. 69+70.53
 US 20 740+40.93

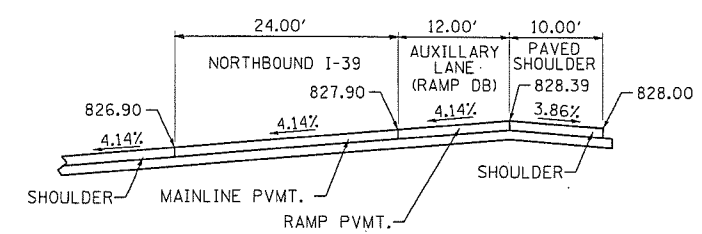


SECTION C-C
 RAMP DB STA. 71+84.03
 US 20 742+54.24

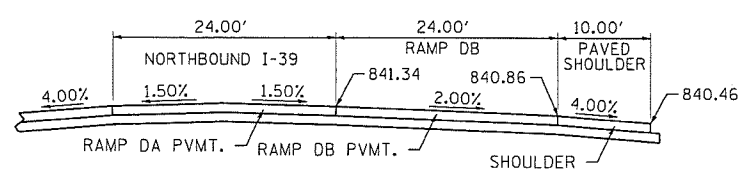


SECTION D-D
 RAMP DB STA. 81+84.05
 US 20 752+54.47

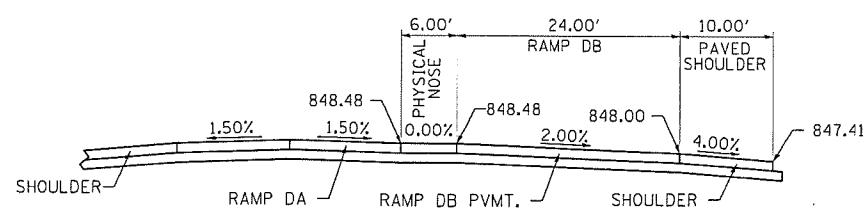
"RAMP DB"
 NORTHBOUND I-39
 MAJOR CONVERGENCE



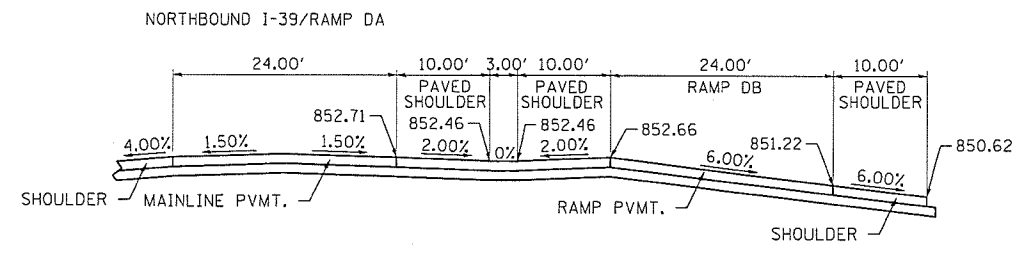
SECTION A-A
 RAMP DB STA. 22+06.14
 I-39 STA. 2534+95.86



SECTION B-B
 RAMP DB STA. 32+06.64
 I-39 STA. 2544+96.28



SECTION C-C
 RAMP DB STA. 37+06.14
 I-39 STA. 2549+95.75



SECTION D-D
 RAMP DB STA. 40+89.44
 I-39 STA. 2553+83.15

"RAMP DB"
 NORTHBOUND I-39
 ONE SIDED MAJOR DIVERGENCE

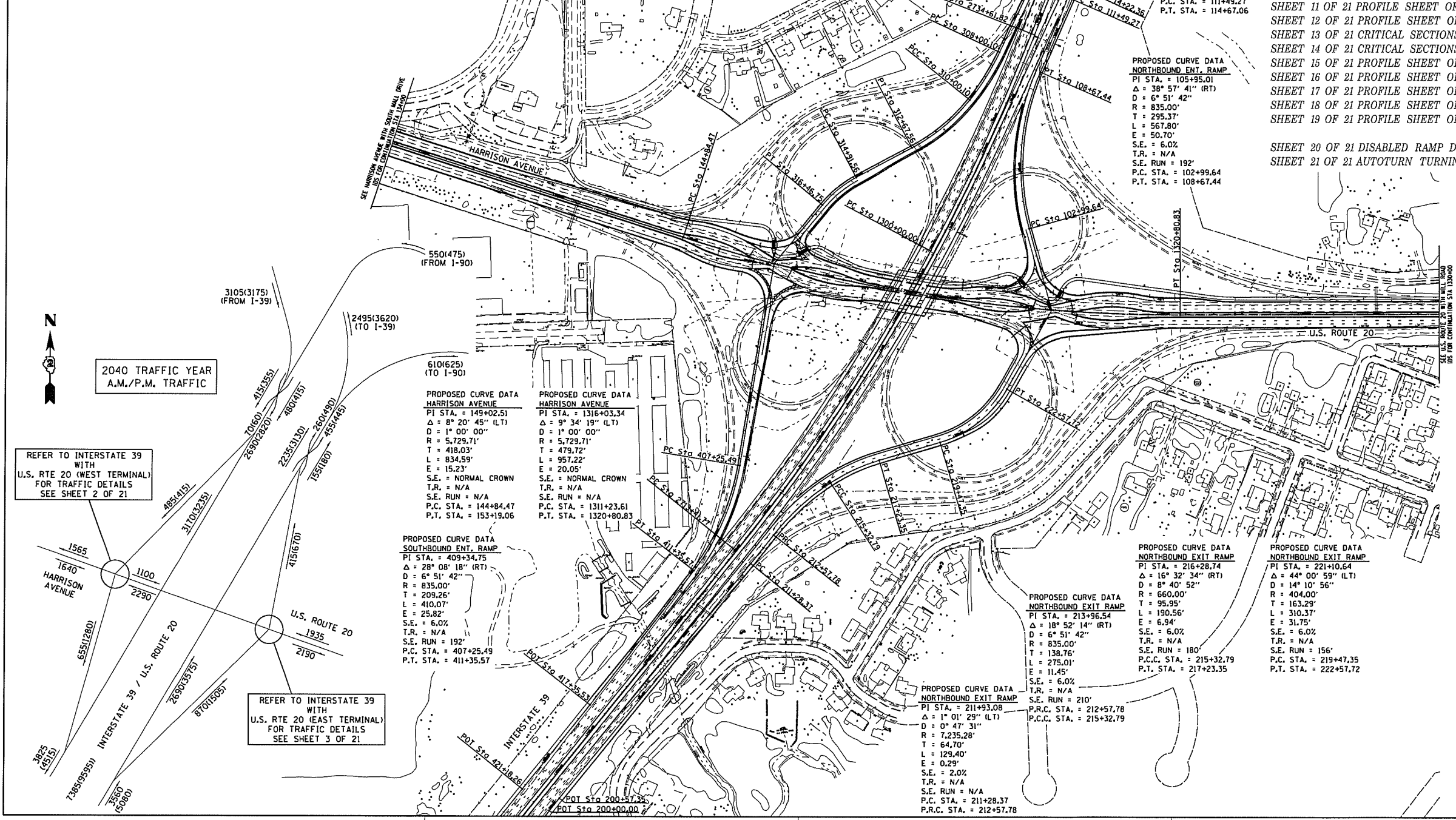
INTERCHANGE DESIGN STUDY
 FAI ROUTE 39 (INTERSTATE 39)
 FAP ROUTE 301 WITH (U.S. ROUTE 20 BYPASS)
 SEC. NO. (201-3)K&(4-1),5K
 SCALE 1"=50' COUNTY WINNEBAGO
 SJN : _____ PROJ. NO. P-92-111-06
 I.D.S. SHEET 23 OF 23

INTERSTATE 39 & U.S. ROUTE 20 WITH HARRISON AVENUE

INDEX OF SHEETS

- SHEET 1 OF 21 COVER SHEET
- SHEET 2 OF 21 IDS SHEET WITH I-39 / U.S. 20 WITH HARRISON AVE. (WEST TERMINAL)
- SHEET 3 OF 21 IDS SHEET WITH I-39 / U.S. 20 WITH HARRISON AVE. (EAST TERMINAL)
- SHEET 4 OF 21 PLAN SHEET WITH HARRISON AVENUE (EAST & WEST PROJECT LIMITS)
- SHEET 5 OF 21 PLAN SHEET WITH I-39 N.B. EXIT AND S.B. ENTRANCE RAMP
- SHEET 6 OF 21 PROFILE SHEET OF SOUTHBOUND ENTRANCE RAMP PROFILE
- SHEET 7 OF 21 PROFILE SHEET OF NORTHBOUND EXIT RAMP PROFILE
- SHEET 8 OF 21 CRITICAL SECTIONS OF NORTHBOUND EXIT RAMP
- SHEET 9 OF 21 CRITICAL SECTIONS OF SOUTHBOUND ENTRANCE RAMP
- SHEET 10 OF 21 PLAN SHEET WITH I-39 N.B. ENTRANCE AND S.B. EXIT RAMP
- SHEET 11 OF 21 PROFILE SHEET OF NORTHBOUND ENTRANCE RAMP PROFILE
- SHEET 12 OF 21 PROFILE SHEET OF SOUTHBOUND EXIT RAMP PROFILE
- SHEET 13 OF 21 CRITICAL SECTIONS OF NORTHBOUND ENTRANCE RAMP
- SHEET 14 OF 21 CRITICAL SECTIONS OF SOUTHBOUND EXIT RAMP
- SHEET 15 OF 21 PROFILE SHEET OF INTERSTATE 39 (2717+00 - 2744+00)
- SHEET 16 OF 21 PROFILE SHEET OF NORTHBOUND I-39 (6+00 - 27+00)
- SHEET 17 OF 21 PROFILE SHEET OF SOUTHBOUND I-39 (1300+00 - 1319+00)
- SHEET 18 OF 21 PROFILE SHEET OF HARRISON AVENUE (134+00 - 1321+00)
- SHEET 19 OF 21 PROFILE SHEET OF HARRISON AVENUE & I-39 / U.S. RTE 20 (1321+00 - 1330+00) (2702+00 - 2717+00)
- SHEET 20 OF 21 DISABLED RAMP DETAIL SHEET
- SHEET 21 OF 21 AUTOTURN TURNING PATH SHEET

INTERCHANGE DESIGN STUDY				
WEAVING ANALYSIS - PROGRAM USED HCS V5.4				
Weaving Section	I-39 SOUTHBOUND		I-39 NORTHBOUND	
Terrain/Grade	Level		Level	
Type of Weave	Type C		Type A	
Number of Total Lanes	3		3	
Peak Hour Factor	0.92		0.92	
Free Flow Speed (FFS)	65 mph		65 mph	
Length of Weaving (ft)	1,700'		1,300'	
Volume 1 (Through Volume)	AM	920	PM	2,960
	PM	920	AM	2,960
Volume 2 (Exit Weave)	AM	140	PM	430
	PM	140	AM	430
Volume 3 (Entrance Weave)	AM	2470	PM	345
	PM	2470	AM	345
Volume 4 (Ramp-Ramp)	AM	375	PM	130
	PM	375	AM	130
TRUCK TYPES	Trucks & Buses, RV		Trucks & Buses, RV	
% Trucks	25%		25%	
Population Factor	1.0		1.0	
Weaving Intensity Factor, WI	Weaving	1.09	Non-Weaving	0.90
	Non-Weaving	1.66	Weaving	0.47
Weaving and Non-weaving Speeds, S1	41.34	35.68	43.97	52.44
Weaving Segment Speed, S	39.30 mph		50.53 mph	
Weaving Segment Density, D	40.12 pc/ml/in		30.90 pc/ml/in	
Level of Service A.M. = P.M.	E		D	
Capacity of Base Condition, cb	5496 pc/h		5994 pc/h	
Constrained/Unconstrained	Unconstrained		Unconstrained	



PROPOSED CURVE DATA SOUTHBOUND I-39 BASELINE
 PI STA. = 1304+25.28
 $\Delta = 6^\circ 42' 35.53''$ (RT)
 $D = 0^\circ 47' 23.23''$
 $R = 7,254.61'$
 $T = 425.28'$
 $L = 849.58'$
 $E = 12.45'$
 $S.E. = 3.0\%$
 $T.R. = 82^\circ$
 $S.E. RUN = 175'$
 P.C. STA. = 1300+00.00
 P.T. STA. = 1308+49.58

PROPOSED CURVE DATA SOUTHBOUND EXIT RAMP
 PI STA. = 315+70.17
 $\Delta = 22^\circ 21' 12''$ (LT)
 $D = 14^\circ 28' 07''$
 $R = 396.00'$
 $T = 78.60'$
 $L = 155.19'$
 $E = 7.73'$
 $S.E. = 6.0\%$
 $T.R. = N/A$
 $S.E. RUN = 156'$
 P.C. STA. = 314+91.56
 P.T. STA. = 316+46.75

PROPOSED CURVE DATA SOUTHBOUND EXIT RAMP
 PI STA. = 309+00.58
 $\Delta = 13^\circ 43' 25''$ (RT)
 $D = 6^\circ 51' 42''$
 $R = 835.00'$
 $T = 100.48'$
 $L = 200.00'$
 $E = 6.02'$
 $S.E. = 6.0\%$
 $T.R. = N/A$
 $S.E. RUN = 210'$
 P.C. STA. = 308+00.10
 P.C.C. STA. = 310+00.10

PROPOSED CURVE DATA SOUTHBOUND EXIT RAMP
 PI STA. = 311+35.69
 $\Delta = 23^\circ 13' 09''$ (RT)
 $D = 8^\circ 40' 52''$
 $R = 660.00'$
 $T = 135.59'$
 $L = 267.47'$
 $E = 13.78'$
 $S.E. = 6.0\%$
 $T.R. = N/A$
 $S.E. RUN = 180'$
 P.C. STA. = 310+00.10
 P.T. STA. = 312+67.56

PROPOSED CURVE DATA NORTHBOUND ENT. RAMP
 PI STA. = 113+08.22
 $\Delta = 3^\circ 40' 46''$ (RT)
 $D = 1^\circ 09' 28''$
 $R = 4,948.52'$
 $T = 158.95'$
 $L = 317.79'$
 $E = 2.55'$
 $S.E. = VARIABLE$
 $T.R. = N/A$
 $S.E. RUN = N/A$
 P.C. STA. = 111+49.27
 P.T. STA. = 114+67.06

PROPOSED CURVE DATA NORTHBOUND ENT. RAMP
 PI STA. = 105+95.01
 $\Delta = 38^\circ 57' 41''$ (RT)
 $D = 6^\circ 51' 42''$
 $R = 835.00'$
 $T = 295.37'$
 $L = 567.80'$
 $E = 50.70'$
 $S.E. = 6.0\%$
 $T.R. = N/A$
 $S.E. RUN = 192'$
 P.C. STA. = 102+99.64
 P.T. STA. = 108+67.44

PROPOSED CURVE DATA INTERSTATE 39
 PI STA. = 2724+10.28
 $\Delta = 17^\circ 40' 00''$ (LT)
 $D = 0^\circ 50' 00''$
 $R = 6,875.65'$
 $T = 1,068.50'$
 $L = 2,120.05'$
 $E = 82.53'$
 $S.E. = 3.0\%$
 $T.R. = N/A$
 $S.E. RUN = N/A$
 P.C. STA. = 2713+41.77
 P.T. STA. = 2734+61.82

PROPOSED CURVE DATA HARRISON AVENUE
 PI STA. = 149+02.51
 $\Delta = 8^\circ 20' 45''$ (LT)
 $D = 1^\circ 00' 00''$
 $R = 5,729.71'$
 $T = 418.03'$
 $L = 834.59'$
 $E = 15.23'$
 $S.E. = NORMAL CROWN$
 $T.R. = N/A$
 $S.E. RUN = N/A$
 P.C. STA. = 144+84.47
 P.T. STA. = 153+19.06

PROPOSED CURVE DATA HARRISON AVENUE
 PI STA. = 1316+03.34
 $\Delta = 9^\circ 34' 19''$ (LT)
 $D = 1^\circ 00' 00''$
 $R = 5,729.71'$
 $T = 479.72'$
 $L = 957.22'$
 $E = 20.05'$
 $S.E. = NORMAL CROWN$
 $T.R. = N/A$
 $S.E. RUN = N/A$
 P.C. STA. = 1311+23.61
 P.T. STA. = 1320+80.83

PROPOSED CURVE DATA SOUTHBOUND ENT. RAMP
 PI STA. = 409+34.75
 $\Delta = 28^\circ 08' 18''$ (RT)
 $D = 6^\circ 51' 42''$
 $R = 835.00'$
 $T = 209.26'$
 $L = 410.07'$
 $E = 25.82'$
 $S.E. = 6.0\%$
 $T.R. = N/A$
 $S.E. RUN = 192'$
 P.C. STA. = 407+25.49
 P.T. STA. = 411+35.57

PROPOSED CURVE DATA NORTHBOUND EXIT RAMP
 PI STA. = 213+96.54
 $\Delta = 18^\circ 52' 14''$ (RT)
 $D = 6^\circ 51' 42''$
 $R = 835.00'$
 $T = 138.76'$
 $L = 275.01'$
 $E = 11.45'$
 $S.E. = 6.0\%$
 $T.R. = N/A$
 $S.E. RUN = 210'$
 P.C. STA. = 212+57.78
 P.C.C. STA. = 215+32.79

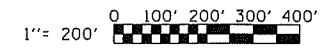
PROPOSED CURVE DATA NORTHBOUND EXIT RAMP
 PI STA. = 216+28.74
 $\Delta = 16^\circ 32' 34''$ (RT)
 $D = 8^\circ 40' 52''$
 $R = 660.00'$
 $T = 95.95'$
 $L = 190.56'$
 $E = 6.94'$
 $S.E. = 6.0\%$
 $T.R. = N/A$
 $S.E. RUN = 180'$
 P.C. STA. = 215+32.79
 P.T. STA. = 217+23.35

PROPOSED CURVE DATA NORTHBOUND EXIT RAMP
 PI STA. = 221+10.64
 $\Delta = 44^\circ 00' 59''$ (LT)
 $D = 14^\circ 10' 56''$
 $R = 404.00'$
 $T = 163.29'$
 $L = 310.37'$
 $E = 31.75'$
 $S.E. = 6.0\%$
 $T.R. = N/A$
 $S.E. RUN = 156'$
 P.C. STA. = 219+47.35
 P.T. STA. = 222+57.72

2040 TRAFFIC YEAR
A.M./P.M. TRAFFIC

REFER TO INTERSTATE 39 WITH U.S. RTE 20 (WEST TERMINAL) FOR TRAFFIC DETAILS SEE SHEET 2 OF 21

REFER TO INTERSTATE 39 WITH U.S. RTE 20 (EAST TERMINAL) FOR TRAFFIC DETAILS SEE SHEET 3 OF 21



INTERCHANGE DESIGN STUDY
 FAI ROUTE 39 WITH (INTERSTATE 39)
 FAP ROUTE 301 WITH (U.S. 20/HARRISON AVE.)

SIGNALIZED CAPACITY DESIGN ANALYSIS

PROGRAM USED: VISSIM, VERSION: 8.0, SIGNAL TYPE: ACTUATED, AREA TYPE: OTHER
 NO. OF PHASES: (A.M.) 2 (P.M.) 2, CYCLE LENGTH: (A.M.) 90 SEC. (P.M.) 90 SEC., PEAK HOUR FACTOR: 0.92
 INTERSECTION DELAY/LEVEL-OF-SERVICE: A.M. 12.84 SECONDS LOS_B, P.M. 15.31 SECONDS LOS_B

APPROACH	A	B	C
LANE GROUP			
NUMBER OF LANES	2	1	3
2040 30TH MAX. HOUR TRAFFIC (veh/h)	A.M. 1325 P.M. 1325	600 690	1215 150
BASE SATURATION FLOW RATE (veh/h)	1900	1900	1900
LANE WIDTH (FT)	12	12	12
VOLUME OF RIGHT TURN ON RED (veh/h)	A.M. P.M.	A.M. P.M.	A.M. P.M.
PEDESTRIANS/HOUR (ped/h)	A.M. P.M.	A.M. P.M.	A.M. P.M.
ARRIVAL TYPE	4	4	3
LANE UTILIZATION ADJ. FACTOR	1.00	1.00	1.00
GREEN TIME (SECONDS)	A.M. 38 P.M. 40	38 35	N/A N/A
GREEN RATIO (g/c)	A.M. 0.42 P.M. 0.44	0.42 0.40	
CAPACITY (c)	A.M. 1880 P.M. 1620	1600 1700	2130 1900
v/c RATIO (X)	A.M. 0.34 P.M. 0.82	0.11 0.39	0.28 0.49
STORAGE QUEUE (FEET OR VEHICLES)	A.M. 207 P.M. 390	0 0	167 240
LANE GROUP DELAY (SECONDS)	A.M. 12.80 P.M. 11.45	2.00 3.88	5.21 9.45
LANE GROUP LEVEL-OF-SERVICE	A.M. B P.M. B	A A	B B

PROPOSED CURVE DATA HARRISON AVENUE
 PI STA. = 149+02.51
 $\Delta = 8^\circ 20' 45''$ (LT)
 $D = 1^\circ 00' 00''$
 $R = 5,729.71'$
 $T = 418.03'$
 $L = 834.59'$
 $E = 15.23'$
 S.E. = NORMAL CROWN
 T.R. = N/A
 S.E. RUN = N/A
 P.C. STA. = 144+84.47
 P.T. STA. = 153+19.06

PROPOSED CURVE DATA SOUTHBOUND EXIT RAMP
 PI STA. = 315+70.17
 $\Delta = 22^\circ 27' 12''$ (LT)
 $D = 14^\circ 28' 07''$
 $R = 396.00'$
 $T = 78.60'$
 $L = 155.19'$
 $E = 7.73'$
 S.E. = 6.0%
 T.R. = N/A
 S.E. RUN = 156'
 P.C. STA. = 314+91.56
 P.T. STA. = 316+46.75

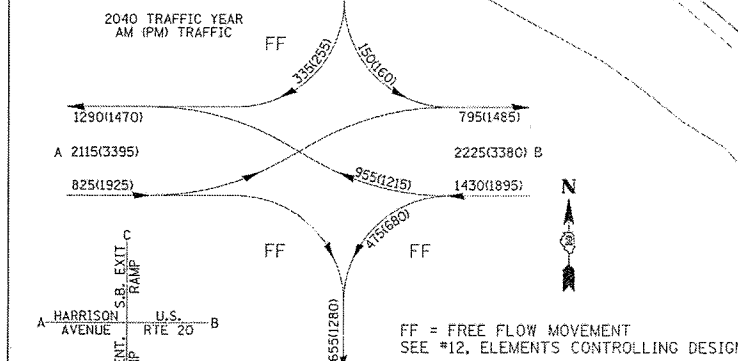
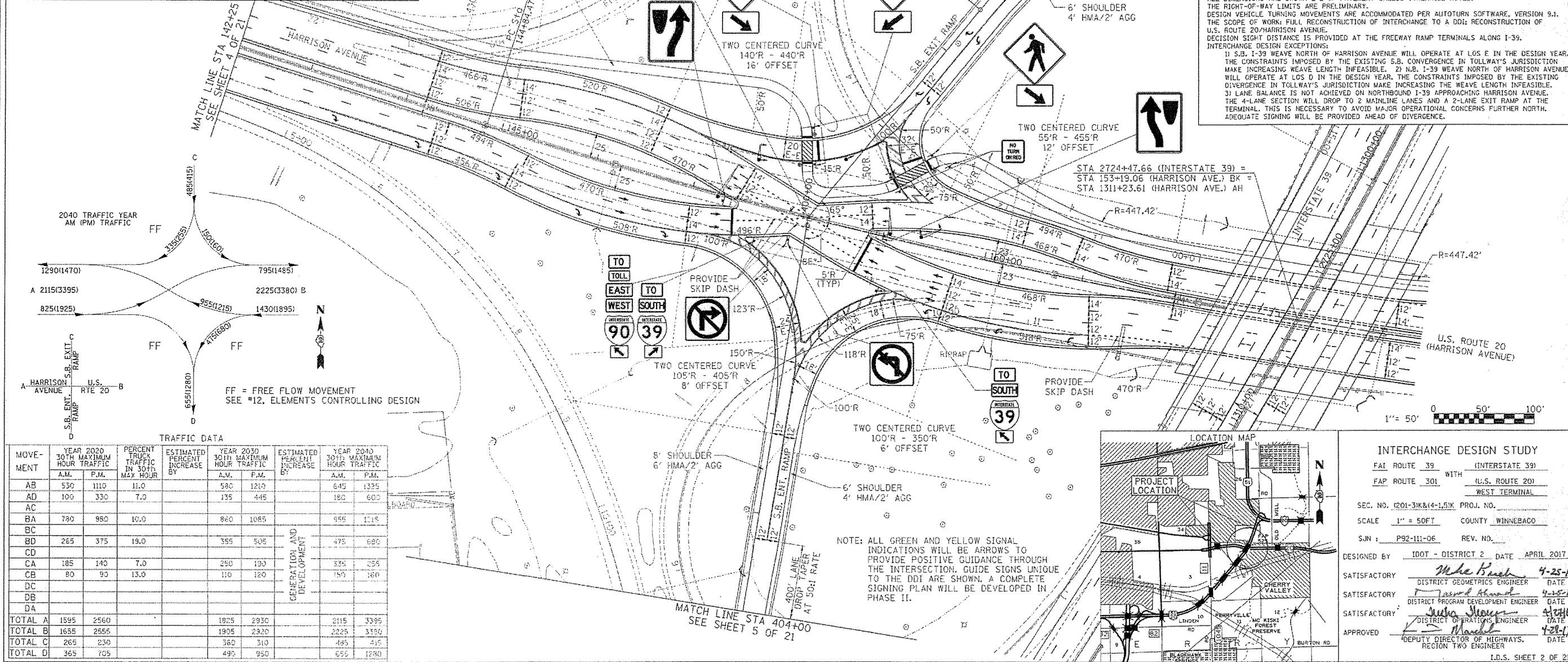
ELEMENTS CONTROLLING DESIGN

PREFERRED ROUTE:
 F.A.P. ROUTE NUMBER: 301, MARKED ROUTE NUMBER: U.S. ROUTE 20.
 STREET NAME: U.S. ROUTE 20, SRA ROUTE? NO.
 FUNCTIONAL CLASSIFICATION: OTHER PRINCIPAL ARTERIAL, OSOW DESIGN? YES.
 EXISTING ADT: 25,600 (2020) VPD, DESIGN YEAR ADT: 33,950 (2040) VPD.
 PROPOSED DESIGN SPEED: 35 MPH, PROPOSED POSTED SPEED: 35 MPH.

SECONDARY ROUTE:
 F.A.P. ROUTE NUMBER: 39 RAMP, MARKED ROUTE NUMBER: INTERSTATE 39 RAMP.
 STREET NAME: INTERSTATE 39 RAMP, SRA ROUTE? NO.
 FUNCTIONAL CLASSIFICATION: INTERSTATE, OSOW DESIGN? YES.
 EXISTING ADT: 7,050 (2020) VPD, DESIGN YEAR ADT: 12,800 (2040) VPD.
 PROPOSED DESIGN SPEED: 35-50 MPH, PROPOSED POSTED SPEED: N/A.

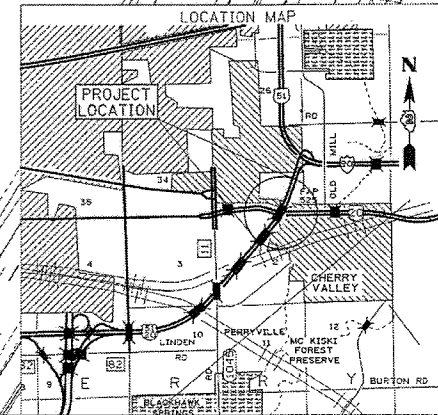
IMPROVEMENT TYPE: FULL RECONSTRUCTION TO A DDI, ANTICIPATED YEAR OF CONSTRUCTION: 2020.
 EXISTING METHOD OF TRAFFIC CONTROL: CLOVERLEAF, PROPOSED METHOD: DDI WITH TRAFFIC SIGNALS.
 SIGNAL WARRANT: MUTCD WARRANT 1.
 DESIGN VEHICLE: WB-65 DESIGN VEHICLE.
 DESIGN YEAR: 2040 WHICH IS A 20 YEAR DESIGN.
 TRUCK ROUTE DESIGNATION: PREFERRED ROADWAY: U.S. 20 (EAST LEG) IS A CLASS II TRUCK ROUTE.
 SECONDARY ROADWAY: I-39 RAMP ARE ENTERING AND EXITING AT CLASS I TRUCK ROUTE.
 DESIGN CRITERIA: SEE CHAPTERS 36 & 37.
 FIELD OBSERVATIONS INDICATE THAT PEDESTRIAN AND BIKE VOLUMES IN THE PROJECT AREA ARE CURRENTLY MINIMAL. PEDESTRIAN PUSH BUTTONS WILL BE INSTALLED WITH THE TRAFFIC SIGNALS TO PROVIDE THE GREEN TIME REQUIRED FOR PEDESTRIAN CROSSINGS. THE S.B. RAMP TO W.B. HARRISON AVENUE, E.B. U.S. 20 TO N.B. RAMP, AND W.B. U.S. 20 TO N.B. RAMP MOVEMENTS WILL BE FREE-FLOW EXCEPT WHEN A PEDESTRIAN IS PRESENT. A PUSH BUTTON WILL BE USED TO CALL A SPECIAL CYCLE THAT WOULD TEMPORARILY STOP EACH FREE-FLOW MOVEMENT ON TURN. FREE-FLOW CONDITIONS WOULD RESUME AFTER APPROPRIATE PED CROSSING TIME IS PROVIDED.

GENERAL NOTES
 ARE PROFILES PROVIDED? YES. IF NOT, STATE REASON WHY:
 TYPE B-6.24 CURB AND GUTTER TO BE USED ON OUTER EDGES OF PAVEMENT.
 TYPE M-6.24 & M-6.06 CURB AND GUTTER TO BE USED ON CHANNELIZING ISLAND.
 ALL DIMENSIONS ARE SHOWN E-T-O OF PAVEMENT UNLESS OTHERWISE NOTED.
 THE RIGHT-OF-WAY LIMITS ARE PRELIMINARY.
 DESIGN VEHICLE TURNING MOVEMENTS ARE ACCOMMODATED PER AUTOTURN SOFTWARE, VERSION 9.1.
 THE SCOPE OF WORK: FULL RECONSTRUCTION OF INTERCHANGE TO A DDI; RECONSTRUCTION OF U.S. ROUTE 20/HARRISON AVENUE.
 DECISION SIGHT DISTANCE IS PROVIDED AT THE FREEWAY RAMP TERMINALS ALONG I-39.
 INTERCHANGE DESIGN EXCEPTIONS:
 1) S.B. I-39 WEAVE NORTH OF HARRISON AVENUE WILL OPERATE AT LOS E IN THE DESIGN YEAR. THE CONSTRAINTS IMPOSED BY THE EXISTING S.B. CONVERGENCE IN TOLLWAY'S JURISDICTION MAKE INCREASING WEAVE LENGTH INFEASIBLE.
 2) N.B. I-39 WEAVE NORTH OF HARRISON AVENUE WILL OPERATE AT LOS D IN THE DESIGN YEAR. THE CONSTRAINTS IMPOSED BY THE EXISTING DIVERGENCE IN TOLLWAY'S JURISDICTION MAKE INCREASING THE WEAVE LENGTH INFEASIBLE.
 3) LANE BALANCE IS NOT ACHIEVED ON NORTHBOUND I-39 APPROACHING HARRISON AVENUE. THE 4-LANE SECTION WILL DROP TO 2 MAINLINE LANES AND A 2-LANE EXIT RAMP AT THE TERMINAL. THIS IS NECESSARY TO AVOID MAJOR OPERATIONAL CONCERNS FURTHER NORTH. ADEQUATE SIGNING WILL BE PROVIDED AHEAD OF DIVERGENCE.



TRAFFIC DATA

MOVEMENT	YEAR 2020 30TH MAXIMUM HOUR TRAFFIC		PERCENT TRUCK TRAFFIC IN 30TH MAX HOUR	ESTIMATED PERCENT INCREASE BY	YEAR 2030 30TH MAXIMUM HOUR TRAFFIC		ESTIMATED PERCENT INCREASE BY	YEAR 2040 30TH MAXIMUM HOUR TRAFFIC	
	A.M.	P.M.			A.M.	P.M.		A.M.	P.M.
AB	530	1110	11.0		580	1210		645	1325
AD	100	330	7.0		135	445		180	600
AC									
BC	780	980	10.0		860	1085		955	1115
BD	265	375	19.0		355	505		475	680
CD									
CA	185	140	7.0		250	190		335	255
CB	80	95	13.0		110	120		150	160
DC									
DB									
DA									
TOTAL A	1595	2560			1825	2930		2115	3395
TOTAL B	1655	2565			1905	2320		2225	3180
TOTAL C	265	250			360	310		485	415
TOTAL D	365	705			490	950		655	1280



INTERCHANGE DESIGN STUDY

FAI ROUTE 39 WITH (INTERSTATE 39)
 FAP ROUTE 301 WITH (U.S. ROUTE 20)
 WEST TERMINAL

SEC. NO. (201-31K&14-1.5K) PROJ. NO.
 SCALE 1" = 50 FT COUNTY WINNEBAGO

SJN : P92-111-06 REV. NO.

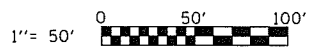
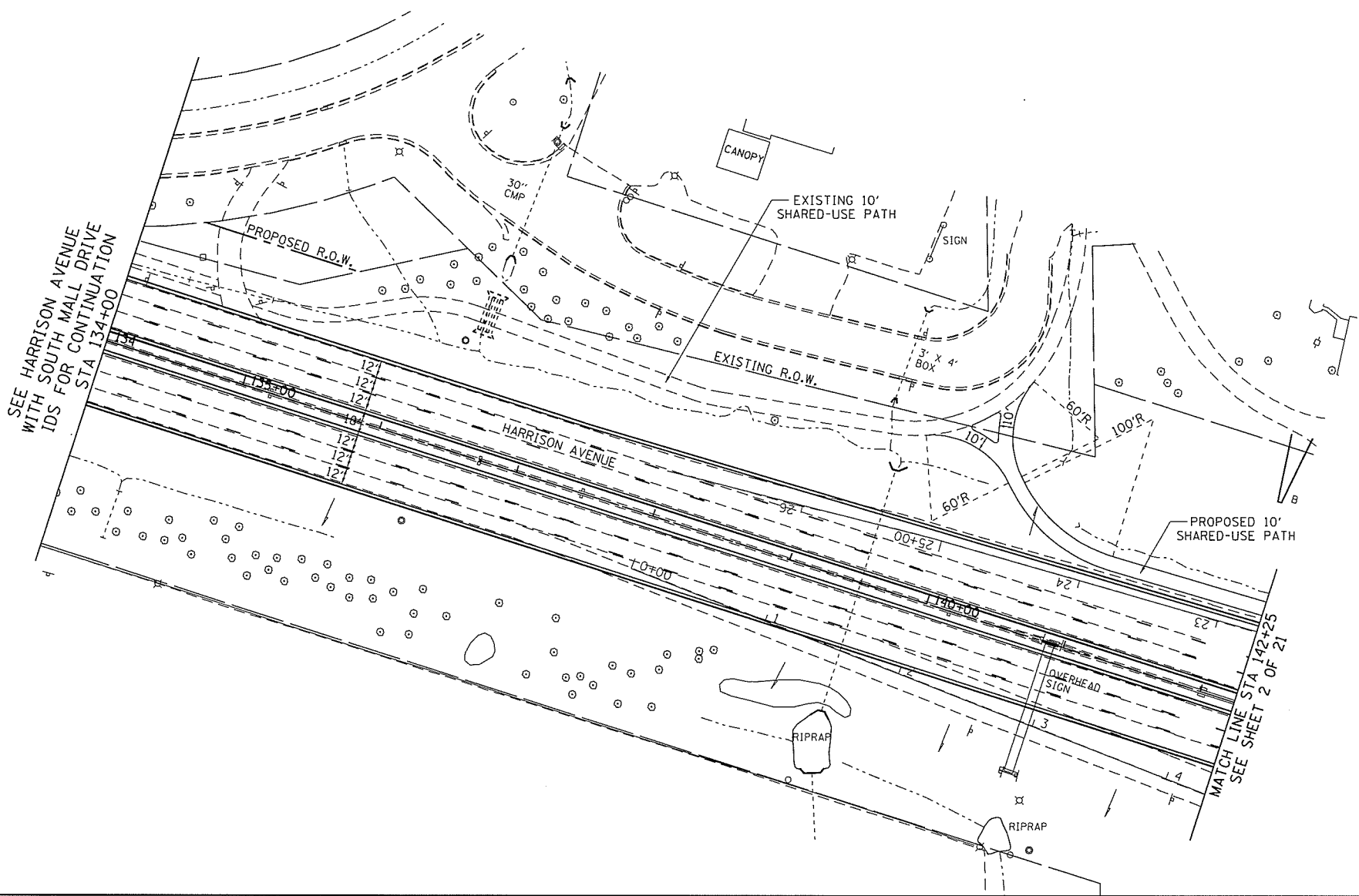
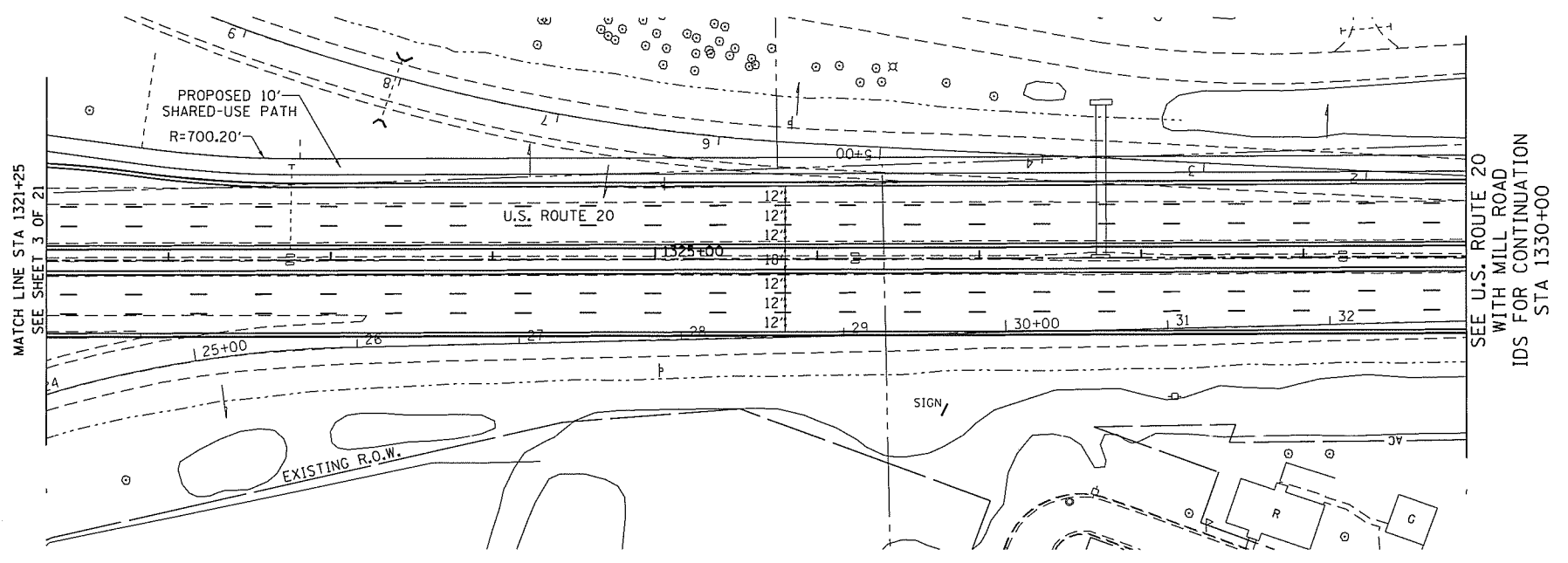
DESIGNED BY IDOT - DISTRICT 2 DATE APRIL 2017

SATISFACTORY *Mike Bueh* 4-25-17 DISTRICT GEOMETRICS ENGINEER DATE
 SATISFACTORY *David Johnson* 4-25-17 DISTRICT PROGRAM DEVELOPMENT ENGINEER DATE
 SATISFACTORY *Julio Alvarez* 4-25-17 DISTRICT OPERATIONS ENGINEER DATE
 APPROVED *David Johnson* 4-25-17 DEPUTY DIRECTOR OF HIGHWAYS, REGION TWO ENGINEER DATE

I.D.S. SHEET 2 OF 21

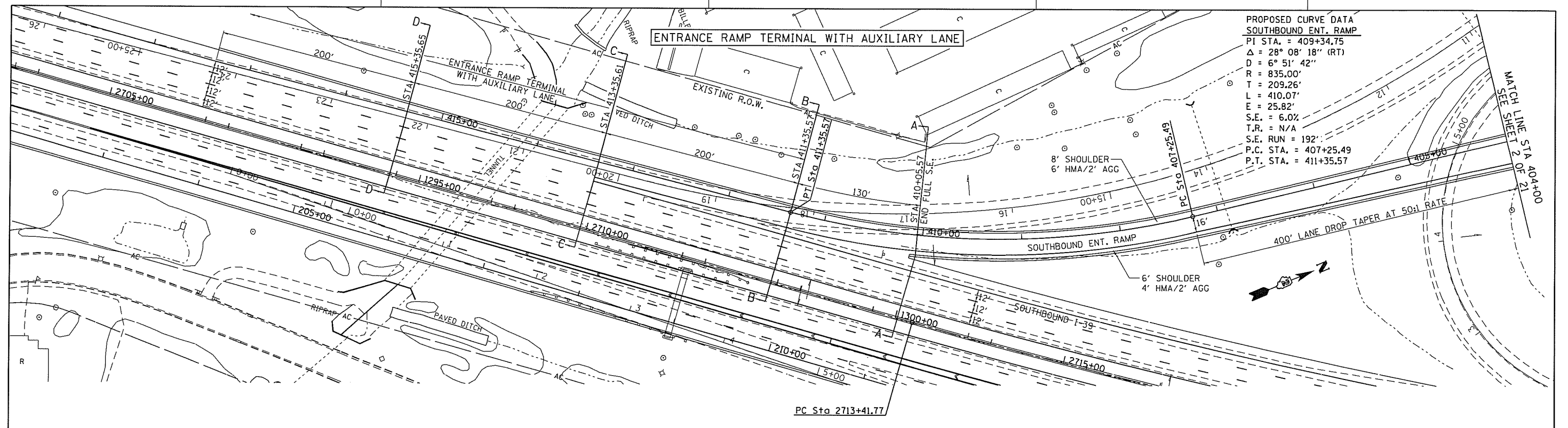
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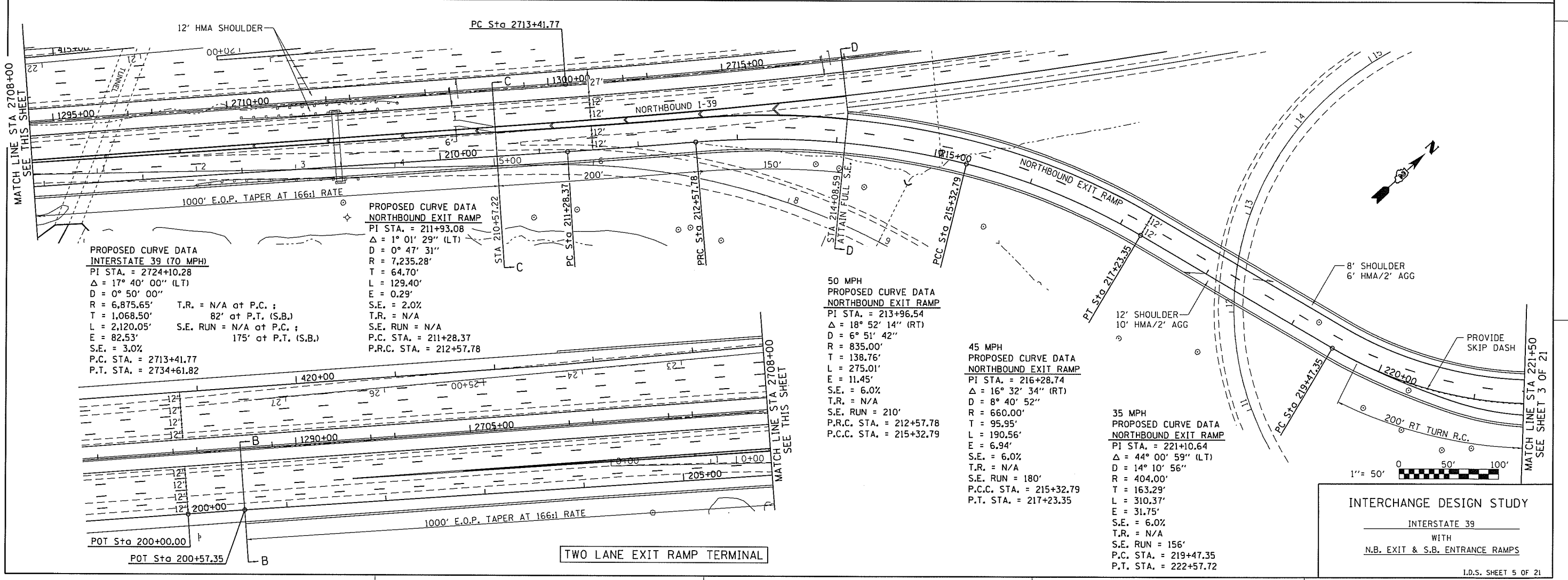


INTERCHANGE DESIGN STUDY
HARRISON AVENUE
WITH
EAST AND WEST PROJECT LIMITS
I.D.S. SHEET 4 OF 21

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**PROPOSED CURVE DATA
 SOUTHBOUND ENT. RAMP**
 PI STA. = 409+34.75
 Δ = 28° 08' 18" (RT)
 D = 6° 51' 42"
 R = 835.00'
 T = 209.26'
 L = 410.07'
 E = 25.82'
 S.E. = 6.0%
 T.R. = N/A
 S.E. RUN = 192'
 P.C. STA. = 407+25.49
 P.T. STA. = 411+35.57



**PROPOSED CURVE DATA
 INTERSTATE 39 (70 MPH)**
 PI STA. = 2724+10.28
 Δ = 17° 40' 00" (LT)
 D = 0° 50' 00"
 R = 6,875.65' T.R. = N/A at P.C. ;
 T = 1,068.50' 82' at P.T. (S.B.)
 L = 2,120.05' S.E. RUN = N/A at P.C. ;
 E = 82.53' 175' at P.T. (S.B.)
 S.E. = 3.0%
 P.C. STA. = 2713+41.77
 P.T. STA. = 2734+61.82

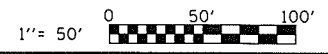
**PROPOSED CURVE DATA
 NORTHBOUND EXIT RAMP**
 PI STA. = 211+93.08
 Δ = 1° 01' 29" (LT)
 D = 0° 47' 31"
 R = 7,235.28'
 T = 64.70'
 L = 129.40'
 E = 0.29'
 S.E. = 2.0%
 T.R. = N/A
 S.E. RUN = N/A
 P.C. STA. = 211+28.37
 P.R.C. STA. = 212+57.78

**50 MPH
 PROPOSED CURVE DATA
 NORTHBOUND EXIT RAMP**
 PI STA. = 213+96.54
 Δ = 18° 52' 14" (RT)
 D = 6° 51' 42"
 R = 835.00'
 T = 138.76'
 L = 275.01'
 E = 11.45'
 S.E. = 6.0%
 T.R. = N/A
 S.E. RUN = 210'
 P.R.C. STA. = 212+57.78
 P.C.C. STA. = 215+32.79

**45 MPH
 PROPOSED CURVE DATA
 NORTHBOUND EXIT RAMP**
 PI STA. = 216+28.74
 Δ = 16° 32' 34" (RT)
 D = 8° 40' 52"
 R = 660.00'
 T = 95.95'
 L = 190.56'
 E = 6.94'
 S.E. = 6.0%
 T.R. = N/A
 S.E. RUN = 180'
 P.C.C. STA. = 215+32.79
 P.T. STA. = 217+23.35

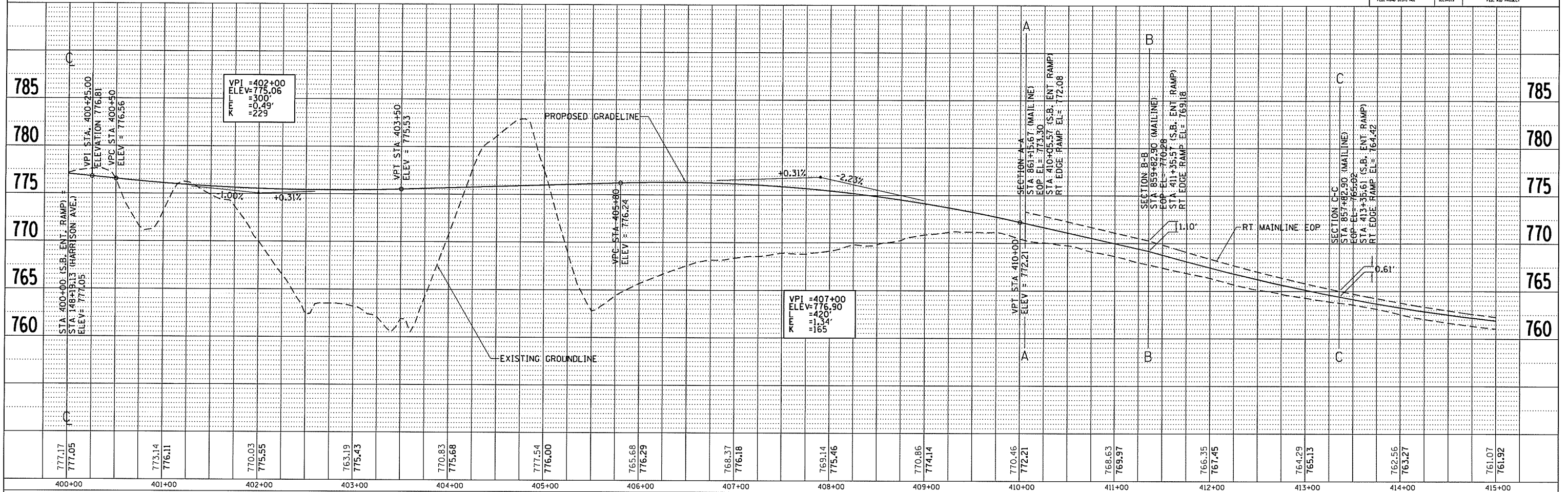
**35 MPH
 PROPOSED CURVE DATA
 NORTHBOUND EXIT RAMP**
 PI STA. = 221+10.64
 Δ = 44° 00' 59" (LT)
 D = 14° 10' 56"
 R = 404.00'
 T = 163.29'
 L = 310.37'
 E = 31.75'
 S.E. = 6.0%
 T.R. = N/A
 S.E. RUN = 156'
 P.C. STA. = 219+47.35
 P.T. STA. = 222+57.72

INTERCHANGE DESIGN STUDY
 INTERSTATE 39
 WITH
 N.B. EXIT & S.B. ENTRANCE RAMPS

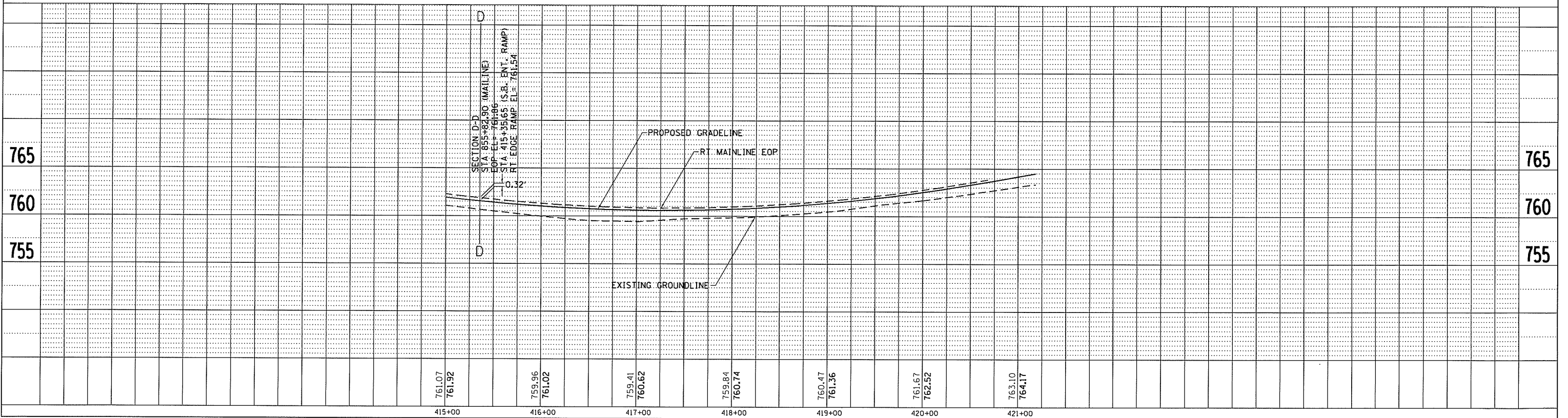


SOUTHBOUND ENTRANCE RAMP PROFILE

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1201-3K&(4-1,5)K	WINNEBAGO	21	6
STA. TO STA.		FED. AID PROJECT	
FED. ROAD DIST. NO.		MILEMIS	

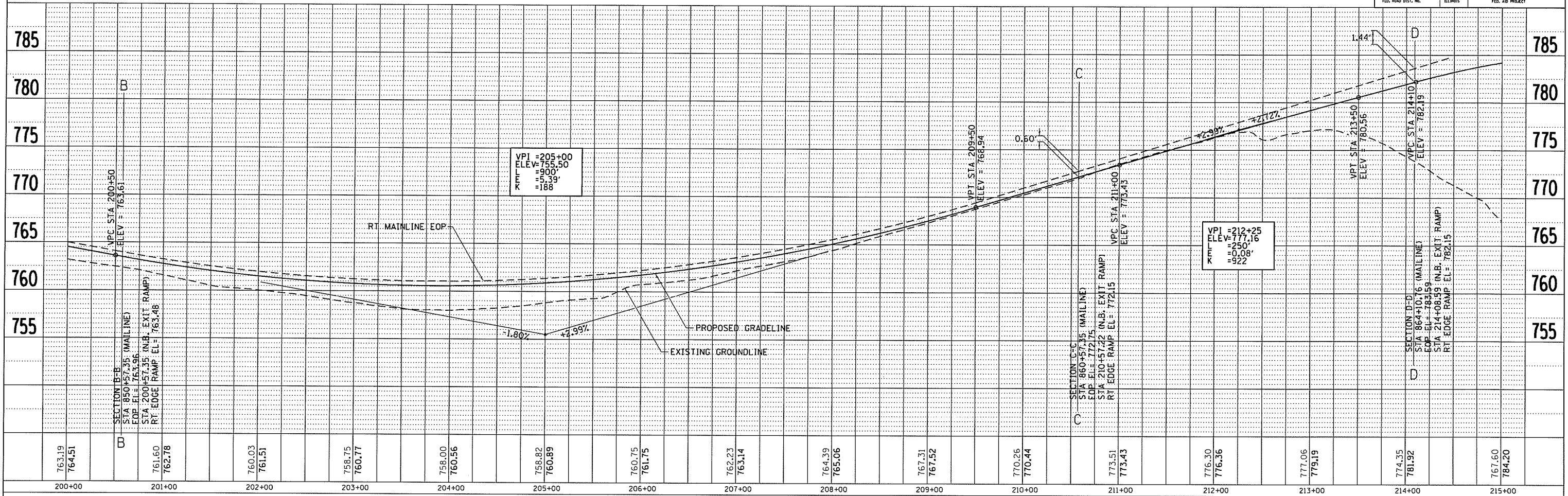


SOUTHBOUND ENTRANCE RAMP PROFILE

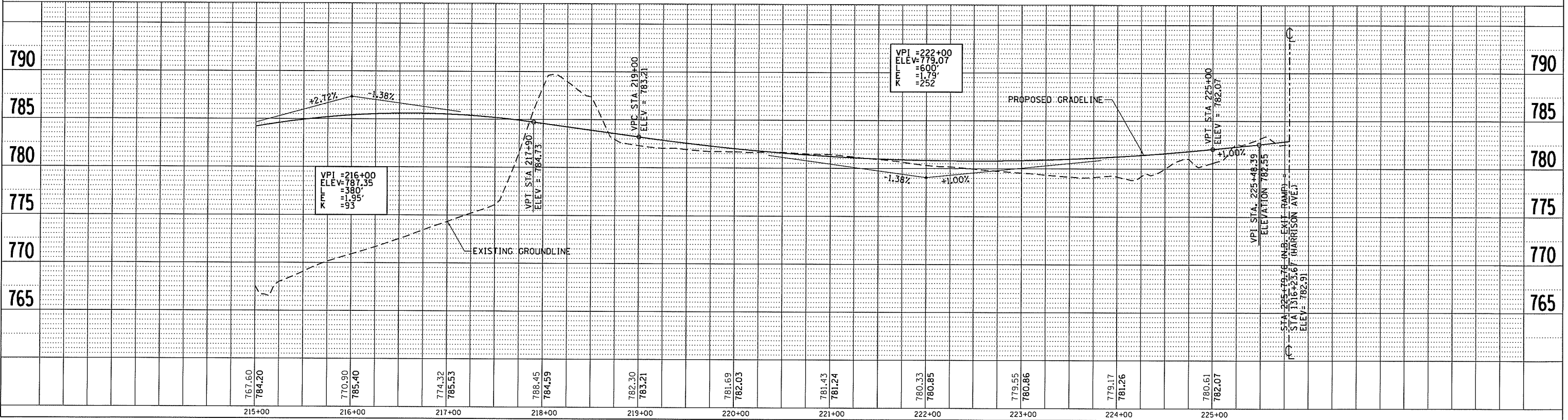


NORTHBOUND EXIT RAMP PROFILE

PA. REL.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
201-31K&(4-1,5)K	WINNEBAGO	21	7	
STA. TO STA.				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

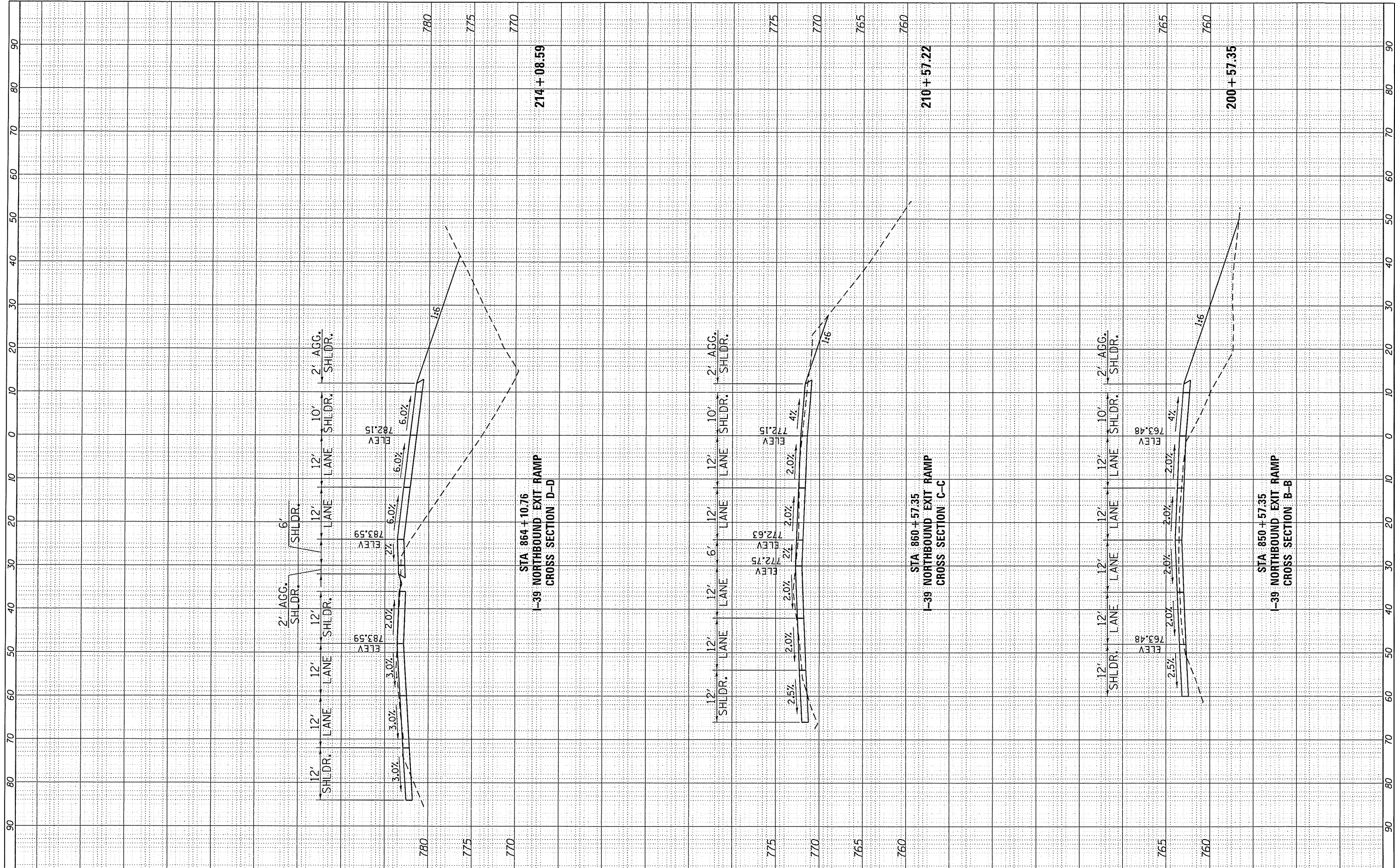


NORTHBOUND EXIT RAMP PROFILE



FINAL SURVEY NO.	SURVEYED	BY	DATE
	PLOTTED		
	TEMPLATE		
	NOTE BOOK		
	AREAS CHECKED		

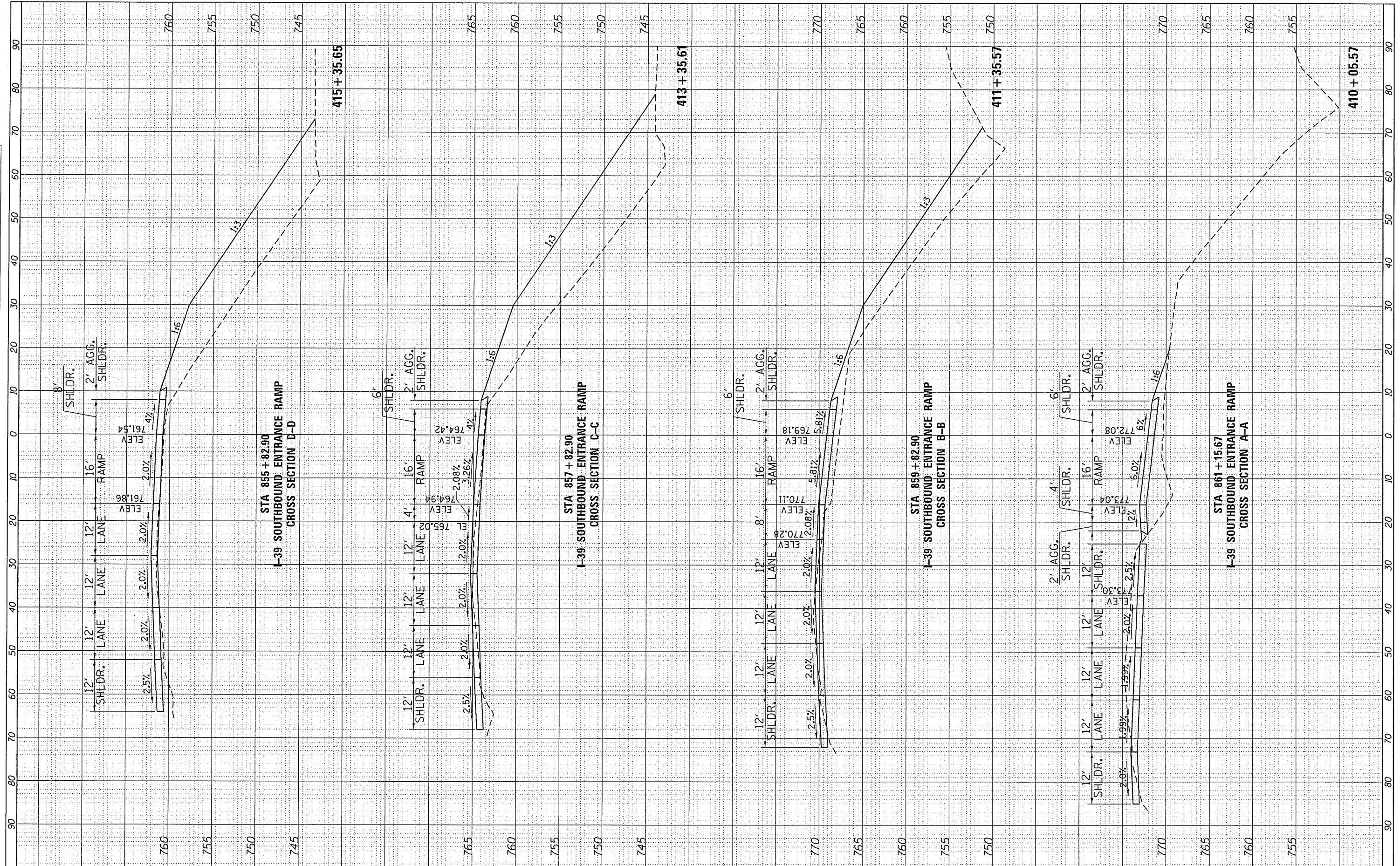
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	PLOTTED		
	TEMPLATE		
	NOTE BOOK		
	AREAS CHECKED		



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PLOT SCALE = 1" = 5ft VERT., 1" = 10ft HORIZ.		DATE -	REVISED -		CONTRACT NO.								
PLOT DATE = Mar-10-2017 08:28:53 AM					[ILLINOIS] FED. AID PROJECT								

FINL SURVEY	REVIEWED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	AREAS CHECKED		

ORIGINAL SURVEY	REVIEWED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	AREAS CHECKED		



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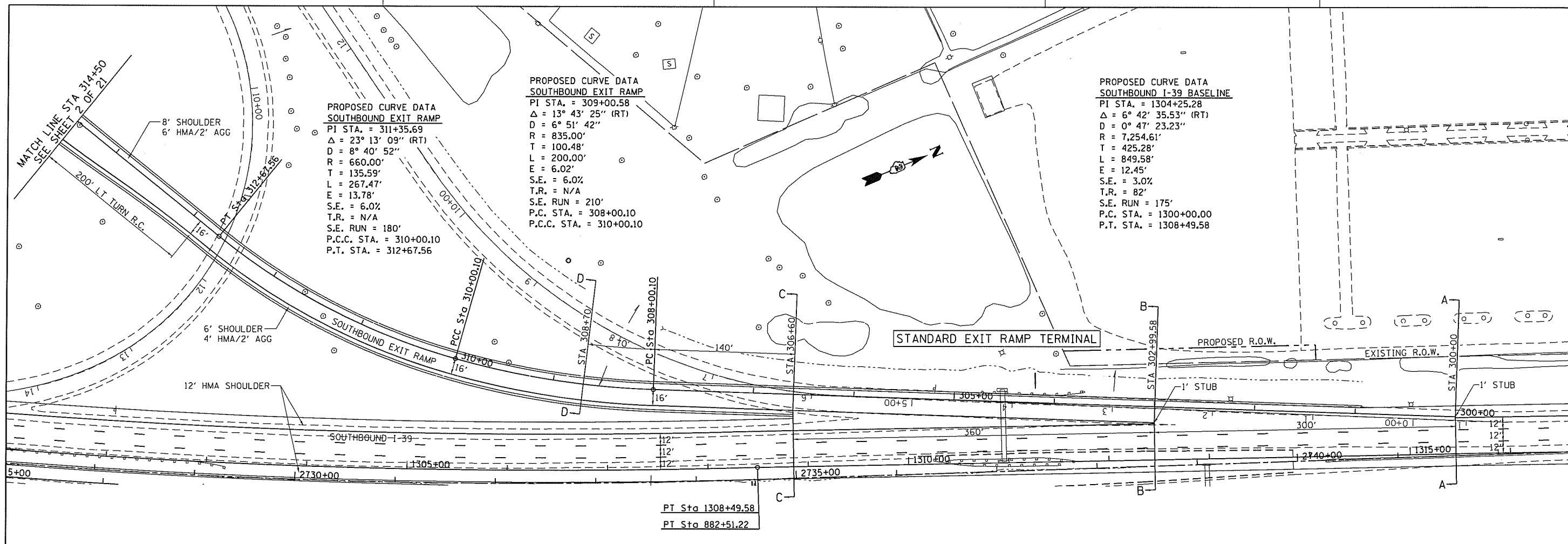
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

I-39 SOUTHBOUND ENTRANCE RAMP
 CRITICAL CROSS SECTIONS
 SCALE: SHEET NO. OF SHEETS STA. 410+05.57 TO STA. 415+35.65

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	(201-3)K (4-1,5)K	WINNEBAGO	21	9
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

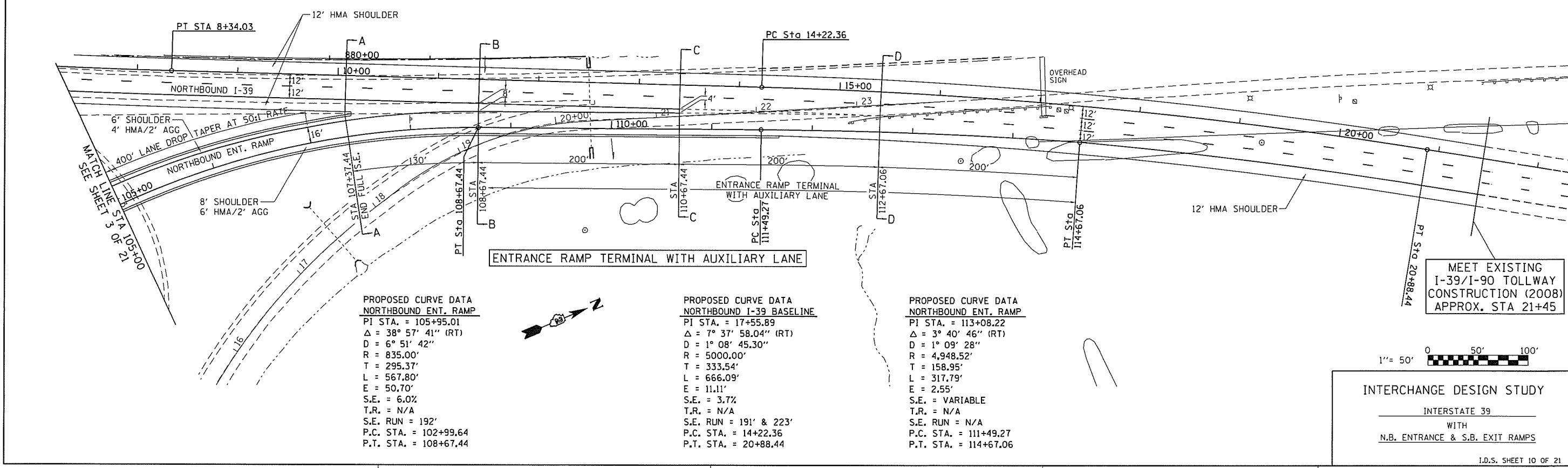
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**PROPOSED CURVE DATA
 SOUTHBOUND EXIT RAMP**
 PI STA. = 311+35.69
 $\Delta = 23^\circ 13' 09''$ (RT)
 D = $8^\circ 40' 52''$
 R = 660.00'
 T = 135.59'
 L = 267.47'
 E = 13.78'
 S.E. = 6.0%
 T.R. = N/A
 S.E. RUN = 180'
 P.C. STA. = 310+00.10
 P.T. STA. = 312+67.56

**PROPOSED CURVE DATA
 SOUTHBOUND EXIT RAMP**
 PI STA. = 309+00.58
 $\Delta = 13^\circ 43' 25''$ (RT)
 D = $6^\circ 51' 42''$
 R = 835.00'
 T = 100.48'
 L = 200.00'
 E = 6.02'
 S.E. = 6.0%
 T.R. = N/A
 S.E. RUN = 210'
 P.C. STA. = 308+00.10
 P.C.C. STA. = 310+00.10

**PROPOSED CURVE DATA
 SOUTHBOUND I-39 BASELINE**
 PI STA. = 1304+25.28
 $\Delta = 6^\circ 42' 35.53''$ (RT)
 D = $0^\circ 47' 23.23''$
 R = 7,254.61'
 T = 425.28'
 L = 849.58'
 E = 12.45'
 S.E. = 3.0%
 T.R. = 82'
 S.E. RUN = 175'
 P.C. STA. = 1300+00.00
 P.T. STA. = 1308+49.58



**PROPOSED CURVE DATA
 NORTHBOUND ENT. RAMP**
 PI STA. = 105+95.01
 $\Delta = 38^\circ 57' 41''$ (RT)
 D = $6^\circ 51' 42''$
 R = 835.00'
 T = 295.37'
 L = 567.80'
 E = 50.70'
 S.E. = 6.0%
 T.R. = N/A
 S.E. RUN = 192'
 P.C. STA. = 102+99.64
 P.T. STA. = 108+67.44

**PROPOSED CURVE DATA
 NORTHBOUND I-39 BASELINE**
 PI STA. = 17+55.89
 $\Delta = 7^\circ 37' 58.04''$ (RT)
 D = $1^\circ 08' 45.30''$
 R = 5000.00'
 T = 333.54'
 L = 666.09'
 E = 11.11'
 S.E. = 3.7%
 T.R. = N/A
 S.E. RUN = 191' & 223'
 P.C. STA. = 14+22.36
 P.T. STA. = 20+88.44

**PROPOSED CURVE DATA
 NORTHBOUND ENT. RAMP**
 PI STA. = 113+08.22
 $\Delta = 3^\circ 40' 46''$ (RT)
 D = $1^\circ 09' 28''$
 R = 4,948.52'
 T = 158.95'
 L = 317.79'
 E = 2.55'
 S.E. = VARIABLE
 T.R. = N/A
 S.E. RUN = N/A
 P.C. STA. = 111+49.27
 P.T. STA. = 114+67.06

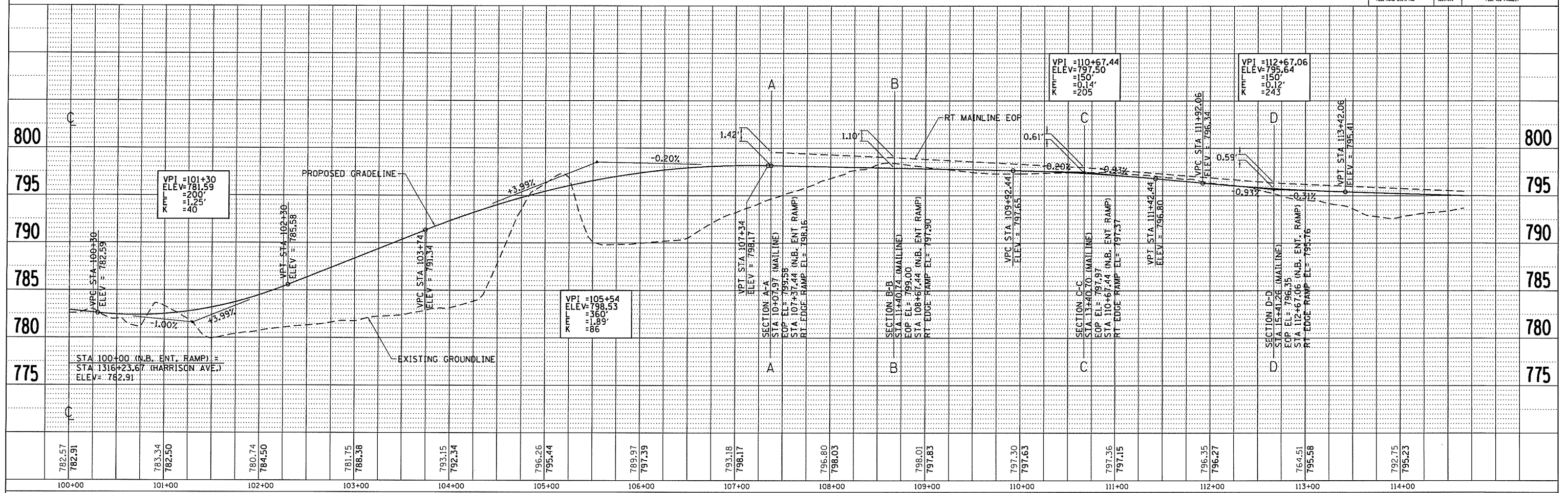
MEET EXISTING
 I-39/I-90 TOLLWAY
 CONSTRUCTION (2008)
 APPROX. STA 21+45



INTERCHANGE DESIGN STUDY
 INTERSTATE 39
 WITH
 N.B. ENTRANCE & S.B. EXIT RAMPS

NORTHBOUND ENTRANCE RAMP PROFILE

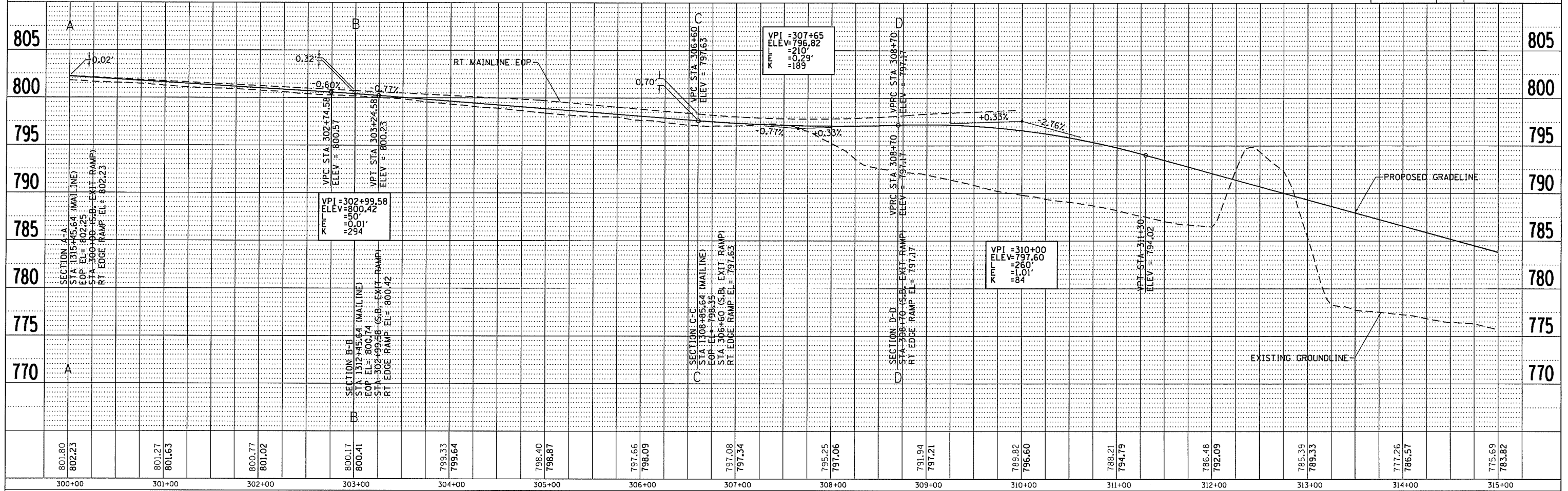
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(201-3)K&(4-1.5)K	WINNEBAGO	21	11
STA. TO STA.		FED. AID PROJECT	
FED. ROAD DIST. NO.	ILLINOIS		



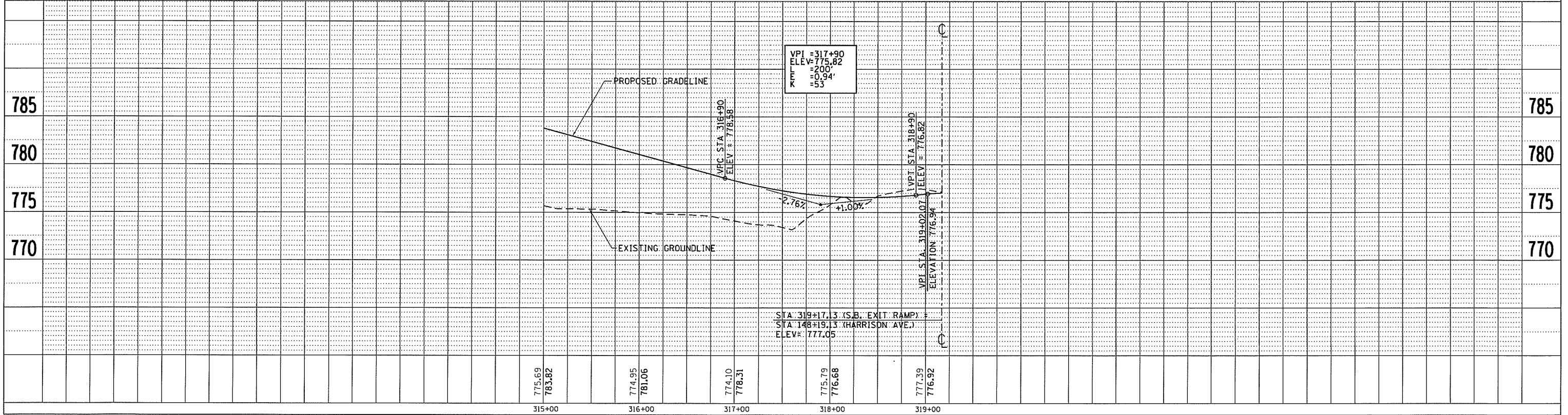
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782.91	782.50	784.50	788.38	792.34	795.44	797.39	798.17	798.03	797.83	797.15	796.27	795.58	795.23	
100+00	101+00	102+00	103+00	104+00	105+00	106+00	107+00	108+00	109+00	110+00	111+00	112+00	113+00	114+00

SOUTHBOUND EXIT RAMP PROFILE

FA MILE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	201-3K&4-1,5K	WINNEBAGO	21	12
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

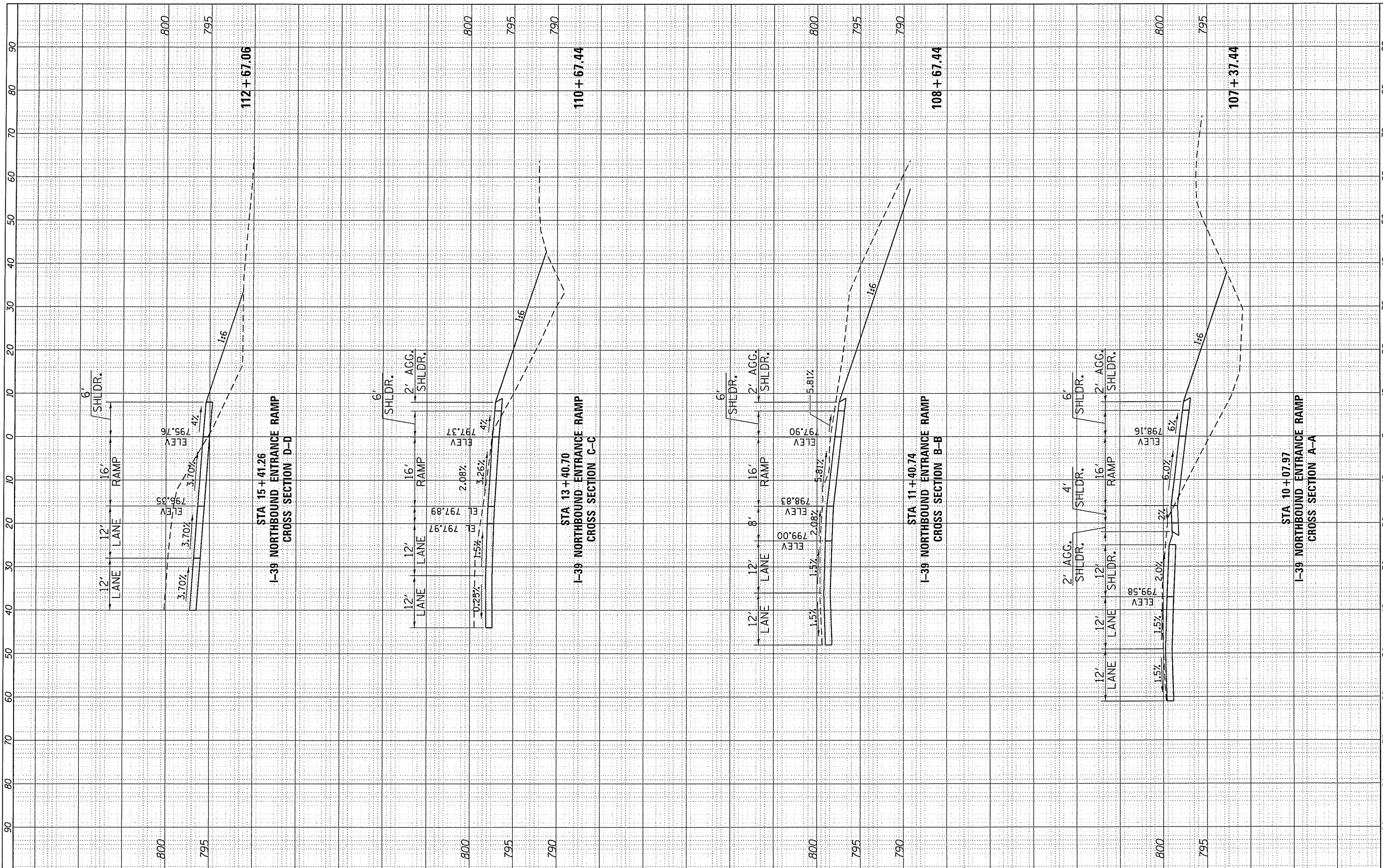


SOUTHBOUND EXIT RAMP PROFILE



FINAL SURVEY	SUBMITTED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		



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		CHECKED -	REVISED -
		DATE -	REVISED -

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CHECKED -	REVISED -
DATE -	REVISED -

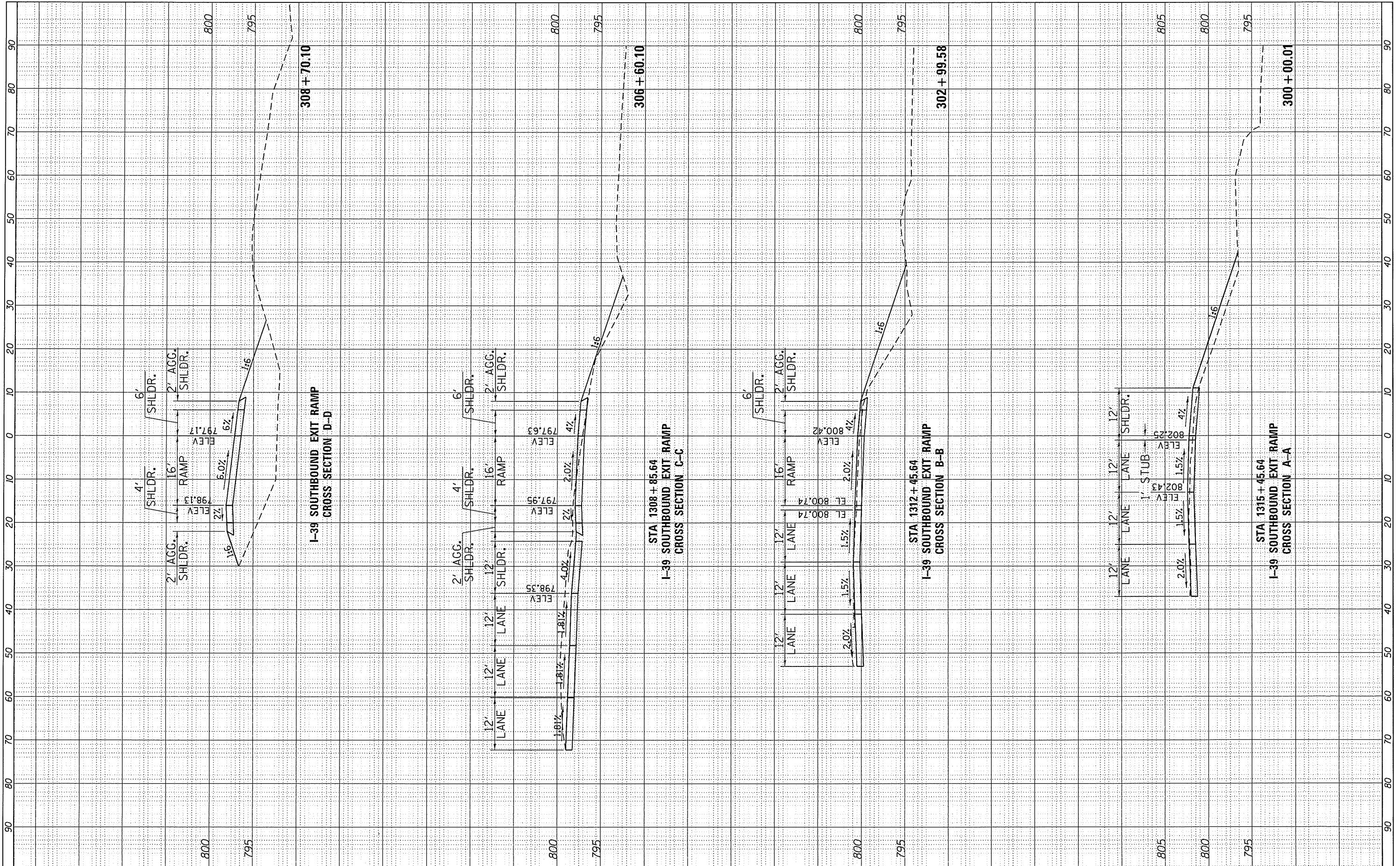
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

I-39 NORTHBOUND ENTRANCE RAMP CRITICAL CROSS SECTIONS			
SCALE:	SHEET NO.	OF SHEETS	STA. 107+37.44 TO STA. 112+67.06

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	(201-3)K (4-1,5)K	WINNEBAGO	21	13
				CONTRACT NO.
				ILLINOIS FED. AID PROJECT

FINAL SURVEY NO.	NO.
REVISED	NO.
PLOTTED	NO.
TEMPLATE	NO.
AREAS	NO.
CHECKED	NO.
BY	DATE

ORIGINAL SURVEY NO.	NO.
REVISED	NO.
PLOTTED	NO.
TEMPLATE	NO.
AREAS	NO.
CHECKED	NO.
BY	DATE



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PLOT SCALE = 1" = 5ft VERT. / 1" = 10ft HORIZ	CHECKED -	REVISED -	REVISED -
PLOT DATE = Mar-10-2017 08:34:05 AM	DATE -	REVISED -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

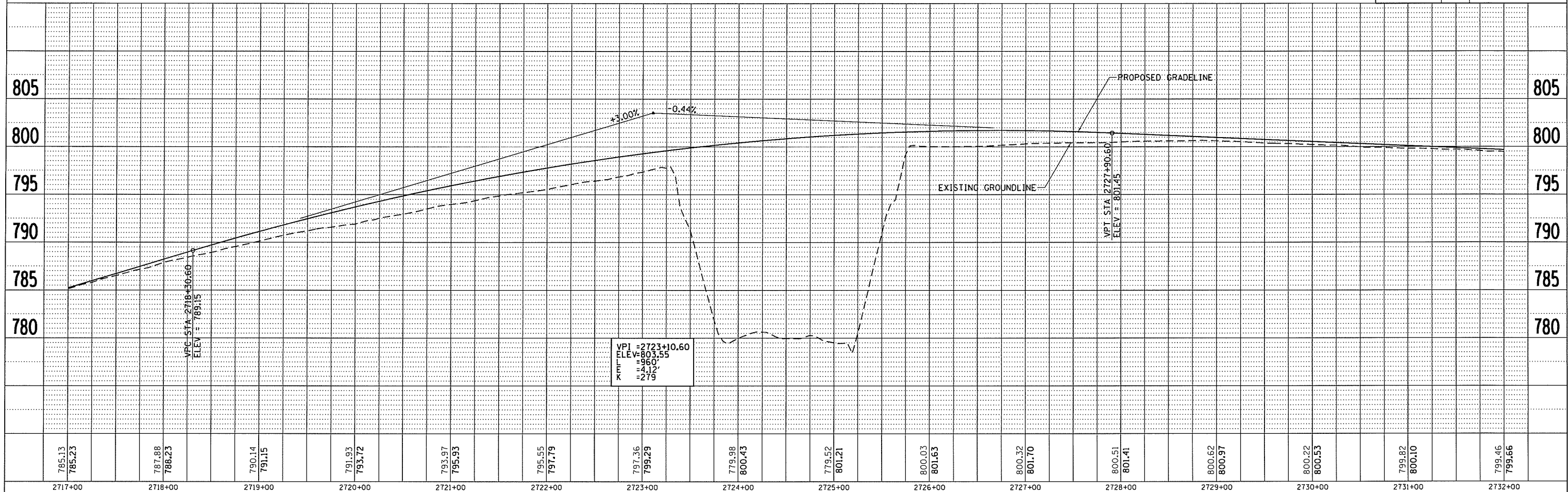
**I-39 SOUTHBOUND EXIT RAMP
CRITICAL CROSS SECTIONS**

SCALE: SHEET NO. OF SHEETS STA. 300+00.01 TO STA. 308+70.10

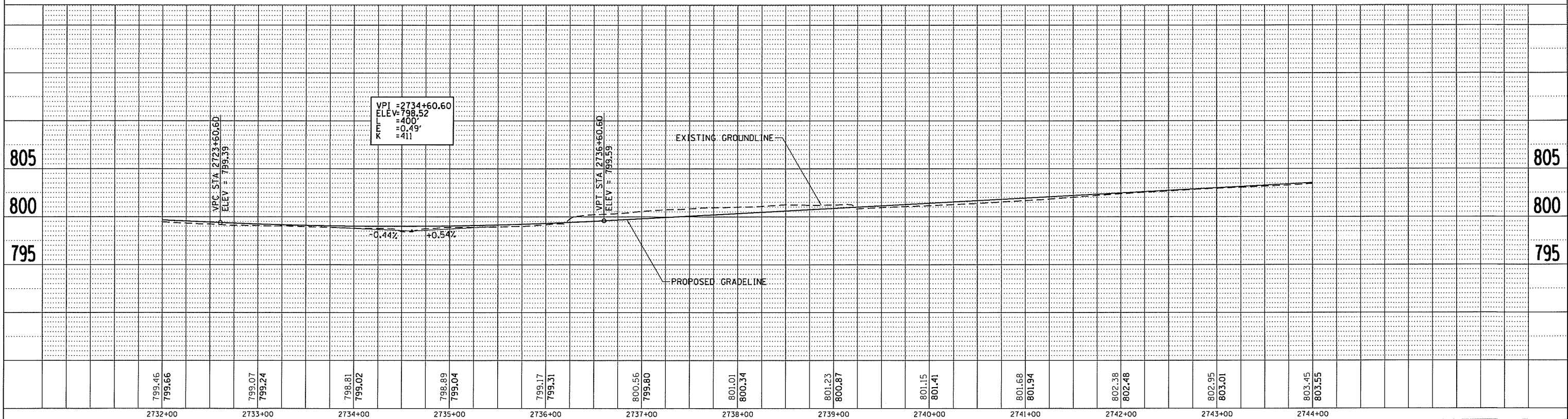
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	(201-31K (4-1.5K	WINNEBAGO	21	14
CONTRACT NO.			ILLINOIS FED. AID PROJECT	

INTERSTATE 39 PROFILE

TA RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	201-3JK&4-1,5JK	WINNEBAGO	21	15
STA. 2717+00		TO STA. 2744+00		
FED. ROAD DIST. NO.		ILLINOIS		FED. AID PROJECT

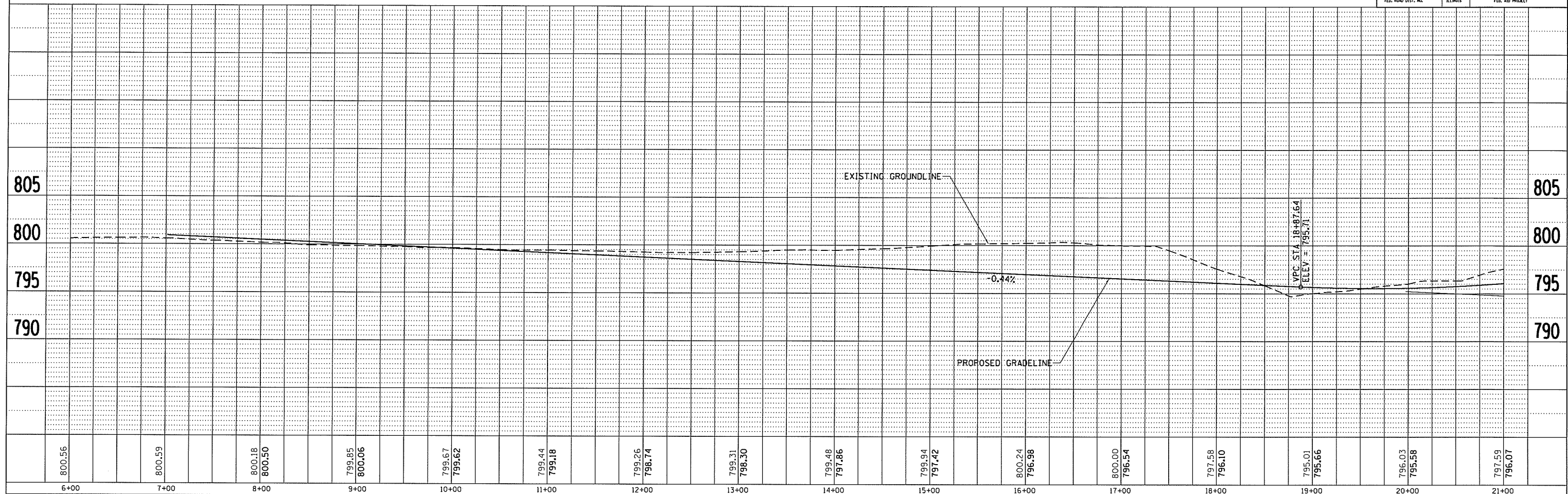


INTERSTATE 39 PROFILE

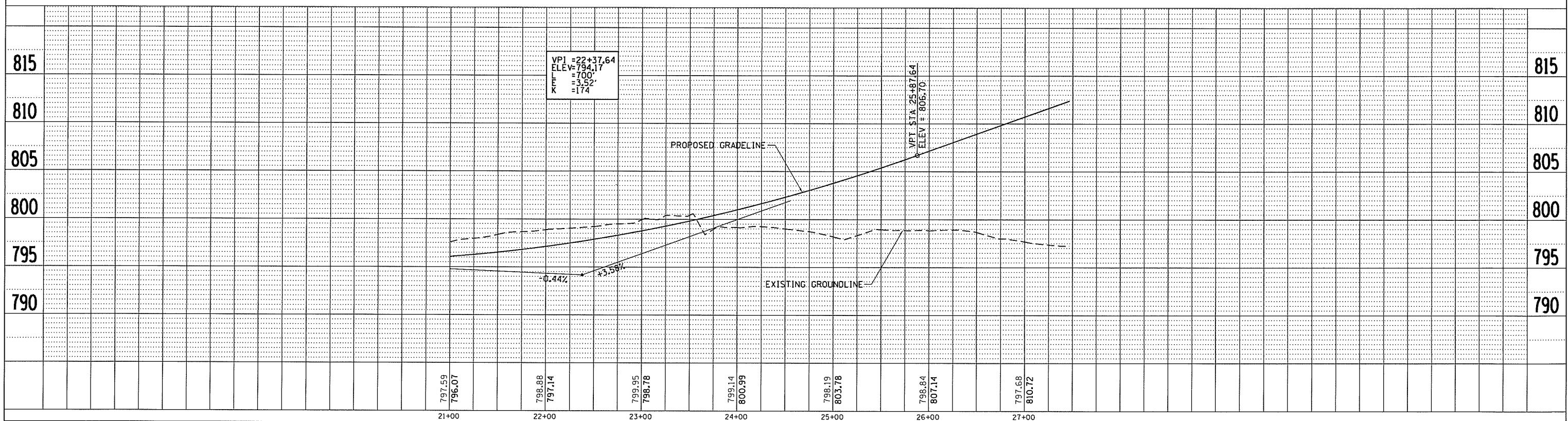


NORTHBOUND I-39 PROFILE

FA RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	201-31K&14-1,51K	WINNEBAGO	21	16
STA. 6+00		TO STA. 27+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

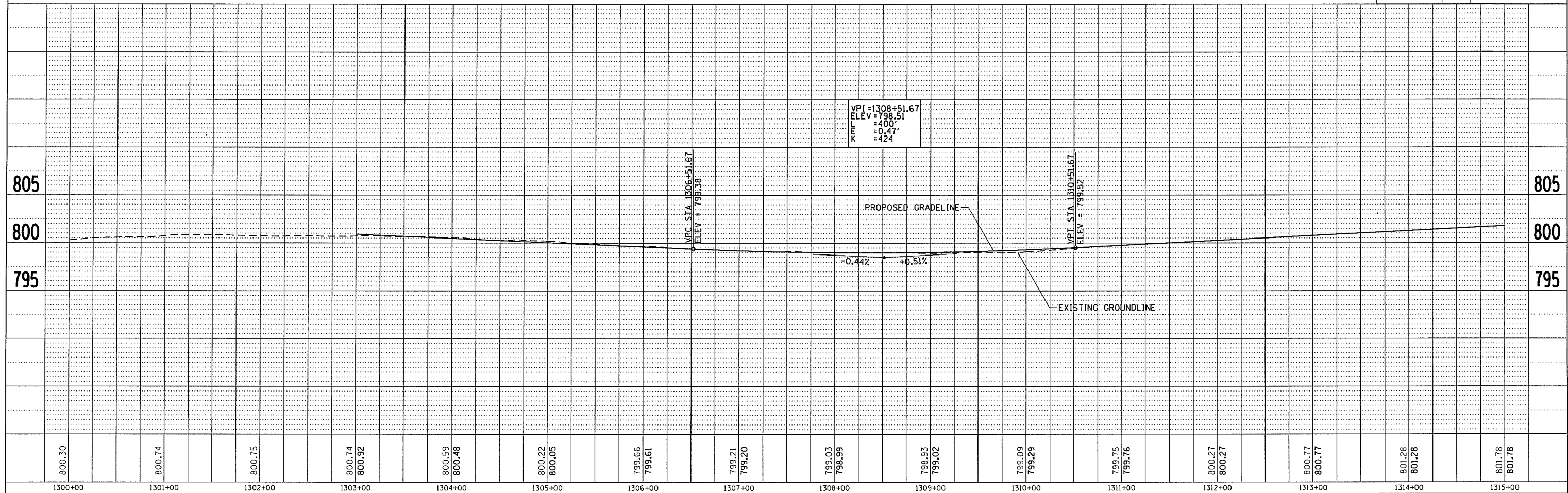


NORTHBOUND I-39 PROFILE

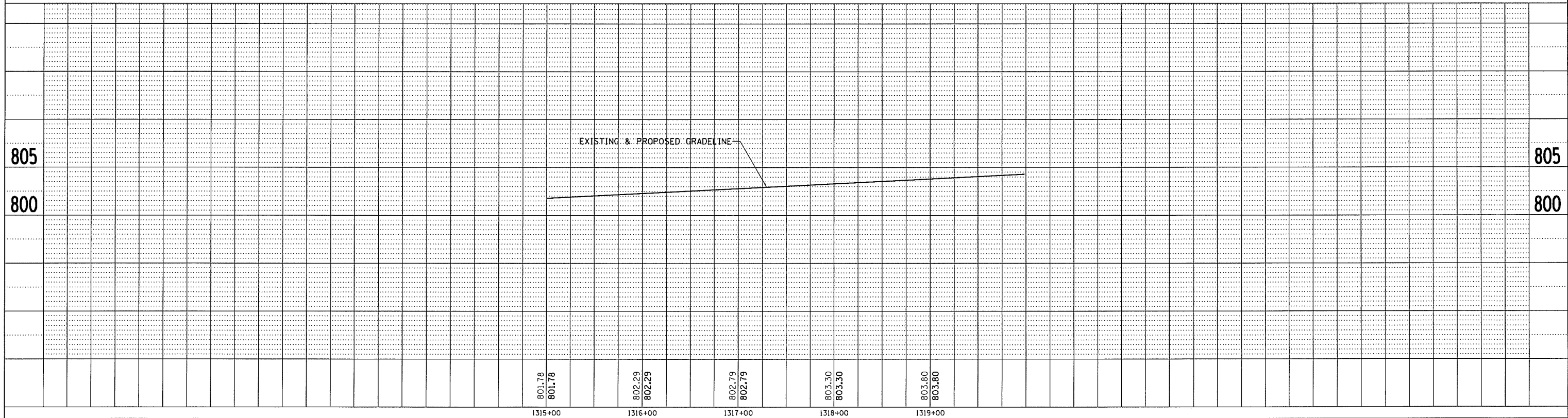


SOUTHBOUND I-39 PROFILE

FA RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	1201-3JK&(4-1,5)K	WINNEBAGO	21	17
STA. 1300+00		TO STA. 1319+00		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

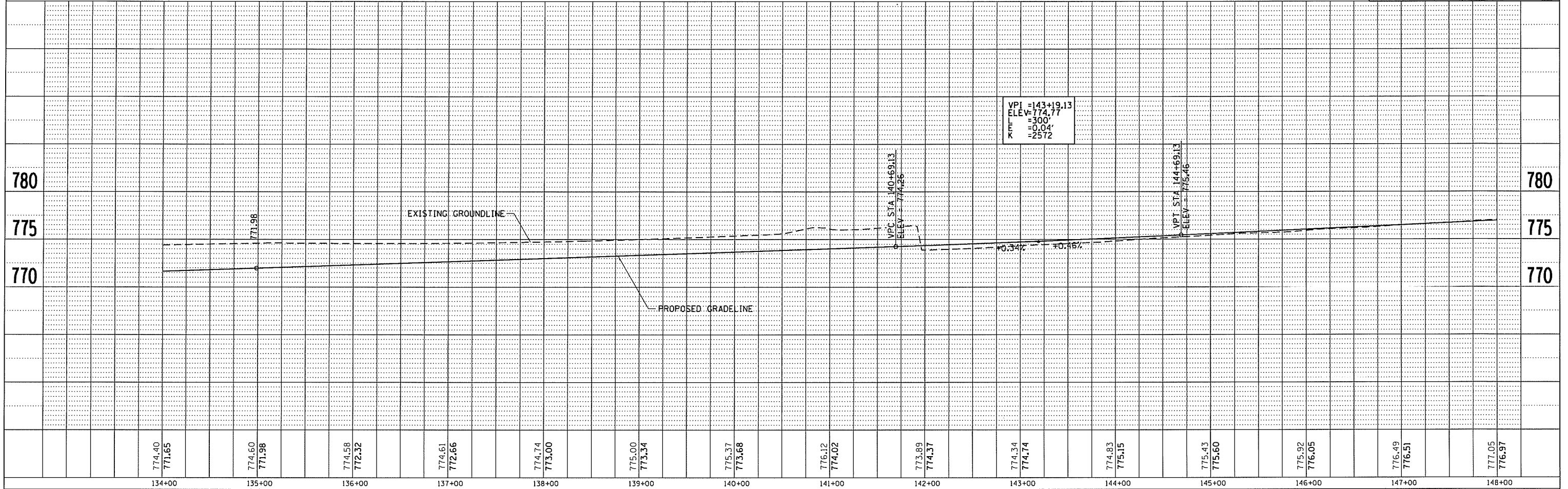


SOUTHBOUND I-39 PROFILE

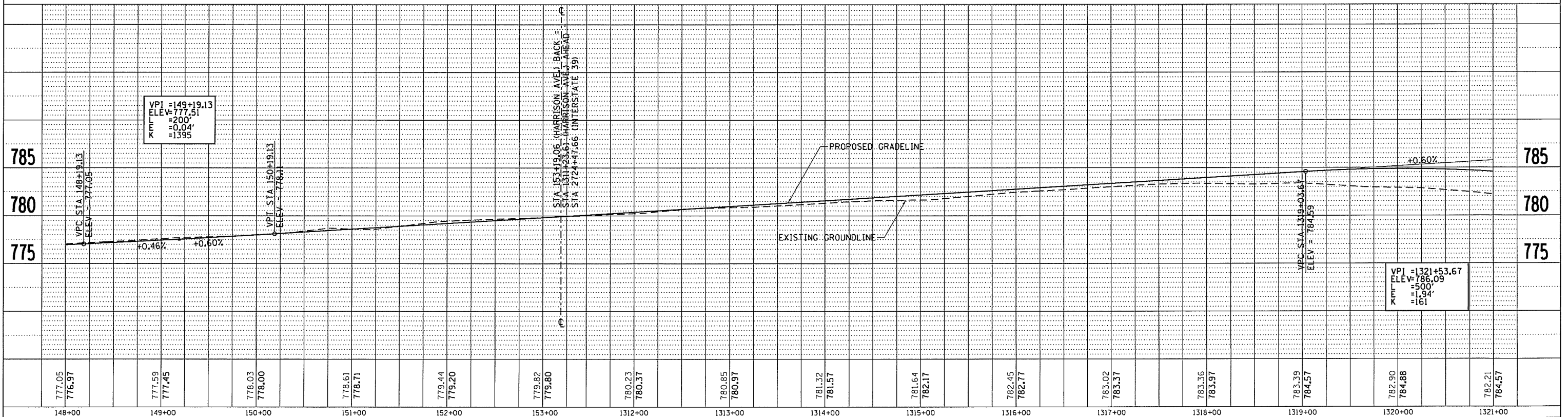


HARRISON AVENUE PROFILE

SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
201-3K&4-1.5K	WINNEBAGO	21	18
STA. 134+00 TO STA. 1321+00			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	

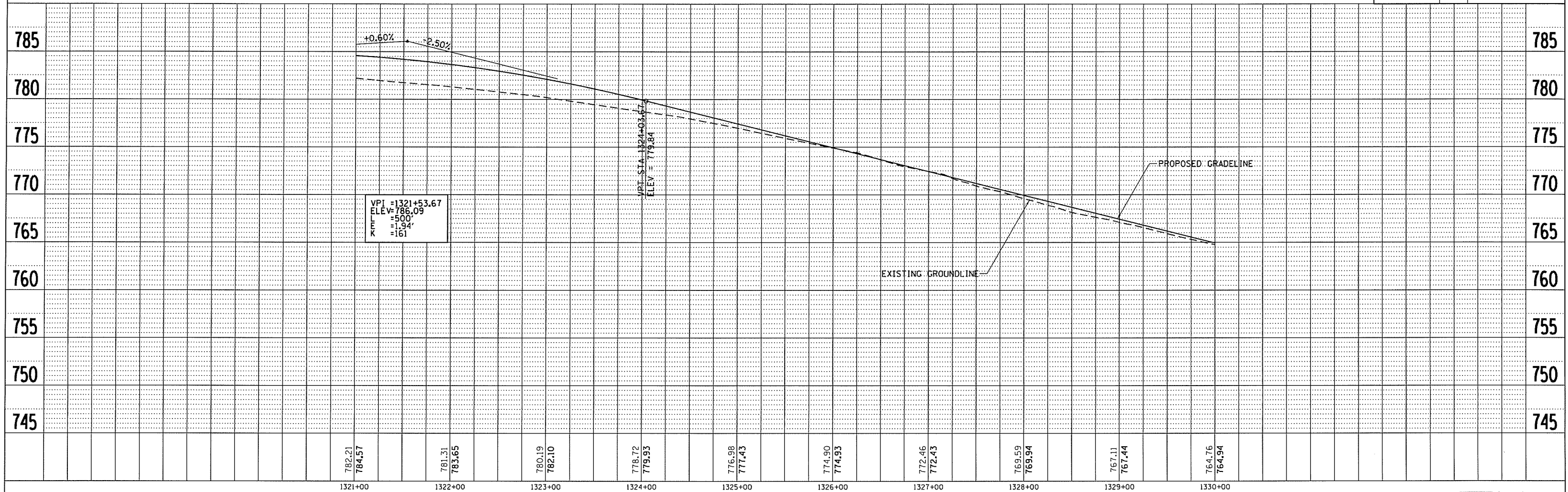


HARRISON AVENUE PROFILE

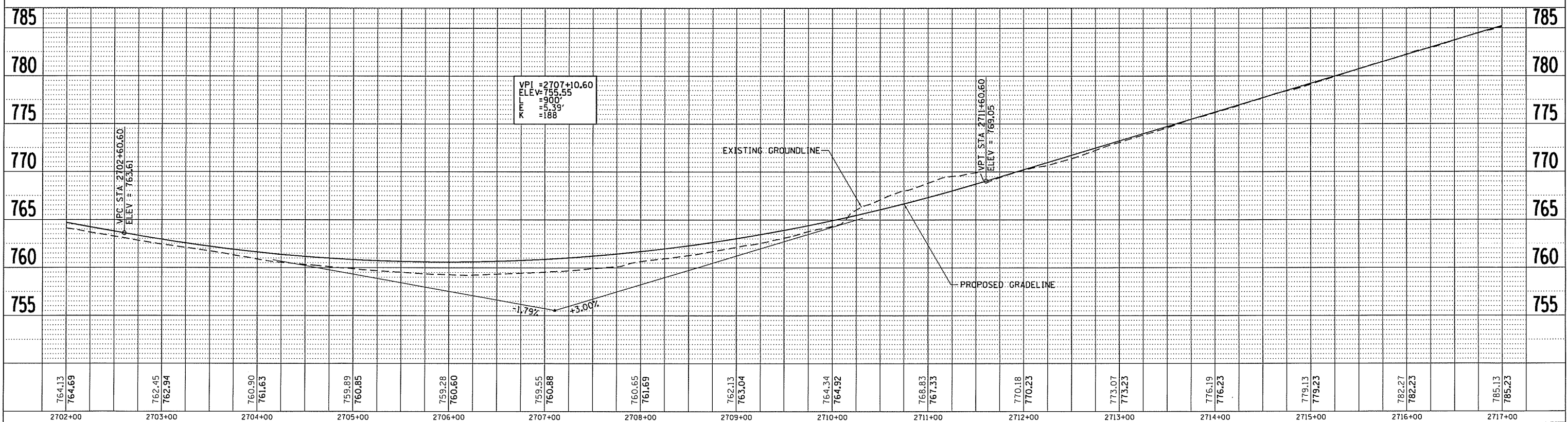


HARRISON AVENUE PROFILE

FA RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	201-3K&(4-1,5)K	WINNEBAGO	21	19
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

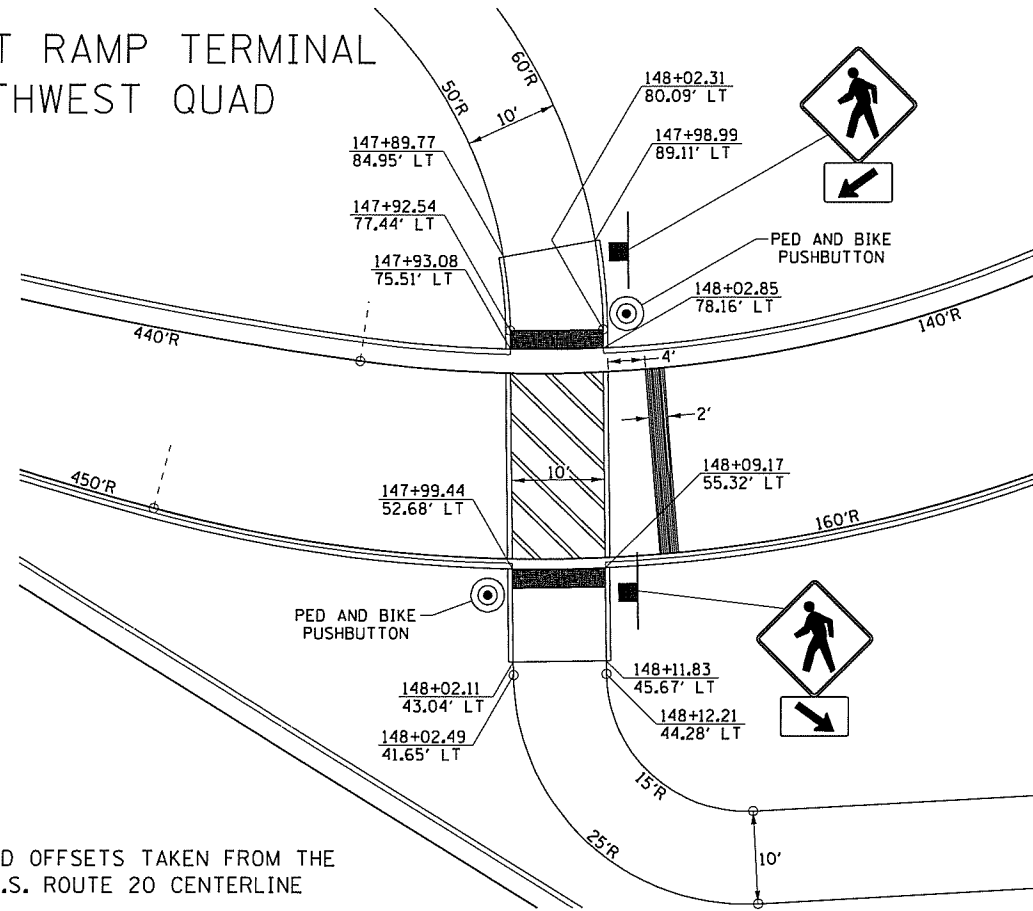


I-39 / U.S. ROUTE 20 PROFILE



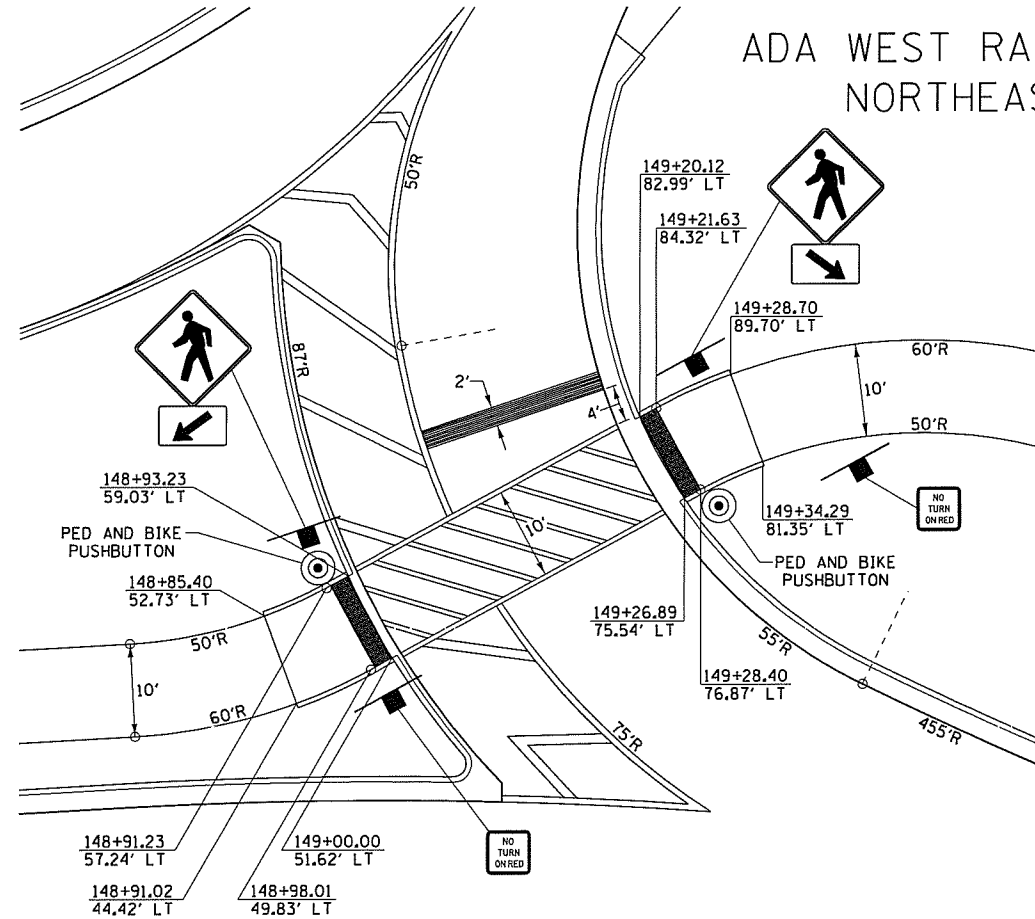
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ADA WEST RAMP TERMINAL NORTHWEST QUAD

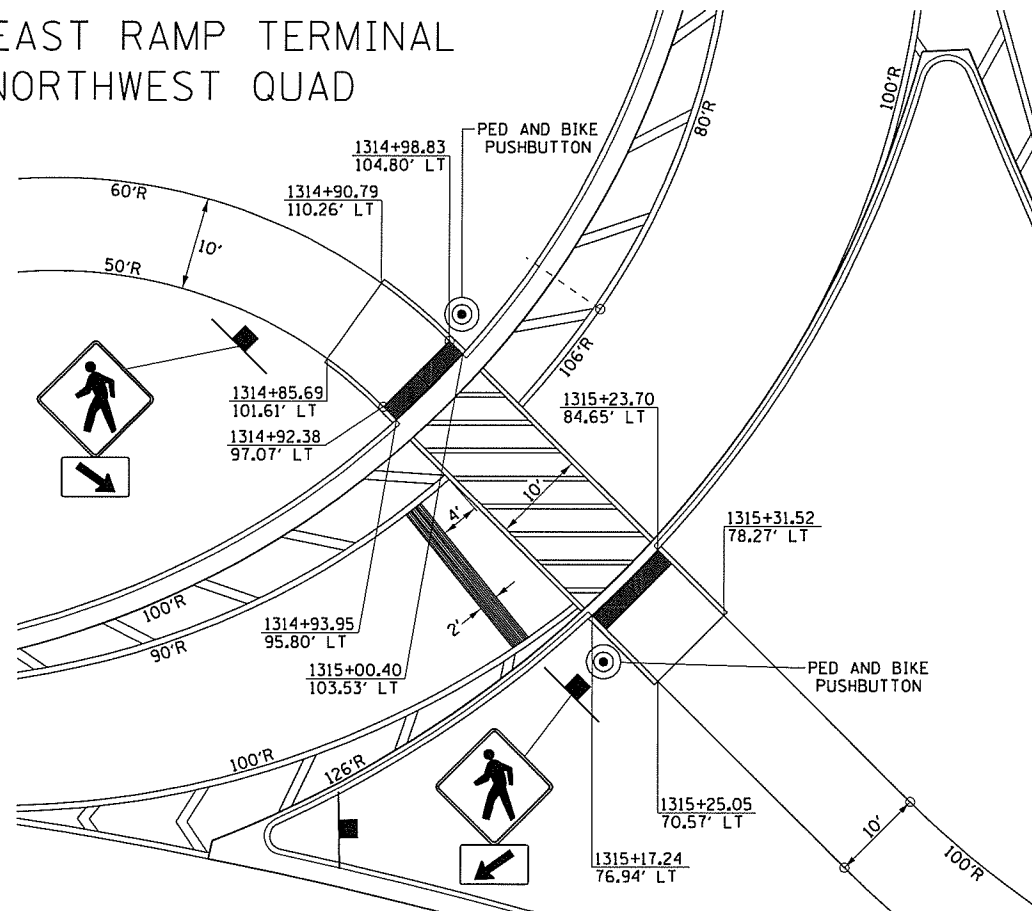


NOTE: ALL STATION AND OFFSETS TAKEN FROM THE HARRISON AVE/U.S. ROUTE 20 CENTERLINE

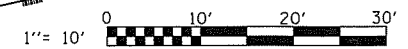
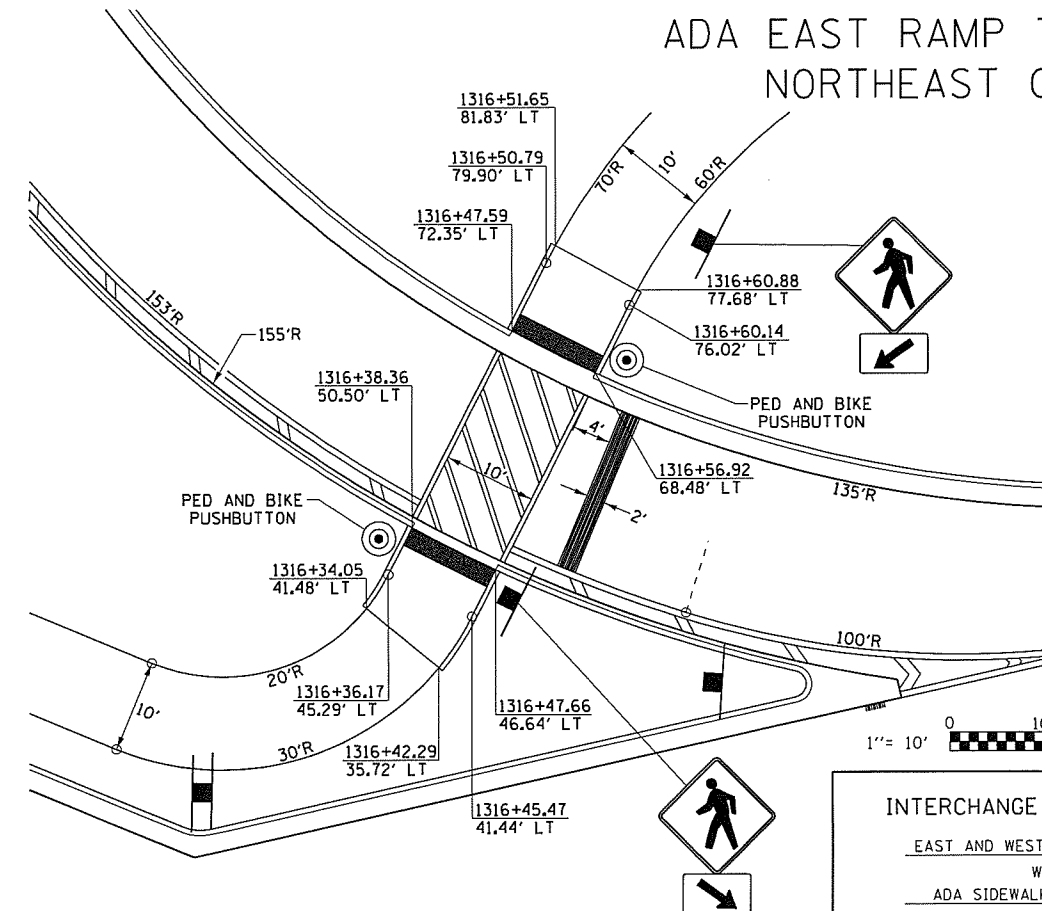
ADA WEST RAMP TERMINAL NORTHEAST QUAD



ADA EAST RAMP TERMINAL NORTHWEST QUAD



ADA EAST RAMP TERMINAL NORTHEAST QUAD

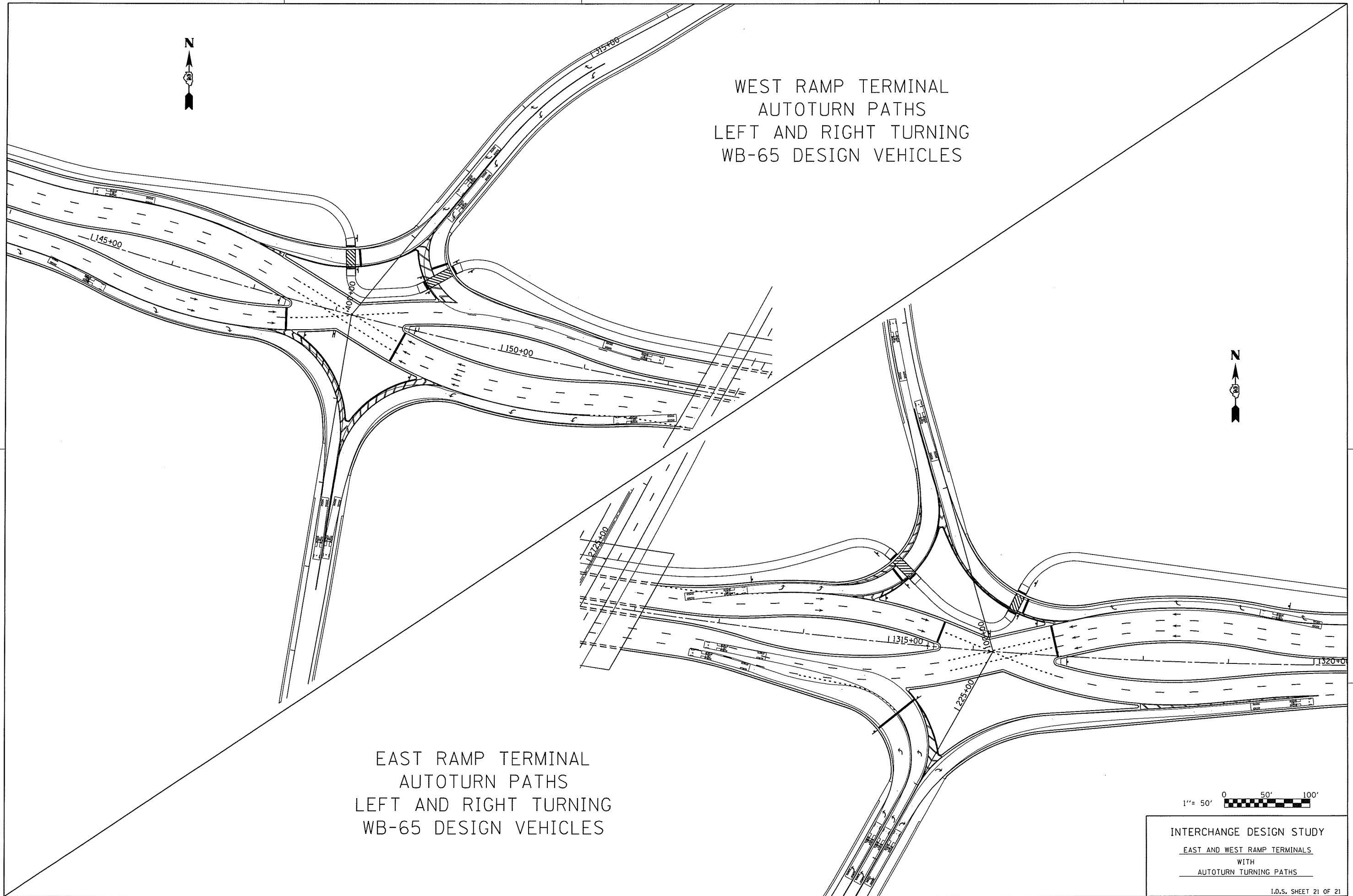


INTERCHANGE DESIGN STUDY
 EAST AND WEST RAMP TERMINALS
 WITH
 ADA SIDEWALK RAMP DETAILS

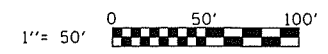
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WEST RAMP TERMINAL
AUTOTURN PATHS
LEFT AND RIGHT TURNING
WB-65 DESIGN VEHICLES



EAST RAMP TERMINAL
AUTOTURN PATHS
LEFT AND RIGHT TURNING
WB-65 DESIGN VEHICLES



INTERCHANGE DESIGN STUDY
EAST AND WEST RAMP TERMINALS
WITH
AUTOTURN TURNING PATHS

SIGNALIZED CAPACITY DESIGN ANALYSIS

PROGRAM USED: HCS 2010 VERSION: 6.65 SIGNAL TYPE: ACTUATED AREA TYPE: OTHER
 NO. OF PHASES: (A.M.) 5 (P.M.) 5 CYCLE LENGTH: (A.M.) 120 SEC. (P.M.) 120 SEC. PEAK HOUR FACTOR: 0.92
 INTERSECTION DELAY/LEVEL-OF-SERVICE: A.M. 22.8 SECONDS LOS C P.M. 25.8 SECONDS LOS C

APPROACH	A (EB)			B (WB)			C (SB)			D (NB)		
	L	TH	R	L	TH	R	L	TH	R	L	TH	R
LANE GROUP	1	3	1	1	3	1	1	3	1	1	3	1
NUMBER OF LANES	1	3	1	1	3	1	1	3	1	1	3	1
2040 30TH MAX. HOUR TRAFFIC (veh/h)	A.M. 115 P.M. 125	1120 2010	40 250	5 1700	100 115	75 25	95 115	135 80	220 180	145 105	10 30	
BASE SATURATION FLOW RATE (veh/h)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	
LANE WIDTH (FT)	12	12	12	12	12	12	12	12	12	12	12	
VOLUME OF RIGHT TURN ON RED (veh/h)	A.M. 0 P.M. 0	A.M. 0 P.M. 0	A.M. 0 P.M. 0	A.M. 0 P.M. 0	A.M. 0 P.M. 0	A.M. 0 P.M. 0	A.M. 0 P.M. 0	A.M. 0 P.M. 0	A.M. 0 P.M. 0	A.M. 0 P.M. 0	A.M. 0 P.M. 0	
PEDESTRIANS/HOUR (ped/h)	A.M. 0 P.M. 0	A.M. 0 P.M. 0	A.M. 0 P.M. 0	A.M. 0 P.M. 0	A.M. 0 P.M. 0	A.M. 0 P.M. 0	A.M. 0 P.M. 0	A.M. 0 P.M. 0	A.M. 0 P.M. 0	A.M. 0 P.M. 0	A.M. 0 P.M. 0	
ARRIVAL TYPE	3			3			3			3		
LANE UTILIZATION ADJ. FACTOR	1.000	.908	1.000	1.000	.908	1.000	1.000	.908	1.000	1.000	1.000	
GREEN TIME (SECONDS)	A.M. 7 P.M. 7	65 65	78 78	7 7	65 65	78 78	7 7	65 65	78 78	7 7	65 65	
GREEN RATIO (g/c)	A.M. 0.60 P.M. 0.60	0.54 0.54	0.62 0.61	0.60 0.60	0.54 0.54	0.61 0.67	0.22 0.25	0.16 0.17	0.22 0.23	0.17 0.11	0.23 0.17	
CAPACITY (c)	A.M. 340 P.M. 224	2670 2670	983 970	340 195	2596 2596	970 1063	272 350	342 310	336 294	317 206	362 268	
v/c RATIO (X)	A.M. 0.368 P.M. 0.606	0.456 0.818	0.044 0.280	0.016 0.223	0.461 0.712	0.112 0.118	0.300 0.388	0.429 0.403	0.712 0.665	0.498 0.554	0.030 0.122	
STORAGE QUEUE (FEET OR VEHICLES)	A.M. 182 P.M. 198	299 537	24 24	5 63	468 303	59 154	118 163	136 100	170 283	327 156	201 40	
LANE GROUP DELAY (SECONDS)	A.M. 12.9 P.M. 25.1	17.3 25.6	9.1 11.8	11.3 21.5	17.4 22.2	10.1 7.5	38.4 37.1	45.3 38.3	40.9 51.4	49.2 52.7	36.3 42.6	
LANE GROUP LEVEL-OF-SERVICE	A.M. B P.M. C	B C	A B	B C	B C	B D	D D	D D	D D	D D	D D	

RELOC. SERVICE DRIVE PROPOSED CURVE DATA
 PI STA. = 501+36.99
 $\Delta = 75^\circ 54' 26''$ (LT)
 $D = 52^\circ 05' 13''$
 $R = 110.00'$
 $T = 85.80'$
 $L = 145.73'$
 $E = 29.50'$
 S.E. = N.C.
 T.R. = N/A
 S.E. RUN = N/A
 P.C. STA. = 500+51.20
 P.T. STA. = 501+96.93

MILL ROAD PROPOSED CURVE DATA
 PI STA. = 408+44.46
 $\Delta = 12^\circ 08' 22''$ (RT)
 $D = 5^\circ 43' 46''$
 $R = 1,000.00'$
 $T = 106.34'$
 $L = 211.88'$
 $E = 5.64'$
 S.E. = N.C.
 T.R. = N/A
 S.E. RUN = N/A
 P.C. STA. = 407+38.12
 P.T. STA. = 409+50.00

ELEMENTS CONTROLLING DESIGN

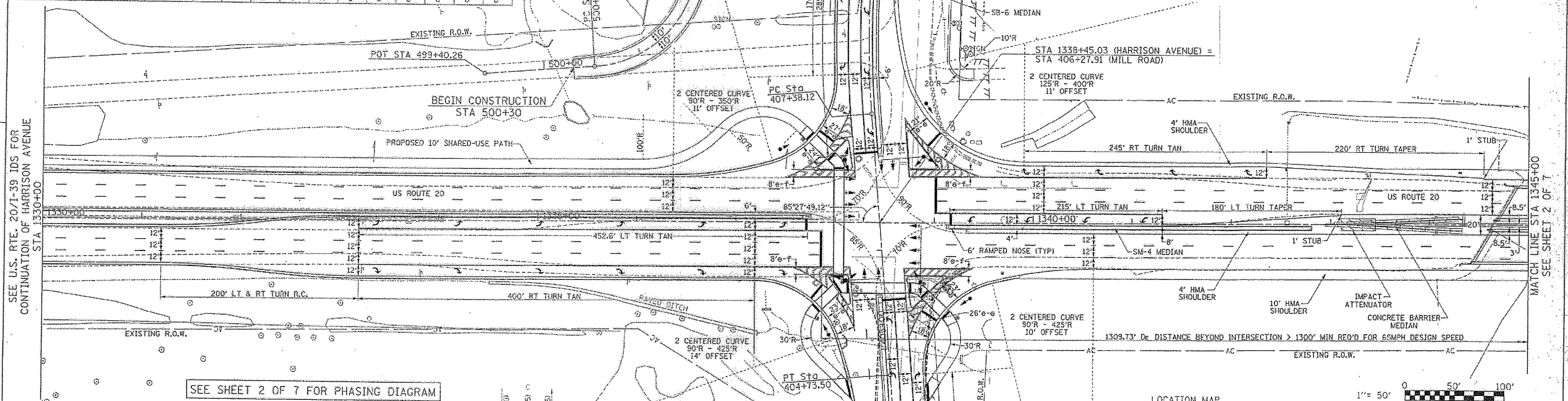
PREFERRED ROUTE:
 F.A.P. ROUTE NUMBER: 301. MARKED ROUTE NUMBER: U.S. ROUTE 20.
 STREET NAME: U.S. ROUTE 20. SRA ROUTE ? NO.
 FUNCTIONAL CLASSIFICATION: OTHER PRINCIPAL ARTERIAL. OSOW DESIGN? NO.
 EXISTING ADT: 30,900 VPD. DESIGN YEAR ADT: 43,450 VPD.
 PROPOSED DESIGN SPEED: 45 MPH. PROPOSED POSTED SPEED: 45 MPH.

SECONDARY ROUTE:
 F.A. ROUTE NUMBER: N/A. MARKED ROUTE NUMBER: N/A.
 STREET NAME: MILL ROAD. SRA ROUTE ? NO.
 FUNCTIONAL CLASSIFICATION: MINOR ARTERIAL (N); MAJOR COLLECTOR (S). OSOW DESIGN? NO.
 EXISTING ADT: 6,600 VPD. DESIGN YEAR ADT: 7,200 VPD.
 PROPOSED DESIGN SPEED: 45 MPH (N); 30 MPH (S). PROPOSED POSTED SPEED: 45 MPH (N); 25 MPH (S).

IMPROVEMENT TYPE: RECONSTRUCTION. ANTICIPATED YEAR OF CONSTRUCTION: 2020
 EXISTING METHOD OF TRAFFIC CONTROL: TRAFFIC SIGNALS. PROPOSED METHOD: TRAFFIC SIGNALS.
 SIGNAL WARRANT: EXISTING SIGNALS.
 DESIGN VEHICLE: INTERSECTION DESIGNED FOR A WB-65 DESIGN VEHICLE.
 DESIGN YEAR: 2040 WHICH IS A 20 YEAR DESIGN.
 TRUCK ROUTE DESIGNATION: PREFERRED ROADWAY: U.S. ROUTE 20 IS A CLASS II TRUCK ROUTE.
 SECONDARY ROADWAY: MILL ROAD, NONE.
 DESIGN CRITERIA: BDE MANUAL - CHAPTER 36.

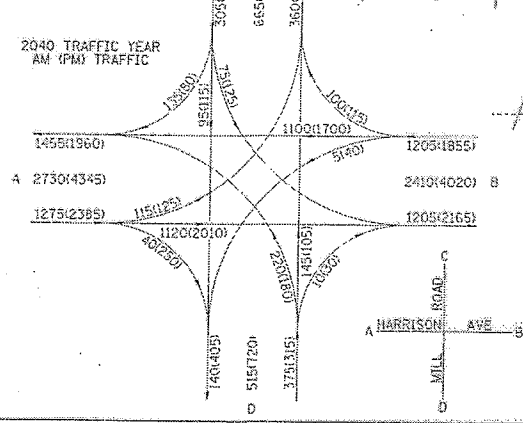
GENERAL NOTES

ARE PROFILES PROVIDED? YES. IF NOT, STATE REASON WHY:
 TYPE B6.24 CURB AND GUTTER ON THE OUTSIDE OF THE ROADWAY/SHOULDERS.
 TYPE B6.06 & B6.24 CURB AND GUTTER ON THE APPROACH MEDIAN; EXCEPT EAST LEG - SM-41.
 TYPE M6.06 & M6.24 CURB AND GUTTER ON THE CORNER ISLANDS.
 ALL DIMENSIONS SHOWN ARE (E-E), UNLESS OTHERWISE NOTED
 THE RIGHT-OF-WAY LIMITS ARE PRELIMINARY.
 DESIGN VEHICLE TURNING MOVEMENTS ARE ACCOMMODATE PER AUTOTURN SOFTWARE, VERSION 9.0.1.256.
 THE SCOPE OF WORK: RECONSTRUCTION OF U.S. ROUTE 20 TO SIX LANES PLUS AUXILIARY LANES THROUGH THE MILL ROAD INTERSECTION AND REALIGNMENT OF MILL ROAD WITH TURN LANES. NEW SIGNALS TO BE INSTALLED AND PED/BIKE FACILITIES COMPLETED ON THE WEST LEG FULLY ADA/FROWG COMPLIANT. SIGNALS TO BE INTERCONNECTED WITH SIGNALS TO THE WEST.
 INTERSECTION DESIGN EXCEPTIONS: WB THROUGH STORAGE QUEUES IN THE AM PEAK WILL OBSTRUCT THE W.B. LEFT & RIGHT TURN LANES IN THE AM PEAK HOUR. DUE TO ENVIRONMENTAL ISSUES, THE ADJACENT BRIDGE IS UNABLE TO BE WIDENED TO EXTEND THE W.B. TURN LANES.

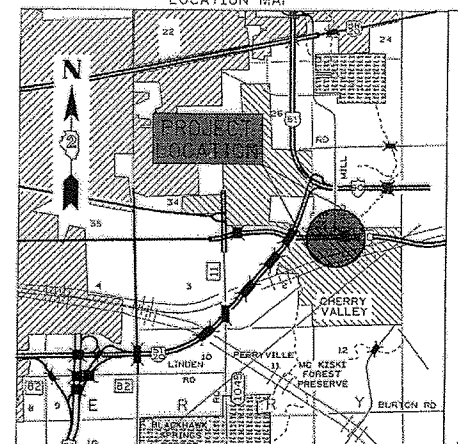


TRAFFIC DATA

MOVEMENT	YEAR 2020 30TH MAXIMUM HOUR TRAFFIC		PERCENT TRUCK TRAFFIC IN 30TH MAX HOUR	ESTIMATED PERCENT INCREASE BY	YEAR 20 30TH MAXIMUM HOUR TRAFFIC		ESTIMATED PERCENT INCREASE BY	YEAR 2040 30TH MAXIMUM HOUR TRAFFIC	
	A.M.	P.M.			A.M.	P.M.		A.M.	P.M.
AB	730	1355	5.0		40	230	1.0	1120	2010
AD	40	230	1.0		5	40	1.0	115	125
AC	105	115	1.0		100	1700		100	1700
BA	735	1160	3.0		45	115		75	125
BC	50	105	1.0		100	115		100	115
BD	5	40			5	40		5	40
CD	85	105	2.0		45	115		75	125
CA	125	70	2.0		145	105		145	105
CB	65	115	1.0		10	30		10	30
DC	135	95			220	160		220	160
DB	10	30			10	30		10	30
DA	200	160	1.0		2730	4345		2730	4345
TOTAL A	1335	3090			2410	4020		2410	4020
TOTAL B	1635	2805			665	865		665	865
TOTAL C	605	605			515	720		515	720
TOTAL D	475	560							



MILL ROAD PROPOSED CURVE DATA
 PI STA. = 403+80.54
 $\Delta = 7^\circ 06' 38''$ (LT)
 $D = 3^\circ 49' 11''$
 $R = 1,500.00'$
 $T = 93.20'$
 $L = 186.15'$
 $E = 2.89'$
 S.E. = N.C.
 T.R. = N/A
 S.E. RUN = N/A
 P.C. STA. = 402+87.35
 P.T. STA. = 404+73.50



INTERSECTION DESIGN STUDY

FAP ROUTE 301 (US ROUTE 20)
 WITH
 MILL ROAD

SEC. NO. I201-3K&4-1.5R PROJ. NO. _____
 SCALE 1" = 50' COUNTY WINNEBAGO
 SJN : P92-III-06 REV. NO. _____

DESIGNED BY IDOT - DISTRICT 2 DATE APRIL 2017

SATISFACTORY *Mike Hansen* 4-25-17
 DISTRICT GEOMETRICS ENGINEER DATE

SATISFACTORY *David Havel* 4-25-17
 DISTRICT PROGRAM DEVELOPMENT ENGINEER DATE

SATISFACTORY *John Shuman* 4-25-17
 DISTRICT OPERATIONS ENGINEER DATE

APPROVED *[Signature]* 4-25-17
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER DATE

LAMP FILE NAME: E 3 I.D.S. SHEET 1 OF 7

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 PLOT SCALE: 1" = 50'
 USER NAME: jshuman

MATCH LINE STA 1345+00
SEE SHEET 2 OF 7

SEE SHEET 2 OF 7 FOR PHASING DIAGRAM

MATCH LINE STA 505+00
SEE SHEET 2 OF 7

MATCH LINE 411+50
SEE SHEET 2 OF 7

MATCH LINE STA 402+50
SEE SHEET 2 OF 7

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**RELOC. SERVICE DRIVE
 PROPOSED CURVE DATA**
 PI STA. = 505+77.33
 $\Delta = 86^\circ 36' 35''$ (RT)
 $D = 81^\circ 51' 04''$
 $R = 70.00'$
 $T = 65.98'$
 $L = 105.81'$
 $E = 26.19'$
 $S.E. = N.C.$
 $T.R. = N/A$
 $S.E. RUN = N/A$
 $P.C. STA. = 505+11.35$
 $P.T. STA. = 506+17.16$

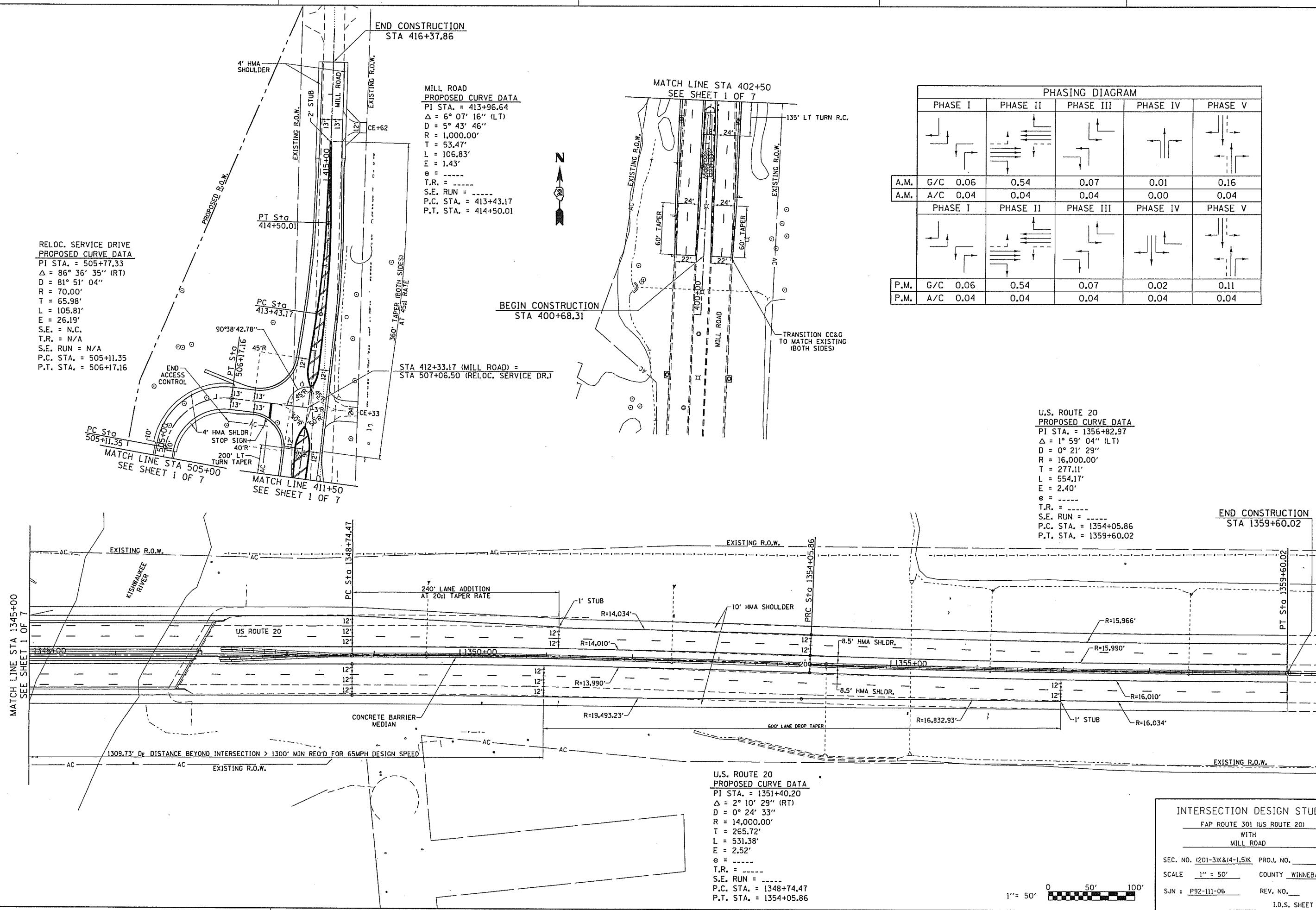
**MILL ROAD
 PROPOSED CURVE DATA**
 PI STA. = 413+96.64
 $\Delta = 6^\circ 07' 16''$ (LT)
 $D = 5^\circ 43' 46''$
 $R = 1,000.00'$
 $T = 53.47'$
 $L = 106.83'$
 $E = 1.43'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 413+43.17$
 $P.T. STA. = 414+50.01$

PHASING DIAGRAM

	PHASE I	PHASE II	PHASE III	PHASE IV	PHASE V
A.M. G/C	0.06	0.54	0.07	0.01	0.16
A.M. A/C	0.04	0.04	0.04	0.00	0.04
P.M. G/C	0.06	0.54	0.07	0.02	0.11
P.M. A/C	0.04	0.04	0.04	0.04	0.04

**U.S. ROUTE 20
 PROPOSED CURVE DATA**
 PI STA. = 1356+82.97
 $\Delta = 1^\circ 59' 04''$ (LT)
 $D = 0^\circ 21' 29''$
 $R = 16,000.00'$
 $T = 277.11'$
 $L = 554.17'$
 $E = 2.40'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 1354+05.86$
 $P.T. STA. = 1359+60.02$

**U.S. ROUTE 20
 PROPOSED CURVE DATA**
 PI STA. = 1351+40.20
 $\Delta = 2^\circ 10' 29''$ (RT)
 $D = 0^\circ 24' 33''$
 $R = 14,000.00'$
 $T = 265.72'$
 $L = 531.38'$
 $E = 2.52'$
 $e = \text{-----}$
 $T.R. = \text{-----}$
 $S.E. RUN = \text{-----}$
 $P.C. STA. = 1348+74.47$
 $P.T. STA. = 1354+05.86$



INTERSECTION DESIGN STUDY

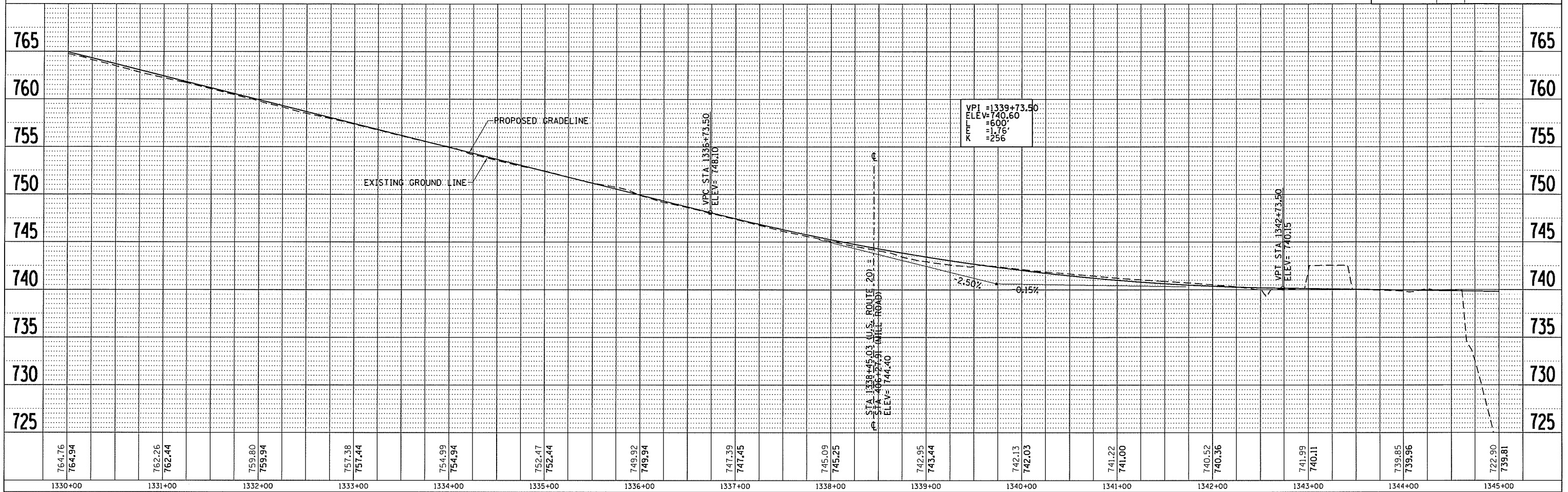
FAP ROUTE 301 (US ROUTE 20)
 WITH
 MILL ROAD

SEC. NO. (201-31K&14-1.5)K PROJ. NO. _____
 SCALE 1" = 50' COUNTY WINNEBAGO
 SJN : P92-111-06 REV. NO. _____
 I.D.S. SHEET 2 OF 7

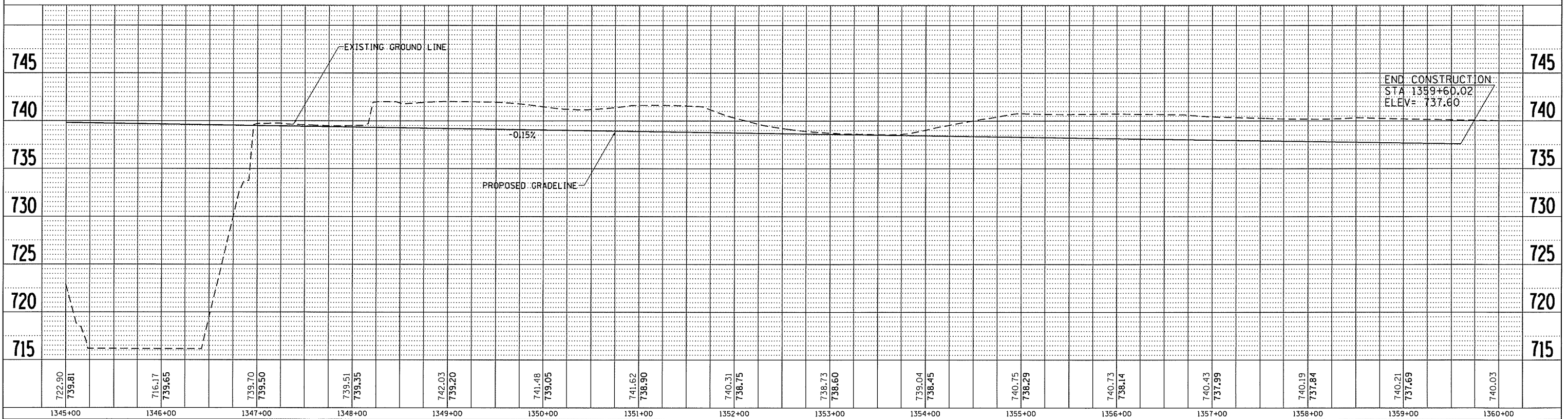


PROPOSED PROFILE U.S. ROUTE 20

FAP RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	201-31K&14-1.5JR	WINNEBAGO	7	3
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS		FED. AID PROJECT

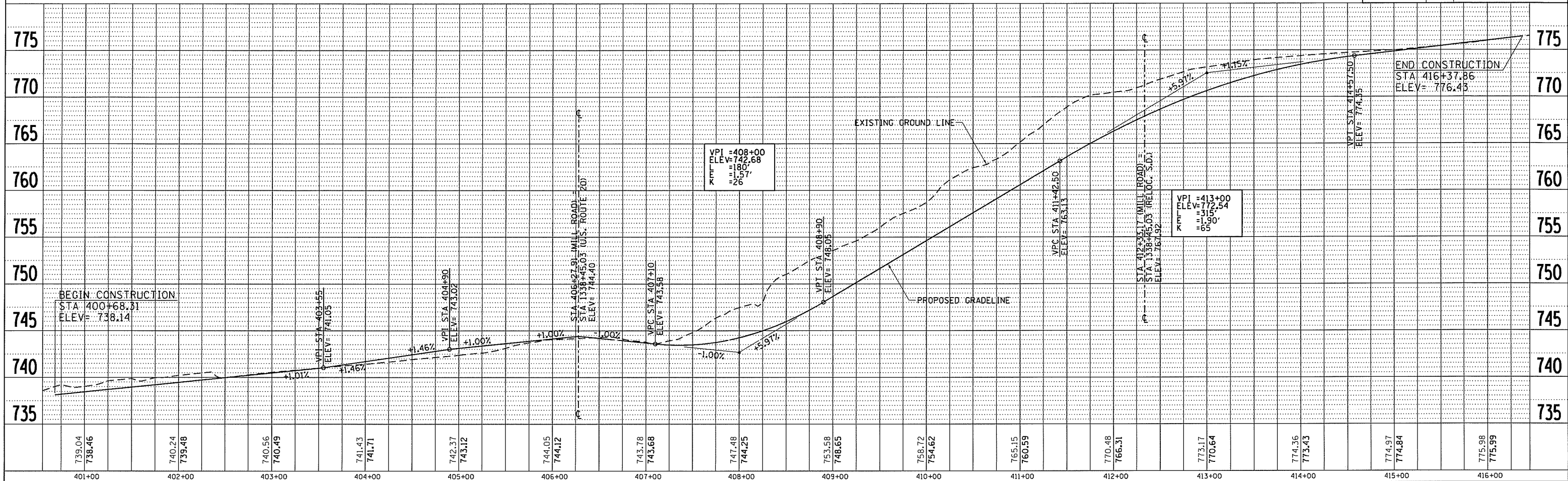


PROPOSED PROFILE U.S. ROUTE 20

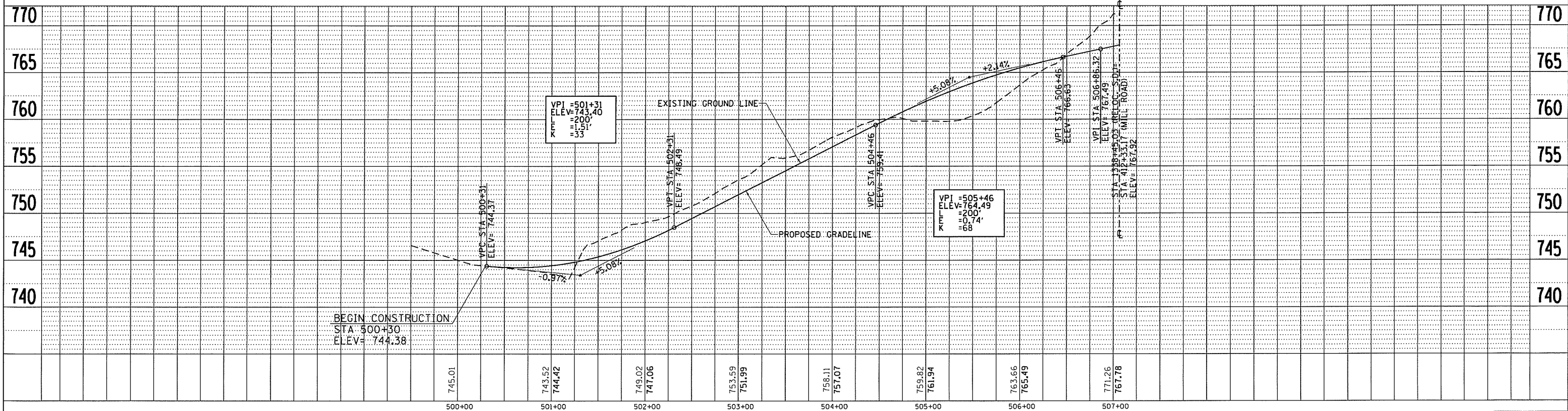


PROPOSED PROFILE MILL ROAD

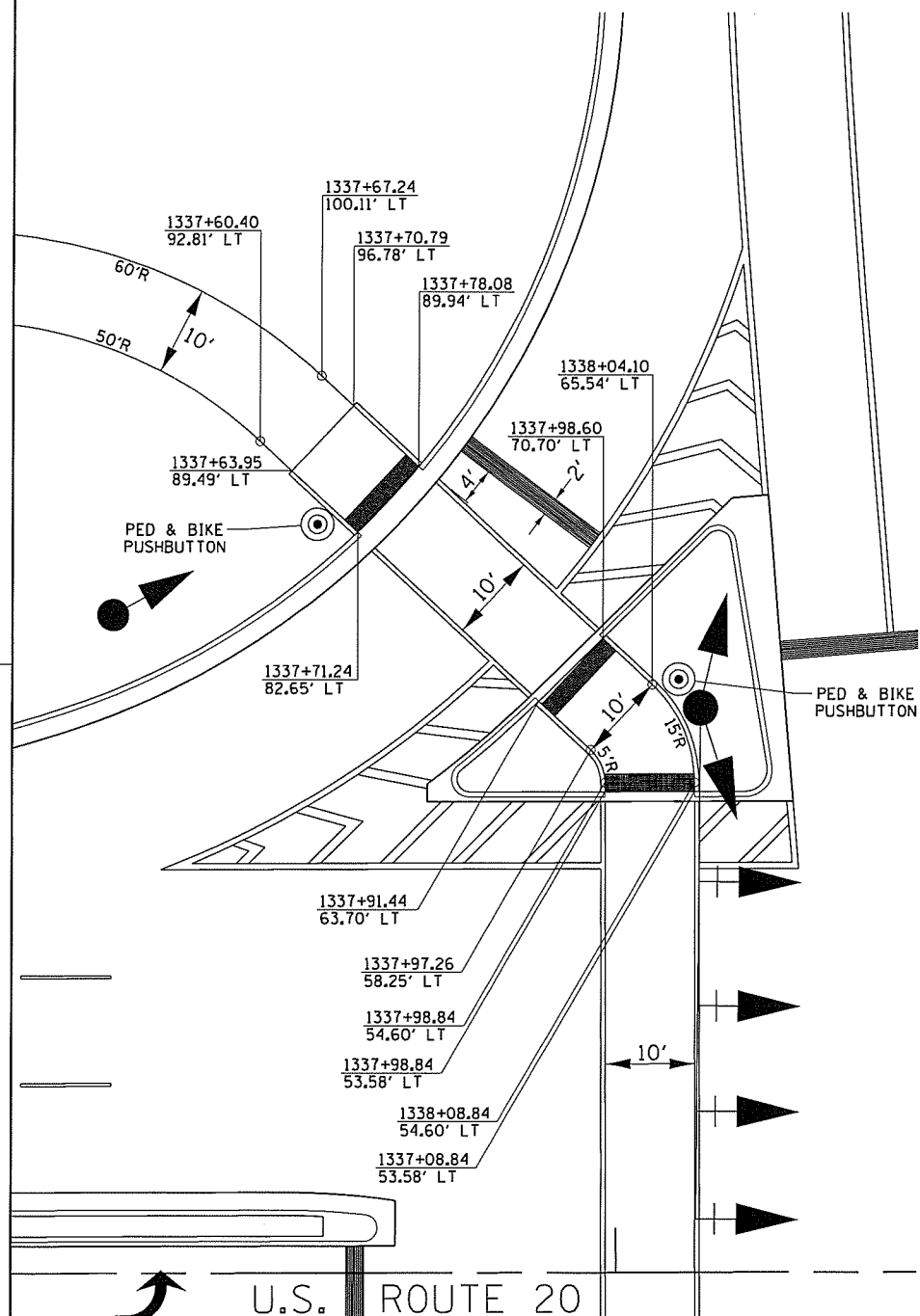
F&P REF.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
301	201-3IK&4-1,5IR	WINNEBAGO	7	4
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



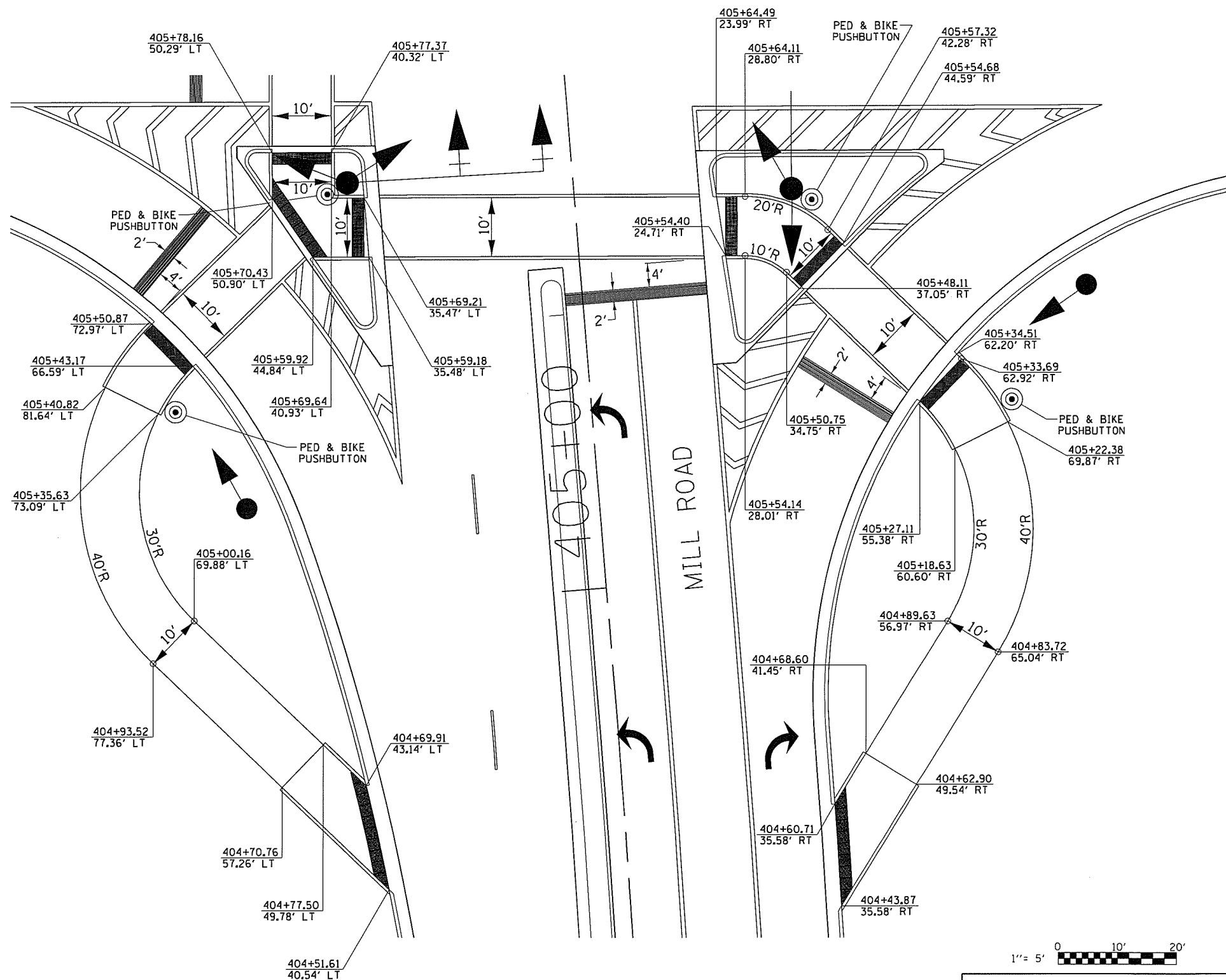
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NOTE: ALL STATION AND OFFSETS TAKEN FROM THE U.S. ROUTE 20 CENTERLINE



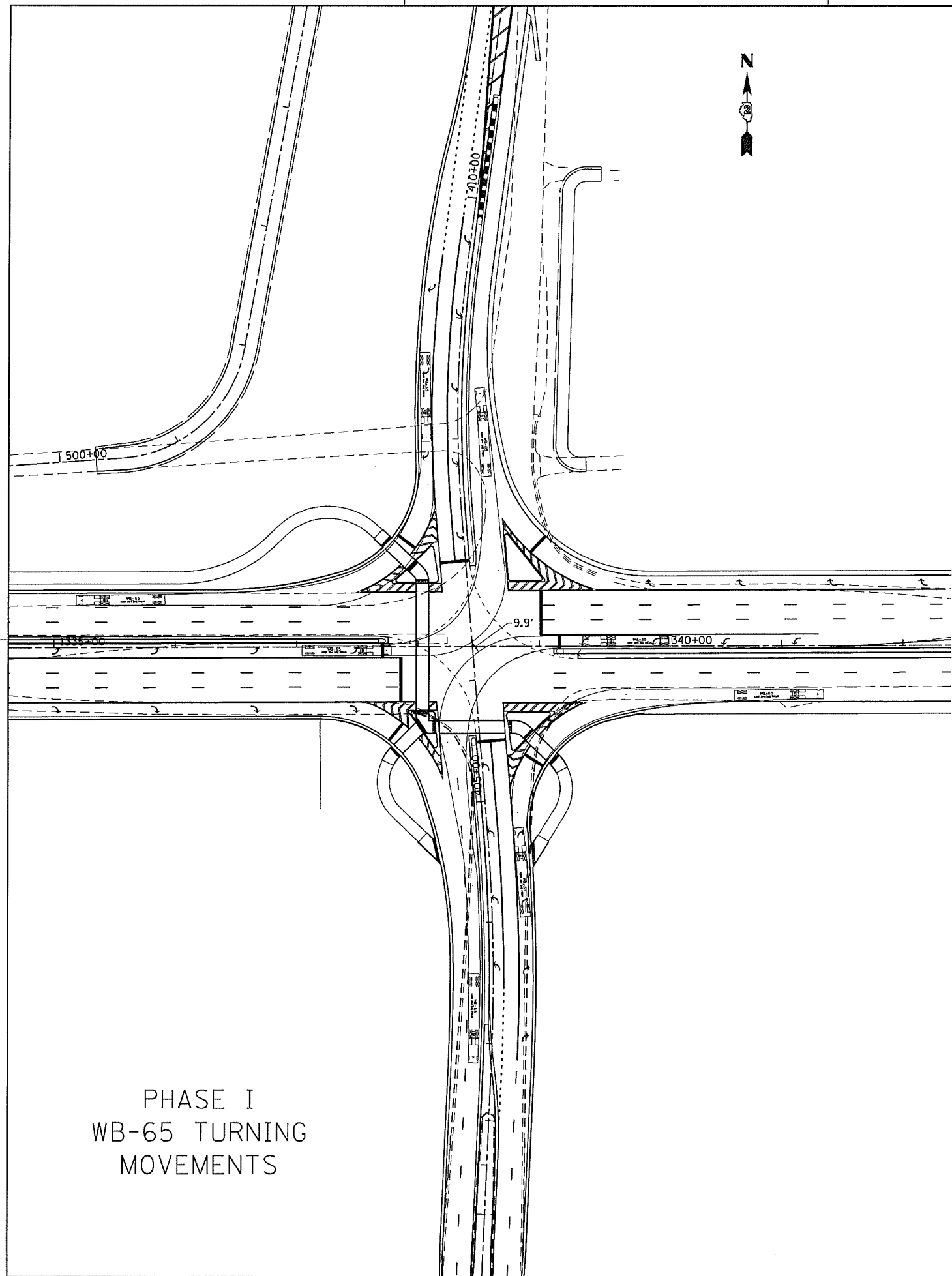
NOTE: ALL STATION AND OFFSETS TAKEN FROM THE MILL ROAD CENTERLINE



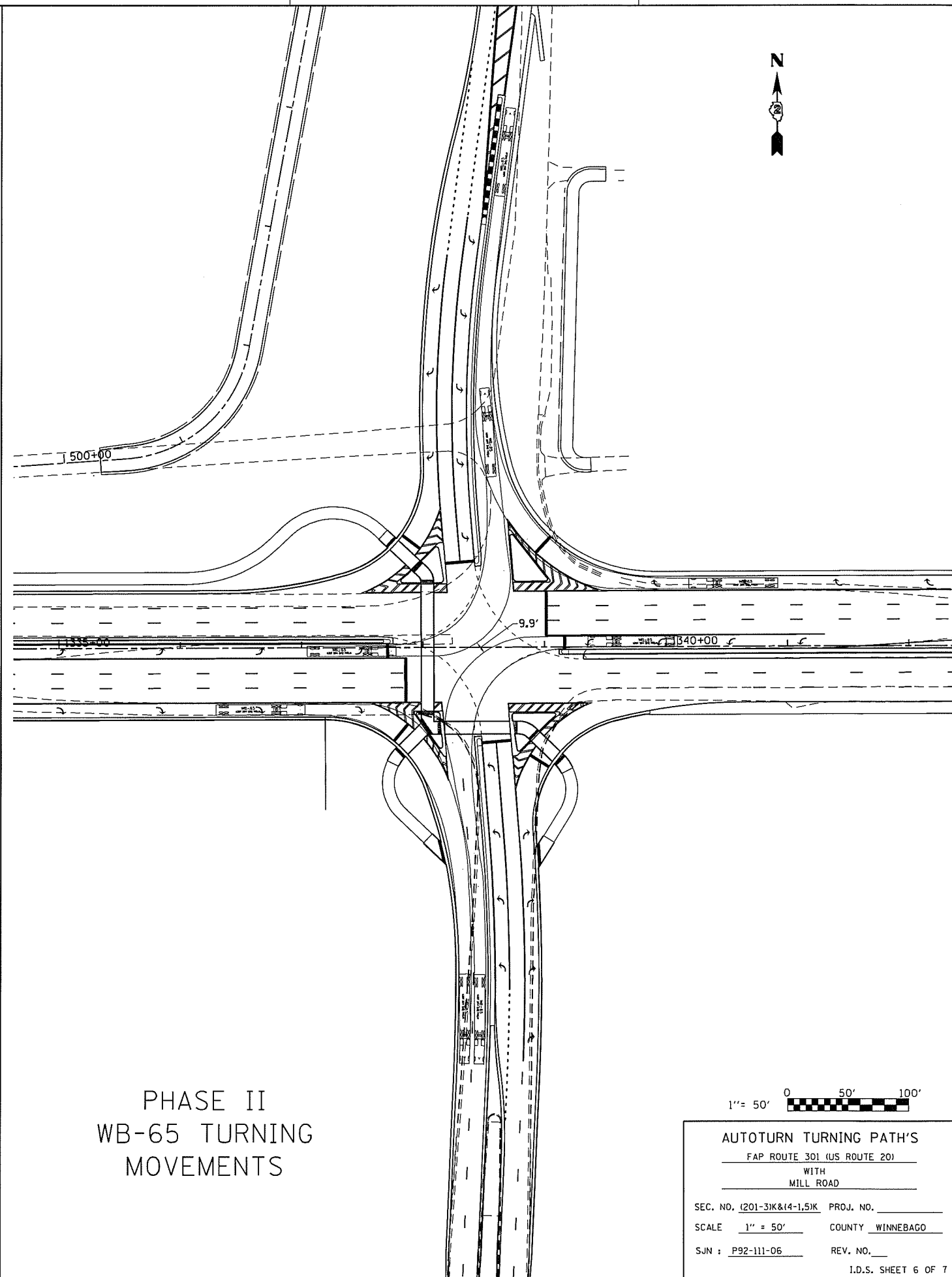
ADA SIDEWALK RAMP DETAILS
 FAP ROUTE 301 (US ROUTE 20)
 WITH
 MILL ROAD

SEC. NO. (201-3)K&(4-1.5)K PROJ. NO. _____
 SCALE 1" = 5' COUNTY WINNEBAGO
 SJN : P92-111-06 REV. NO. _____
 I.D.S. SHEET 5 OF 7

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USER NAME = Griffith



PHASE I
WB-65 TURNING
MOVEMENTS



PHASE II
WB-65 TURNING
MOVEMENTS

1" = 50' 0 50' 100'

AUTOTURN TURNING PATH'S
FAP ROUTE 301 (US ROUTE 20)
WITH
MILL ROAD

SEC. NO. (201-3)K&I4-1.5)K PROJ. NO. _____

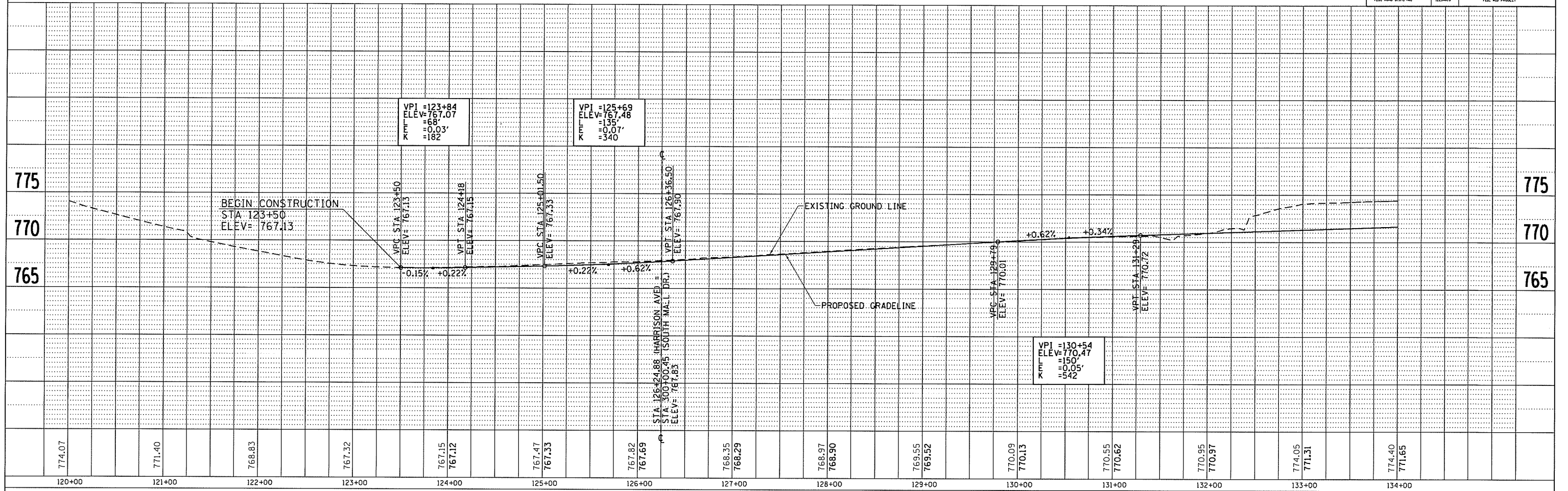
SCALE 1" = 50' COUNTY WINNEBAGO

SJN : P92-111-06 REV. NO. _____

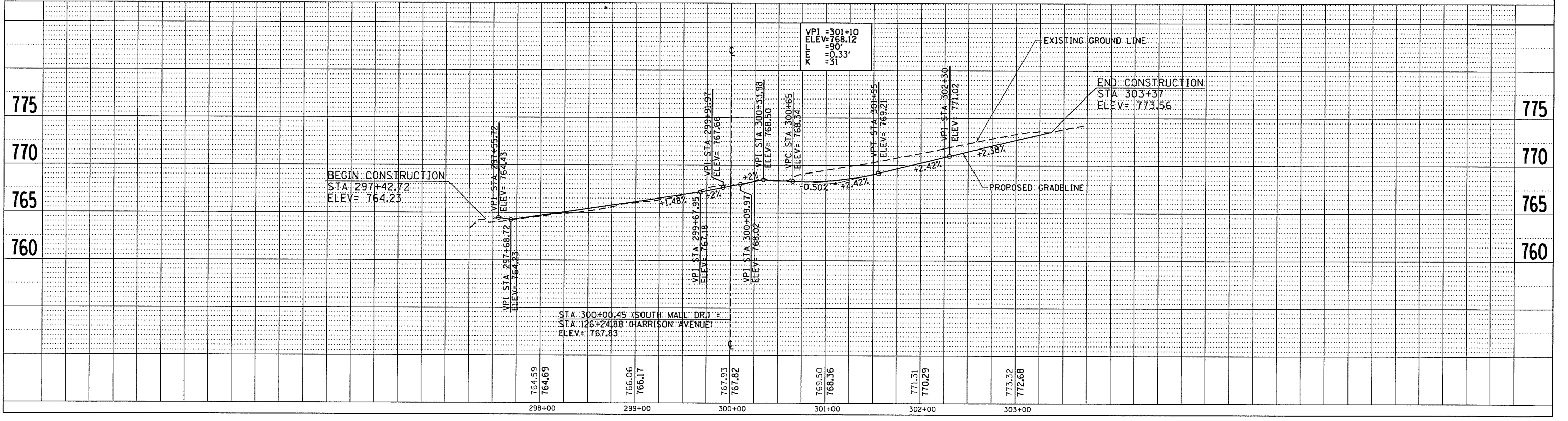
I.D.S. SHEET 6 OF 7

PROPOSED PROFILE HARRISON AVENUE

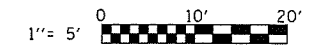
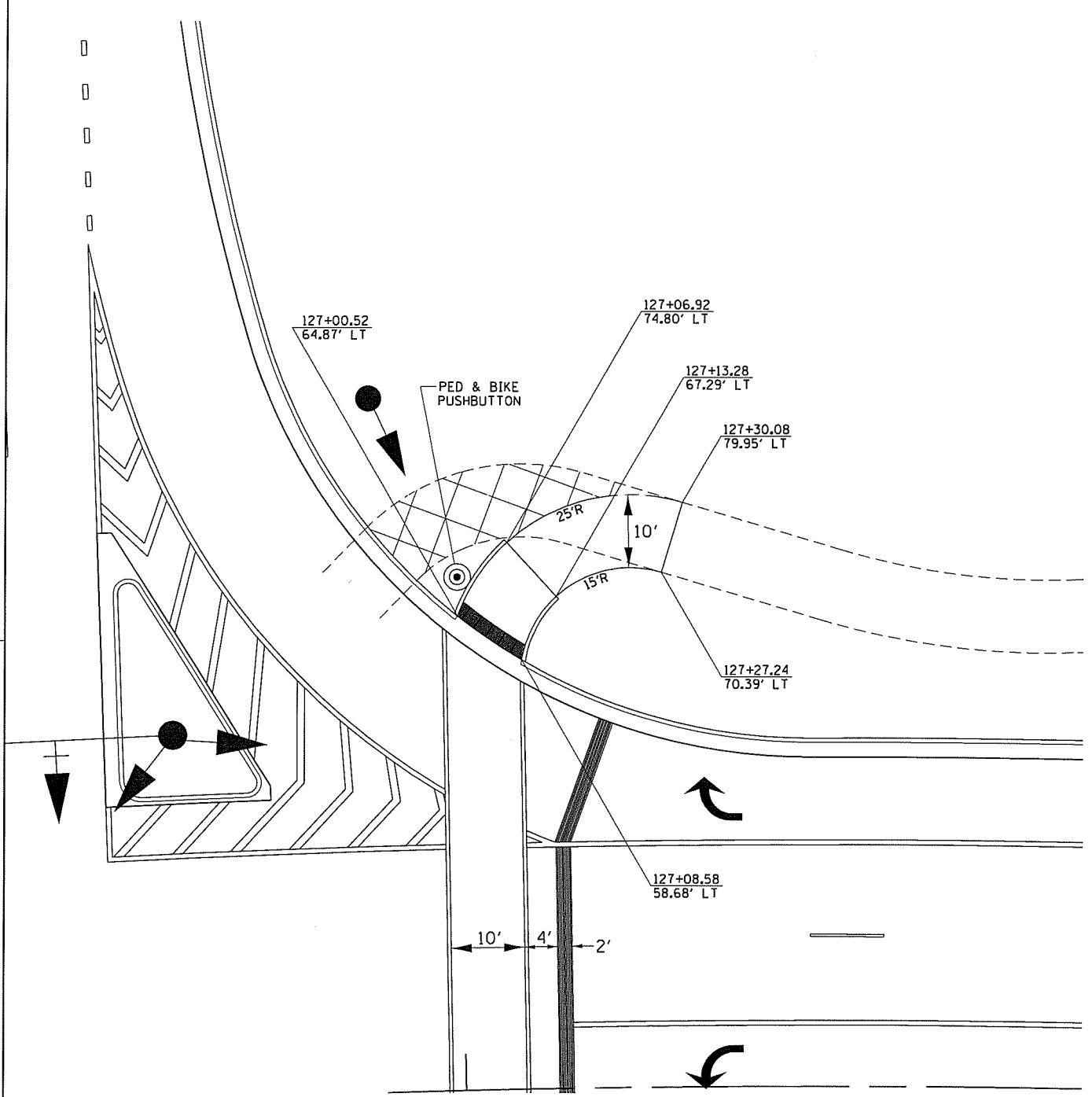
SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
201-31K&14-1,51R	WINNEBAGO	4	2
STA. TO STA.			
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT



PROPOSED PROFILE SOUTH MALL DRIVE

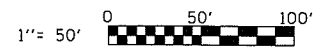
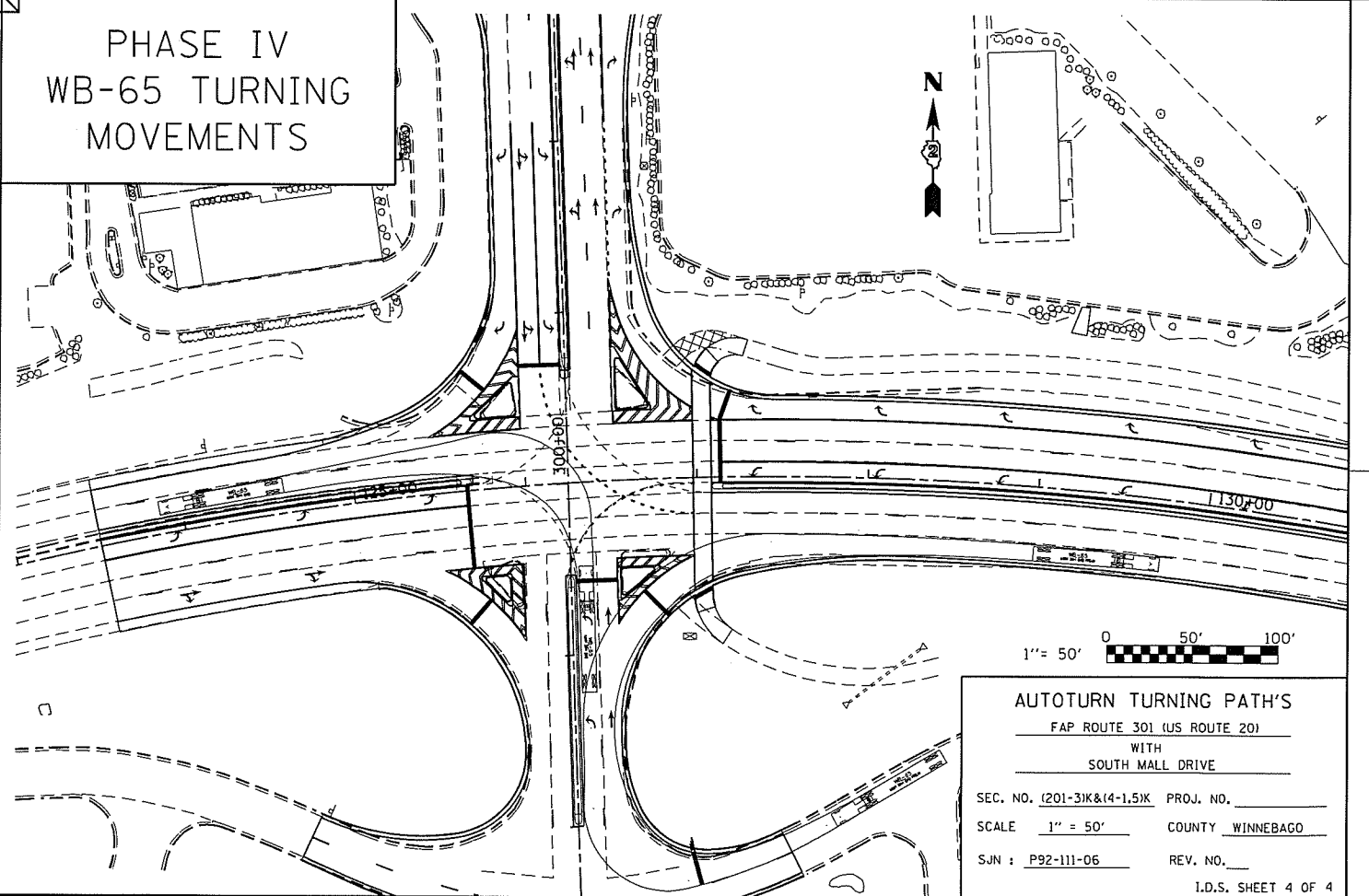
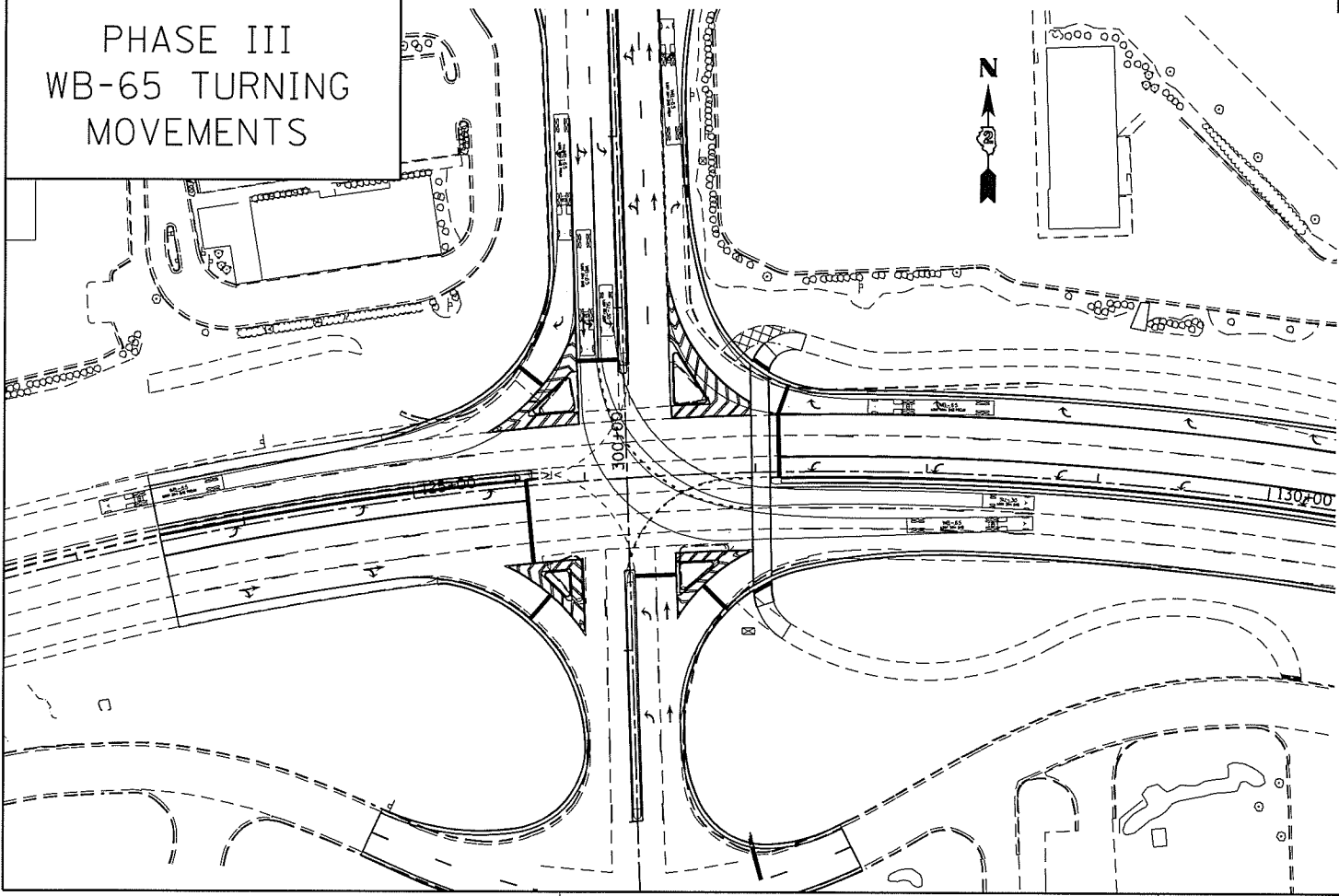
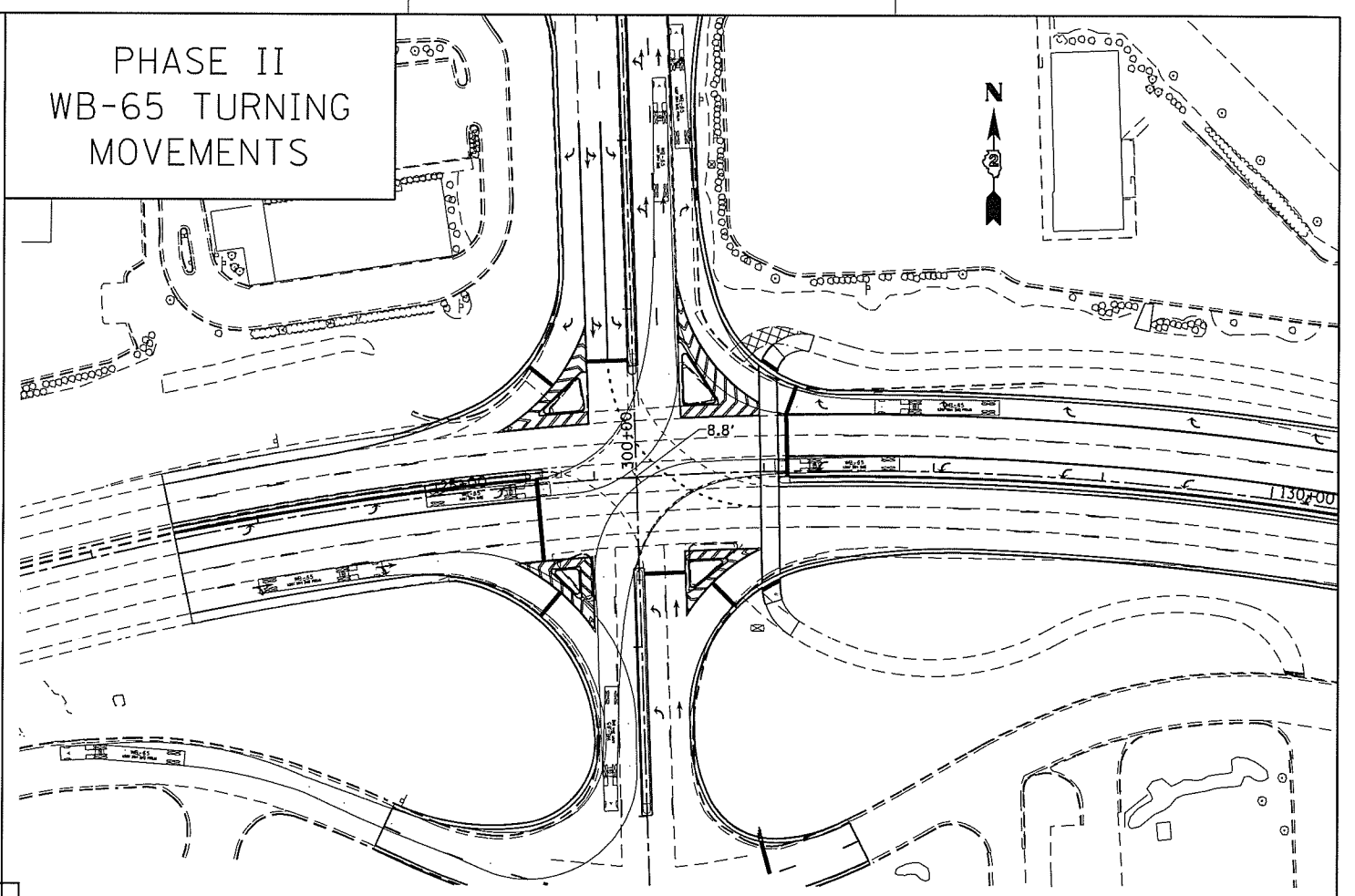
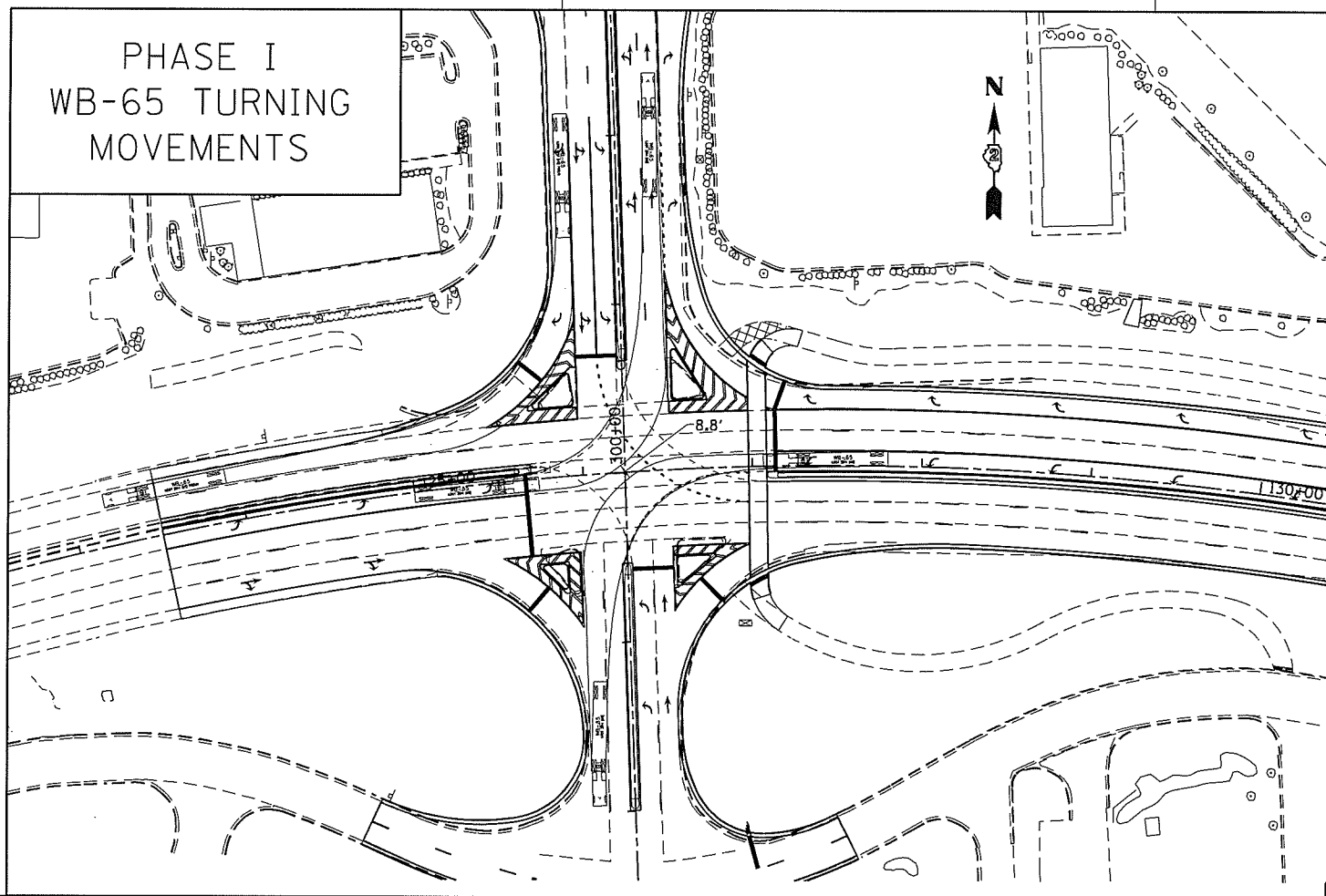


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 PLOT SCALE = 50.000' / 1"
 USER NAME = criffi.bndj



ADA SIDEWALK RAMP DETAILS	
FAP ROUTE 301 (US ROUTE 20)	
WITH	
SOUTH MALL DRIVE	
SEC. NO. (201-31K&4-1.5K	PROJ. NO. _____
SCALE 1" = 5'	COUNTY WINNEBAGO
SJN : P92-111-06	REV. NO. _____
I.D.S. SHEET 3 OF 4	

PLOT DATE = Mar-17-2017 08:06:40 AM
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 PLOT SCALE = 50.000 / in.
 USER NAME = Criff, bjd



AUTOTURN TURNING PATH'S
 FAP ROUTE 301 (US ROUTE 20)
 WITH
 SOUTH MALL DRIVE

SEC. NO. (201-31K&(4-1.5)K PROJ. NO. _____
 SCALE 1" = 50' COUNTY WINNEBAGO
 SJN : P92-111-06 REV. NO. _____
 I.D.S. SHEET 4 OF 4

TYPE, SIZE, & LOCATION STUDIES

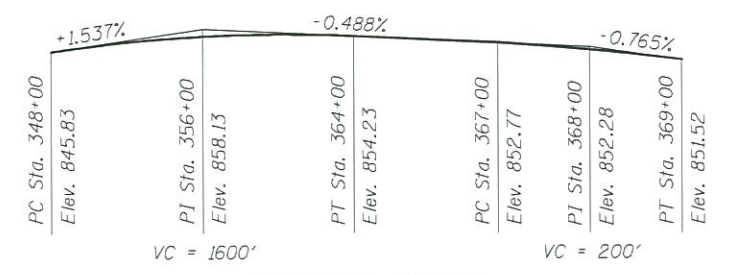
FAI 39 (I-39) and FAP 301 (US 20)
 Section (201-3)K and (4-1, 5)R
 Winnebago County
 Job No. P-92-111-06
 Contract No. 64C62 & 64B13
 PTB 141/004
 Consultant: Hanson Professional Services Inc.

Proposed SN	Existing SN	Description	Action	Contract No.	TSL	SGR	Designer	TSL & SGR Submitted	TSL Comments	SGR Comments	TSL & SGR Resub.	TSL Approved	SGR Approved	TSL Status	SGR Status	Action	ETC
101-0204	none	I-39 NB (Ramp DA) over I-39 SB (Ramp BC) new structure due to ramp BD realignment		64C62	x	x	Fehr Graham	4/20/2016	6/2/2016	6/6/2016	10/4/2016 3/8/2017 9/7/2016	1/11/2017 3/8/2017	1/9/2017	completed, stamped Approved TSL received on 3/8/2017	completed		
101-0206	101-0098	Perryville Road over I-39 & US 20	Complete Replacement - realignment	64C24	x	x	Hanson	2/17/2016	8/2/2016	4/14/2016	3/7/2017 9/7/2016	X	11/3/2016	completed, waiting on approval of integral abutment policy change	completed	IDOT	
101-0207	101-0131	Mulford Road over I-39 & US 20	Complete Replacement - realignment	64C24	x	x	Hanson	2/17/2016	8/2/2016	5/4/2016	3/7/2017	X	11/3/2016	completed, waiting on approval of integral abutment policy change	completed	IDOT	
101-0208/09	101-0067/68	US-20 over CNRR	Complete Replacement	64C24	x	x	Infrastructure	2/17/2017	3/23/2017	3/23/2017	3/24/2017 1/19/2017		3/31/2017	completed, waiting on RR approval	completed	IDOT	
101-0210/11	101-0069/70	I-39/US 20 over Union Pacific Railroad	Complete Replacement	64C24	x	x	Infrastructure	8/18/2016	11/11/2016	11/18/2016	3/7/2017 12/1/2016	X	2/2/2017	IDOT approved, waiting on RR approval	completed	IDOT	
101-0212	none	NB I-39 (Ramp DB) over Linden Rd.	new structure due to ramp DB realignment	64C62	x	x	Fehr Graham	6/30/2016	8/8/2016	10/11/2016	2/13/2017 3/8/2017	1/24/2017 3/8/2017	2/14/2017	completed, stamped Approved TSL received on 3/8/2017	completed		
101-0213/14	101-0071/72	I-39/US 20 over Harrison Ave.	Complete Replacement	64C24	x	x	Hanson	10/5/2016	11/29/2016	2/6/2017	11/23/2016 12/27/2016	3/17/2017	3/7/2017	3/22/2017	completed	completed	IDOT
101-0215	none	I-39 SB (Ramp BD) over US 20	new structure due to ramp BD realignment	64C62	x	x	Hanson	1/16/2017	3/3/2017	3/28/2017	4/5/2017 12/1/2016			completed, need IDOT approval	completed, need IDOT approval	IDOT	
101-0216	none	Linden Road over I-39 SB (Ramp BD)	new structure due to ramp BD realignment	64C62	x	x	Fehr Graham	8/18/2016	10/20/2016	10/6/2016	12/13/2016 3/8/2017	12/15/2016 3/8/2012	12/21/2016	completed, stamped Approved TSL received on 3/8/2017	completed		
101-2053	101-2025	I-39 over Madigan Creek	Remove and replace culvert	64C24 & 64B13	x	x	Fehr Graham							no TSL required per IDOT			
101-0073/74	101-0073/74	US 20 over Kishwaukee	deck replacement with structure widening	64C24 & 64B13	x		Fehr Graham							design deferred to Phase II			
N/A	401-0136	Ramp BD over US 20 Bypass	to be removed - ramp realignment														
N/A	401-0139	Ramp BD - US 20 WB to I-39 SB over Linden	to be removed - ramp realignment														
N/A	401-0140	Ramp DB: I-39 NB to US 20 EB over Linden	to be removed - ramp realignment														

Benchmark: Cut "□" south side of eastern base of 30 mph ramp sign located east of the ramp connecting I-39 NB to US 20 West (Ramp DA) 0.1 mile north of the centerline of Linden Road. Elev. 851.37, 42°-13'-06.37" N, 89°-00'-39.64" W.

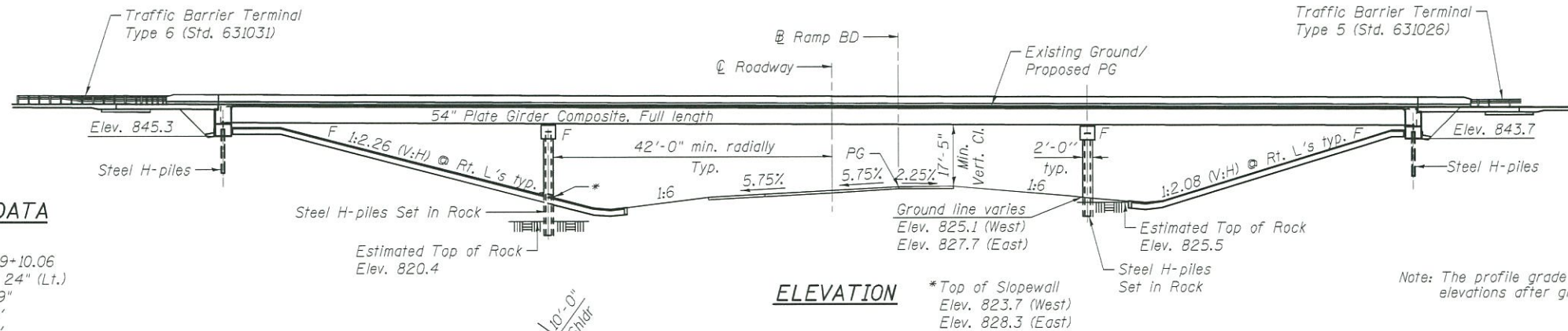
Existing Structure: None

Stage Construction will be utilized to maintain one lane of traffic at all times. Stage I Traffic will be maintained on existing Ramp DA.



CURVE DATA

Ramp BD
 P.I. Sta. = 169+10.06
 $\Delta = 107^\circ 00' 24''$ (Lt.)
 $D = 2^\circ 14' 49''$
 $R = 2,550.00'$
 $T = 3,446.54'$
 $L = 4,762.42'$
 $E = 1,737.32$
 $SE = 5.75\%$
 P.C. Sta. = 134+63.52
 P.T. Sta. = 182+25.95

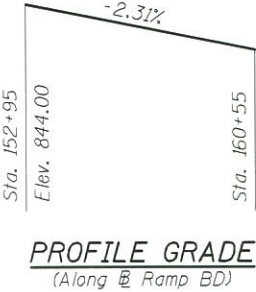


ELEVATION

*Top of Slope wall
 Elev. 823.7 (West)
 Elev. 828.3 (East)

Note: The profile grade shows the final elevations after grinding

PROFILE GRADE
 (Along Ramp DA)



HIGHWAY CLASSIFICATION

F.A.I. Rte. 39 - I-39 NB Ramp DA	F.A.I. Rte. 39 - I-39 SB Ramp BD
Functional Class: Interstate	Functional Class: Interstate
ADT: 4050(2013); 6400(2040)	ADT: 10,000(2013); 28,000(2040)
ADTT: 1000(2013); 1600(2040)	ADTT: 4200(2013); 11,700(2040)
DHV: 640 (2040)	DHV: 2250 (2040)
Design Speed: 50 m.p.h.	Design Speed: 70 m.p.h.
Posted Speed: 45 m.p.h.	Posted Speed: 65 m.p.h.
One-Way Traffic	One-Way Traffic

LOADING HL-93

Allow 50#/sq. ft. for future wearing surface.

DESIGN SPECIFICATIONS

2014 AASHTO LRFD Bridge Design Specifications, 7th Edition with 2015 and 2016 Interims

DESIGN STRESSES

FIELD UNITS

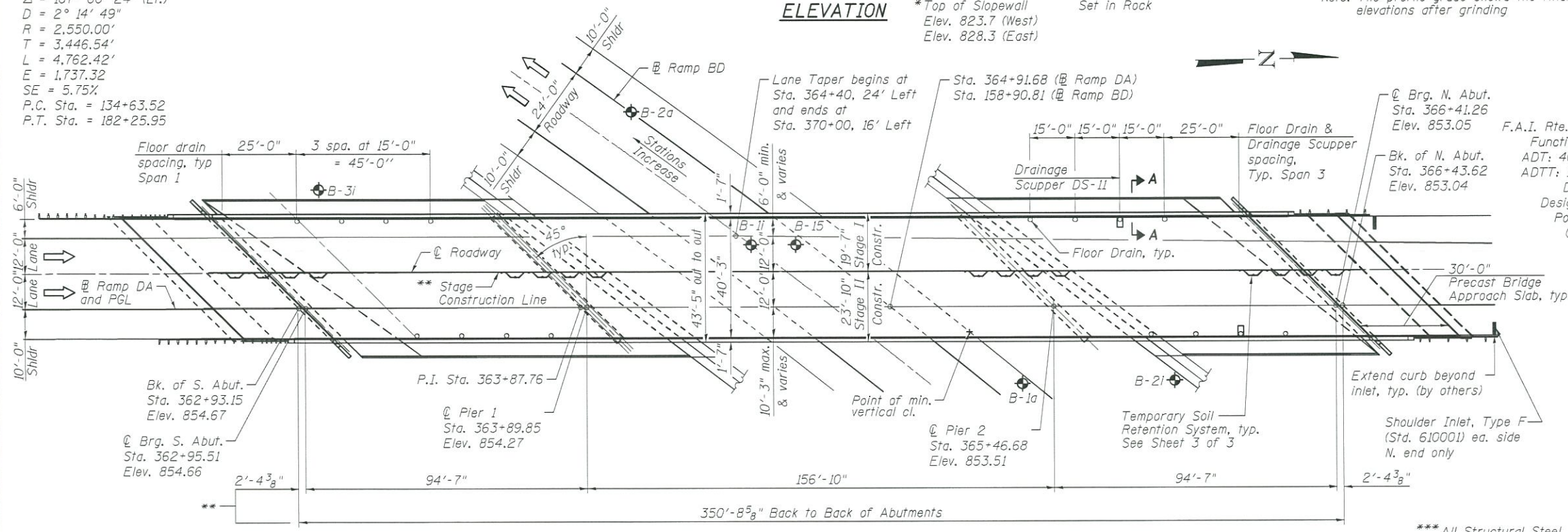
$f'_c = 3,500$ psi
 $f'_c = 4,000$ (Superstructure Concrete)
 $f_y = 60,000$ psi (Reinforcement)
 $f_y = 50,000$ psi (M270 Grade 50) ***

PRECAST UNITS

$f'_c = 6,000$ psi

SEISMIC DATA

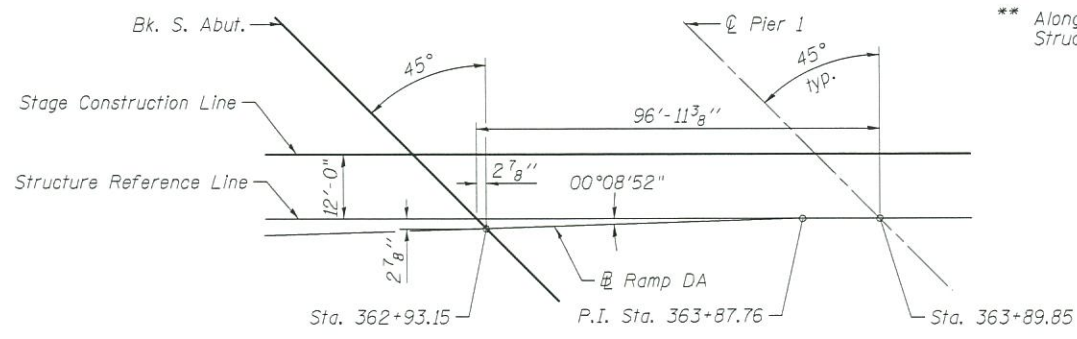
Seismic Performance Zone (SPZ) = 1
 Design Spectral Acceleration at 1.0 sec. (S_{d1}) = 0.056 g
 Design Spectral Acceleration at 0.2 sec. (S_{ps}) = 0.10 g
 Soil Site Class = C



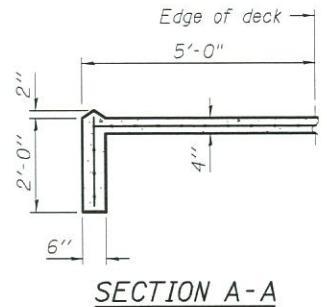
PLAN

Note: Up to 1/4 inch may be ground off the bridge deck and the bridge approach slabs.

*** All Structural Steel shall be metalized

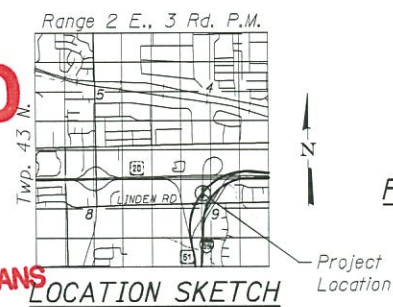


OFFSET SKETCH



SECTION A-A

APPROVED
 MAR 08 2017
 AS A BASIS FOR
 PREPARATION OF DETAILED PLANS



LOCATION SKETCH

GENERAL PLAN
 I-39 NB (RAMP DA) OVER
 I-39 SB (RAMP BD)
 F.A.I. RTE. 39 SEC. (201-3)K & (4-1.5)R
 WINNEBAGO COUNTY
 STATION 364+91.68
 STRUCTURE NO. 101-0204

FILE NAME: \\N01204-64622-001-1.dgn

FEHR GRAHAM
 ENGINEERING & ENVIRONMENTAL
 ILLINOIS DESIGN FIRM NO. 184-003525
 FEHR GRAHAM PROJECT NUMBER: 15-1002

USER NAME = myyoung
 PLOT SCALE = 42.6667' / 1" = 3/8" / 2017
 CB PROJ. NO.: 06085

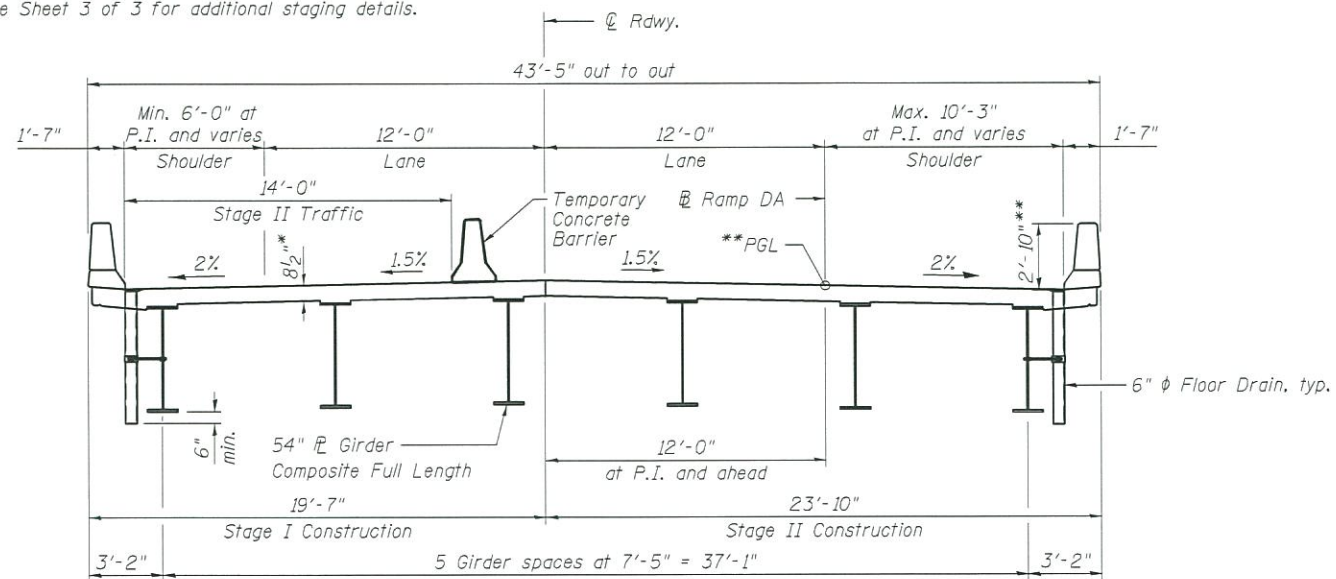
DESIGNED - GB/CME	REVISED -
CHECKED - MCB	REVISED -
DRAWN - MMY	REVISED -
CHECKED - MCB	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHEET NO. 1 OF 3 SHEETS

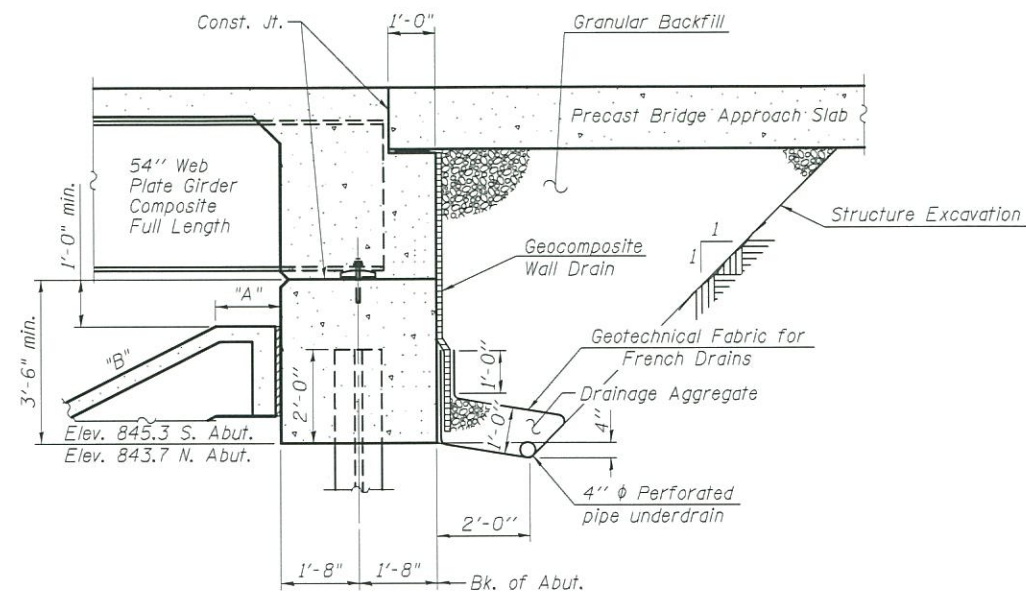
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1.5)R	WINNEBAGO		
CONTRACT NO. 64C62				
ILLINOIS FED. AID PROJECT				

Note: Stage I Traffic is located on existing roadway.
See Sheet 3 of 3 for additional staging details.



CROSS SECTION
(Looking North)
Dimensions at rt. L's to structure

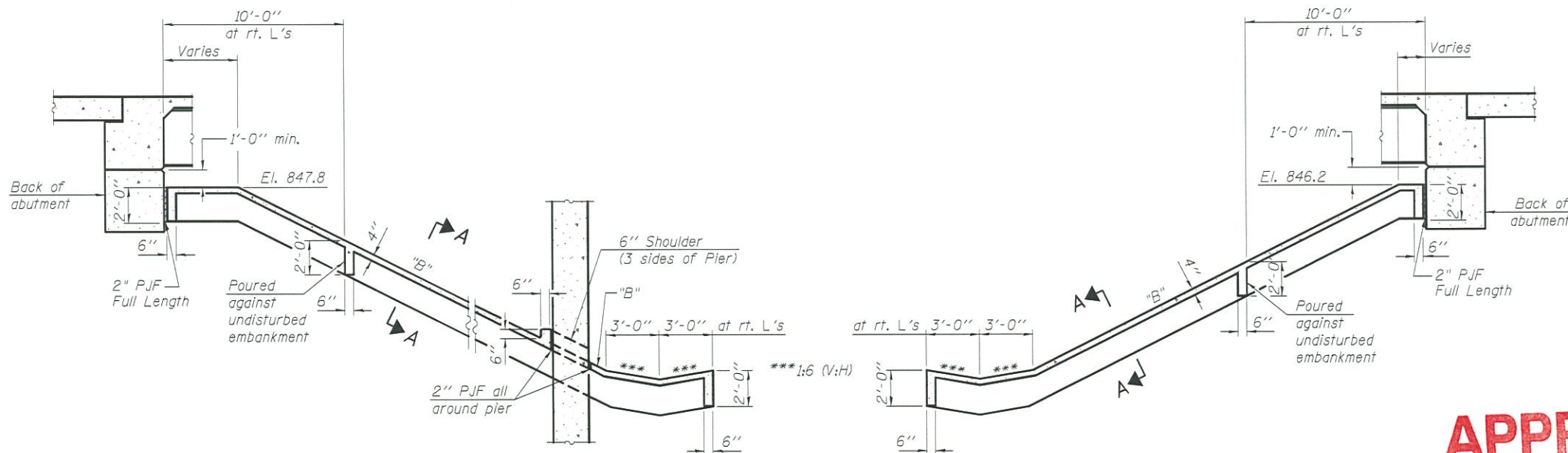
*Prior to Grinding
**After Grinding



SECTION THRU INTEGRAL ABUTMENT
(Horiz. dim. @ Rt. L's)

"A" - Berm width Varies
0" to 12'-3 1/2" S. Abut.
0" to 7'-9 3/8" N. Abut.

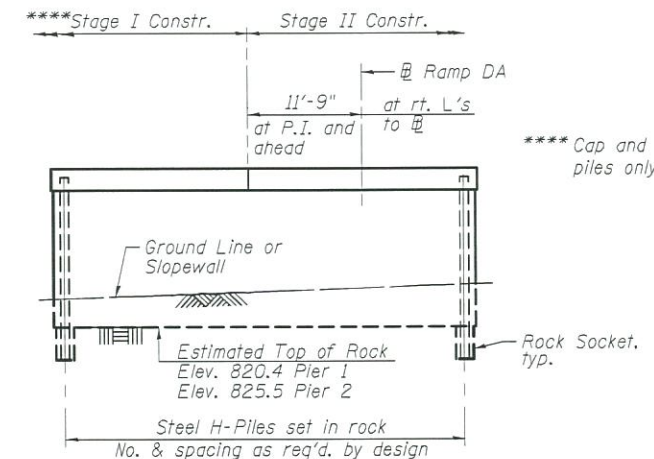
"B" - Slope at right angles
1:2.26 (V:H) S. Abut.
1:2.08 (V:H) N. Abut.



SECTION THRU CONCRETE SLOEWALL
South Abutment

Note: See Sheet 1 of 3 for Section A-A.

SECTION THRU CONCRETE SLOEWALL
North Abutment



PIER SKETCH
(Looking North)

APPROVED

MAR 08 2017

AS A BASIS FOR
PREPARATION OF DETAILED PLANS

GENERAL PLAN
I-39 NB (RAMP DA) OVER
I-39 SB (RAMP BD)
F.A.I. RTE. 39 SEC. (201-3)K & (4-1,5)R
WINNEBAGO COUNTY
STATION 364+91.68
STRUCTURE NO. 101-0204

FILE NAME: \\ND0204-GICE2-002.tbl.dgn

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525
FEHR GRAHAM PROJECT NUMBER: 15-1002

USER NAME = myoung
DESIGNED - GB/CME
CHECKED - MCB
DRAWN - MMY
CHECKED - MCB

DESIGNED - GB/CME
CHECKED - MCB
DRAWN - MMY
CHECKED - MCB

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

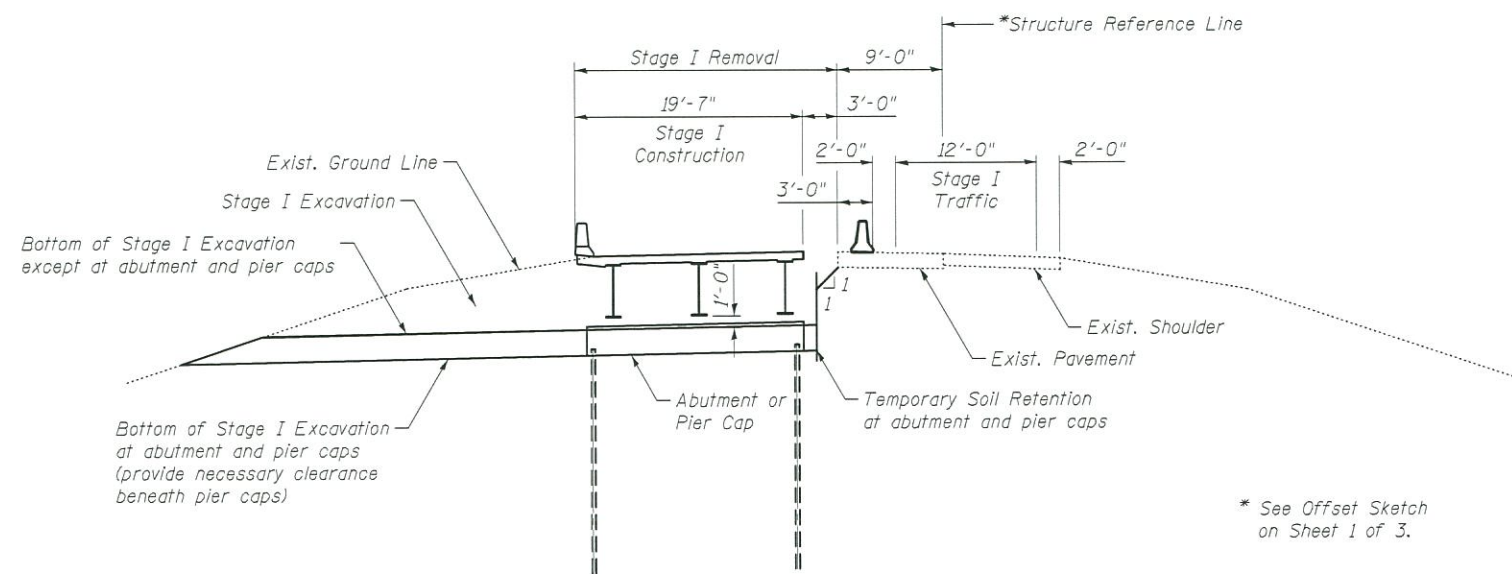
SHEET NO. 2 OF 3 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1,5)R	WINNEBAGO		
				CONTRACT NO. 64C62
ILLINOIS FED. AID PROJECT				

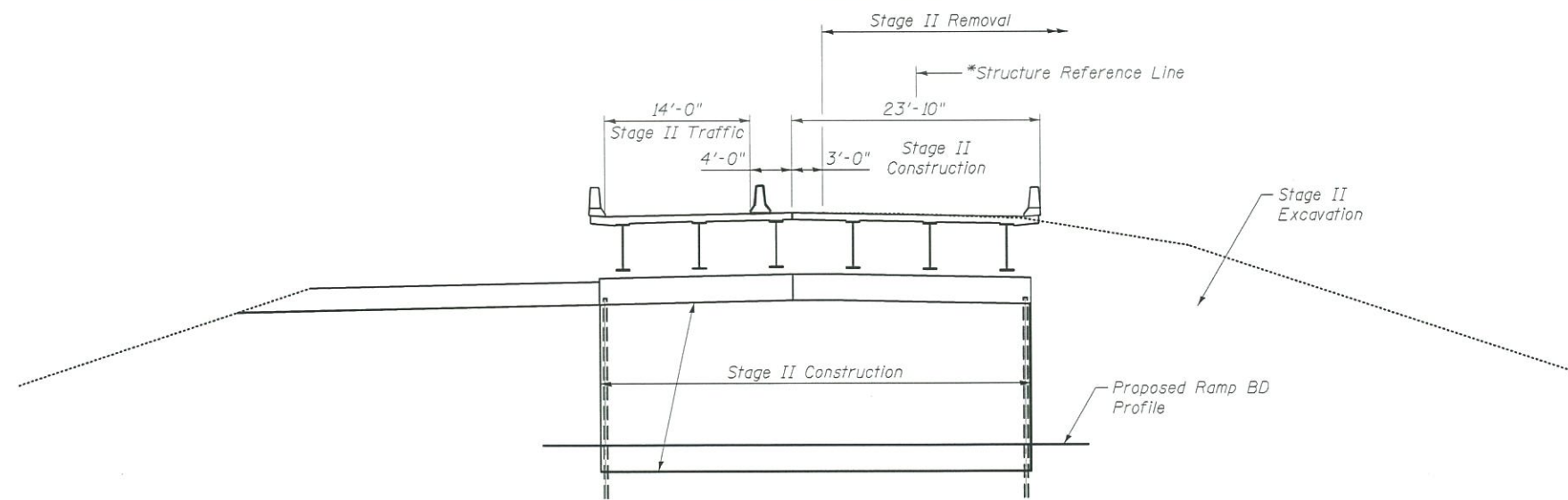
CONSTRUCTION SEQUENCE

Ramp DA over Ramp BD will be constructed using a top-down construction method to maintain one lane of traffic on existing/proposed Ramp DA at all times as indicated below.

1. Direct Ramp DA traffic to Stage I Traffic.
2. Proceed with Stage I Excavation to 1'-0" below the bottom of the proposed girders across the spans using slopes laid back at 1:1 as shown on Stage I Construction Detail.
3. Install Temporary Soil Retention System as required at the abutments and piers and excavate as required to construct the caps.
4. Install Stage I piles per SGR recommendations.
5. Proceed with Stage I Construction of abutment and pier caps, superstructure and approach slabs.
6. Direct Ramp DA traffic to Stage II Traffic.
7. Proceed with Stage II Excavation beneath Stage I Construction and as required for Stage II Construction.
8. Proceed with Stage II Construction.



STAGE I CONSTRUCTION



STAGE II CONSTRUCTION
(Showing Pier construction, abutment construction similar)

APPROVED

MAR 08 2017

**AS A BASIS FOR
PREPARATION OF DETAILED PLANS**

GENERAL PLAN
I-39 NB (RAMP DA) OVER
I-39 SB (RAMP BD)
F.A.I. RTE. 39 SEC. (201-3)K & (4-1,5)R
WINNEBAGO COUNTY
STATION 364+91.68
STRUCTURE NO. 101-0204

FILE NAME = ... \N000204-64C62-003-003.dwg

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525

USER NAME = myoung	DESIGNED - CME	REVISED -
PLLOT SCALE = 1/8" = 1'-0"	CHECKED - MCB	REVISED -
PLLOT DATE = 3/8/2017	DRAWN - CFC	REVISED -
	CHECKED - MCB	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHEET NO. 3 OF 3 SHEETS

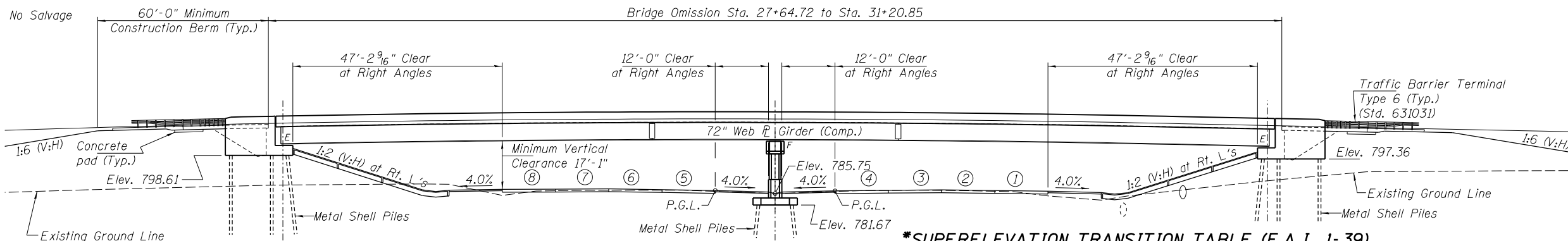
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1,5)R	WINNEBAGO		
CONTRACT NO. 64C62				
ILLINOIS FED. AID PROJECT				

BM: #401 - Cut square on Southerly of center crash wall of bridge under Perryville Rd. - Elevation = 788.53

Note: Up to 1/4 inch may be ground off the bridge deck and the approach slabs.

Existing Structure (No. 101-0098)

Originally constructed in 1963 under FAP 194, Section 4-HB-3, the structure is a 4 span, concrete deck on continuous steel beam superstructure with concrete piers and pile bent abutments. The structure was repainted in 1980. The west fascia beam between piers 2 and 3 was replaced in 1999 and again in 2007. The structure has a Bk. to Bk. = 304'-6" with an out-out of deck of 33'-8". Traffic shall be maintained on the existing structure during construction of new structure.



ELEVATION

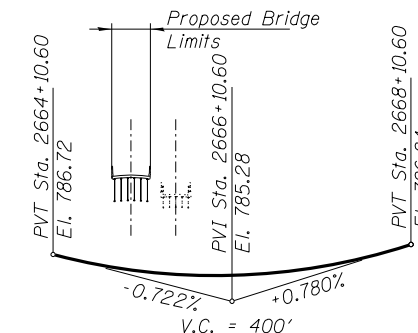
*SUPERELEVATION TRANSITION TABLE (F.A.I. 1-39)

Lane Designation	Superelevation		Re-Establish Normal Crown			
	-3.5%	3.5%	-2.0%	-1.5%	1.5%	2.0%
①	2661+19.80	-	2662+03.20	-	-	-
②	2661+19.80	-	-	2662+86.50	-	-
③	2661+19.80	-	-	-	2664+04.80	-
④	2661+19.80	-	-	-	-	2664+39.80
⑤	-	2661+19.80	-	-	-	2662+03.20
⑥	-	2661+19.80	-	-	2662+86.50	-
⑦	-	2661+19.80	-	2664+04.80	-	-
⑧	-	2661+19.80	2664+39.80	-	-	-

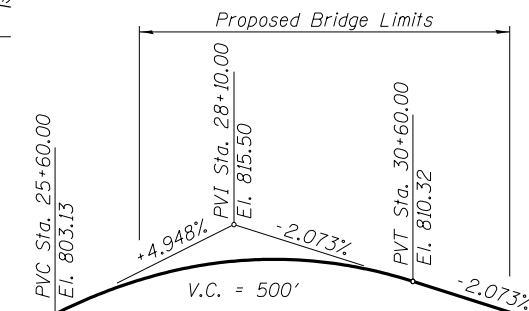
* Positive Cross Slopes Slope Upward from the P.G.L.
Negative Cross Slopes Slope Downward from the P.G.L.

CURVE DATA
F.A.I. 1-39

Δ = 50°-41'-00"
D = 0°-59'-59.92"
T = 2713.52'
L = 5068.45'
E = 610.07'
R = 5729.70'
S.E. = 3.5% (See Table)
P.C. = Sta. 2611+31.39
P.T. = Sta. 2661+99.84
P.I. = Sta. 2638+44.91



PROPOSED PROFILE
Along I-39 Inside E.O.P.

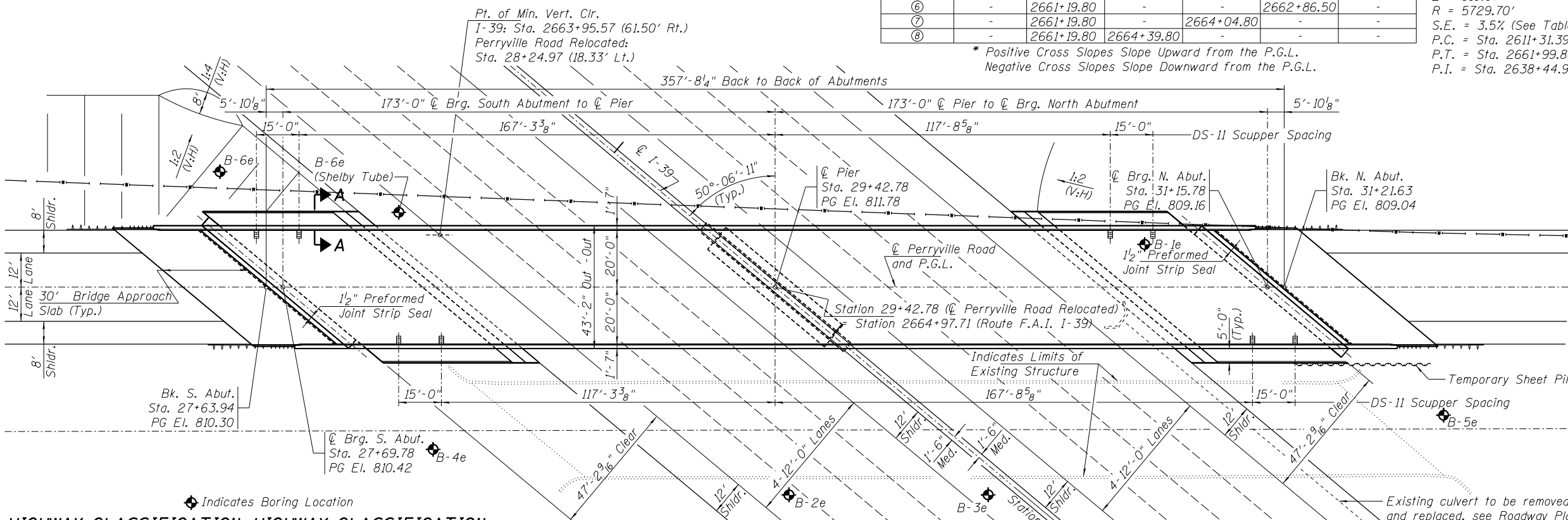


PROPOSED PROFILE

Along Perryville Road Relocated.
The profile grade shows the final elevations after grinding.

SEISMIC DATA

Seismic Performance Zone (SPZ) = 1
Design Spectral Acceleration at 1.0 sec. (S_{D1}) = 0.06
Design Spectral Acceleration at 0.2 sec. (S_{D5}) = 0.10
Soil Site Class = C



PLAN

HIGHWAY CLASSIFICATION

F.A.U. Rte. 5148 - Perryville
Functional Class: Minor Arterial
ADT: 8150 (2013); 15550 (2040)
ADTT: 489 (2013); 933 (2040)
DHV: 1555 (2040)
Design Speed: 45 m.p.h.
Posted Speed: 45 m.p.h.
2 - Way Traffic
Directional Distribution: 50:50

F.A.I. Rte. 39 - I-39
Functional Class: Interstate
ADT: 44600 (2013); 106610 (2040)
ADTT: 12950 (2013); 32000 (2040)
DHV: 10600 (2040)
Design Speed: 70 m.p.h.
Posted Speed: 65 m.p.h.
2 - Way Traffic
Directional Distribution: 50:50

DESIGN STRESSES

FIELD UNITS
f'_c = 3,500 psi
f'_c = 4,000 psi (Superstructure Concrete)
f_y = 60,000 psi (Reinforcement)
f_y = 50,000 psi (M270 Grade 50)*
* All Structural Steel shall be Metalized.

DESIGN SPECIFICATIONS

2014 AASHTO LRFD Bridge Design Specifications, 7th Edition with 2016 Interims

LOADING HL-93

Allow 50#/sq. ft. for future wearing surface.

GENERAL PLAN

PERRYVILLE ROAD over

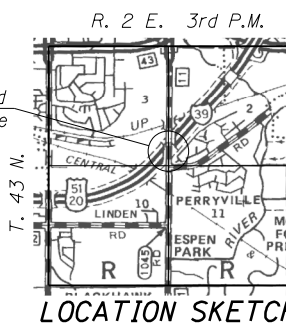
I-39 & U.S.20

F.A.U. 5148 - SECTION (201-3)K & (4-1, 5)R

WINNEBAGO COUNTY

STA. 29+42.78

STRUCTURE NUMBER -101-0206



DESIGNED	MNM	02/14/08
DRAWN	RAH	02/14/08
REVIEWED	MNM	12/05/15

I:\05 jobs\0652055\CADD\Struct\Sheet\1010206-XXXX-TSL-Perryville.dgn



USER NAME	= Johns00944
PLOT SCALE	= 0.166667' / in.
PLOT DATE	= 3/7/2017

DESIGNED	- MNM
CHECKED	- TEH
DRAWN	- RAH/RSJ
CHECKED	- MNM

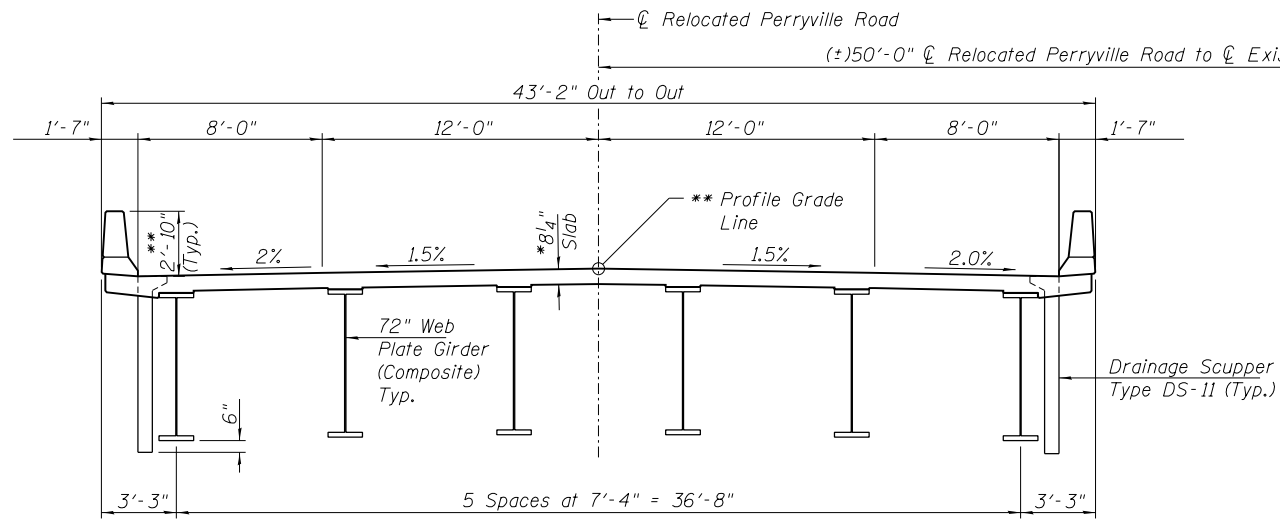
REVISED	
REVISED	
REVISED	
REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STRUCTURE NUMBER 101-0206

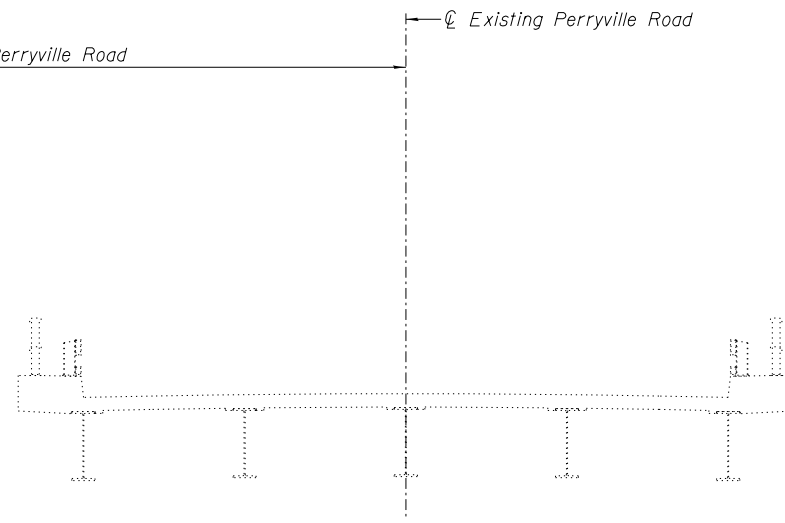
SHEET NO. 1 OF 2 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5148	(201-3)K & (4-1, 5)R	WINNEBAGO	2	1
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

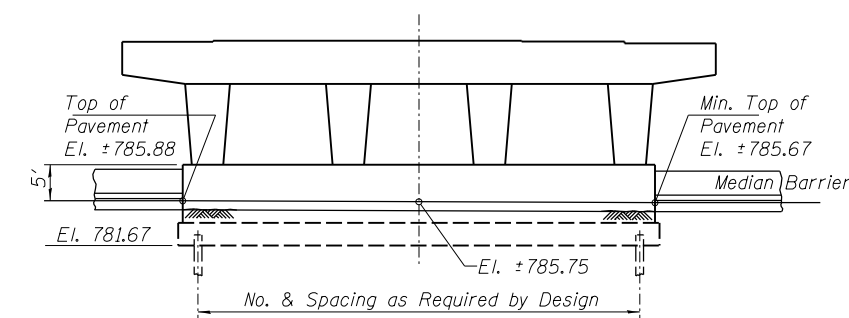


PROPOSED CROSS SECTION

* Prior to Grinding
** After Grinding

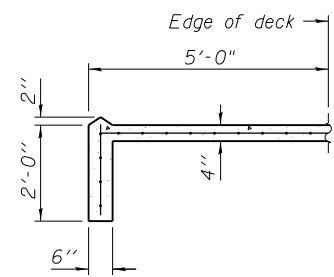


EXISTING CROSS SECTION

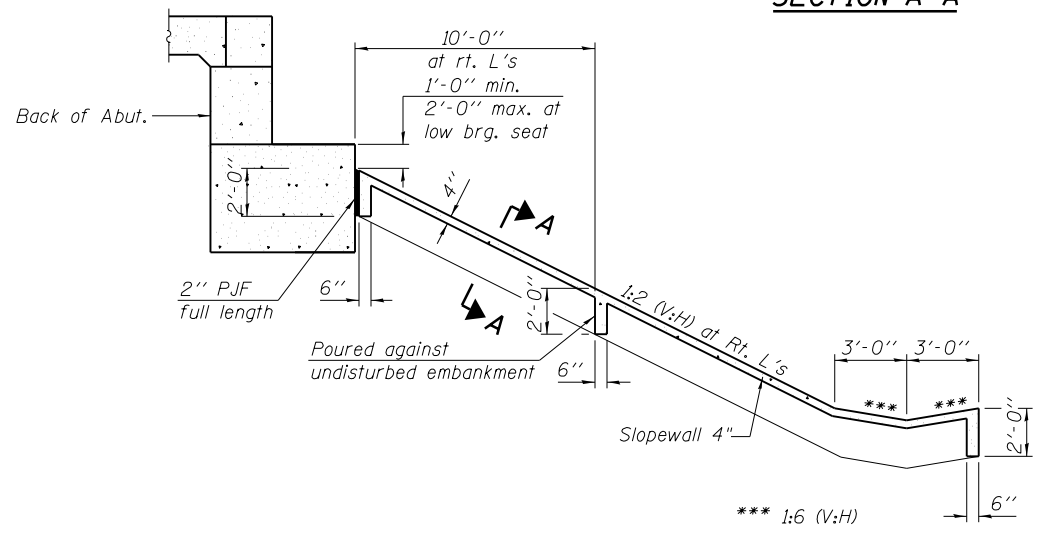


PIER SKETCH

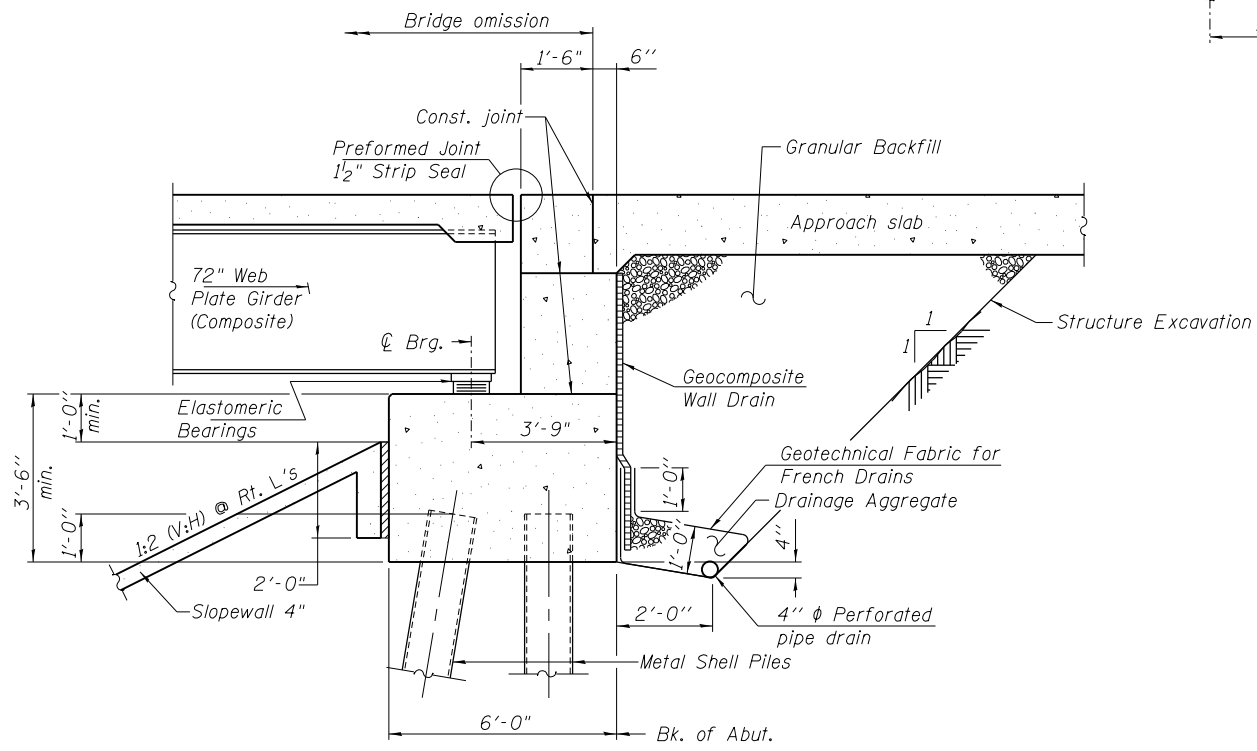
(Looking West)



SECTION A-A



SECTION THRU CONCRETE SLOPEWALL



SECTION THRU ABUTMENT

(Horiz. dim. @ Rt. L's)

DETAILS
PERRYVILLE ROAD over
I-39 & U.S.20
F.A.U. 5148 - SECTION (201-3)K & (4-1.5)R
WINNEBAGO COUNTY
STA. 29+42.78
STRUCTURE NUMBER -101-0206

DESIGNED	MNM	02/14/08
DRAWN	RAH	02/14/08
REVIEWED	MNM	12/05/15

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 © Copyright Hanson Professional Services Inc. 2017

USER NAME = Johns00944	DESIGNED - MNM	REVISIONS	REVISIONS
PLOT SCALE = 0.166667' / in.	CHECKED - TEH	REVISIONS	REVISIONS
PLOT DATE = 3/7/2017	DRAWN - RAH/RSJ	REVISIONS	REVISIONS
	CHECKED - MNM	REVISIONS	REVISIONS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STRUCTURE NUMBER 101-0206

SHEET NO. 2 OF 2 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5148	(201-3)K & (4-1.5)R	WINNEBAGO	2	2
CONTRACT NO.				

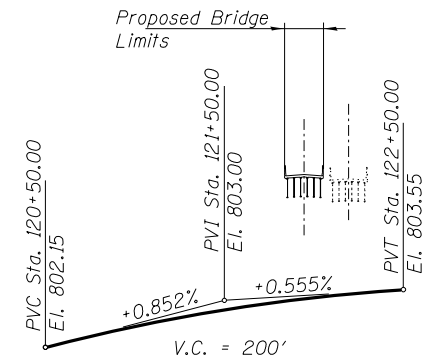
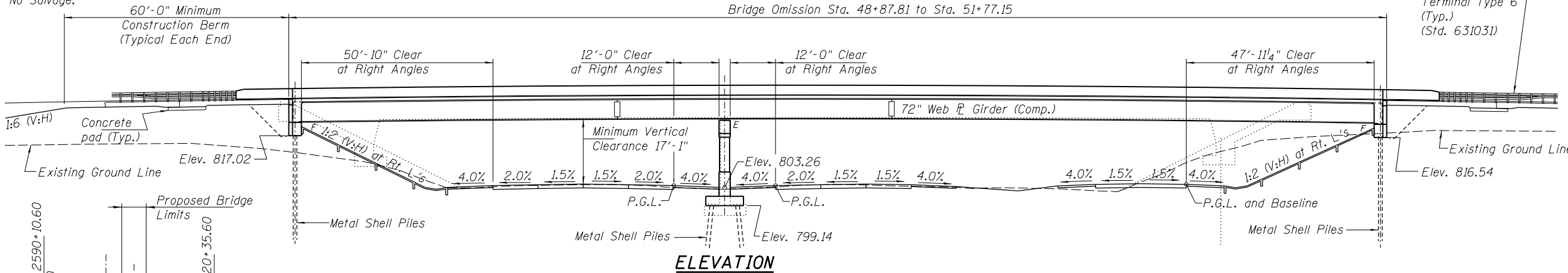
ILLINOIS FED. AID PROJECT

BM: # 316 2" Aluminum Disk on Approx. C/L of U.S. 20 - on pier base under Mulford Rd. - Elevation 805.75

Note: Up to 1/4 inch may be ground off the bridge deck and the approach slabs.

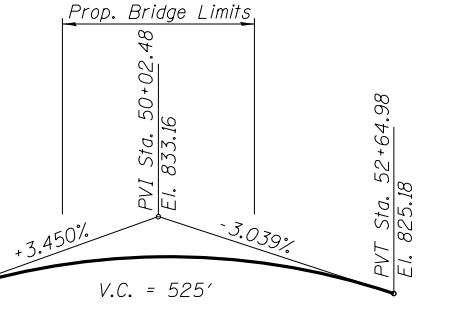
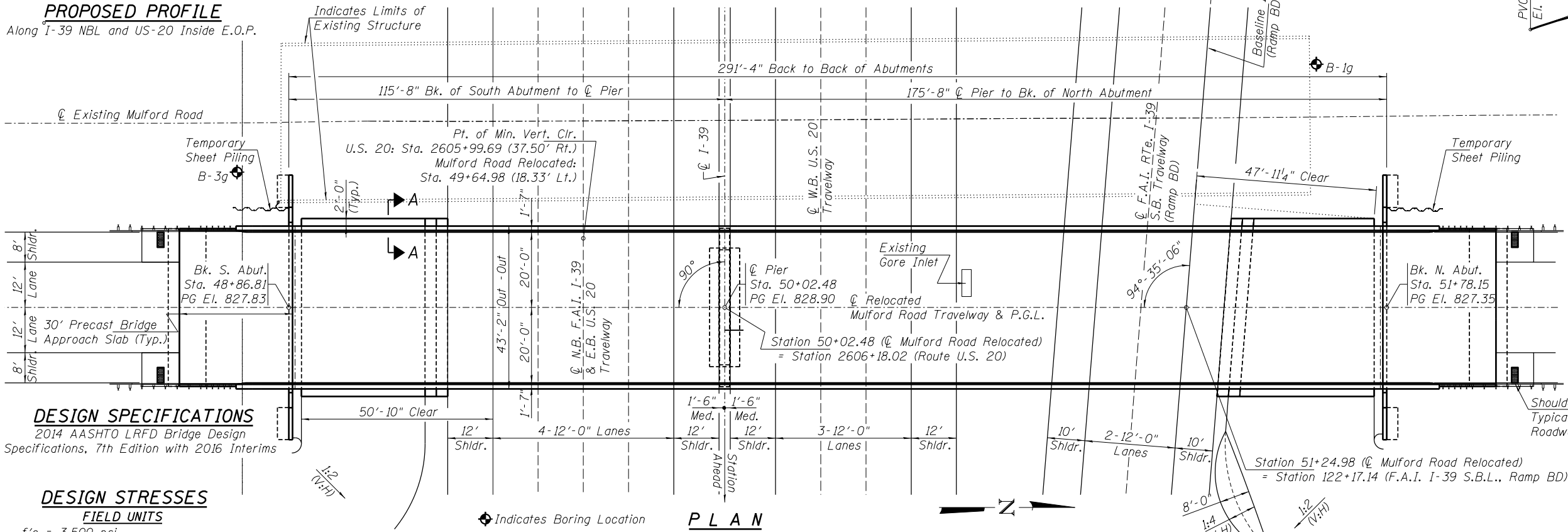
Existing Structure (No. 101-0131)

Originally constructed in 1963 under FA 194, Section 4-HB-2, the structure was a 4 span, concrete deck on steel beam superstructure with concrete piers and pile bent abutments. The structure was reconstructed in 1976 under FA 194, Section 201-3HB-3 as a 2 span concrete deck on continuous steel girder superstructure with concrete piers and closed sand filled pile bent abutments. The structure has a Bk. to Bk. = 274'-0" with an out-out of deck of 42'-0". The structure was repainted in 1999. Traffic shall be maintained on the existing structure during construction of the new structure. No Salvage.



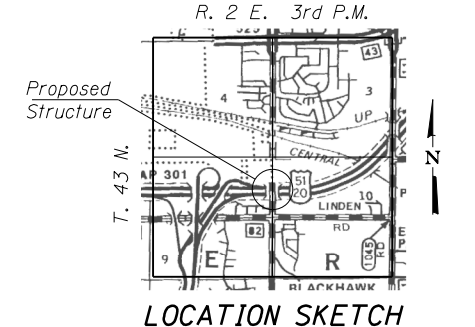
PROPOSED PROFILE

Along I-39 NBL and US-20 Inside E.O.P.



PROPOSED PROFILE

Along C Mulford Road Relocated. The profile shows the final elevations after grinding.



DESIGNED	MNM	02/14/08
DRAWN	RAH	02/14/08
REVIEWED	MNM	12/05/15

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HANSON

PROFESSIONAL DESIGN FIRM LICENSE #184-001084

DESIGNED	-	MNM	REVISED	
CHECKED	-	TEH	REVISED	
DRAWN	-	RAH/RSJ	REVISED	
CHECKED	-	MNM	REVISED	

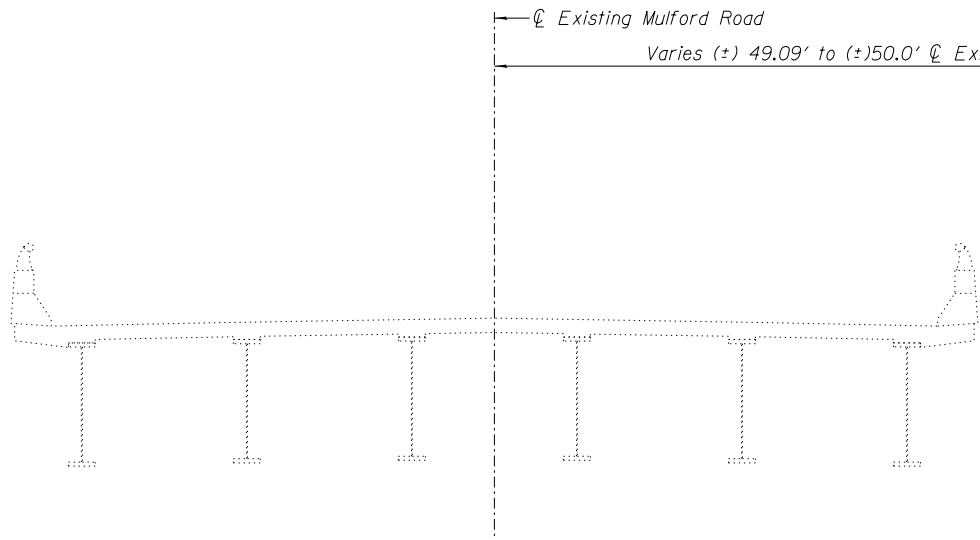
STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

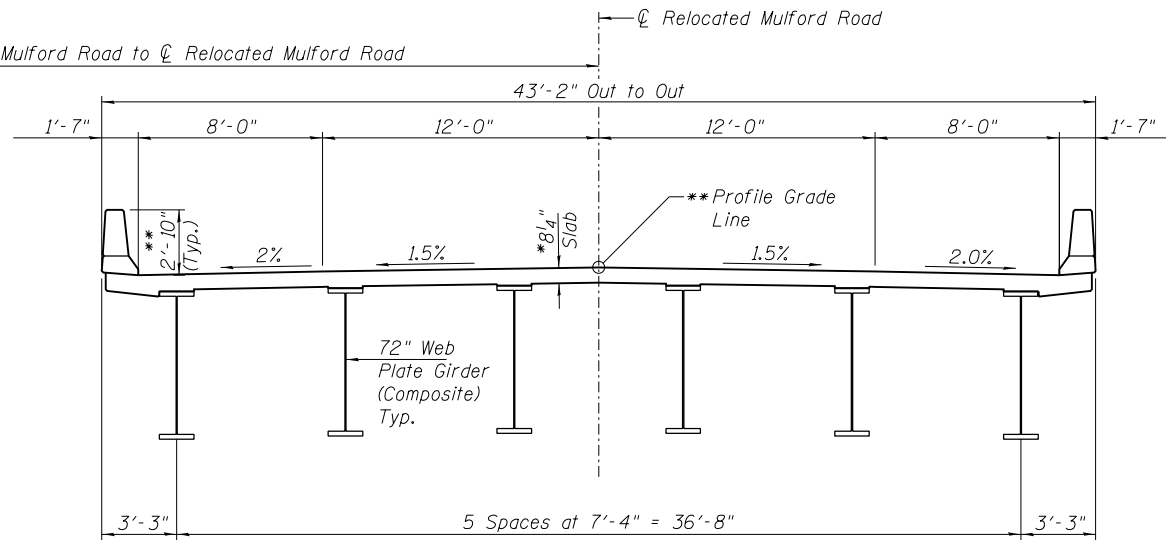
STRUCTURE NUMBER 101-0207

SHEET NO. 1 OF 2 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5147	(201-3)K & (4-1.5)R	WINNEBAGO	2	1
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

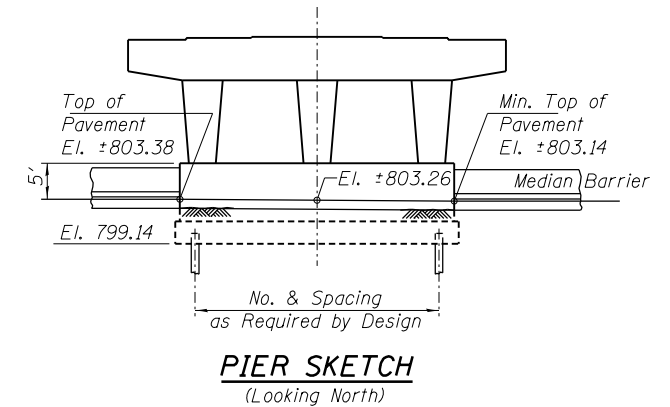


EXISTING CROSS SECTION

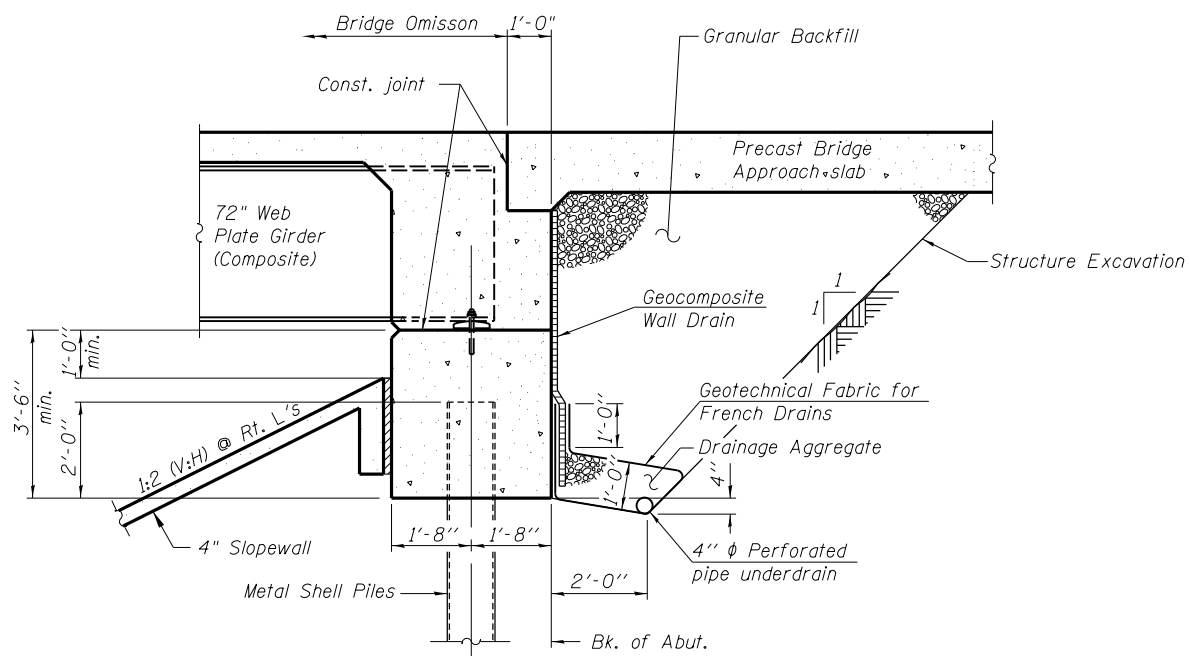


PROPOSED CROSS SECTION

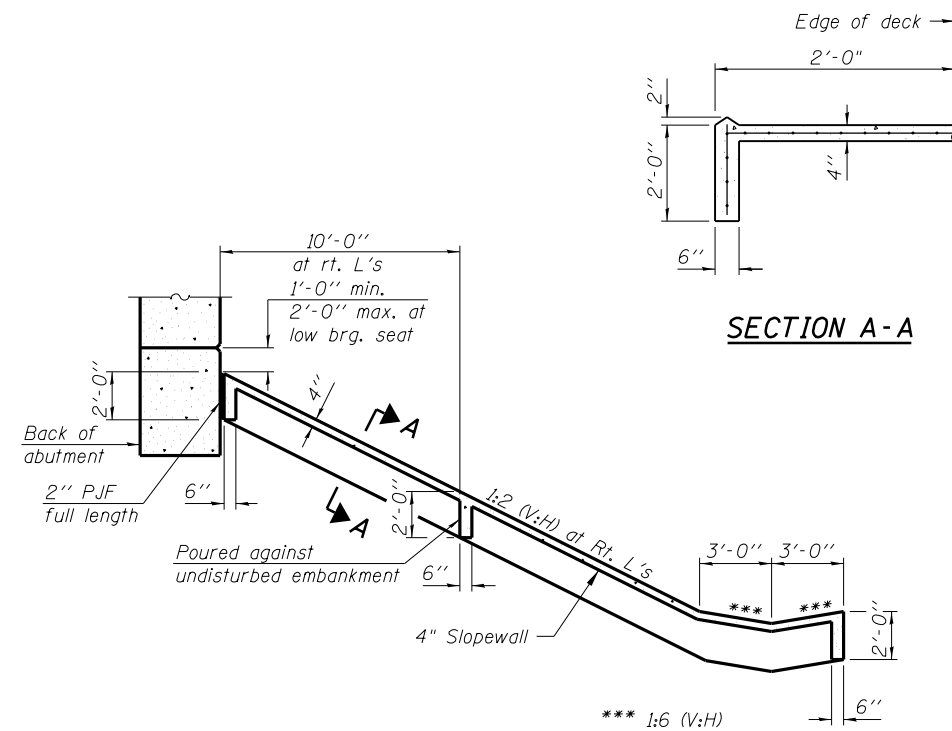
* Prior to Grinding
** After Grinding



PIER SKETCH
(Looking North)



SECTION THRU INTEGRAL ABUTMENT
(Horiz. dim. @ Rt. L's)



SECTION A-A

SECTION THRU CONCRETE SLOPEWALL

DETAILS
MULFORD ROAD over
I-39 & U.S.20
F.A.U. 5147 SECTION (201-3)K & (4-1.5)R
WINNEBAGO COUNTY
STA. 50+02.48
STRUCTURE NUMBER -101-0207

DESIGNED	MNM	02/14/08
DRAWN	RAH	02/14/08
REVIEWED	MNM	12/05/15

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USER NAME =	Johns0944
PLOT SCALE =	0.166667' / in.
PLOT DATE =	3/7/2017

DESIGNED -	MNM	REVISED	
CHECKED -	TEH	REVISED	
DRAWN -	RAH/RSJ	REVISED	
CHECKED -	MNM	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STRUCTURE NUMBER 101-0207

SHEET NO. 2 OF 2 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
5147	(201-3)K & (4-1, 5)R	WINNEBAGO	2	2
CONTRACT NO.				

ILLINOIS FED. AID PROJECT

Bench Mark: B.M. 400-Cut "□" on southeast corner of S.N. 101-0067. Elevation = 800.52

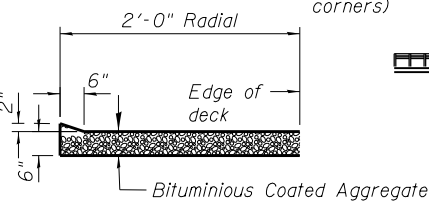
Existing Structure: S.N. 101-0067 (E.B.) and S.N. 101-0068 (W.B.) built as F.A. Route 194, Section 4 VB at Station 796 +28.52 in 1963. Bridges widened and deck replaced in 1991 as F.A.I. Route 39, Section 4 VBY. Each superstructure consists of 3 spans of continuous composite rolled steel beams. Each substructure consists of concrete stub abutments and concrete hammerhead piers supported on piles. Length = 169'-8" (back-to-back of abutments). Width of Eastbound Bridge = 42'-3" (out-to-out deck). Width of Westbound Bridge = 43'-2" (out to out deck). Traffic is to be maintained utilizing stage construction.

No Salvage

Traffic Barrier Terminal
Type 6, Std. 631031
(S.E. & N.W. bridge corners)

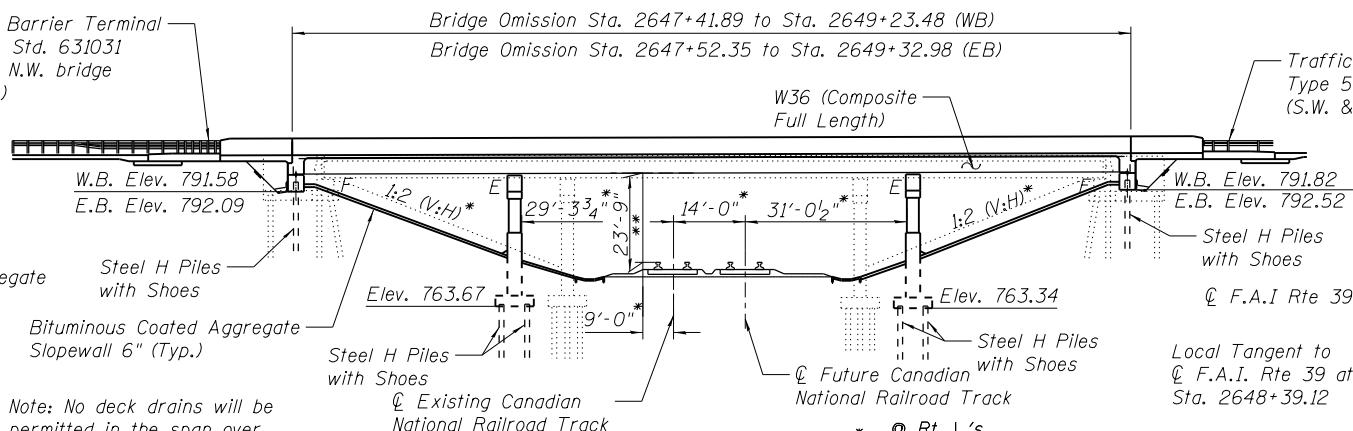
Bridge Omission Sta. 2647+41.89 to Sta. 2649+23.48 (WB)
Bridge Omission Sta. 2647+52.35 to Sta. 2649+32.98 (EB)

Traffic Barrier Terminal
Type 5, Std. 631026
(S.W. & N.E. bridge corners)

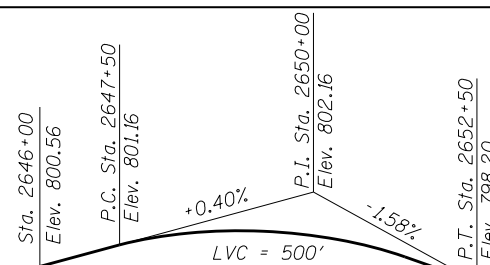


SECTION B-B

Note: No deck drains will be permitted in the span over track or within 10' of crossarms of a railroad pole line.

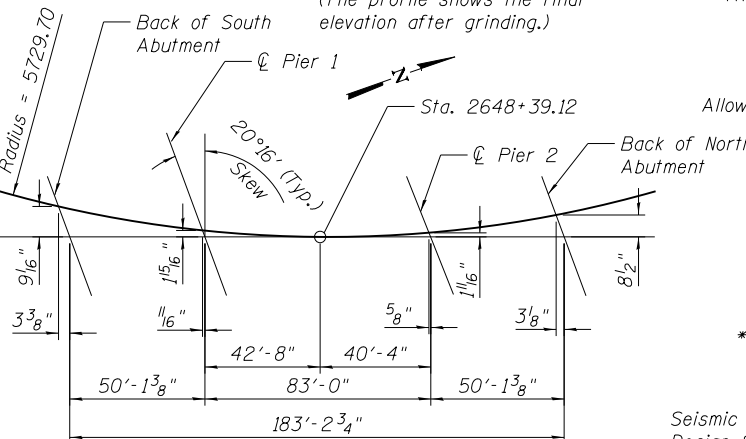


ELEVATION



PROFILE GRADE

(Along Inside Edge of Roadways)
(The profile shows the final elevation after grinding.)



OFFSET SKETCH

HIGHWAY CLASSIFICATION

F.A.I. Rte. 39 - I-39/U.S. Rte. 20
Functional Class: Interstate
ADT: 44,600 (2013); 106,610 (2040)
ADTT: 12,950 (2013); 32,000 (2040)
DHW: 10,600 (2040)
Design Speed: 70 m.p.h.
Posted Speed: 65 m.p.h

DESIGN SPECIFICATIONS

2014 AASHTO LRFD Bridge Design Specifications, 7th Edition with 2015 and 2016 Interims

LOADING HL-93

Allow 50#/sq. ft. for future wearing surface.

DESIGN STRESSES

FIELD UNITS

f'c = 3,500 psi
f'c = 4,000 psi (Superstructure Concrete)
fy = 60,000 psi (Reinforcement)
fy = 50,000 psi (M270 Grade 50)***

*** Structural steel to be metalized.

SEISMIC DATA

Seismic Performance Zone (SPZ) = 1
Design Spectral Acceleration at 1.0 sec. (SD1) = 0.056g
Design Spectral Acceleration at 0.2 sec. (SDS) = 0.101g
Soil Site Class = C

Sta. 7+37.75	Elev. 772.45	Sta. 9+37.75	Elev. 771.49	Sta. 11+57.35	Elev. 770.49
Sta. 8+37.75	Elev. 771.95	Sta. 10+57.35	Elev. 770.93	Sta. 12+57.35	Elev. 770.12
Sta. 9+97.63	Elev. 771.14				

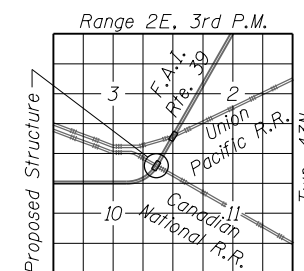
TOP OF RAIL ELEVATIONS

(Canadian National Railroad)

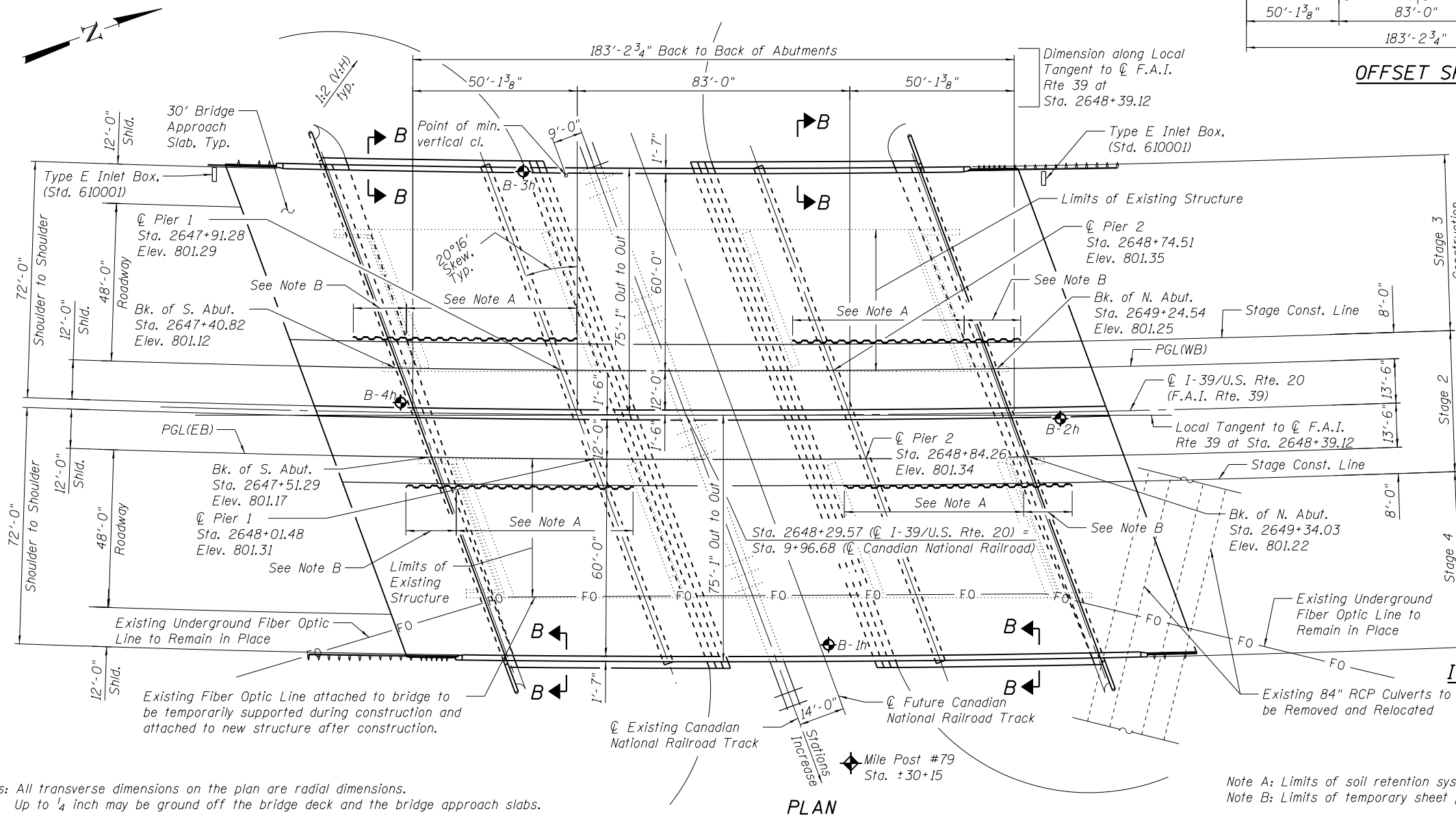
◆ Indicates Soil Boring

HORIZ. CURVE DATA

∠ I-39/US Rte 20 (Exist.)
Δ = 50° 41' 00" (Lt)
D = 1° 00' 00"
R = 5,729.70'
T = 2713.52'
L = 5,068.45'
E = 610.07'
S.E. = 3.5%
P.C. Sta. = 2611+31.39
P.I. Sta. = 2638+44.91
P.T. Sta. = 2661+99.84



LOCATION SKETCH



GENERAL PLAN & ELEVATION

I-39/U.S. ROUTE 20 OVER CANADIAN NATIONAL RAILROAD

F.A.I. RTE. 39 - SECTION (4-1.5)R

WINNEBAGO COUNTY

STA. 2648+29.57

STRUCTURE NUMBER 101-0208 (EB)

STRUCTURE NUMBER 101-0209 (WB)

Notes: All transverse dimensions on the plan are radial dimensions. Up to 1/4 inch may be ground off the bridge deck and the bridge approach slabs.

Note A: Limits of soil retention system
Note B: Limits of temporary sheet piling

PLAN

3/24/2017 11:49:33 AM P:\P-06\1878\00\N\CADD Sheets\1010208-64C24-TSL-001.dgn

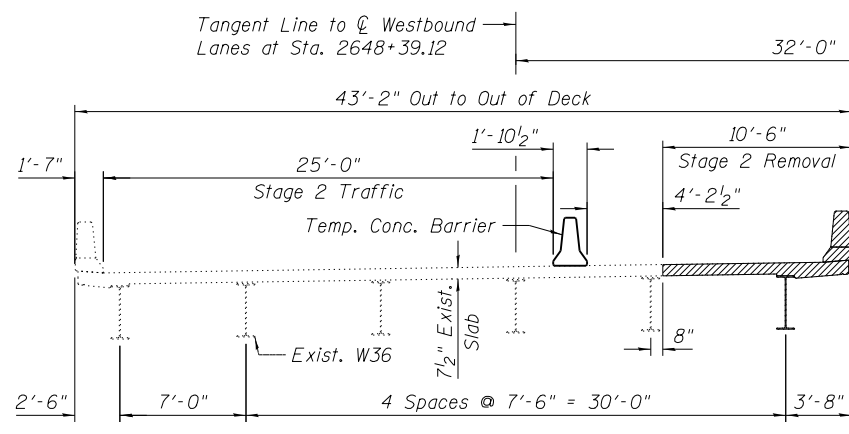


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CHECKED - PK	CHECKED - SPK	REVISED -
PLOT SCALE =	DRAWN - RD/PK	REVISED -
PLOT DATE =	CHECKED - SPK	REVISED -

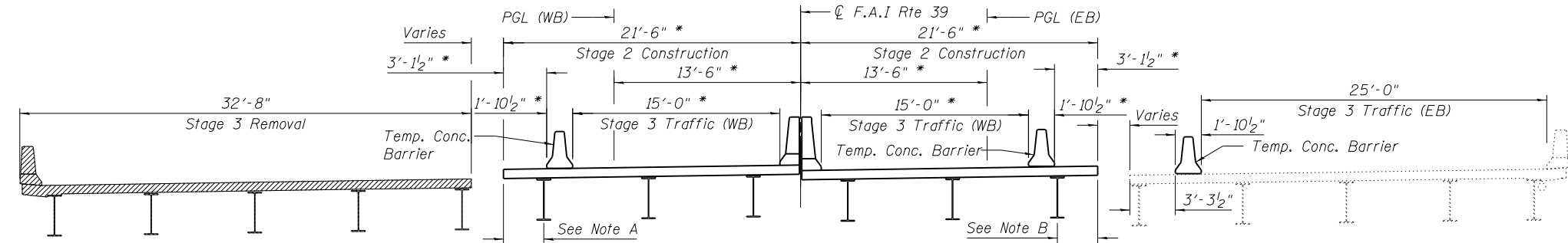
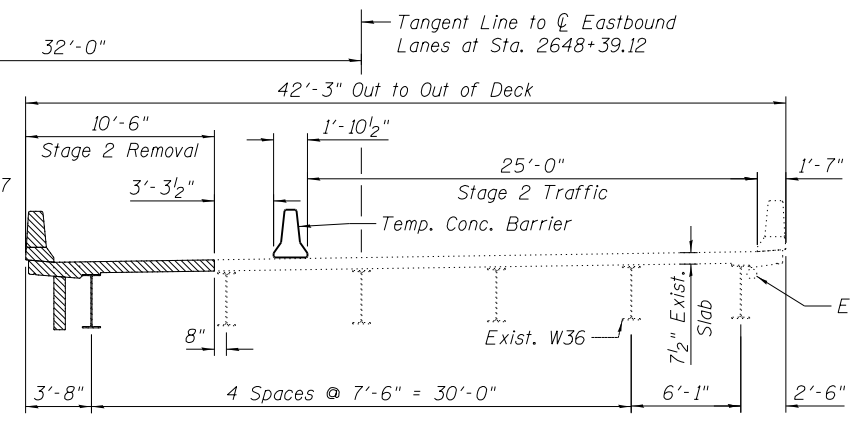
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHEET NO. 1 OF 3 SHEETS

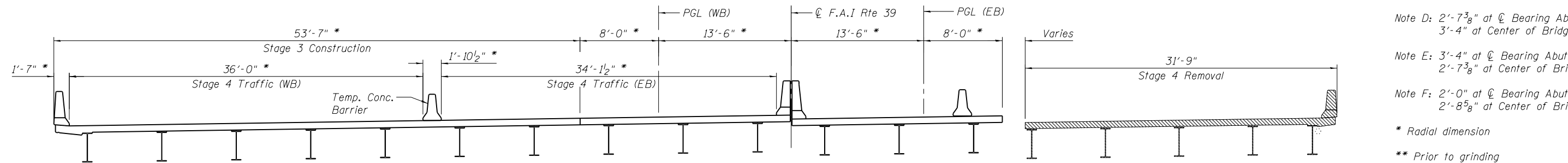
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(4-1.5)R	WINNEBAGO		
CONTRACT NO. 64C24			ILLINOIS FED. AID PROJECT	



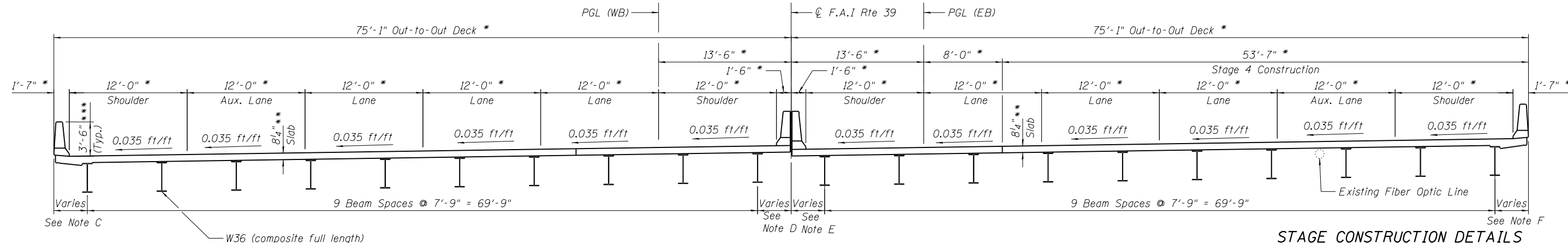
STAGE 2 REMOVAL



STAGE 3 REMOVAL & STAGE 2 CONSTRUCTION



STAGE 4 REMOVAL & STAGE 3 CONSTRUCTION



STAGE 4 CONSTRUCTION

- Notes:**
 All staging cross sections are looking North. Hatched area indicates Removal of Existing Structure. Stage I construction consists of strengthening the outside shoulders adjacent to the existing roadway pavement.
- Note A: 3'-4⁵/₈" at \bar{C} Bearing Abutments
2'-8" at Center of Bridge
 - Note B: 2'-8" at \bar{C} Bearing Abutments
3'-4⁵/₈" at Center of Bridge
 - Note C: 2'-8⁵/₈" at \bar{C} Bearing Abutments
2'-0" at Center of Bridge
 - Note D: 2'-7³/₈" at \bar{C} Bearing Abutments
3'-4" at Center of Bridge
 - Note E: 3'-4" at \bar{C} Bearing Abutments
2'-7³/₈" at Center of Bridge
 - Note F: 2'-0" at \bar{C} Bearing Abutments
2'-8⁵/₈" at Center of Bridge

- * Radial dimension
- ** Prior to grinding
- *** After grinding

STAGE CONSTRUCTION DETAILS
I-39/U.S. ROUTE 20 OVER CANADIAN NATIONAL RAILROAD
F.A.I. RTE. 39 - SECTION (4-1.5)R
WINNEBAGO COUNTY
STA. 2648+29.57
STRUCTURE NUMBER 101-0208 (EB)
STRUCTURE NUMBER 101-0209 (WB)

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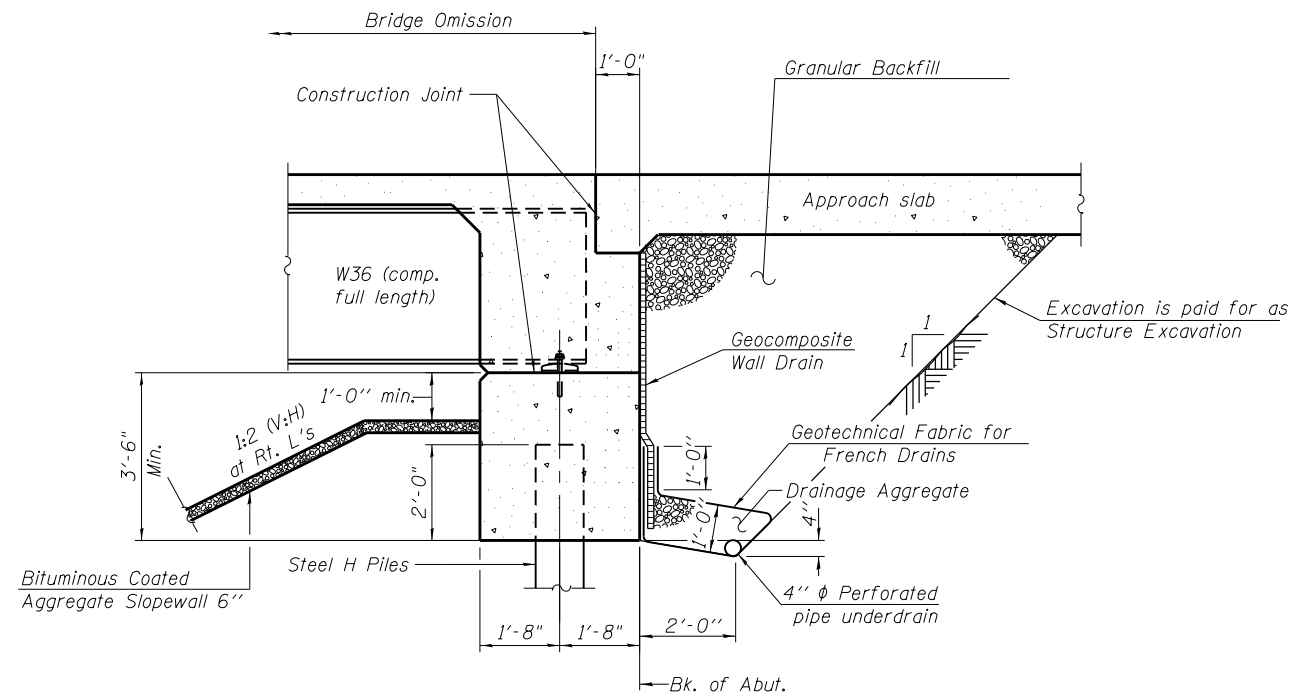
INFRASTRUCTURE ENGINEERING INCORPORATED
 33 West Monroe | Suite 1540 | Chicago, IL 60603
 P 312.425.9998 | F 312.425.9994 | www.infrastructure-eng.com

USER NAME =	DESIGNED - SPK	REVISED -
CHECKED - PK	CHECKED - PK	REVISED -
PLOT SCALE =	DRAWN - RD/PK	REVISED -
PLOT DATE =	CHECKED - SPK	REVISED -

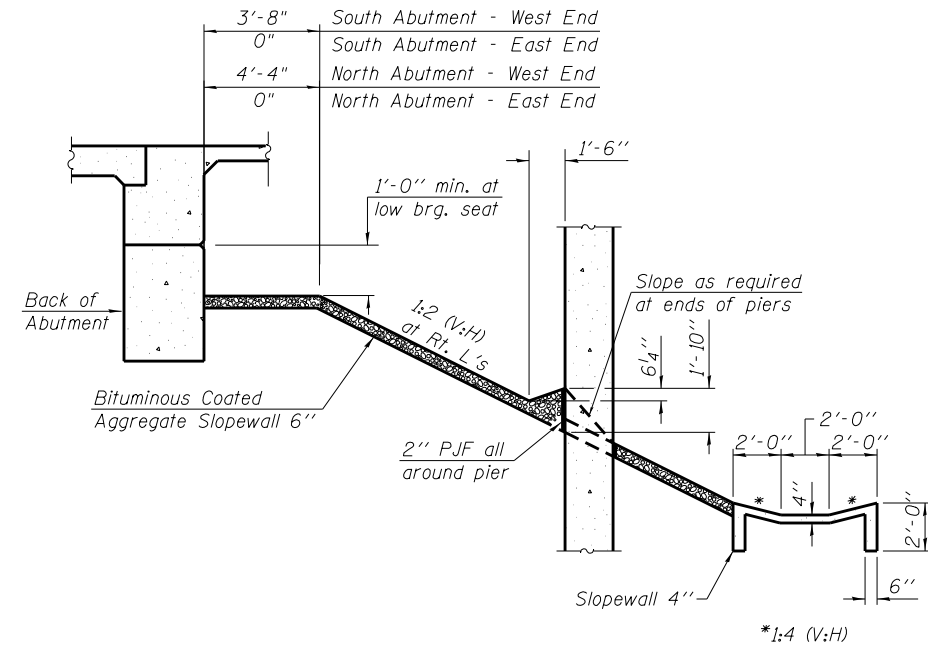
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHEET NO. 2 OF 3 SHEETS

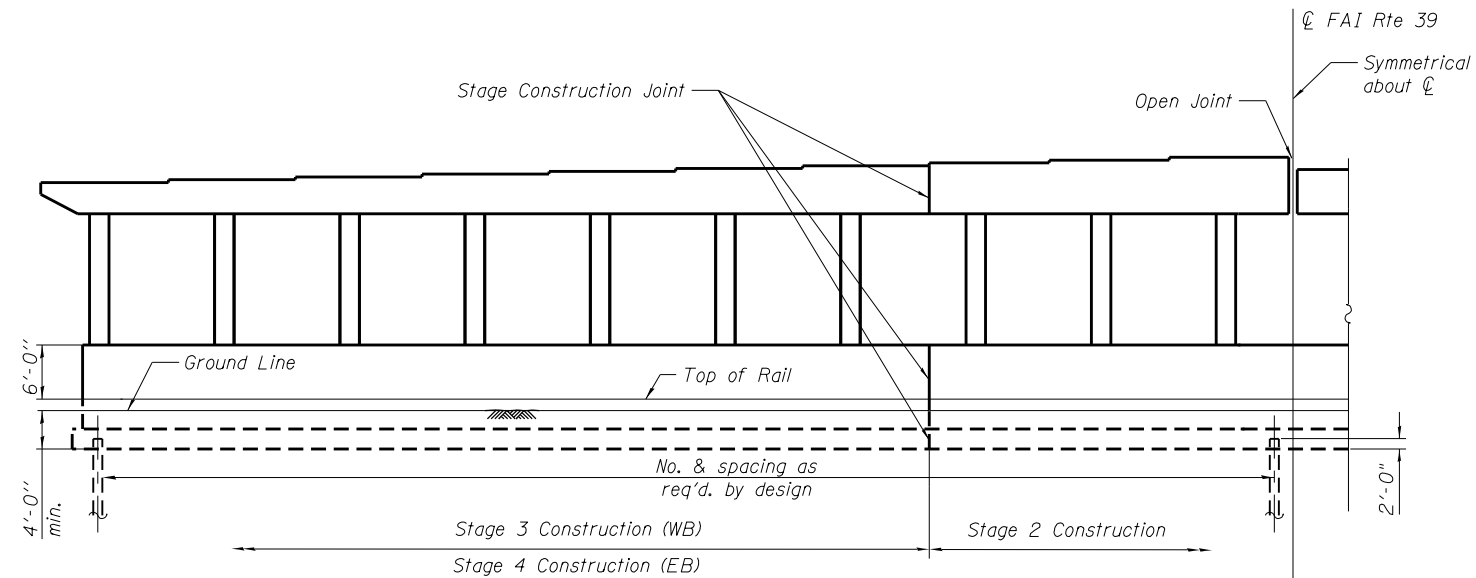
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(4-1.5)R	WINNEBAGO		
CONTRACT NO. 64C24				
ILLINOIS FED. AID PROJECT				



SECTION THRU ABUTMENT
(Horiz. dim. @ Rt. L's)



SECTION THRU BITUMINOUS COATED AGGREGATE SLOPEWALL
(Horiz. dim. @ Rt. L's)



PIER SKETCH

Note: Number of columns to be determined in design phase.

MISCELLANEOUS DETAILS
I-39/U.S. ROUTE 20 OVER CANADIAN NATIONAL RAILROAD
F.A.I. RTE. 39 - SECTION (4-1.5)R
WINNEBAGO COUNTY
STA. 2648+29.57
STRUCTURE NUMBER 101-0208 (EB)
STRUCTURE NUMBER 101-0209 (WB)

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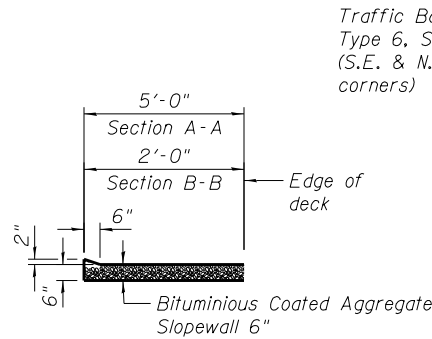
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PLOT DATE =	CHECKED - SPK	REVISED -

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(4-1.5)R	WINNEBAGO		
CONTRACT NO. 64C24				

Bench Mark: BM#402 - Cut square in southeast wingwall of S.N. 101-0070. Elevation 796.69

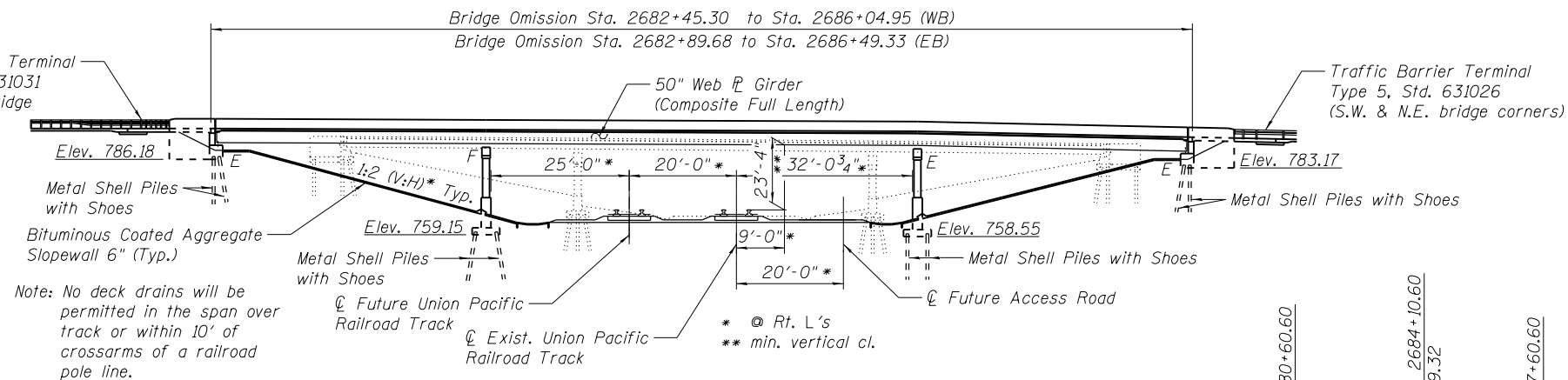
Existing Structure: S.N. 101-0069 (W.B.) and S.N. 101-0070 (E.B.) built as F.A.I. Route 194, Section 4 VB-1 at Station 832 +49.16 in 1963. Bridges widened and deck replaced in 1991 as F.A.I. Route 39, Section 4 VBY-1. Each superstructure consists of 3 spans of continuous composite rolled steel beams with partial length flange cover plates. Each substructure consists of concrete stub abutments and concrete 5 column piers supported on piles. Length = 279'-8 1/2" (back-to-back of abutments). Width = 42'-2" (out-to-out deck). Traffic is to be maintained utilizing stage construction.

No Salvage



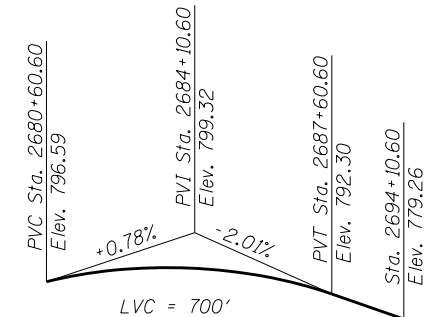
SECTION A-A

SECTION B-B



ELEVATION

Note A: Limits of soil retention system
Note B: Limits of temporary sheet piling



PROFILE GRADE F.A.I. RTE 39

(Along Inside Edge of Roadways)
(The profile grade shows the final elevation after grinding.)

HIGHWAY CLASSIFICATION

F.A.I. Rte. 39 - I-39/U.S. Rte. 20
Functional Class: Interstate
ADT: 44,600 (2013); 106,610 (2040)
ADTT: 12,950 (2013); 32,000 (2040)
DHW: 10,600 (2040)
Design Speed: 70 m.p.h.
Posted Speed: 65 m.p.h.

DESIGN SPECIFICATIONS

2014 AASHTO LRFD Bridge Design Specifications, 7th Edition with 2015 and 2016 Interims

LOADING HL-93

Allow 50#/sq. ft. for future wearing surface.

DESIGN STRESSES

FIELD UNITS

$f'_c = 3,500$ psi
 $f'_c = 4,000$ psi (Superstructure Concrete)
 $f_y = 60,000$ psi (Reinforcement)
 $f_y = 50,000$ psi (M270 Grade 50) ***

*** Structural steel to be metalized.

SEISMIC DATA

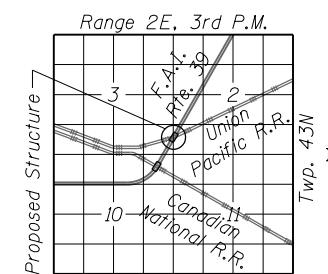
Seismic Performance Zone (SPZ) = 1
Design Spectral Acceleration at 1.0 sec. (S_{D1}) = 0.079g
Design Spectral Acceleration at 0.2 sec. (S_{D5}) = 0.135g
Soil Site Class = D

Sta.	Elev.	Sta.	Elev.	Sta.	Elev.	Sta.	Elev.	Sta.	Elev.
6+96.99	768.46	7+96.99	767.91	8+96.99	767.47	10+04.09	766.91	11+11.79	766.23
						12+11.79	765.88	13+11.79	765.47

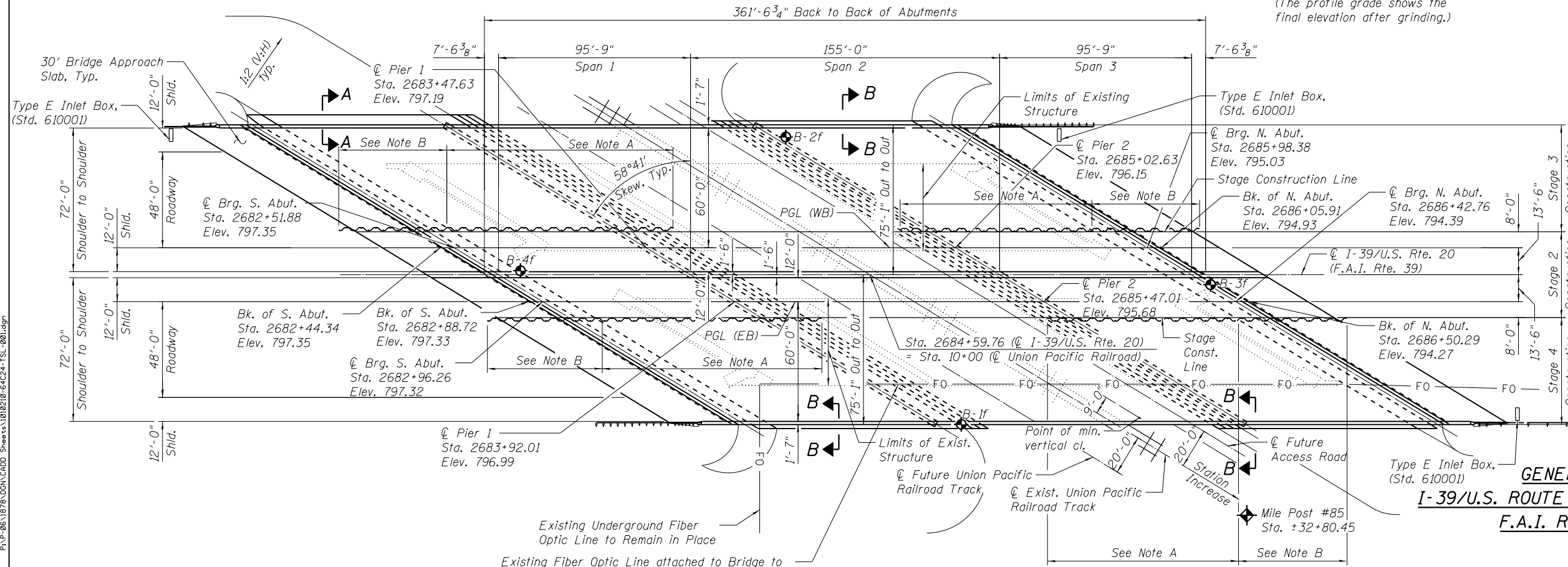
TOP OF RAIL ELEVATIONS

(Union Pacific Railroad)

◆ Indicates Soil Boring Location



LOCATION SKETCH



PLAN

Note: Up to 1/4 inch may be ground off the bridge deck and the bridge approach slabs.

GENERAL PLAN & ELEVATION

I-39/U.S. ROUTE 20 OVER UNION PACIFIC RAILROAD

F.A.I. RTE. 39 - SECTION (4-1,5)R

WINNEBAGO COUNTY

STA. 2684+59.76

STRUCTURE NUMBER 101-0210 (WB)

STRUCTURE NUMBER 101-0211 (EB)

3/7/2017 2:18:51 PM P:\P-06\1878\00\N\CADD_Sheets\1010210-64C24-TS1-001.dgn

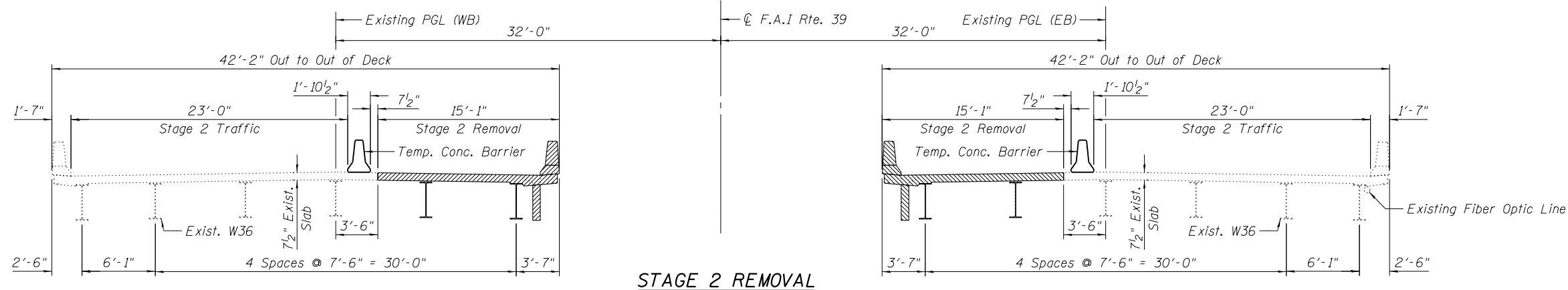
INFRASTRUCTURE ENGINEERING INCORPORATED
33 West Monroe | Suite 1540 | Chicago, IL 60603
P 312.425.9960 | F 312.425.9964 | www.infrastructure-eng.com

USER NAME =	DESIGNED - SPK	REVISD -
CHECKED - PK	REVISD -	
PLOT SCALE =	DRAWN - PK	REVISD -
PLOT DATE =	CHECKED - SPK	REVISD -

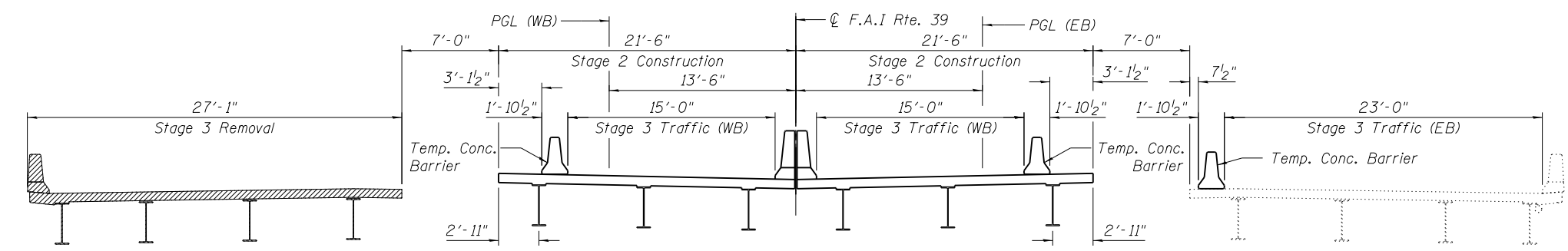
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHEET NO. 1 OF 3 SHEETS

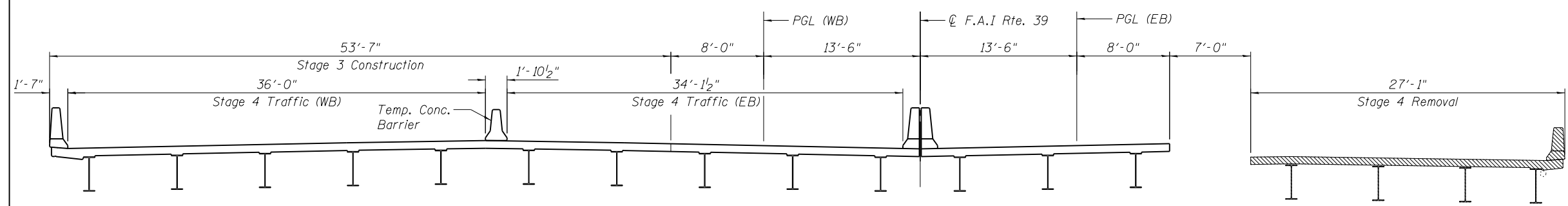
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(4-1,5)R	WINNEBAGO		
ILLINOIS FED. AID PROJECT			CONTRACT NO. 64C24	



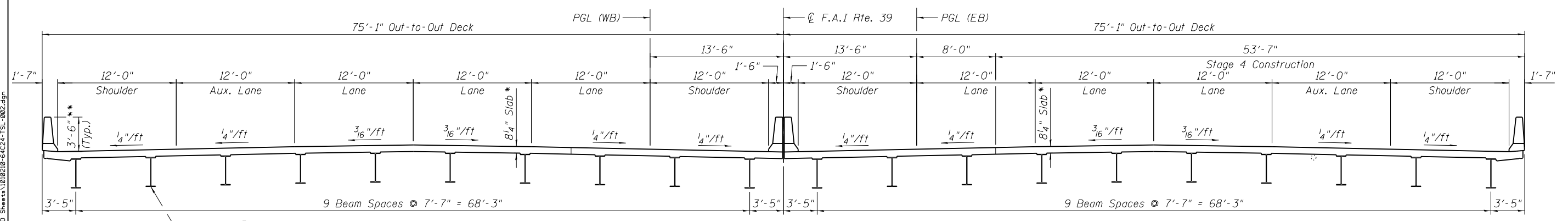
STAGE 2 REMOVAL



STAGE 3 REMOVAL & STAGE 2 CONSTRUCTION



STAGE 4 REMOVAL & STAGE 3 CONSTRUCTION



STAGE 4 CONSTRUCTION

Notes:
 All staging cross sections are looking North.
 Hatched area indicates Removal of Existing Structure.
 Stage I consists of strengthening the outside shoulders adjacent to the existing roadway pavement.

Existing piers supporting the westbound lanes are to remain in place until Stage 3 superstructure removal has been completed.

Existing piers supporting the eastbound lanes are to remain in place until Stage 4 superstructure removal has been completed.

* Prior to grinding
 ** After grinding

STAGE CONSTRUCTION DETAILS
I-39/U.S. ROUTE 20 OVER UNION PACIFIC RAILROAD
F.A.I. RTE. 39 - SECTION (4-1.5)R
WINNEBAGO COUNTY
STA. 2684+59.76
STRUCTURE NUMBER 101-0210 (WB)
STRUCTURE NUMBER 101-0211 (EB)

3/7/2017 2:19:55 PM P:\P-06\1878\VDGN\CADD_Sheets\1010210-64C24-TSL-002.dgn

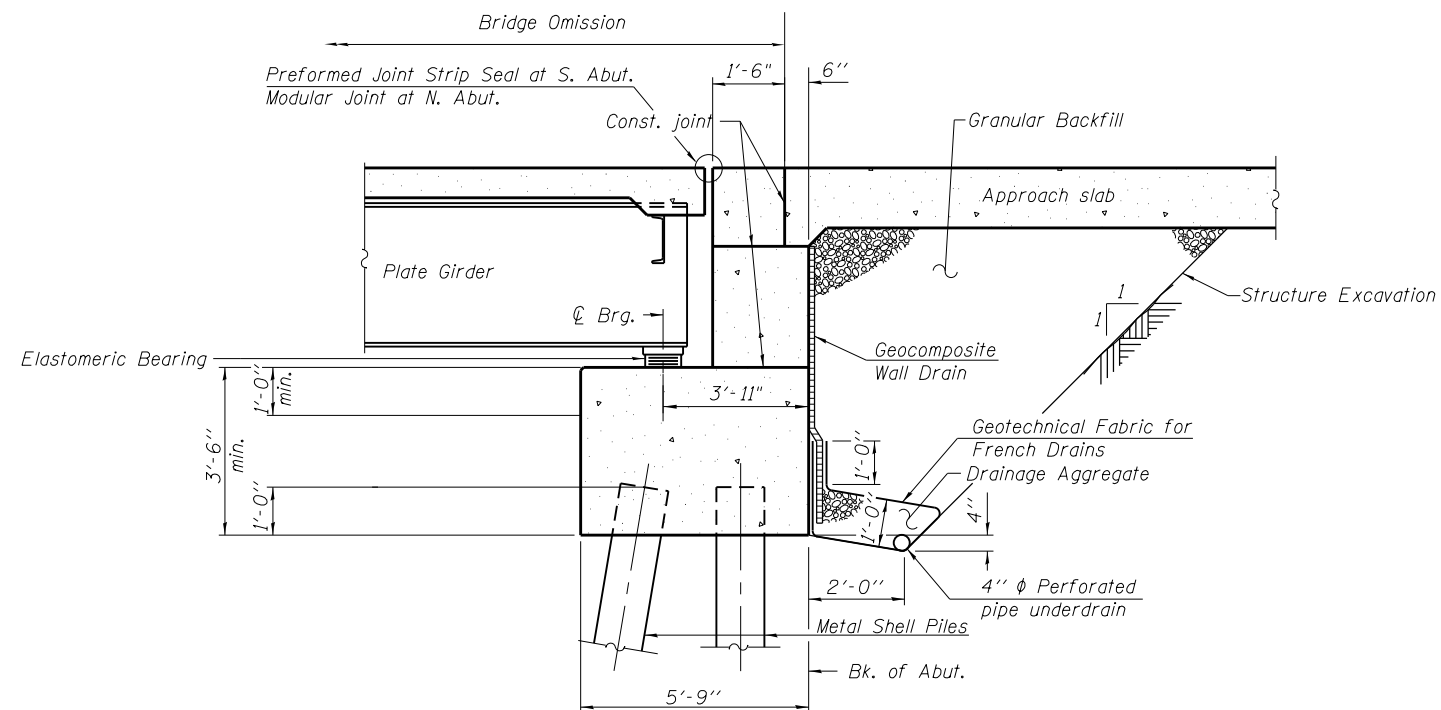


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PLOT DATE =	DRAWN - PK	REVISED -
	CHECKED - SPK	REVISED -

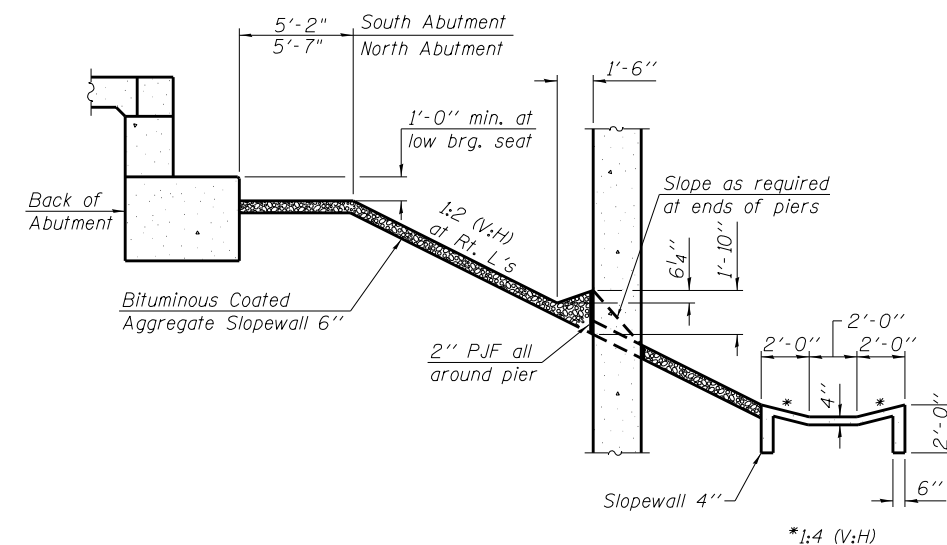
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHEET NO. 2 OF 3 SHEETS

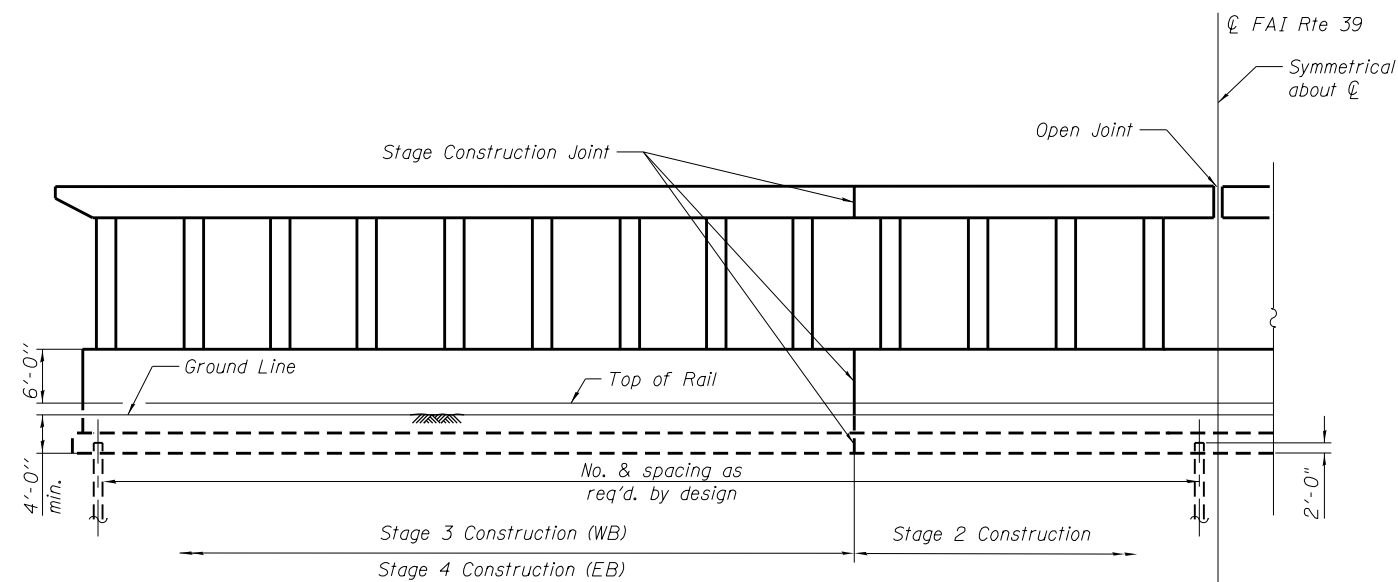
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(4-1.5)R	WINNEBAGO		
CONTRACT NO. 64C24			ILLINOIS FED. AID PROJECT	



SECTION THRU ABUTMENT
(Horiz. dim. @ Rt. L's)



**SECTION THRU BITUMINOUS
COATED AGGREGATE SLOPEWALL**
(Horiz. dim. @ Rt. L's)



PIER SKETCH

MISCELLANEOUS DETAILS
I-39/U.S. ROUTE 20 OVER UNION PACIFIC RAILROAD
F.A.I. RTE. 39 - SECTION (4-1.5)R
WINNEBAGO COUNTY
STA. 2684+59.76
STRUCTURE NUMBER 101-0210 (WB)
STRUCTURE NUMBER 101-0211 (EB)

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CHECKED - PK	REVISOR -	REVISED -
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PLOT DATE =	CHECKED - SPK	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

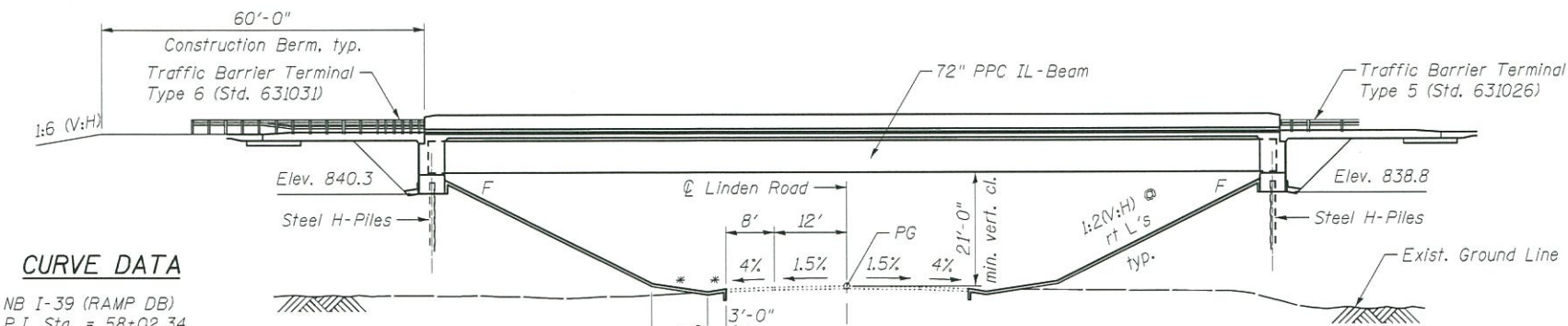
SHEET NO. 3 OF 3 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(4-1.5)R	WINNEBAGO		
CONTRACT NO. 64C24				

ILLINOIS FED. AID PROJECT

Benchmark: Cut "□" south side of eastern base of 30 mph ramp sign located east of the ramp connecting I-39 NB to US 20 West (Ramp DA) 0.1 mile north of the centerline of Linden Road. Elev. 851.37, 42°-13'-06.37" N, 89°-00'-39.64" W.

Existing Structure: None

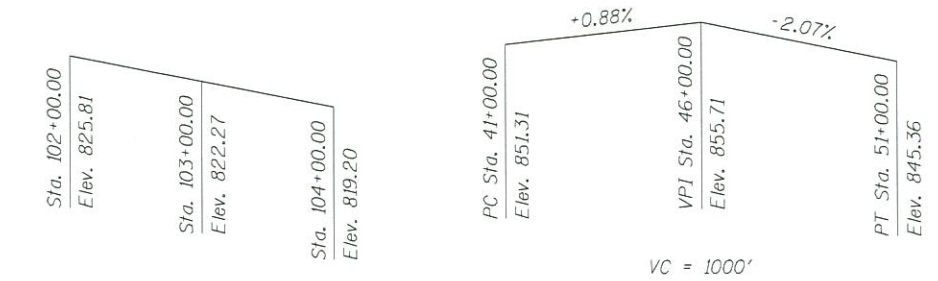


CURVE DATA

NB I-39 (RAMP DB)
 P.I. Sta. = 58+02.34
 $\Delta = 87^\circ 28' 22''$ (Rt.)
 $D = 2^\circ 47' 42''$
 $R = 2,050.00'$
 $T = 1,961.51'$
 $L = 3,129.71'$
 $E = 787.26$
 $SE = 6\%$
 P.C. Sta. = 38+40.82
 P.T. Sta. = 69+70.53

* 1:6 (V:H) at rt L's, typ.

ELEVATION



EXISTING PROFILE GRADE

Along Linden Road

PROFILE GRADE

Along BL NB I-39 (Ramp DB)

Note: The profile grade shows the final elevations after grinding

HIGHWAY CLASSIFICATION

F.A.U. Rte. 5118 - Linden Road	F.A.I. Rte. 39 - NB I-39 Ramp DB
Functional Class: Minor Arterial	Functional Class: Interstate
ADT: 6650 (2013); 18,000 (2040)	ADT: 10,500 (2013); 30,200 (2040)
ADTT: 330 (2013); 900 (2040)	ADTT: 4400 (2013); 12,700 (2040)
DHV: 1805 (2040)	DHV: 3000 (2040)
Design Speed: 45 m.p.h.	Design Speed: 70 m.p.h.
Posted Speed: 45 m.p.h.	Posted Speed: 65 m.p.h.
Two-Way Traffic	One-Way Traffic
Directional Distribution: 50/50	

LOADING HL-93

Allow 50#/sq. ft. for future wearing surface.

DESIGN SPECIFICATIONS

2014 AASHTO LRFD Bridge Design Specifications, 7th Edition with 2015 and 2016 Interims

DESIGN STRESSES

FIELD UNITS

$f'_c = 3,500$ psi
 $f'_c = 4,000$ psi (Superstructure Concrete)
 $f_y = 60,000$ psi (Reinforcement)

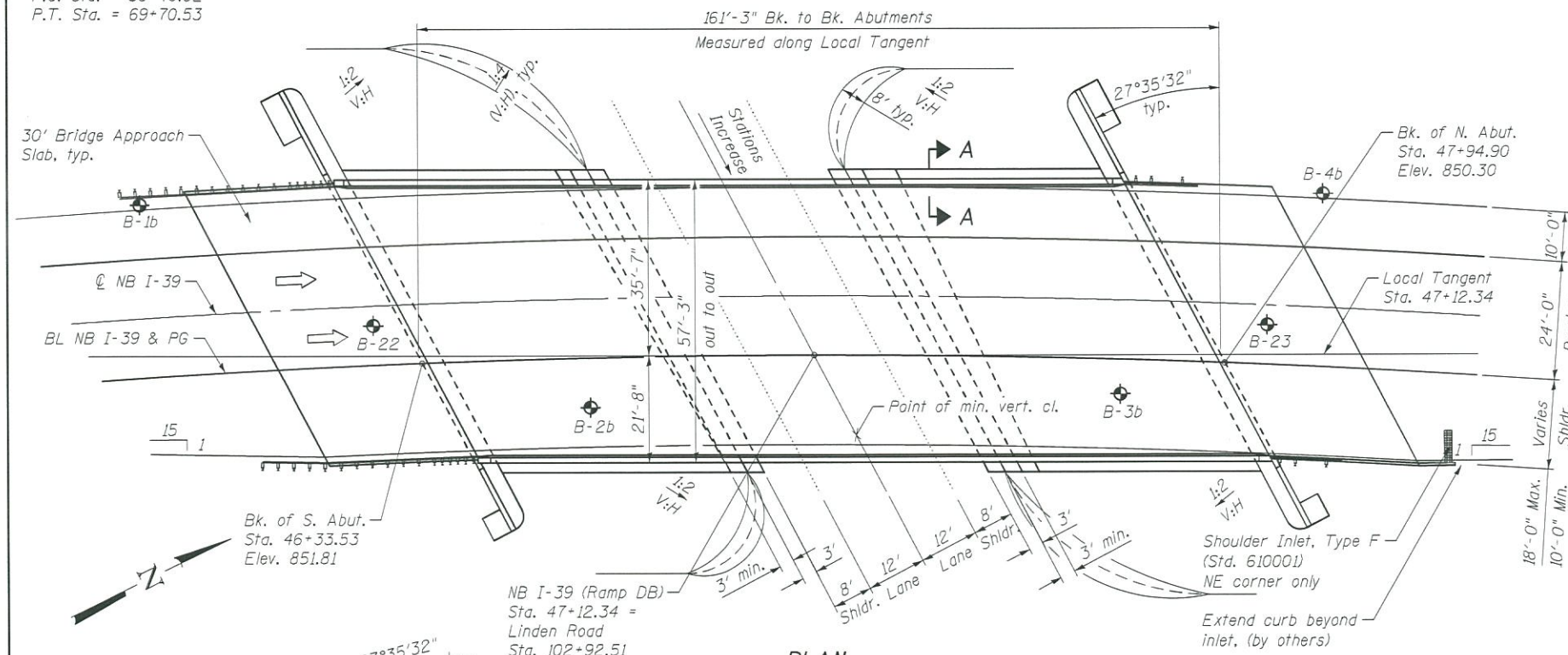
PRECAST PRESTRESSED UNITS

$f'_c = 8,500$ psi
 $f'_c = 7,000$ psi
 $f_{pu} = 270,000$ psi (0.6" ϕ low lax. strands)
 $f_{pbt} = 202,300$ psi (0.6" ϕ low lax. strands)

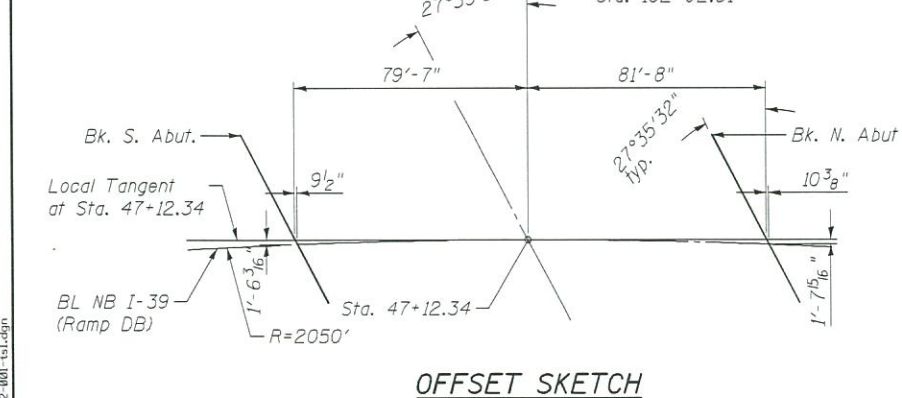
SEISMIC DATA

Seismic Performance Zone (SPZ) = 1
 Design Spectral Acceleration at 1.0 sec. (S_{D1}) = 0.056 g
 Design Spectral Acceleration at 0.2 sec. (S_{D5}) = 0.102 g
 Soil Site Class = C

Note: Up to 1/4 inch may be ground off the bridge deck and the bridge approach slabs.



PLAN

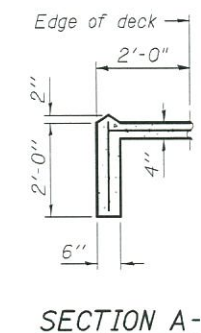


OFFSET SKETCH

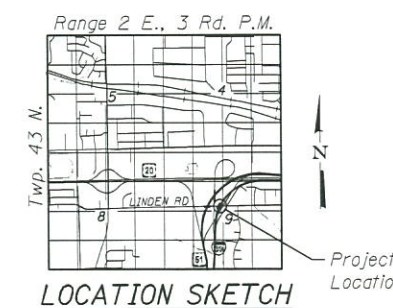
APPROVED

MAR 08 2017

AS A BASIS FOR PREPARATION OF DETAILED PLANS



SECTION A-A



LOCATION SKETCH

GENERAL PLAN
 NB I-39 (RAMP DB) OVER LINDEN ROAD
 F.A.I. RTE. 39 SEC. (201-3)K & (4-1.5)K
 WINNEBAGO COUNTY
 STATION 47+12.34
 STRUCTURE NO. 101-0212

FEHR GRAHAM
 ENGINEERING & ENVIRONMENTAL
 ILLINOIS DESIGN FIRM NO. 04-003525

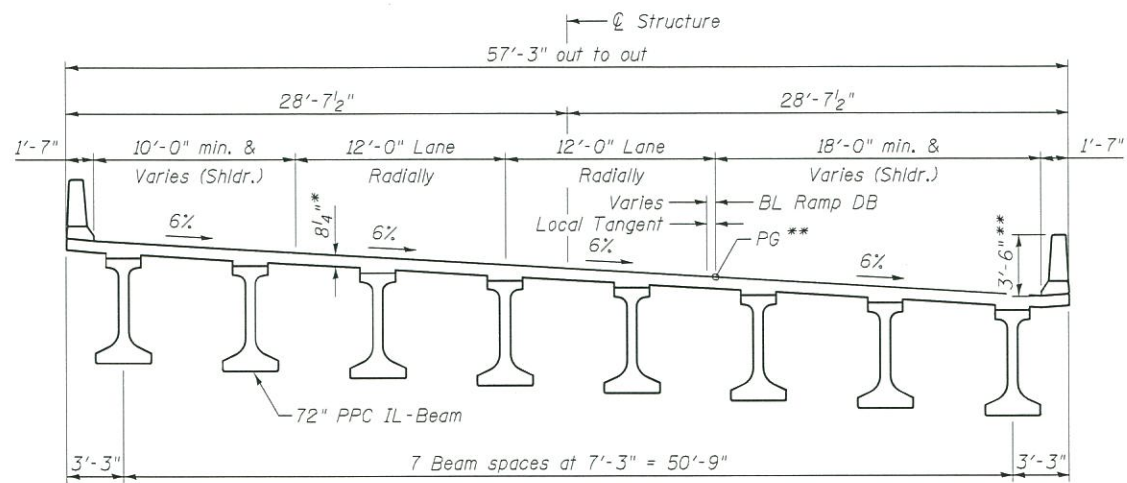
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PLOT DATE = 3/8/2017	DRAWN - CFC	REVISED -
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SHEET NO. 1 OF 2 SHEETS

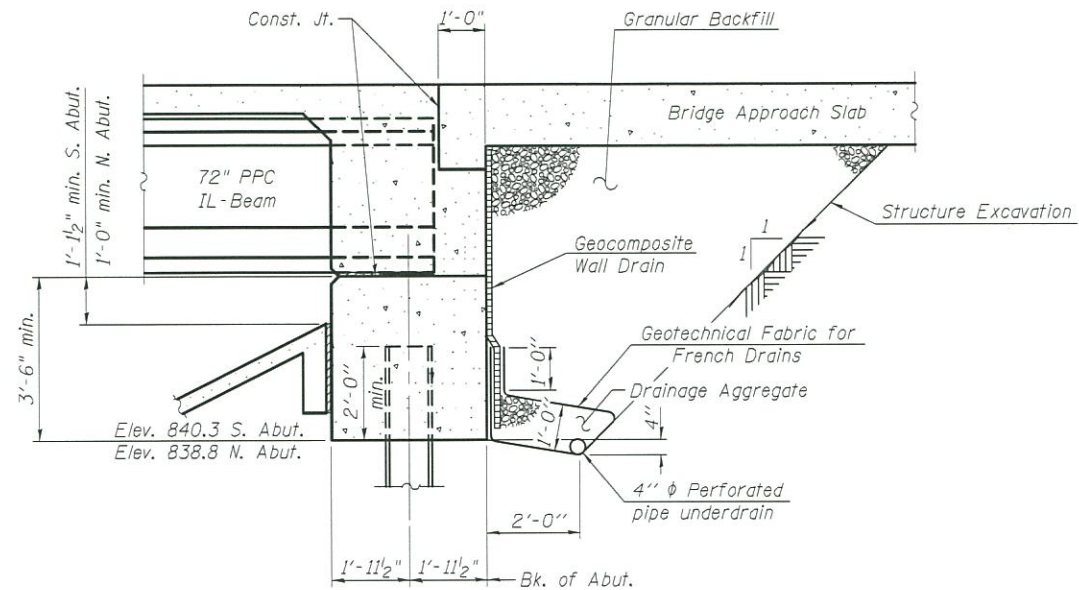
F.A.I. RTE. 39	SECTION (201-3)K & (4-1.5)K	COUNTY WINNEBAGO	TOTAL SHEETS	SHEET NO.
			CONTRACT NO. 64C62	
ILLINOIS FED. AID PROJECT				

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 FEHR GRAHAM PROJECT NUMBER: 15-1002 CB PROJ. NO.: 06085



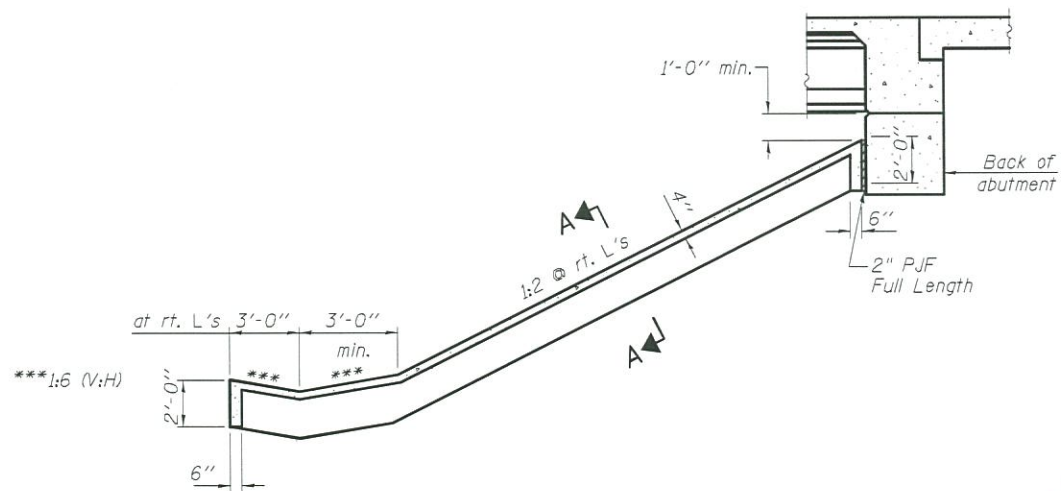
CROSS SECTION
(Looking North)

* Prior to Grinding
** After Grinding



SECTION THRU INTEGRAL ABUTMENT
(Horiz. dim. @ Rt. L's)

Note:
Pile embedment depth may need to be increased due to superelevation increasing cap depth.



SECTION THRU CONCRETE SLOPEWALL

Note: See Sheet 1 of 2 for Section A-A.

APPROVED

MAR 08 2017

AS A BASIS FOR PREPARATION OF DETAILED PLANS

GENERAL PLAN
NB I-39 (RAMP DB) OVER LINDEN ROAD
F.A.I. RTE. 39 SEC. (201-3)K & (4-1,5)K
WINNEBAGO COUNTY
STATION 47+12.34
STRUCTURE NO. 101-0212

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FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 84-003525

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CHECKED - MCB
DRAWN - CFC
PLOT SCALE = 32:0.000000 1" / 32'
PLOT DATE = 3/8/2017

DESIGNED - CME
CHECKED - MCB
REVISOR -
REVISIONS -
REVISIONS -
REVISIONS -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHEET NO. 2 OF 2 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1,5)K	WINNEBAGO		
CONTRACT NO. 64C62				
ILLINOIS FED. AID PROJECT				

Bench Mark: B.M. 405 - N-2030863.68, E-2621037.13 - Elev. 770.67 - N.W. bolt on northerly overhead sign post of "Belvidere Exit 122A" sign.
Approx. 400' into ramp. (East side of Ramp).

Existing SN 101-0071 & SN 101-0072 built as Section 5-HB in 1963. In 1990 the dual structures were widened and rehabilitated. Both structures consist of four span concrete deck on continuous steel wide flange superstructures supported by concrete piers and spill through abutments. Both structures are 228'-1" back to back abutments. SN 101-0071 width varies from 52'-8" to 56'-7". SN 101-0072 width varies from 52'-10" to 56'-4 3/4". Traffic to be maintained utilizing staged construction.

No Salvage

Bridge Omission Sta. 2723+28.51 to Sta. 2725+66.07

HIGHWAY CLASSIFICATION

F.A.P. Rte. 0525 - Harrison
Functional Class: Other Principal Arterial
ADT: 29,000 (2015); 41,250 (2035)
ADTT: 2,610 (2015); 3,713 (2035)
DHW: 4,125 (2035)
Design Speed: 35 m.p.h.
Posted Speed: 35 m.p.h.
2 -Way Traffic
Directional Distribution: 50:50

HIGHWAY CLASSIFICATION

F.A.I. Rte. 39 - I-39/U.S. 20
Functional Class: Interstate
ADT: 63,800 (2015); 104,800 (2035)
ADTT: 18,502 (2015); 30,392 (2035)
DHW: 10,480 (2035)
Design Speed: 70 m.p.h.
Posted Speed: 65 m.p.h.
2 -Way Traffic
Directional Distribution: 50:50

DESIGN STRESSES

FIELD UNITS

f'c = 3,500 psi (Substructure Concrete)
f'c = 4,000 psi (Superstructure Concrete)
fy = 60,000 psi (Reinforcement)
fy = 50,000 psi (Structural Steel) ***
(M270 Grade 50)

*** All structural steel shall be metalized.

LOADING HL-93

Allow 50#/sq. ft. for future wearing surface.

DESIGN SPECIFICATIONS

2014 AASHTO LRFD Bridge Design Specifications, 7th Edition with 2015 and 2016 Interims

SEISMIC DATA

Seismic Performance Zone (SPZ) = 1
Design Spectral Acceleration at 1.0 sec. (SD1) = 0.056g
Design Spectral Acceleration at 0.2 sec. (SDS) = 0.101g
Soil Site Class = C

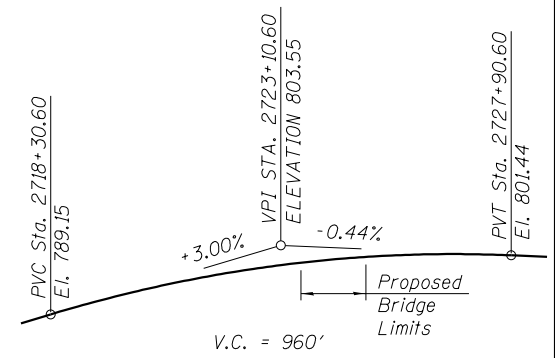
CURVE DATA HARRISON AVE.

PI STA. = 149+02.51	PI STA. = 1316+03.34
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D = 1° 00' 00"	D = 1° 00' 00"
R = 5,729.71'	R = 5,729.71'
T = 418.03'	T = 479.72'
L = 834.59'	L = 957.22'
E = 15.23'	E = 20.05'
P.C. STA. = 144+84.47	P.C. STA. = 1311+23.61
P.T. STA. = 153+19.06	P.T. STA. = 1320+80.83

CURVE DATA

F.A.I. I-39

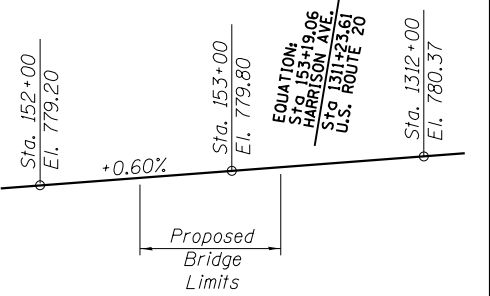
PI STA. = 2724+10.28
Δ = 17° 40' 00" (LT)
D = 0° 50' 00"
R = 6,875.65'
T = 1,068.50'
L = 2,120.05'
E = 82.53'
S.E. = 3.0%
P.C. STA. = 2713+41.77
P.T. STA. = 2734+61.82



PROPOSED PROFILE

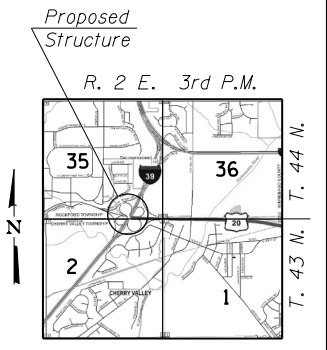
Along I-39/U.S. 20

Note: The profile grade shows the final elevations after grinding. Up to 1/4 inch may be ground off the bridge deck and the bridge approach slabs.



PROPOSED PROFILE

Along Harrison Ave.



LOCATION SKETCH

GENERAL PLAN

I-39/U.S. 20 over HARRISON AVE.

F.A.I. RTE. 39

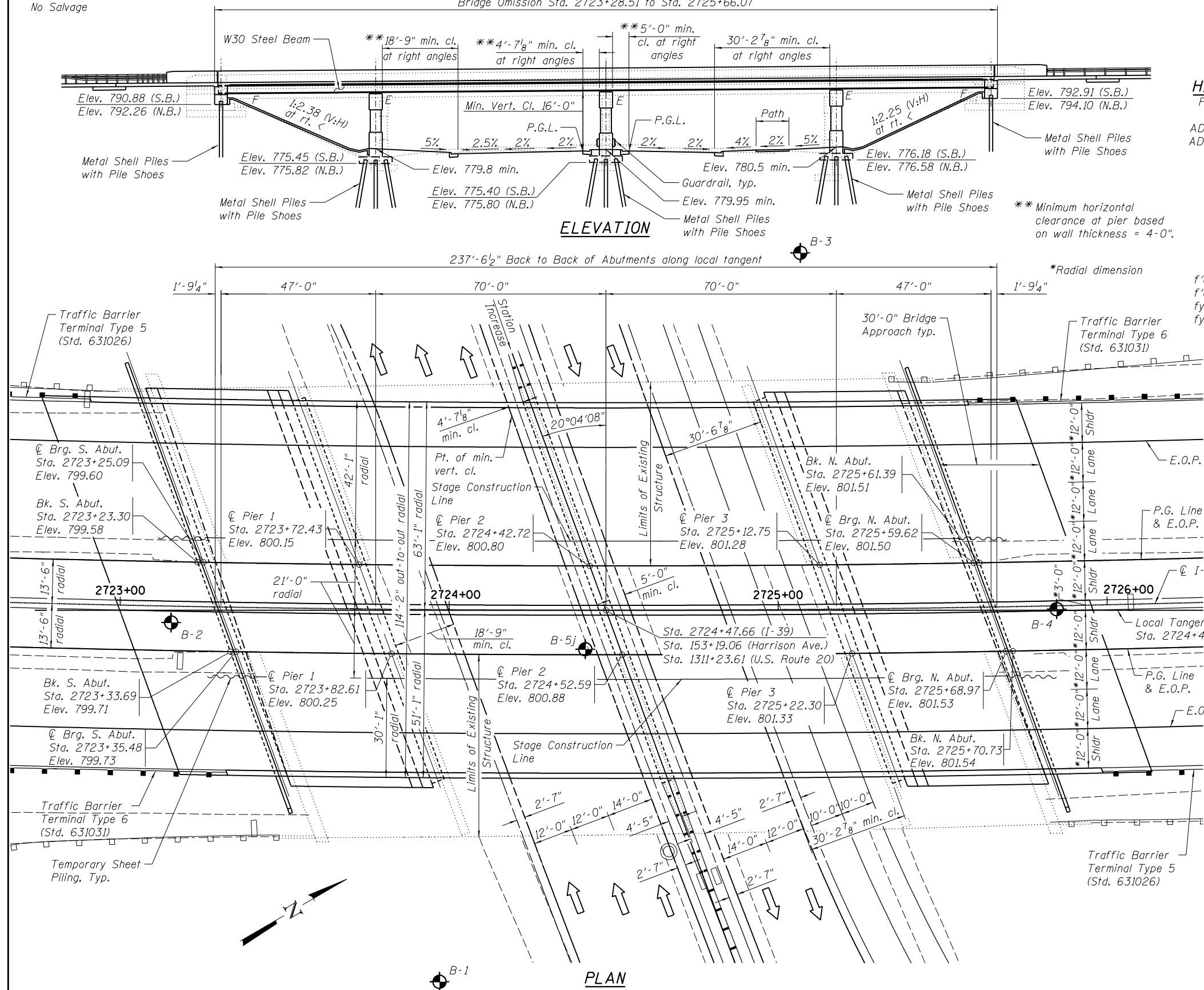
SECTION (201-3)K & (4-1. 5)R

WINNEBAGO COUNTY

STATION 2724+47.66

STRUCTURE NUMBER 101-0213 (N.B.)

STRUCTURE NUMBER 101-0214 (S.B.)



DESIGNED	JKL	05/25/16
DRAWN	MGM	12/06/16
REVIEWED	FLN	12/06/16

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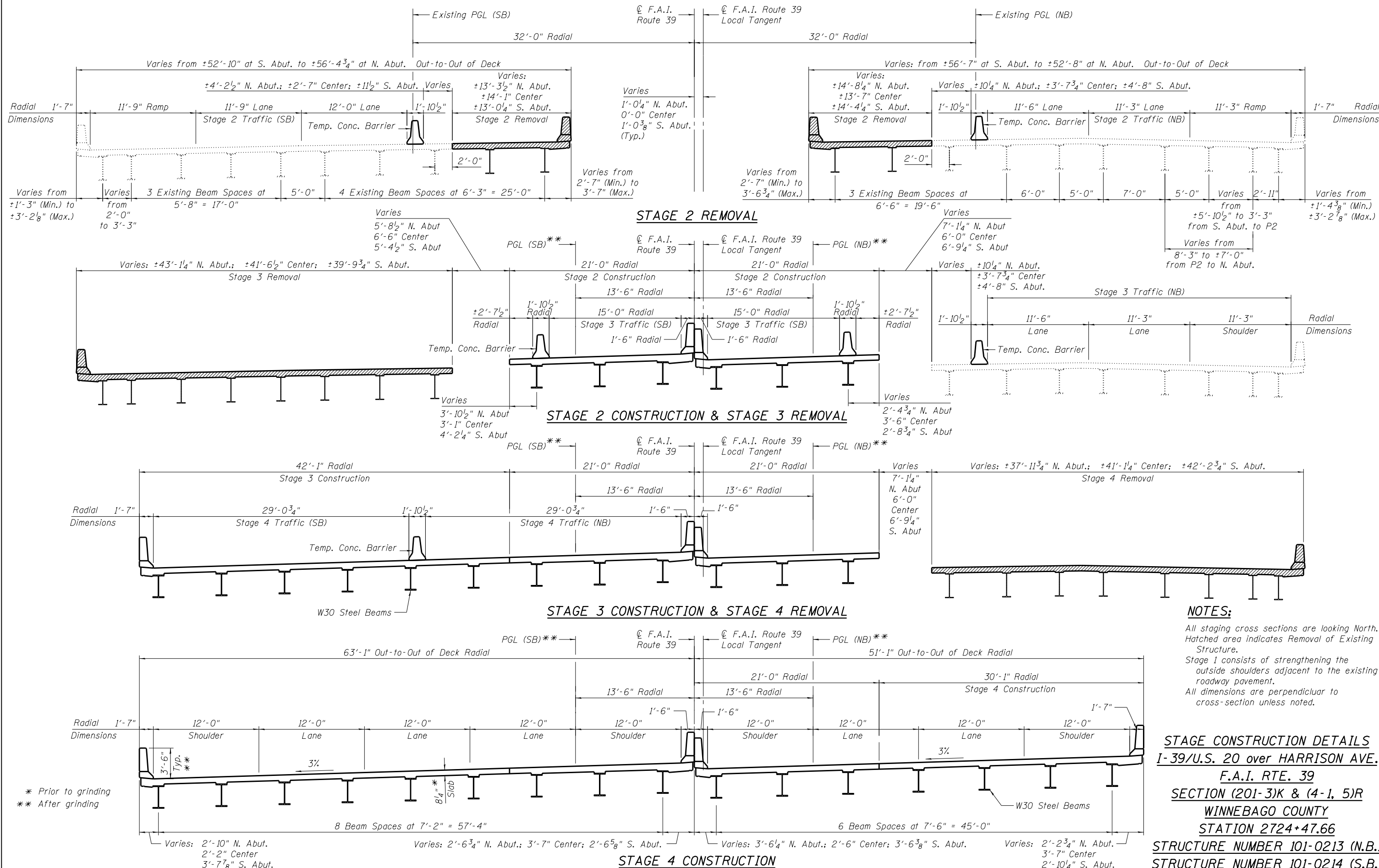
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		CHECKED -	JKL	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN & ELEVATION
STRUCTURE NO. 101-0213 & STRUCTURE NO. 101-0214

SHEET NO. 1 OF 3 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1. 5)R	WINNEBAGO	3	1
CONTRACT NO. 64C24				
ILLINOIS FED. AID PROJECT				



NOTES:
 All staging cross sections are looking North. Hatched area indicates Removal of Existing Structure.
 Stage 1 consists of strengthening the outside shoulders adjacent to the existing roadway pavement.
 All dimensions are perpendicular to cross-section unless noted.

STAGE CONSTRUCTION DETAILS
I-39/U.S. 20 over HARRISON AVE.
F.A.I. RTE. 39
SECTION (201-3)K & (4-1, 5)R
WINNEBAGO COUNTY
STATION 2724+47.66
STRUCTURE NUMBER 101-0213 (N.B.)
STRUCTURE NUMBER 101-0214 (S.B.)

* Prior to grinding
 ** After grinding

DESIGNED	JKL	05/25/16
DRAWN	MGM	12/06/16
REVIEWED	FLN	12/06/16

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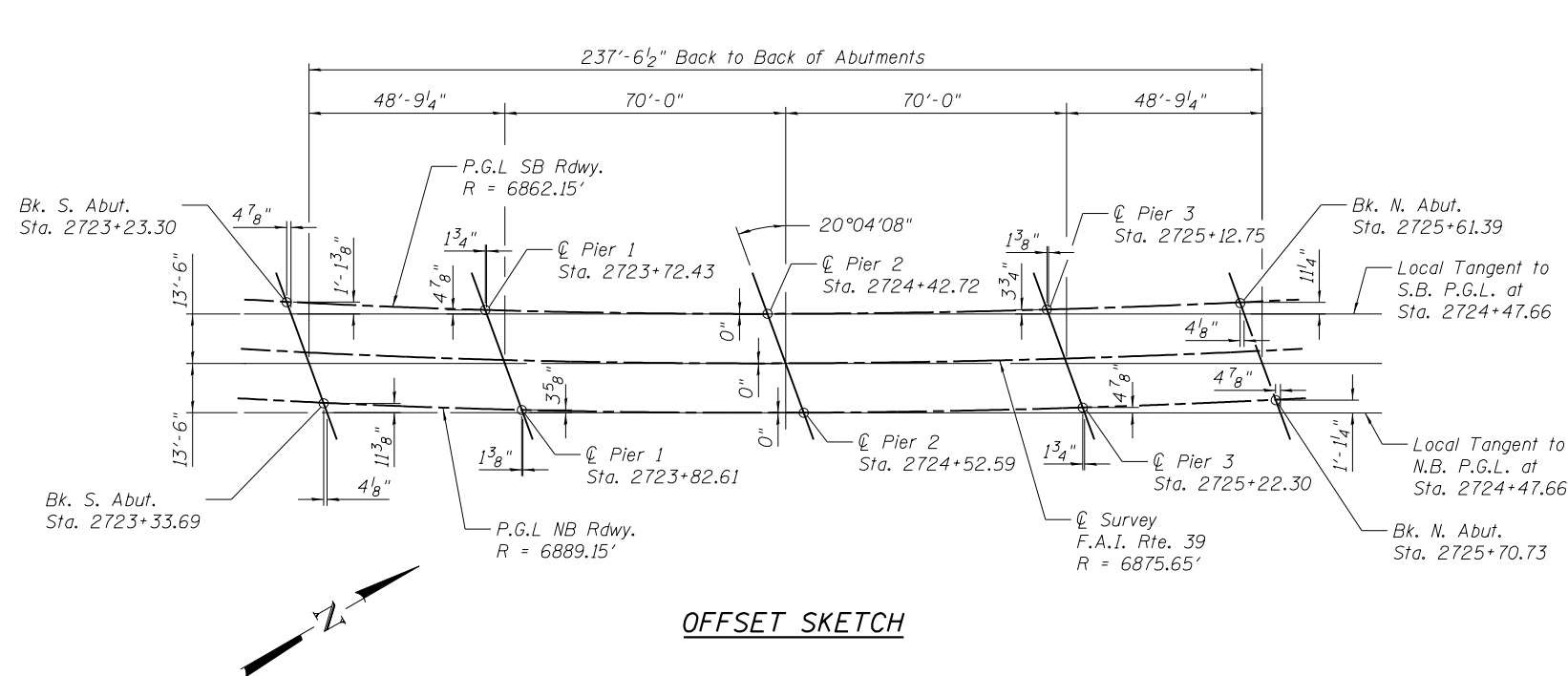
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PLOT DATE =	CHECKED - JKL	REVISD -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

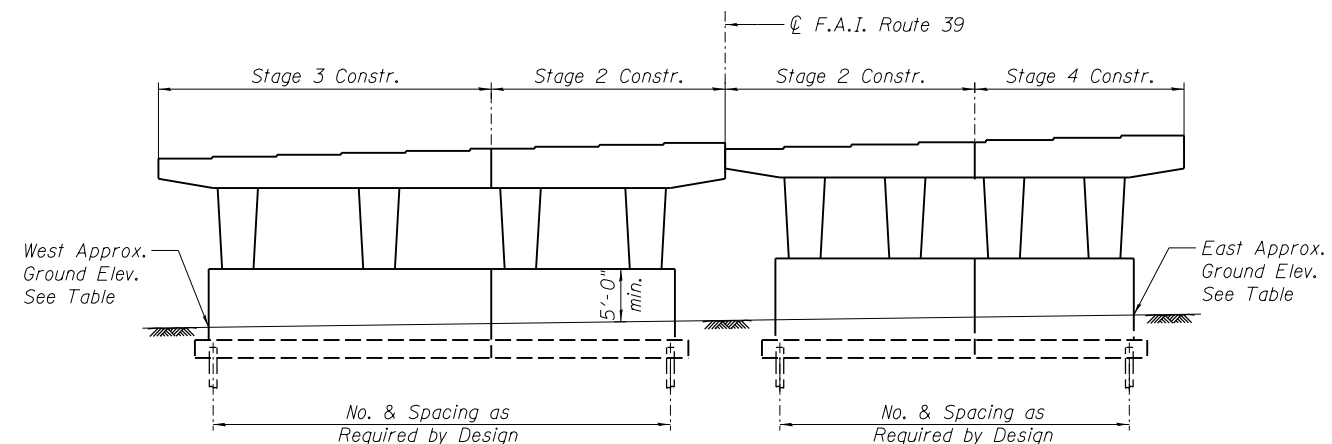
STAGE CONSTRUCTION DETAILS
STRUCTURE NO. 101-0213 & STRUCTURE NO. 101-0214

SHEET NO. 2 OF 3 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1, 5)R	WINNEBAGO	3	2
			CONTRACT NO. 64C24	
ILLINOIS FED. AID PROJECT				



OFFSET SKETCH



PIER SKETCH
(Looking North)

Notes:

Provide two columns minimum per structure per stage.

All cap cantilevers created by partial removal of existing piers shall be evaluated and temporarily supported to the crashwall below, as required.

APPROX. GROUND ELEV.

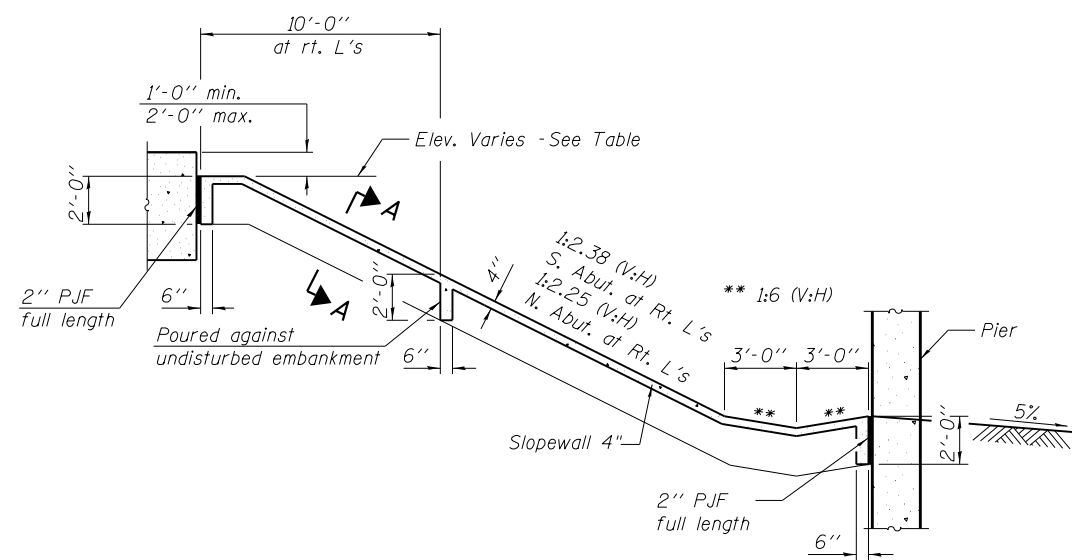
Pier	End	Elev.
Pier 1	East	780.5
	West	779.8
Pier 2	East	780.7
	West	780.0
Pier 3	East	781.2
	West	780.5

DIMENSION "A"

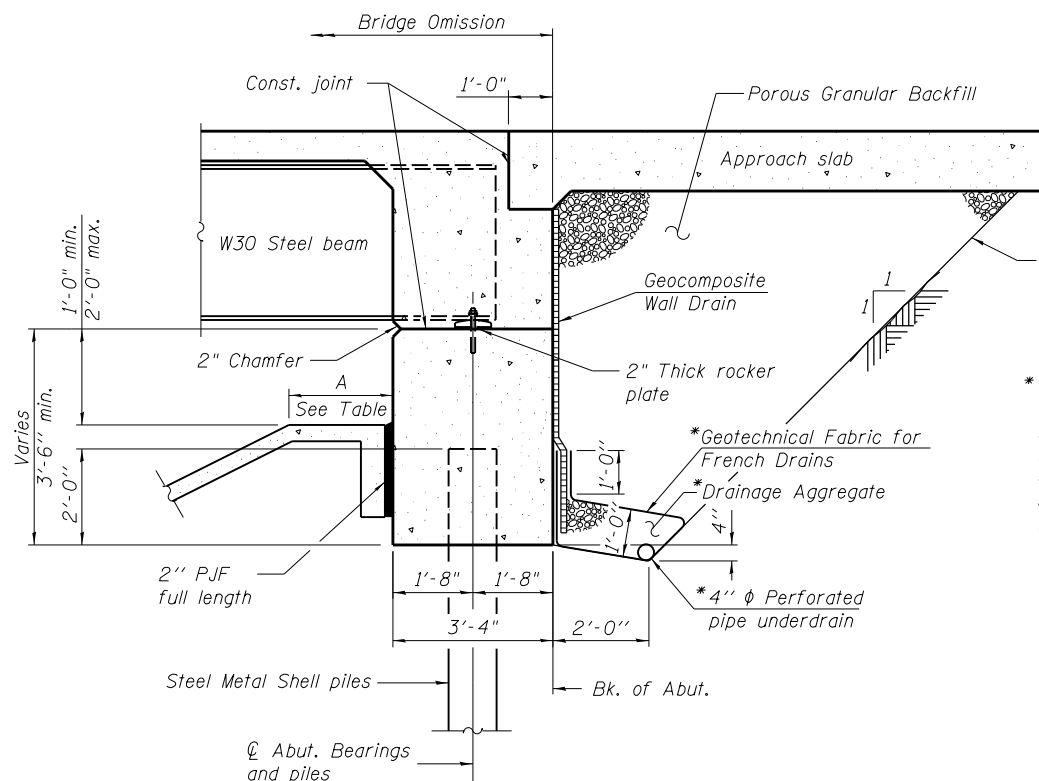
Abut.	End	Value
South	East	0'-0"
	West	3'-2 7/8"
North	East	0'-0"
	West	2'-2 1/2"

TOP OF BERM ELEVATIONS

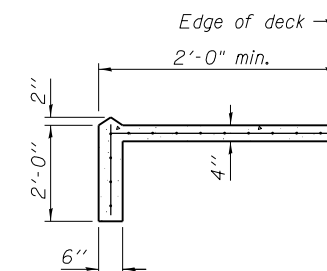
Abut.	End	Elev.
South	East	795.31
	West	793.38
North	East	797.02
	West	795.41



SECTION THRU CONCRETE SLOPEWALL
(Horiz. dim. @ Rt. L's)



SECTION THRU INTEGRAL ABUTMENT
(Horiz. dim. @ Rt. L's)



SECTION A-A

* Included in the cost of Pipe Underdrains for structures.

All drainage system components shall extend to 2'-0" from the end of each wingwall except an outlet pipe shall extend until intersection with the side slopes. The pipes shall drain into concrete headwalls. (See Article 601.05 of the Standard Specifications and Highway Standard 601101)

GENERAL DETAILS
I-39/U.S. 20 over HARRISON AVE.
F.A.I. RTE. 39
SECTION (201-3)K & (4-1, 5)R
WINNEBAGO COUNTY
STATION 2724+47.66
STRUCTURE NUMBER 101-0213 (N.B.)
STRUCTURE NUMBER 101-0214 (S.B.)

DESIGNED	JKL	05/25/16
DRAWN	MGM	12/06/16
REVIEWED	FLN	12/06/16

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PLOT DATE = 3/16/2017	DRAWN - MGM / MWH	REVISION
	CHECKED - JKL	REVISION

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL DETAILS
STRUCTURE NUMBER 101-0213 & 101-0214

SHEET NO. 3 OF 3 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1, 5)R	WINNEBAGO	3	3

CONTRACT NO. 64C24

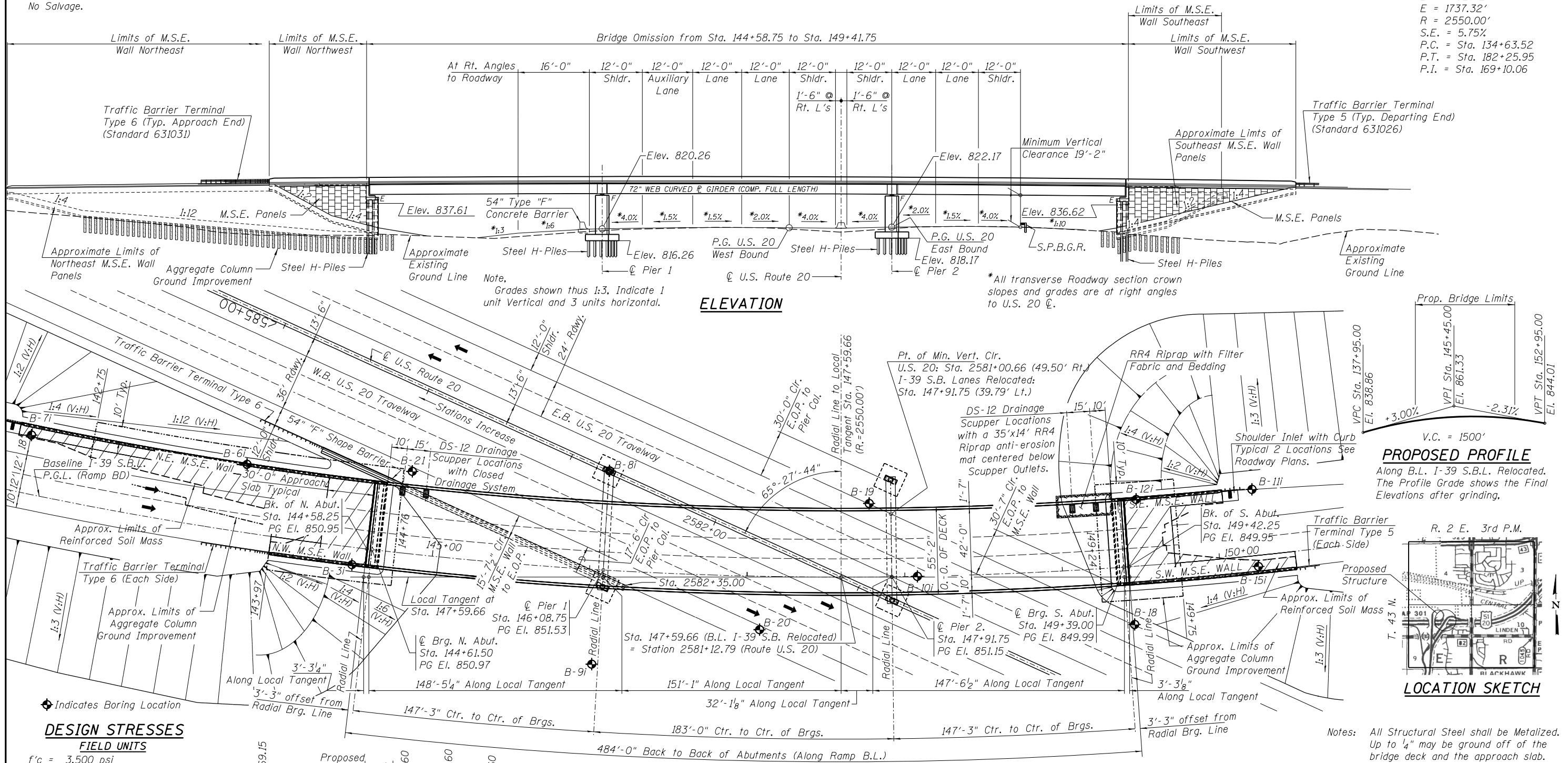
ILLINOIS FED. AID PROJECT

BM: #454 - Cut Square South side of Westerly sign base for a 30MPH ramp 0.1 miles North of the Centerline of Linden Rd. - West side of I-39 Elevation = 850.53
 #455 - Cut Square South side of Easterly sign base for a 30MPH ramp 0.1 miles North of the Centerline of Linden Rd. - East side of I-39 Elevation = 851.37
 Existing Structure (SN 101-0136)

Originally constructed in 1981 as F.A. Route 194, Section 201-3HB-2.
 The structure consists of a 3 Span, 56'-25'-55" Rt Fwd. Skewed, concrete deck on curved steel girders, supported on pile bent concrete abutments and 2 (4 Trapezoidal columns) concrete piers on pile supported foundations. The deck width is 28'-0", and the span lengths are 86'-0", 143'-6" and 155'-0" developing a back to back length of 397'-8 1/2".
 Traffic shall be maintained on the existing structure during construction of the new structure. See Maintenance of Traffic Plans for more staging information.
 No Salvage.

CURVE DATA - I-39 SBL

$\Delta = 107^{\circ}00' - 24"$ (Lt.)
 $D = 2^{\circ} - 14' - 49"$
 $T = 3446.54'$
 $L = 4762.42'$
 $E = 1737.32'$
 $R = 2550.00'$
 $S.E. = 5.75\%$
 $P.C. = \text{Sta. } 134+63.52$
 $P.T. = \text{Sta. } 182+25.95$
 $P.I. = \text{Sta. } 169+10.06$

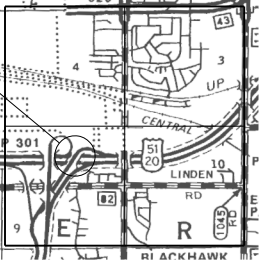


ELEVATION

PROPOSED PROFILE

Along B.L. I-39 S.B.L. Relocated. The Profile Grade shows the Final Elevations after grinding.

LOCATION SKETCH



Notes: All Structural Steel shall be Metalized. Up to 1/4" may be ground off of the bridge deck and the approach slab.

DESIGN STRESSES

FIELD UNITS

$f'_c = 3,500$ psi
 $f'_c = 4,000$ psi (Superstructure Concrete)
 $f_y = 60,000$ psi (Reinforcement)
 $f_y = 50,000$ psi (M270 Grade 50)

PRECAST UNITS

$f'_c = 4,500$ psi (Precast Panels)

LOADING HL-93

Allow 50#/#sq. ft. for future wearing surface.

DESIGN SPECIFICATIONS

2014 AASHTO LRFD Bridge Design Specifications, 7th Edition, with 2015 and 2016 Interims
 I:\06\jobs\0652055\CADD\Struct\Model\110519_139_SB_Ramp_BD_over_US20_TSL.dgn

PROPOSED PROFILE

Along US-20 Inside E.O.P.

PLAN

SEISMIC DATA

Seismic Performance Zone (SPZ) = 1
 Design Spectral Acceleration at 1.0 sec. (S_{D1}) = 0.056
 Design Spectral Acceleration at 0.2 sec. (S_{D5}) = 0.101
 Soil Site Class = C

HIGHWAY CLASSIFICATION

F.A.I. Rte. 39 - I-39 S.B. Lanes
 Functional Class: Interstate
 ADT: 10,000 (2013); 28,000 (2040)
 ADTT: 4,200 (2013); 11,700 (2040)
 DHV: 2250 (2040)
 Design Speed: 70 m.p.h.
 Posted Speed: 65 m.p.h.
 1-Way Traffic

HIGHWAY CLASSIFICATION

FAP Rte 301 - U.S. Route 20
 Functional Class: Freeway and Expressway (Urban)
 ADT: 33,400 (2013); 50,050 (2040)
 ADTT: 6,000 (2013); 9,000 (2040)
 DHV: 5970 (2040)
 Design Speed: 70 m.p.h.
 Posted Speed: 65 m.p.h.
 2-Way Traffic
 Directional Distribution: 50:50

GENERAL PLAN

I-39 S.B.L. (Ramp BD) over U.S.20
F.A.I. 39 - SECTION (201-3)K & (4-1, 5)R
WINNEBAGO COUNTY
STA. 147+59.66
STRUCTURE NUMBER - 101-0215

DESIGNED	1/1
DRAWN	1/1
REVIEWED	1/1



USER NAME =	DESIGNED - MNM	REVISIONS	REVISOR
PLOT SCALE =	CHECKED - TEH	DATE	DATE
PLOT DATE =	DRAWN - ROD		
	CHECKED - MNM		

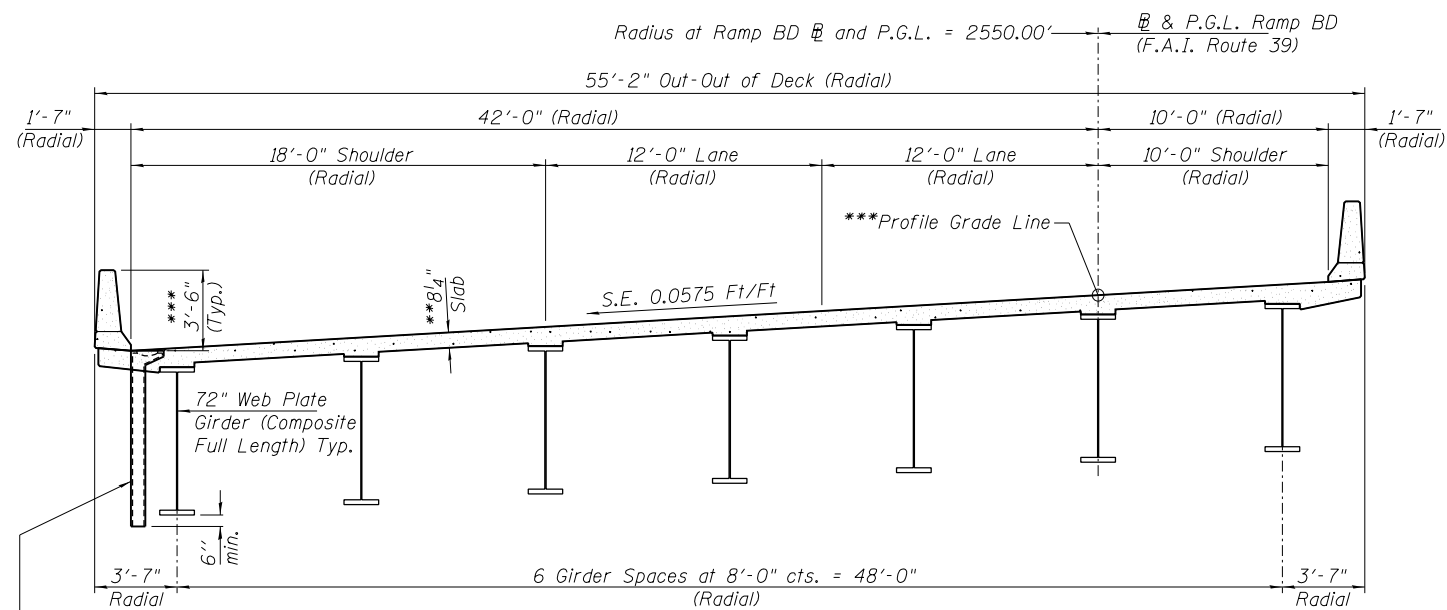
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

STRUCTURE NO. 101-0215

SHEET NO. 01 OF 04 SHEETS

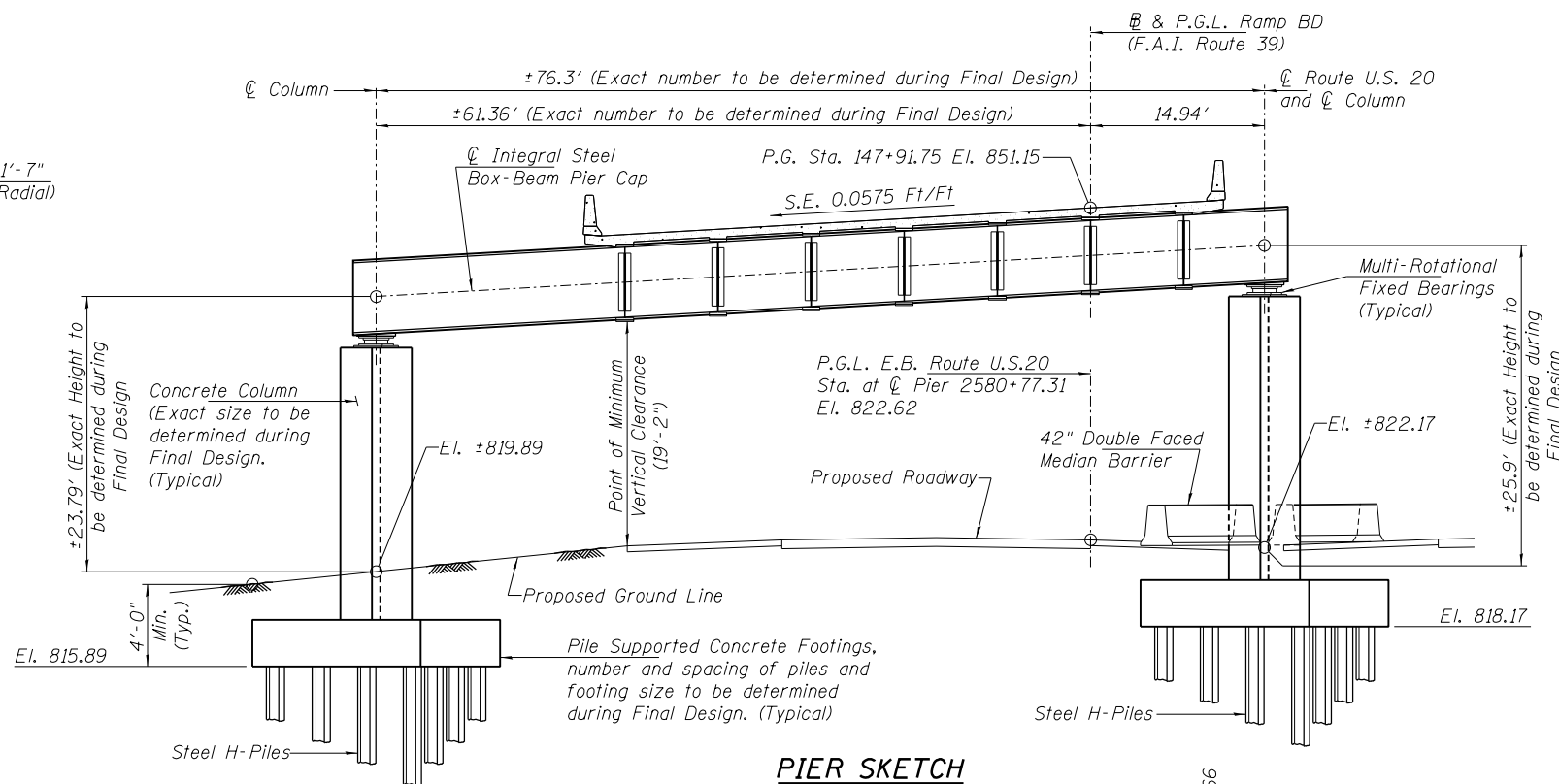
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1, 5)R	WINNEBAGO	4	1
CONTRACT NO. 64C62				
ILLINOIS FED. AID PROJECT				

** Prior to Grinding
 *** After Grinding



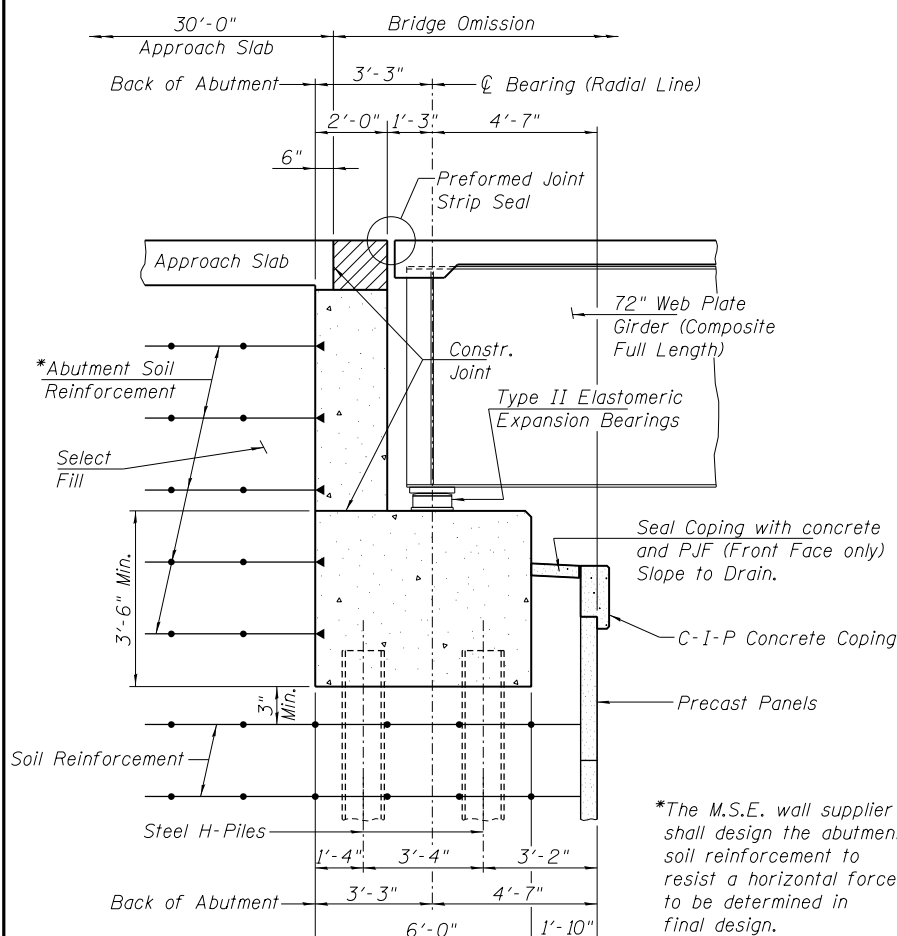
PROPOSED CROSS SECTION
 (Looking South)

DS-12 Scupper typ.
 (See Plan for Spacing)
 Closed Drainage System
 Required at N. Abut.



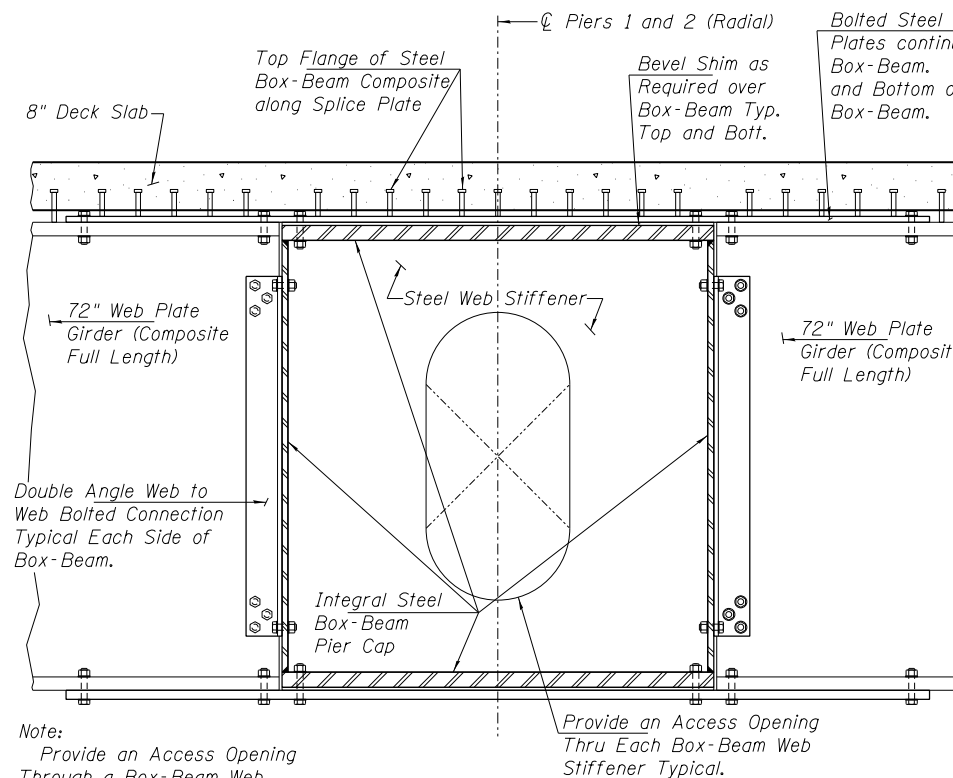
PIER SKETCH

(Looking South)
 (Pier No. 2 Dimensions and Elevations Shown, Pier No. 1 Similar)



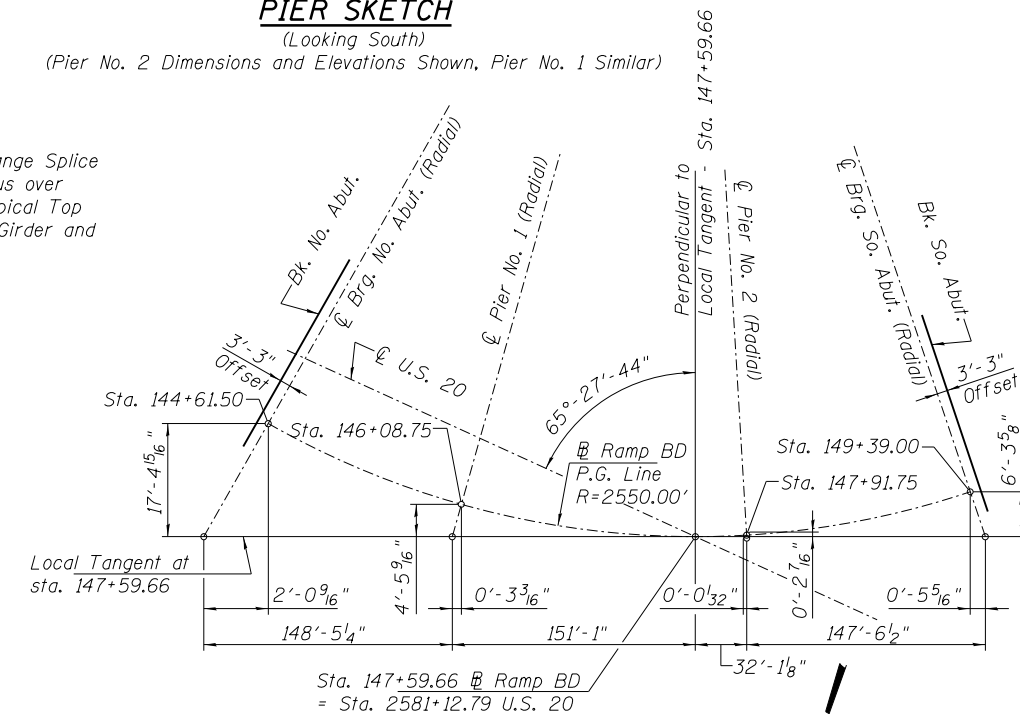
SECTION THRU ABUTMENT
 (Horiz. dim. @ Rt. L's)

*The M.S.E. wall supplier shall design the abutment soil reinforcement to resist a horizontal force to be determined in final design.



SECTION THRU BOX-BEAM PIER CAP
 (Horiz. dim. @ Rt. L's)

Note:
 Provide an Access Opening Through a Box-Beam Web or Flange Member with a Bolted and Sealed Cover.
 Provide Drain Hole in the Bottom Flange of the Box-Beam at the low end. Bird screens are required over the drain hole.



OFFSET SKETCH

DETAILS
I-39 S.B.L. (Ramp BD) over U.S.20
F.A.I. 39 - SECTION (201-3)K & (4-1, 5)R
WINNEBAGO COUNTY
STA. 147+59.66
STRUCTURE NUMBER - 101-0215

DESIGNED	1/1
DRAWN	1/1
REVIEWED	1/1

J:\06 jobs\0652055\CADD\Struct\Model\110519_139_SB_Ramp-BD_over-US20.TSL.dgn



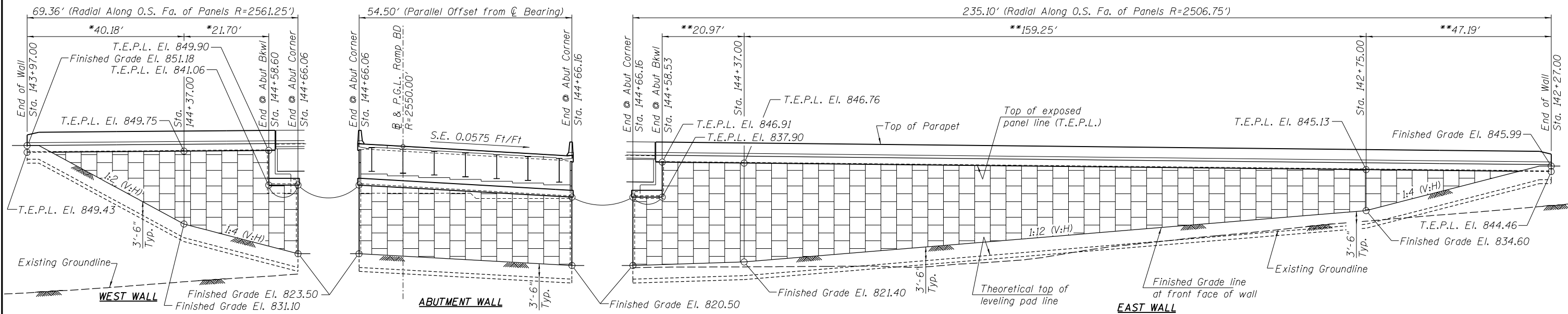
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PLOT SCALE =	CHECKED - TEH	REVISD -
PLOT DATE =	DRAWN - ROD	REVISD -
	CHECKED - MNM	REVISD -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

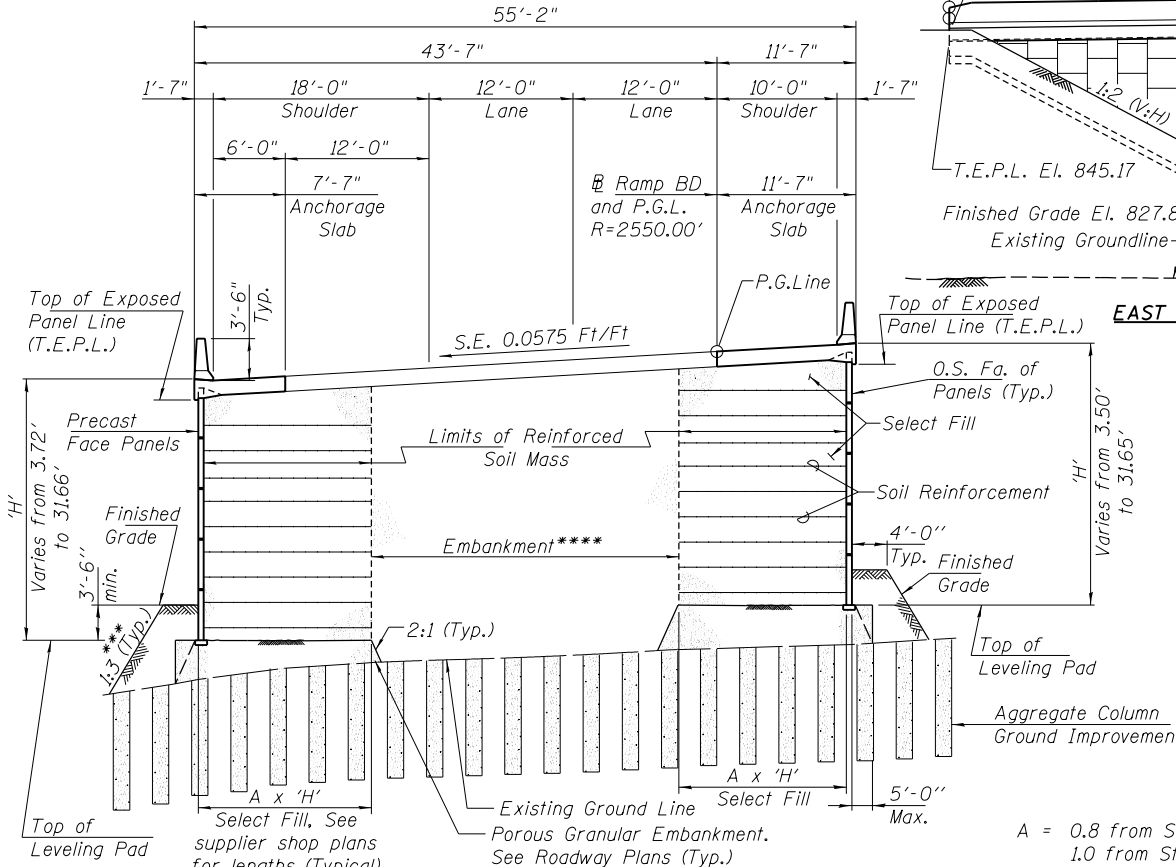
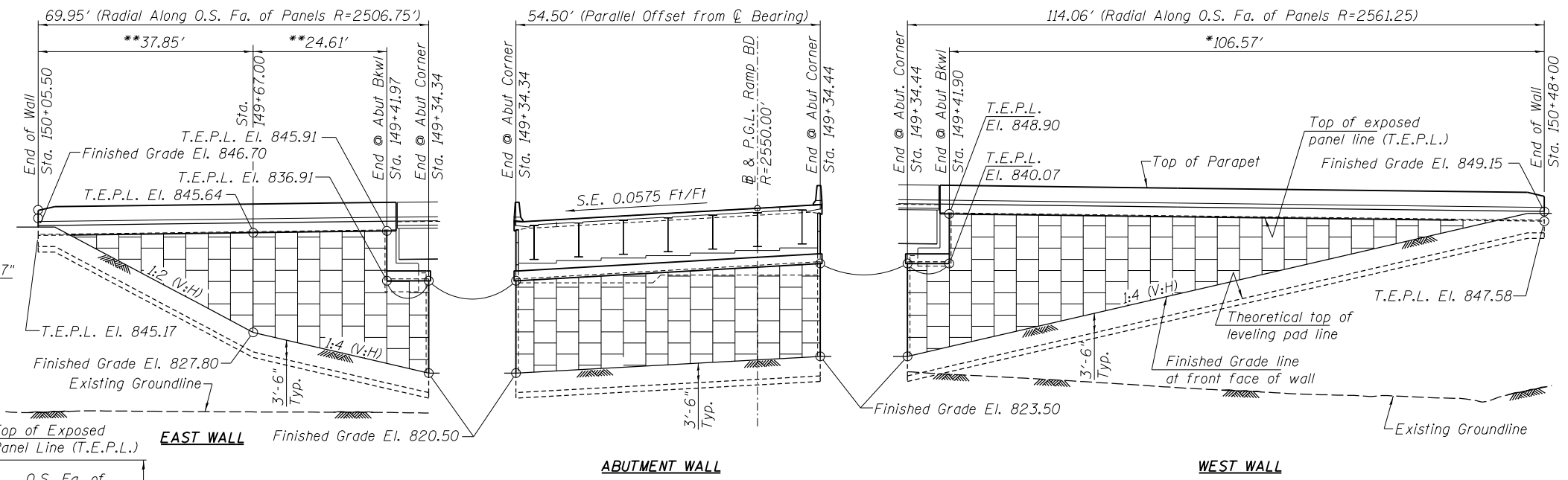
STRUCTURE NO. 101-0215

SHEET NO. 02 OF 04 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1, 5)R	WINNEBAGO	4	2
CONTRACT NO. 64C62				
ILLINOIS FED. AID PROJECT				



* Radial along O.S. Face - R=2561.25'
 ** Radial along O.S. Face - R=2506.75'
 *** 1:2 max. Near End of Wall
 **** Embankment Material between MSE Walls from Sta. 143+97 to Sta. 150+05.50 Shall be Restricted to the same material as used for select fill in MSE Wall.



M.S.E. WALL DETAILS
I-39 S.B.L. (Ramp BD) over U.S.20
F.A.I. 39 - SECTION (201-3)K & (4-1, 5)R
WINNEBAGO COUNTY
STA. 147+59.66
STRUCTURE NUMBER - 101-0215

DESIGNED: / / /
 DRAWN: / / /
 REVIEWED: / / /

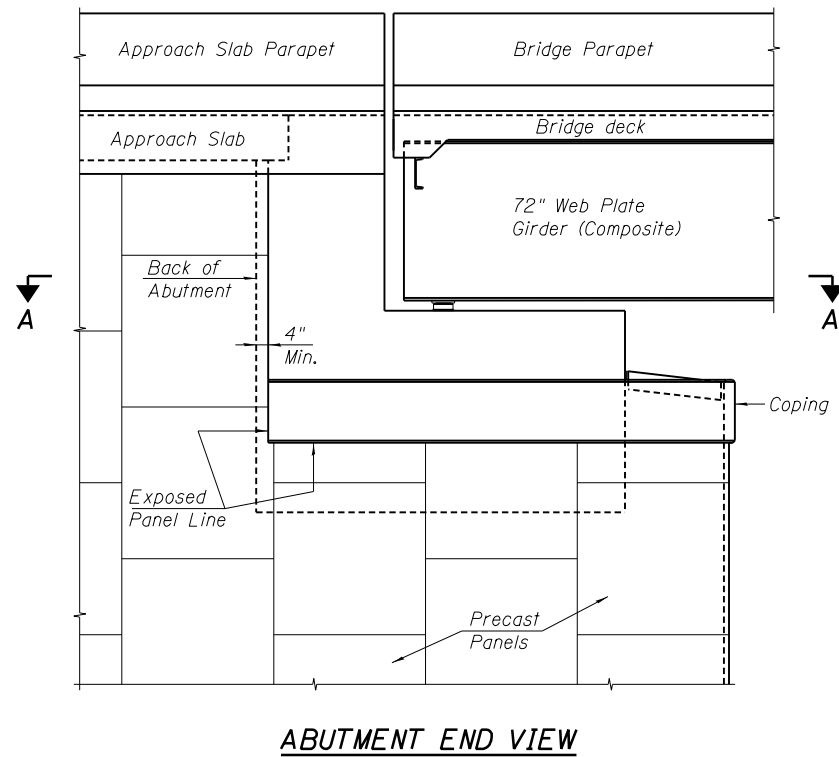
HANSON
 Copyright Hanson Professional Services Inc. 2017

USER NAME =	DESIGNED - MNM	REVISED -
PLOT SCALE =	CHECKED - TEH	REVISED -
PLOT DATE =	DRAWN - ROD	REVISED -
	CHECKED - MNM	REVISED -

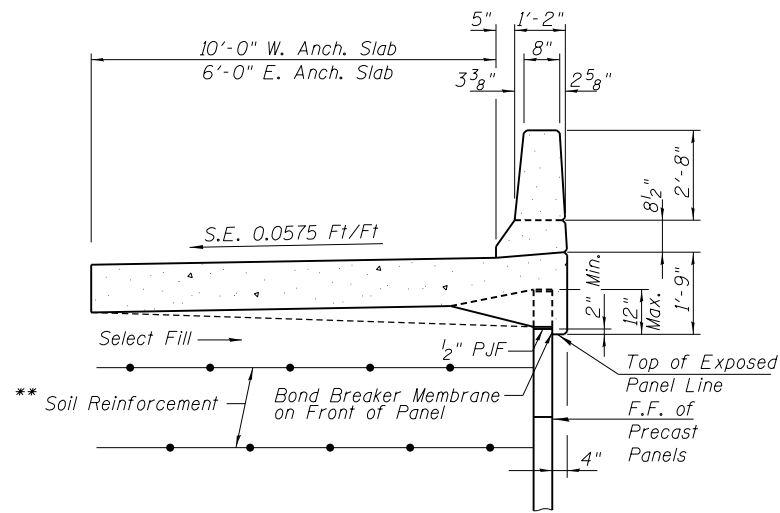
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STRUCTURE NO. 101-0215
 SHEET NO. 03 OF 04 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1, 5)R	WINNEBAGO	4	3
CONTRACT NO. 64C62				
ILLINOIS FED. AID PROJECT				

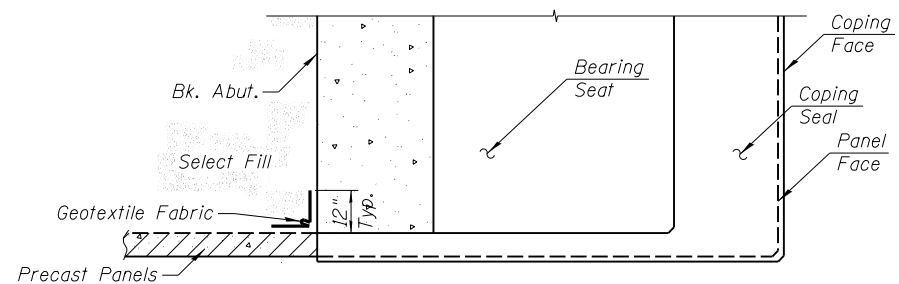


ABUTMENT END VIEW

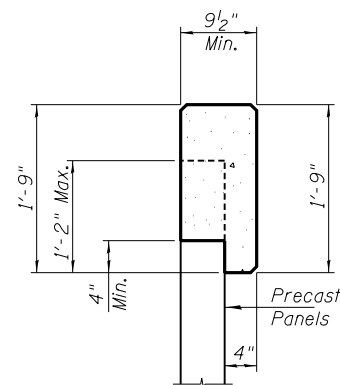


SECTION THRU C.I.P. PARAPET AND ANCHORAGE SLAB

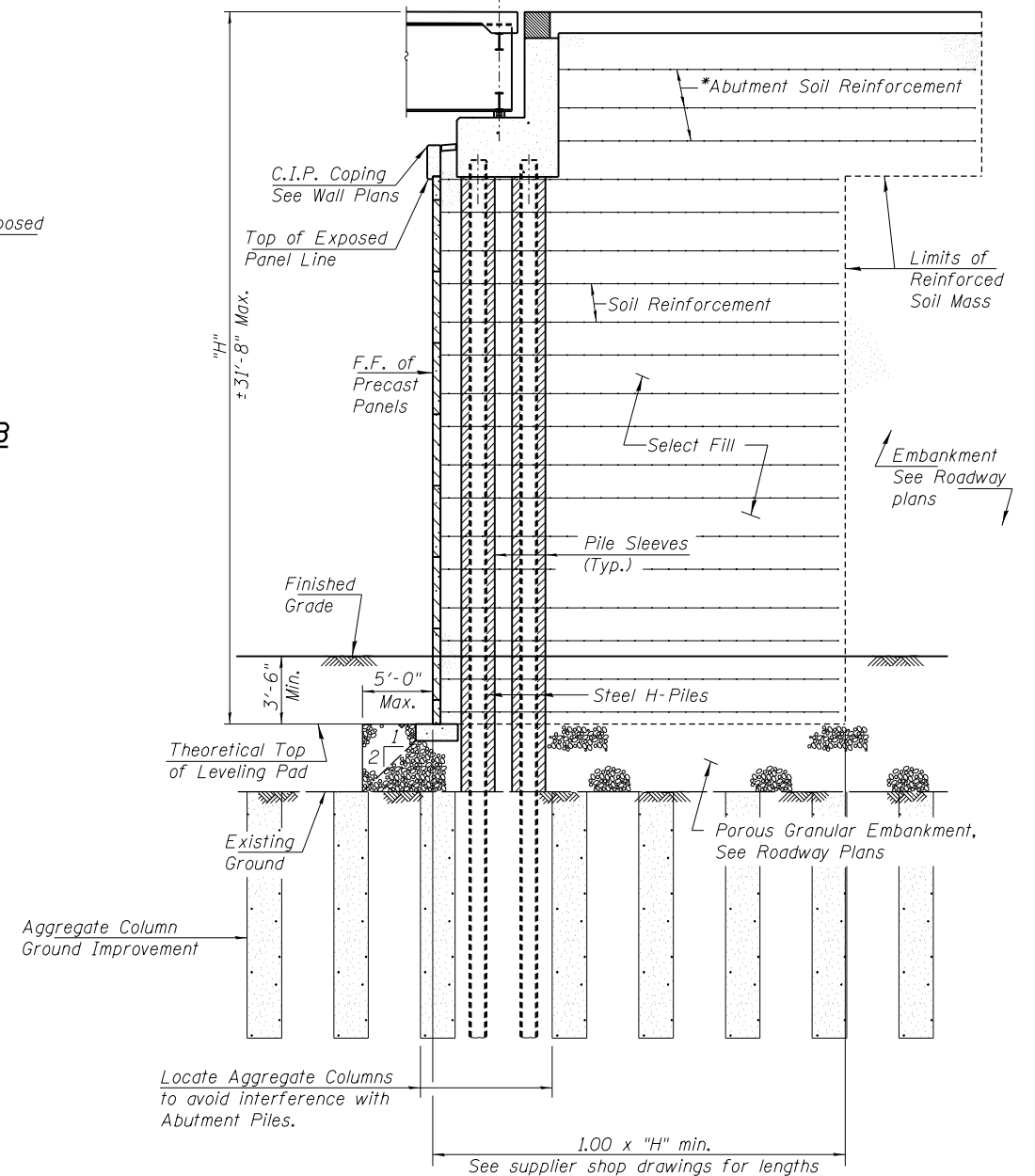
** The M.S.E. Wall supplier's internal stability design shall account for the anchorage slab's bearing pressure surcharge of 1.0 ksf and horizontal sliding force of 0.5 kips/ft. of wall.



SECTION A-A



SECTION THRU C.I.P. COPING



TYPICAL M.S.E. WALL SECTION AT ABUTMENT

(Section at Rt. L's)

* The MSE wall supplier shall design the abutment soil reinforcement to resist a horizontal force to be determined during final design.

M.S.E. WALL DETAILS
I-39 S.B.L. (Ramp BD) over U.S.20
F.A.I. 39 - SECTION (201-3)K & (4-1, 5)R
WINNEBAGO COUNTY
STA. 147+59.66
STRUCTURE NUMBER - 101-0215

DESIGNED	1/1
DRAWN	1/1
REVIEWED	1/1

J:\06 jobs\0652055\CADD\Struct\Mode\110519_139_SB_Ramp-BD_over-US20.TSL.dgn



USER NAME =	DESIGNED - MNM	REVISD -
	CHECKED - TEH	REVISD -
PLOT SCALE =	DRAWN - ROD	REVISD -
PLOT DATE =	CHECKED - MNM	REVISD -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STRUCTURE NO. 101-0215

SHEET NO. 04 OF 04 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1, 5)R	WINNEBAGO	4	4
CONTRACT NO. 64C62				
ILLINOIS FED. AID PROJECT				

Benchmark: Cut "□" south side of eastern base of 30 mph ramp sign located east of the ramp connecting I-39 NB to US 20 West (Ramp DA) 0.1 mile north of the centerline of Linden Road. Elev. 851.37, 42°-13'-06.37" N. 89°-00'-39.64" W.

Existing Structure: None

Stage Construction will be utilized to maintain one lane of traffic at all times. Stage I Traffic will be maintained on existing Linden Rd.

APPROVED

MAR 08 2017

Note: Up to 1/4 inch may be ground off the bridge deck and the bridge approach slabs.

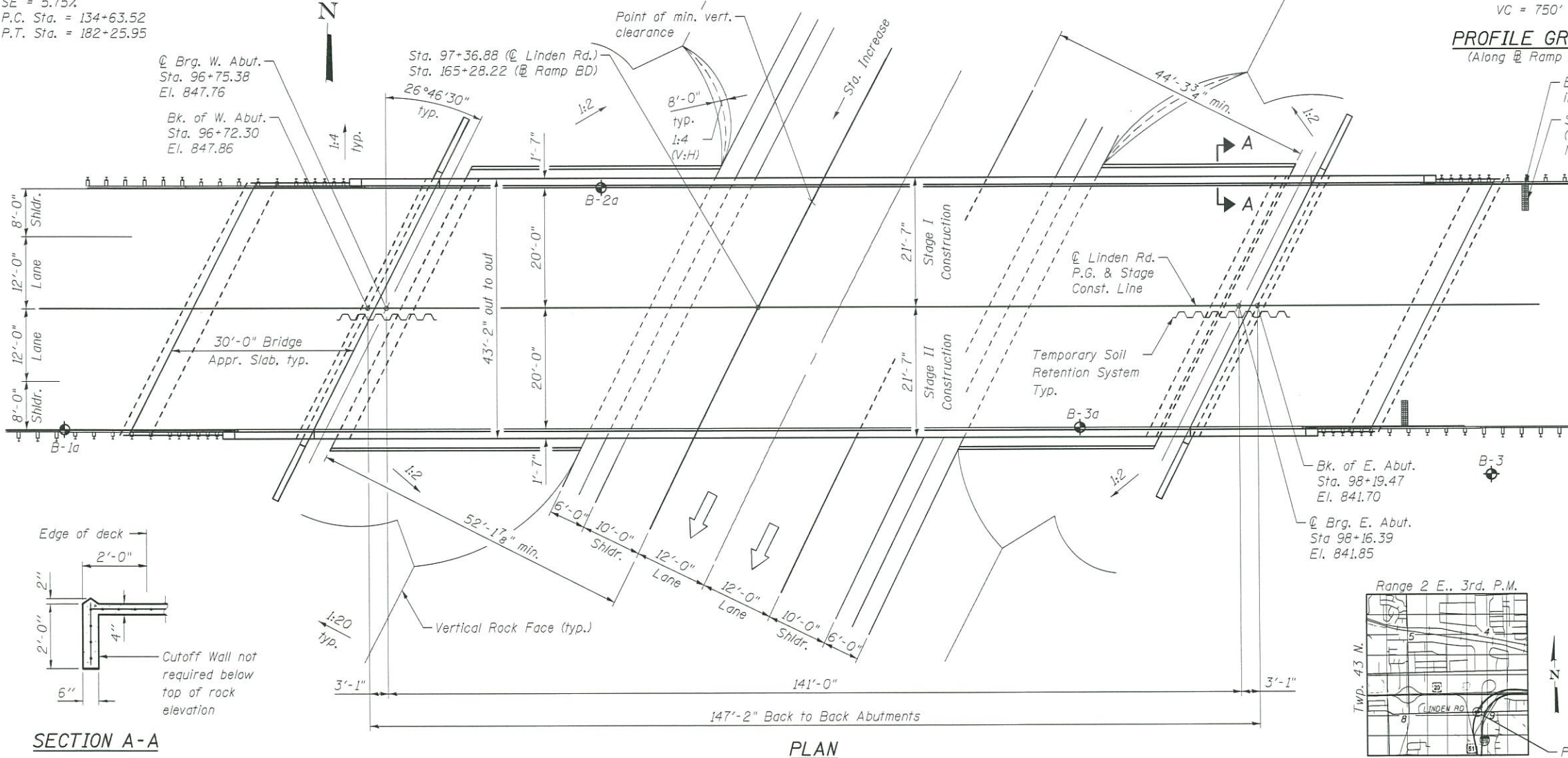
AS A BASIS FOR
PREPARATION OF DETAILED PLANS

CURVE DATA

Ramp BD
P.I. Sta. = 169+10.06
 $\Delta = 107^\circ 00' 24''$ (Lt.)
 $D = 2^\circ 14' 49''$
 $R = 2,550.00'$
 $T = 3,446.54'$
 $L = 4,762.42'$
 $E = 1,737.32$
 $SE = 5.75\%$
P.C. Sta. = 134+63.52
P.T. Sta. = 182+25.95

* W. Abut. approx. bottom of footing Elev. 831.1

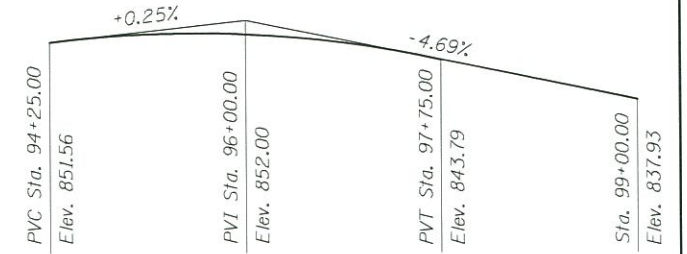
* Embed footing 6" into non-weathered rock.



SECTION A-A

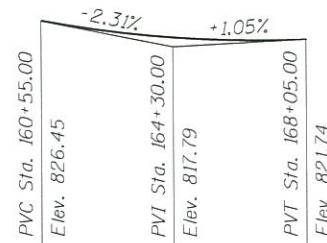
PLAN

LOCATION SKETCH



PROFILE GRADE
(Along Linden Rd.)

Note: The profile grade shows the final elevations after grinding



PROFILE GRADE
(Along Ramp BD)

HIGHWAY CLASSIFICATION

F.A.U. Rte. 5118 - Linden Road
Functional Class: Minor Arterial
ADT: 6650 (2013); 18,000 (2040)
ADTT: 330 (2013); 900 (2040)
DHW: 1805 (2040)
Design Speed: 45 m.p.h.
Posted Speed: 45 m.p.h.
Two-Way Traffic
Directional Distribution: 50:50

F.A.I. Rte. 39 - I-39 SB (Ramp BD)
Functional Class: Interstate
ADT: 10,000 (2013); 28,000 (2040)
ADTT: 4200 (2013); 11,700 (2040)
DHW: 2250 (2040)
Design Speed: 70 m.p.h.
Posted Speed: 65 m.p.h.
One-Way Traffic

LOADING HL-93

Allow 50#/sq. ft. for future wearing surface.

DESIGN SPECIFICATIONS

2014 AASHTO LRFD Bridge Design Specifications, 7th Edition with 2015 & 2016 Interims

DESIGN STRESSES

FIELD UNITS

$f'_c = 3,500$ psi
 $f'_c = 4,000$ psi (Superstructure Concrete)
 $f_y = 60,000$ psi (Reinforcement)

PRECAST PRESTRESSED UNITS

$f'_c = 8,500$ psi
 $f'_{ci} = 7,000$ psi
 $f_{pu} = 270,000$ psi (0.6" ϕ low lax. strands)
 $f_{pbt} = 202,300$ psi (0.6" ϕ low lax. strands)

SEISMIC DATA

Seismic Performance Zone (SPZ) = 1
Design Spectral Acceleration at 1.0 sec. (S_{D1}) = 0.033 g
Design Spectral Acceleration at 0.2 sec. (S_{D5}) = 0.085 g
Soil Site Class = B

GENERAL PLAN

LINDEN ROAD OVER I-39 SB (RAMP BD)
F.A.I. RTE. 39 SEC. (201-3)K & (4-1.5)R
WINNEBAGO COUNTY
STATION 97+36.88
STRUCTURE NO. 101-0216



USER NAME = myoung	DESIGNED - CME	REVISD -
PLOT SCALE = 214.000001' / 1" =	CHECKED - MCB	REVISD -
PLOT DATE = 3/8/2017	DRAWN - CFC	REVISD -
	CHECKED - MCB	REVISD -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHEET NO. 1 OF 2 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1.5)R	WINNEBAGO		
				CONTRACT NO. 64C62
ILLINOIS FED. AID PROJECT				

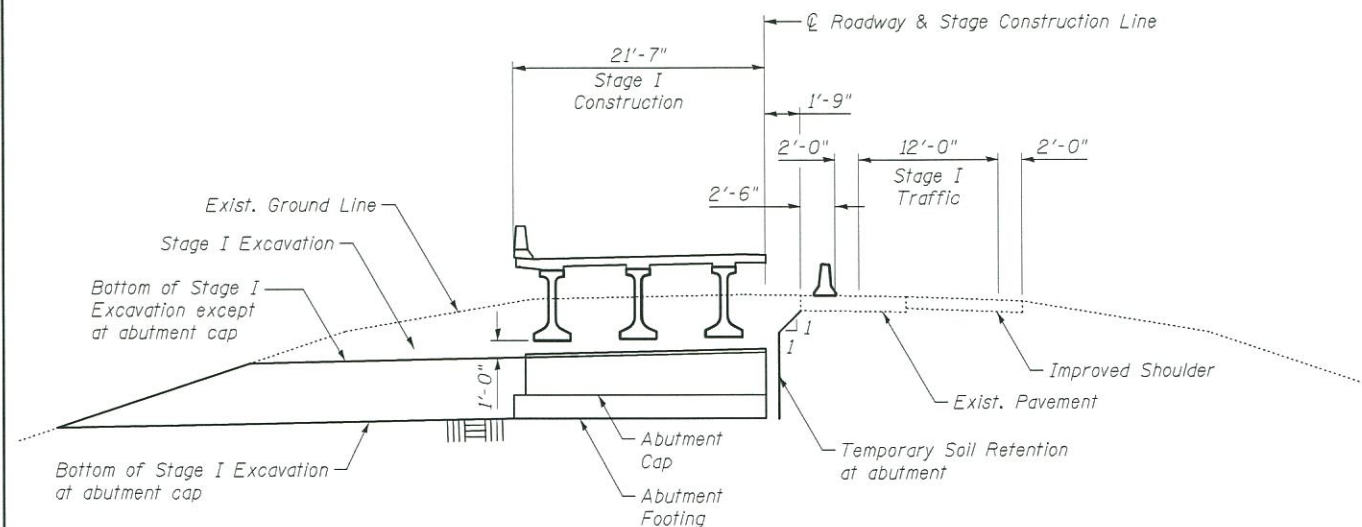
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FEHR GRAHAM PROJECT NUMBER: 15-1002 CB PROJ. NO.: 06085

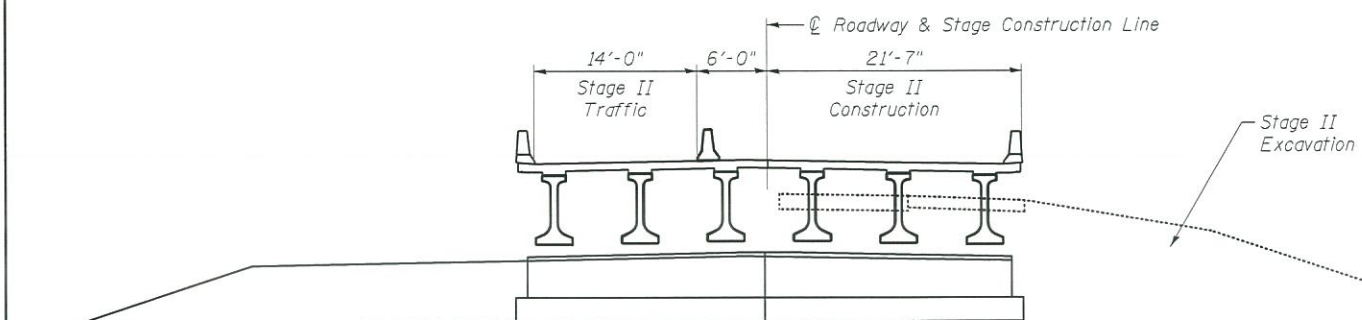
CONSTRUCTION SEQUENCE

Linden Road over Ramp BD will be constructed using a top-down construction method to maintain one lane of traffic on existing/proposed Linden Road at all times as indicated below.

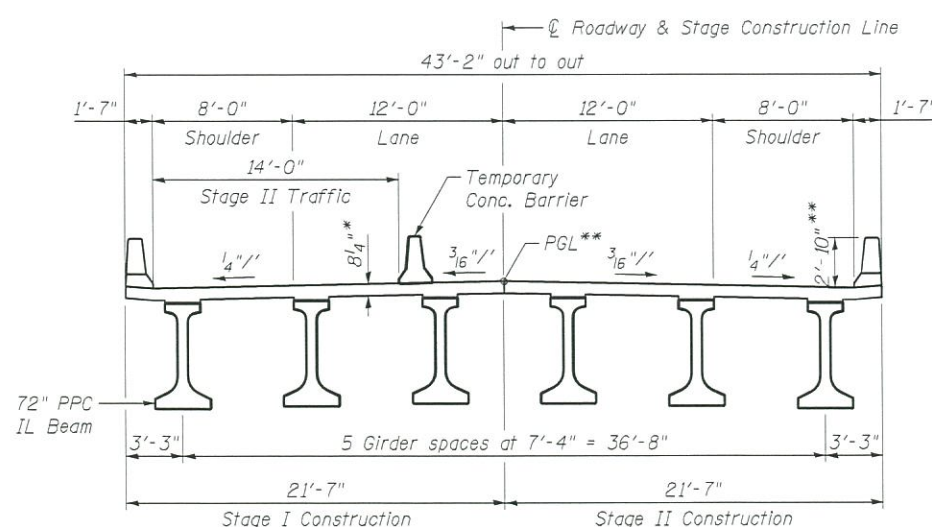
1. Direct Linden Road traffic to Stage I Traffic.
2. Proceed with Stage I Excavation to 1'-0" below the bottom of the proposed beams across the span using slopes laid back at 1:1 as shown on Stage I Construction Detail.
3. Install Temporary Soil Retention System and excavate as required to construct the abutments.
4. Proceed with Stage I Construction of abutments, superstructure and approach slabs.
5. Direct Linden Road traffic to Stage II Traffic.
6. Proceed with Stage II Excavation beneath Stage I Construction and as required for Stage II Construction.
7. Proceed with Stage II Construction.



STAGE I CONSTRUCTION



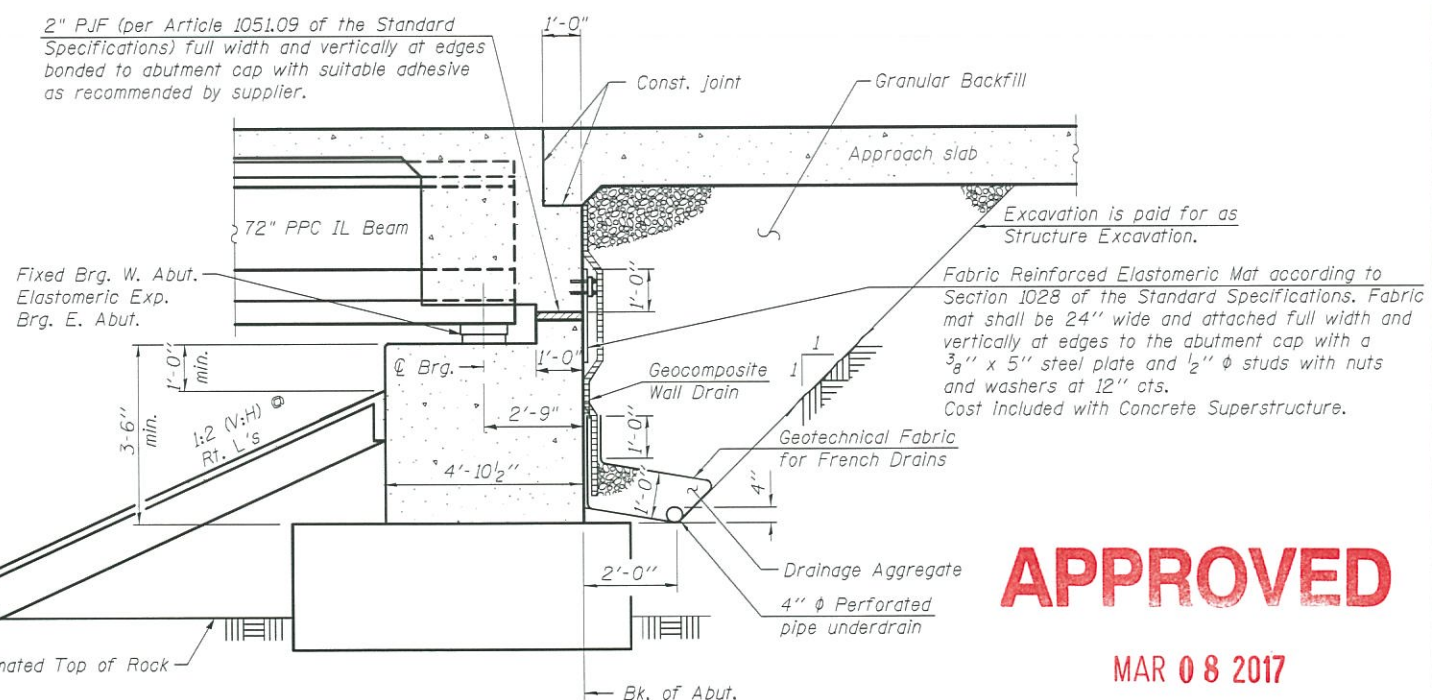
STAGE II CONSTRUCTION



CROSS SECTION

Looking East

* Prior to Grinding
** After Grinding



SECTION THRU SEMI-INTEGRAL ABUTMENT

(Horiz. dim. @ Rt. L's)

Note:
Cast slopewall onto rock and terminate cutoff wall below top of rock elevation.
Embed footing 6" into non-weathered rock.

APPROVED

MAR 08 2017

AS A BASIS FOR
PREPARATION OF DETAILED PLANS

DETAILS

LINDEN ROAD OVER I 39 SB (RAMP BD)
F.A.I. RTE. 39 SEC. (201-3)K & (4-1.5)R

WINNEBAGO COUNTY

STATION 97+36.88

STRUCTURE NO. 101-0216

FILE NAME: \\s0101\2016\10020216-002-101.dgn

FEHR GRAHAM
ENGINEERING & ENVIRONMENTAL
ILLINOIS DESIGN FIRM NO. 184-003525

USER NAME = myoung
DESIGNED - CME
CHECKED - MCB
DRAWN - CFC
CHECKED - MCB
PLOT SCALE = 16:0.000000 1/4" = 1'-0"
PLOT DATE = 3/8/2017

DESIGNED - CME
CHECKED - MCB
DRAWN - CFC
CHECKED - MCB
REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHEET NO. 2 OF 2 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
39	(201-3)K & (4-1.5)R	WINNEBAGO		
				CONTRACT NO. 64C62
ILLINOIS FED. AID PROJECT				

FEHR GRAHAM PROJECT NUMBER: 15-1002 CB PROJ. NO.: 06085